

INDEX DATA	RPS INFORMATION
Scheme Title M25 junction 7-8, <del>Wickhampton Lane, Reigate</del> Reigate	Details Archaeological monitoring at Martlets Lane, Reigate
Road Number M25	Date <del>August</del> 1995
Contractor RPS	
County ?	
OS Reference ?	
Single sided ✓ Double sided A3 2 Colour 7	

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**M25 JUNCTIONS 7 TO 8,  
WIDENING TO D4M:**

**Archaeological Monitoring at  
Markedge Lane, Reigate**

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Deanes Close  
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**August 1995**

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## *M25 JUNCTIONS 7 TO 8, WIDENING TO D4M:*

### *ARCHAEOLOGICAL MONITORING AT MARKEDGE LANE, REIGATE*

#### *Introduction*

- 1.1 RPS Clouston were commissioned by Parkman in March 1994 to undertake a watching brief of the construction of the retaining wall at Markedge Lane (photograph 1).
- 1.2 Reference was made to the attached engineering contract drawings 10204/OB220/01,02.
- 1.3 The site was visited on the occasions listed below and the observations recorded.
- 1.4 The Environmental Statement, Volume II, Part 7, Cultural Heritage concludes in Section 4:

"There is one area of known archaeological significance which might be affected by the construction of retaining walls."

This area lies in the valley known as Gatton Bottom around and between the Gatton Bottom and Markedge Lane motorway bridges.

Monitoring was undertaken in order to:

- i) assess the archaeological potential of sediments affected; and
- ii) record any archaeological finds or features disturbed during the works.

#### *Record of Visits*

##### (A) WESTBOUND CARRIAGEWAY

##### 2.1 March 30th 1994

The area to the south-west of Markedge Lane had been partially stripped of topsoil and a temporary road covering covered with terram sheeting and brick rubble had been laid (photograph 2).

The sheeting obscured observation of the stripped area, but as part of the existing topsoil and the subsoil remained in situ, observation would not have been informative. Monitoring of the spoil heaps produced no surface finds. A small drainage ditch to guide surface water away from the workface via temporary underground plastic hoses was being constructed on the southern side of the stripped easement. A section

through this showed the continued existence of a partially truncated topsoil (photograph 3).

## 2.2 April 4th 1994

On this visit vegetation was stripped from the existing (man-made) embankment, which was of no archaeological interest. ■

## 2.3 April 25th 1994

The purpose of this visit was to inspect the construction of footings for the retaining wall. It was observed that the material being cut back to receive these was the man-made bank (photograph 4) and that the line of the footings correspond to the area previously stripped during the initial construction phase (photographs 5, 6 and 7). This was confirmed by the inspection of the inner (southern) section of the cut where it could be seen that no top or sub-soil remained (photograph 8).

The cut for the footings and the drainage gully was inspected but no archaeological features were seen in section and no artefacts retrieved.

North-east of Markedge Lane the width of the easement was limited by established woodland. Topsoil had not been cleared on the southern side of the rubble road (photograph 9). Here again earthmoving operations were restricted to previously disturbed areas (photograph 10). At the north-eastern end of the works, beyond the woods, the area south of the rubble road had been disturbed during the earlier phase by the construction of a storm water lagoon.

## (B) EASTBOUND CARRIAGEWAY

## 2.4 1st March 1995

The area immediately to the north of the east bound carriageway, between Markedge Lane and Gatton Bottom, within the highway boundary was being excavated by contractors in order to insert retaining walls. In the western portion of the site the retaining wall was already constructed and in place, but to the east was under construction. All exposed soil profiles and geological sections were inspected for archaeological features. Even with the retaining wall in place it was possible to examine exposed profiles (photograph 11). No archaeological finds or features were seen. A drainage ditch (photograph 12) had been excavated immediately within the highway boundary. It was apparent that the original ground surface (pre-motorway) was being destroyed, but there was no indication of any past activity.

## 2.5 23rd March 1995

Import of material for widening of embankments was taking place (photograph 13). Retaining wall excavations in this area were completed, and the remaining exposed soil/geological profiles inspected for archaeological features. No archaeological features or archaeologically significant finds were observed.

## *Conclusions*

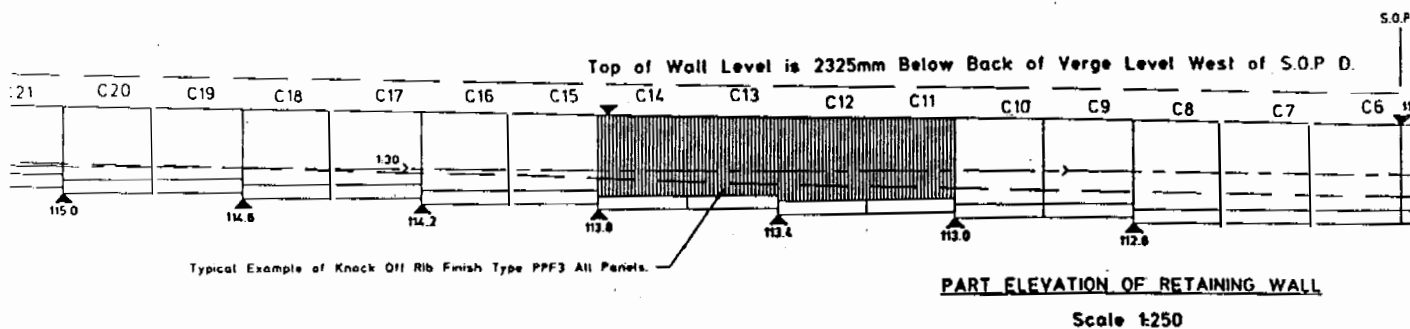
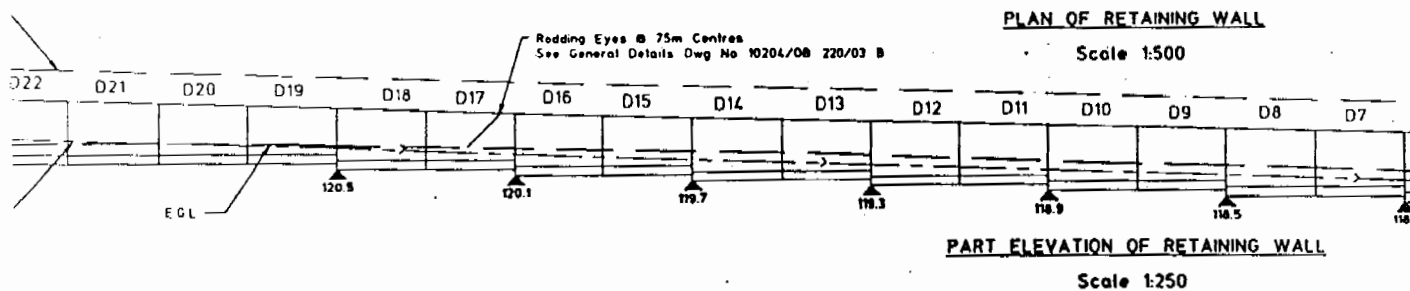
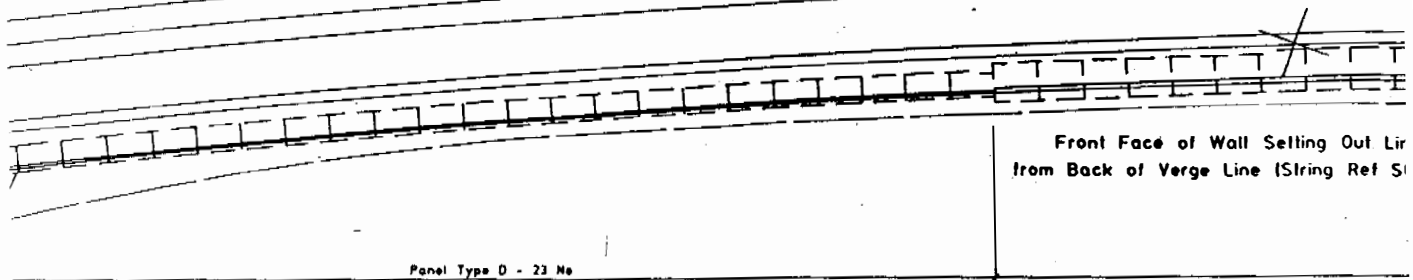
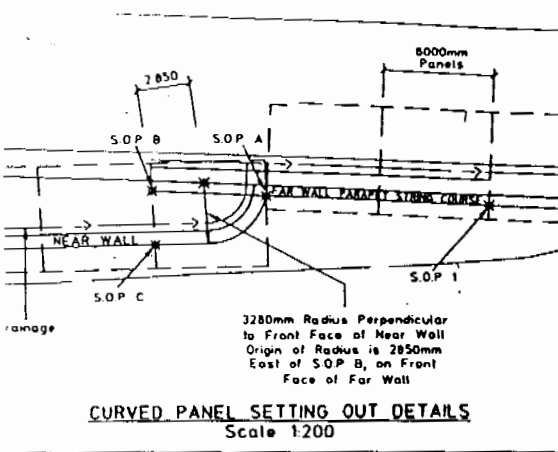
### (A) WESTBOUND CARRIAGEWAY

- 3.1 It is concluded that work on the retaining wall to the south of the M25 is not a threat to possible archaeological deposits and that previously undisturbed areas are safeguarded by protective sheeting and the existence of partial topsoil. Minimal disturbance has been caused by the drainage gully and inspection of this did not reveal any archaeological material or features.
- 3.2 Further archaeological monitoring is still required for works associated with the eastbound carriageway at Gatton Bottom as and when they occur.

### (B) EASTBOUND CARRIAGEWAY

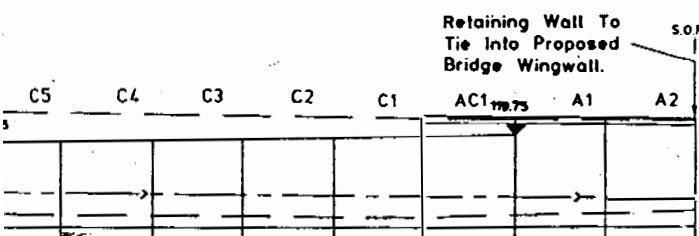
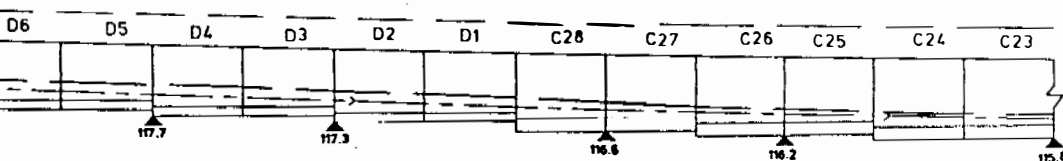
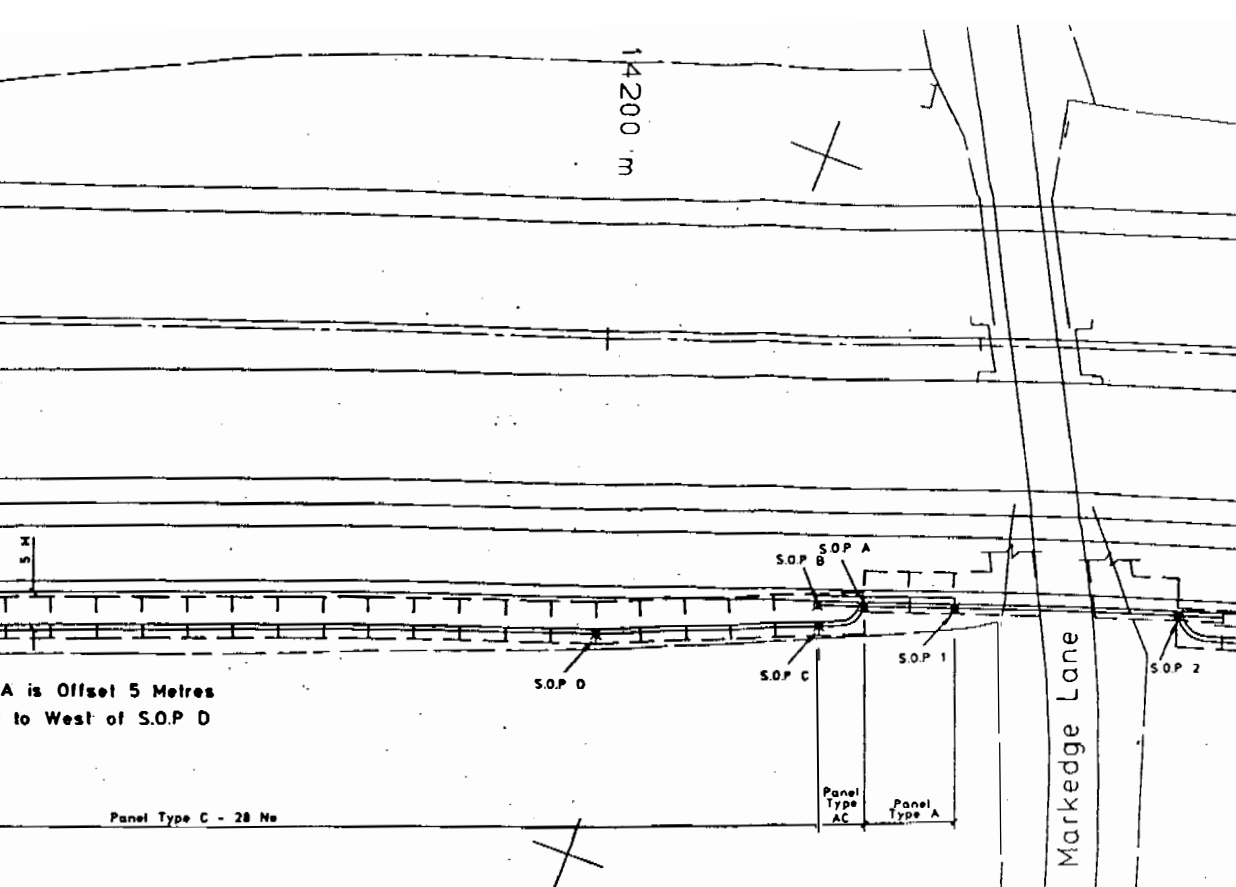
- 3.3 Although it is concluded that previously undisturbed deposits were being destroyed there was no indication that these deposits contained nothing of archaeological interest or significance. If there had been significant past activity in this area, then archaeological features or finds would have been in evidence.

***PLANS***

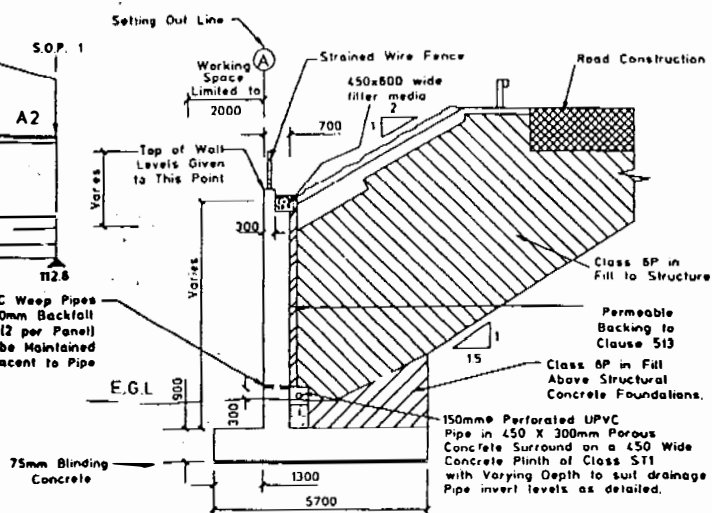


S.O.P.	EASTING	NORTHING
1	17737.29	13648.45
A	17725.86	13644.77
B	17719.95	13642.87
C	17721.18	13640.19
D	17693.25	13629.07

Drawn by	Checked by	Approved by	Date	NOTES
MRF	A.S.	A.S.	9/93	1. This drawing shall be read in conjunction with drawing Nos. 220/01 to 220/06 inclusive.
	A.S.	S.P.	1/94	2. Materials of Construction
				Structural Concrete - Class 90/20
				Blinding Concrete - Standard Mix B1
				Reinforcement - High Yield Steel to BS 4449
				3. Finishes
				All formed surfaces - 25 Pol Broad Prof Red Formwork Type PPF3
				Exposed concrete surfaces - F1
				All unformed surfaces - U2
				Exposed surfaces - U1
				Top surfaces of wet 1/2 parapet piers - U3
				4. Exposed concrete surfaces listed in Appendix 17/2 shall be pitted impregnated in accordance with the requirements of BS 43/90.
				5. All buried surfaces accessible after casting shall be waterproofed with two coats of bitumen up to 100mm below ground level.
				6. Pits and chambers (20mm x 20mm) shall be provided to all corners unless otherwise shown.
				7. Safety fences, lighting columns, drainage, kerbs and road construction above formation level are detailed and measured as Roadworks.
				8. The required safe bearing pressure of foundation formation level is 200kN/m².



75mm UPVC Weep Pipes With 50mm Backfall @ 3000mm Cts. (2 per Panel) Nominal Cover to be Maintained to Reinforcement adjacent to Pipe



TYPICAL SECTION (PANEL TYPE C)

Scale 1:100

For Dimensions of Panel Types A & D See RC Dwg No 10204/08 220/04 A

ORIGINAL AT A3

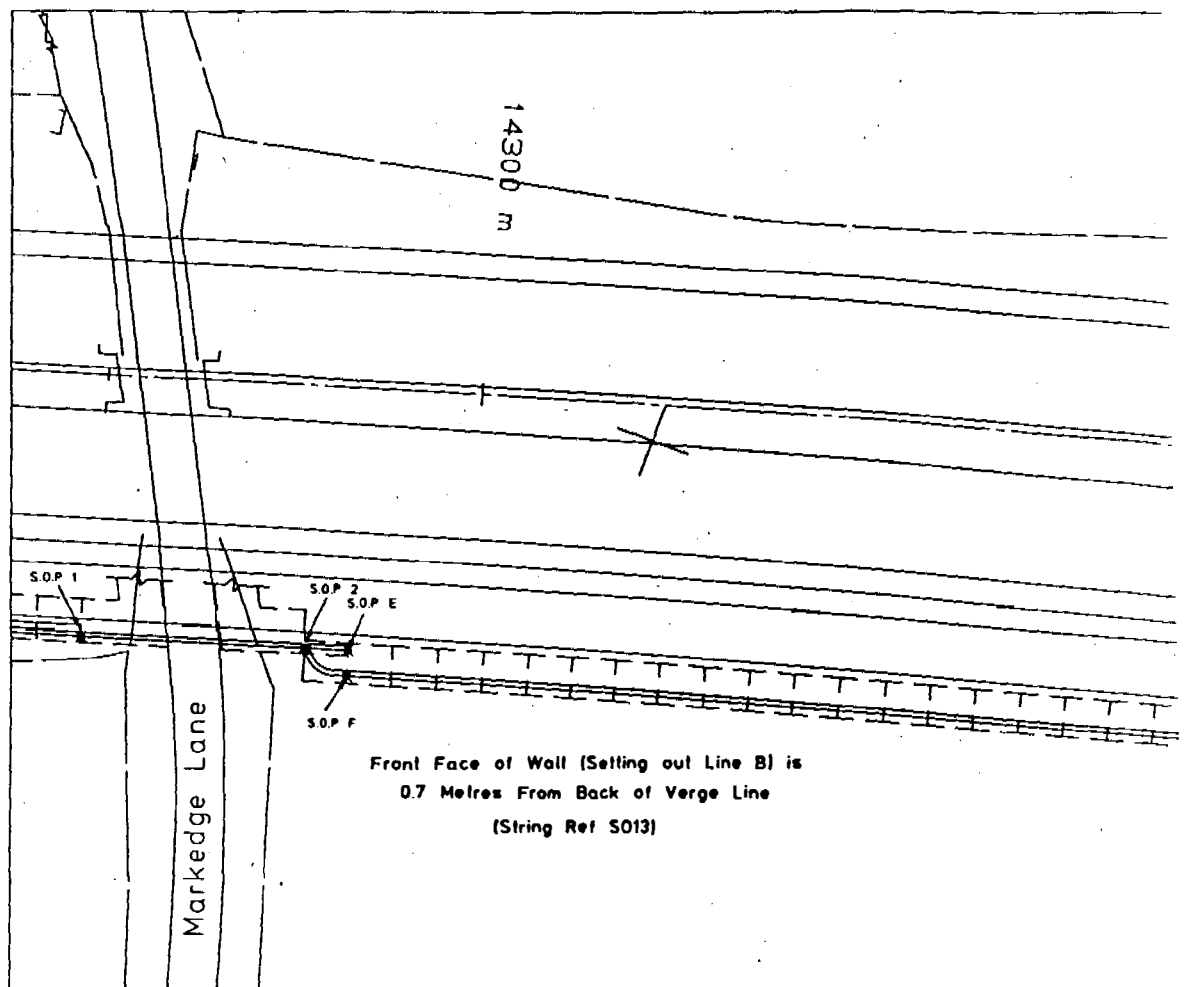
CONTRACT  
DRAWING

Client DEPARTMENT OF TRANSPORT  
Scheme M25 IMPROVEMENTS JUNCTIONS 7-8  
Title MARKEDGE LANE RETAINING WALLS  
GENERAL ARRANGEMENT-WEST WALL

**PARKMAN**  
PLANNING ENGINEERING - ARCHITECTURE

Office of Issue	Telephone no	Drawing no	Rev
MOLD	0352-750122	10204/08220/01	E

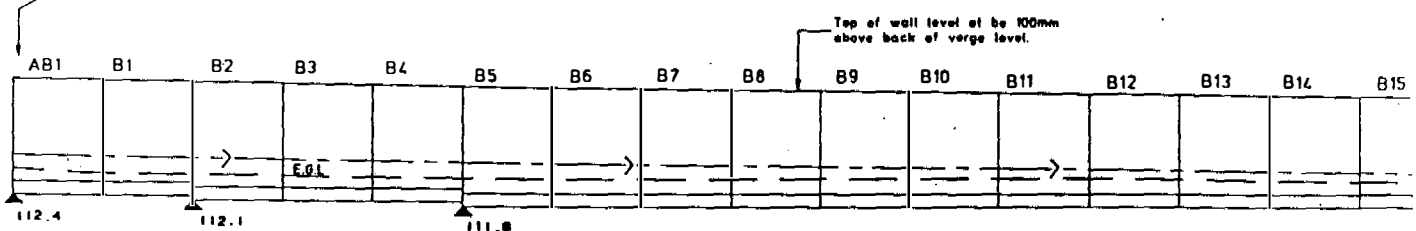




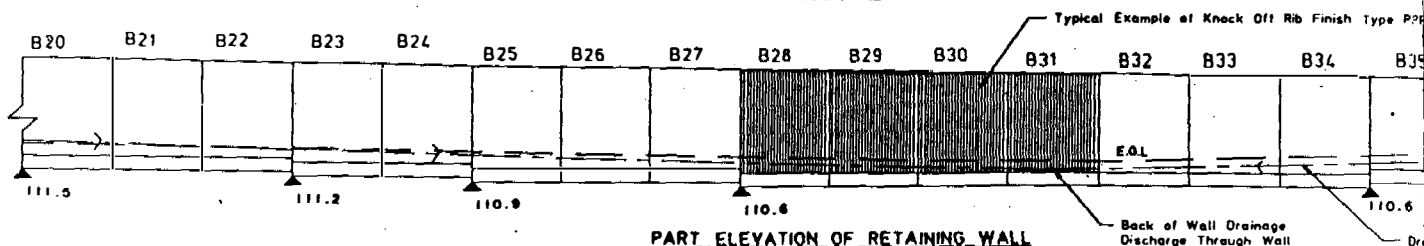
PLAN OF RET  
Scale

Retaining Wall to  
Tie into Proposed  
Wingwalls.

S.O.P.	EASTING	NORTHING
2	17765.72	13657.80
E	17771.39	13659.43
F	17772.45	13656.13

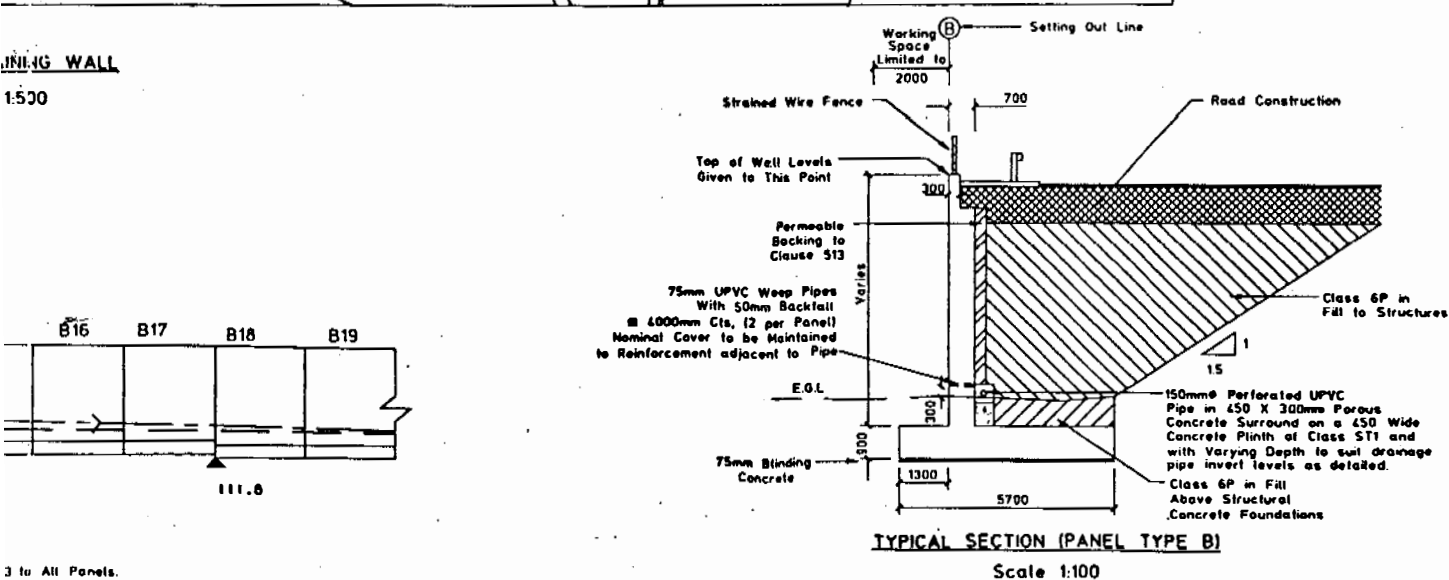
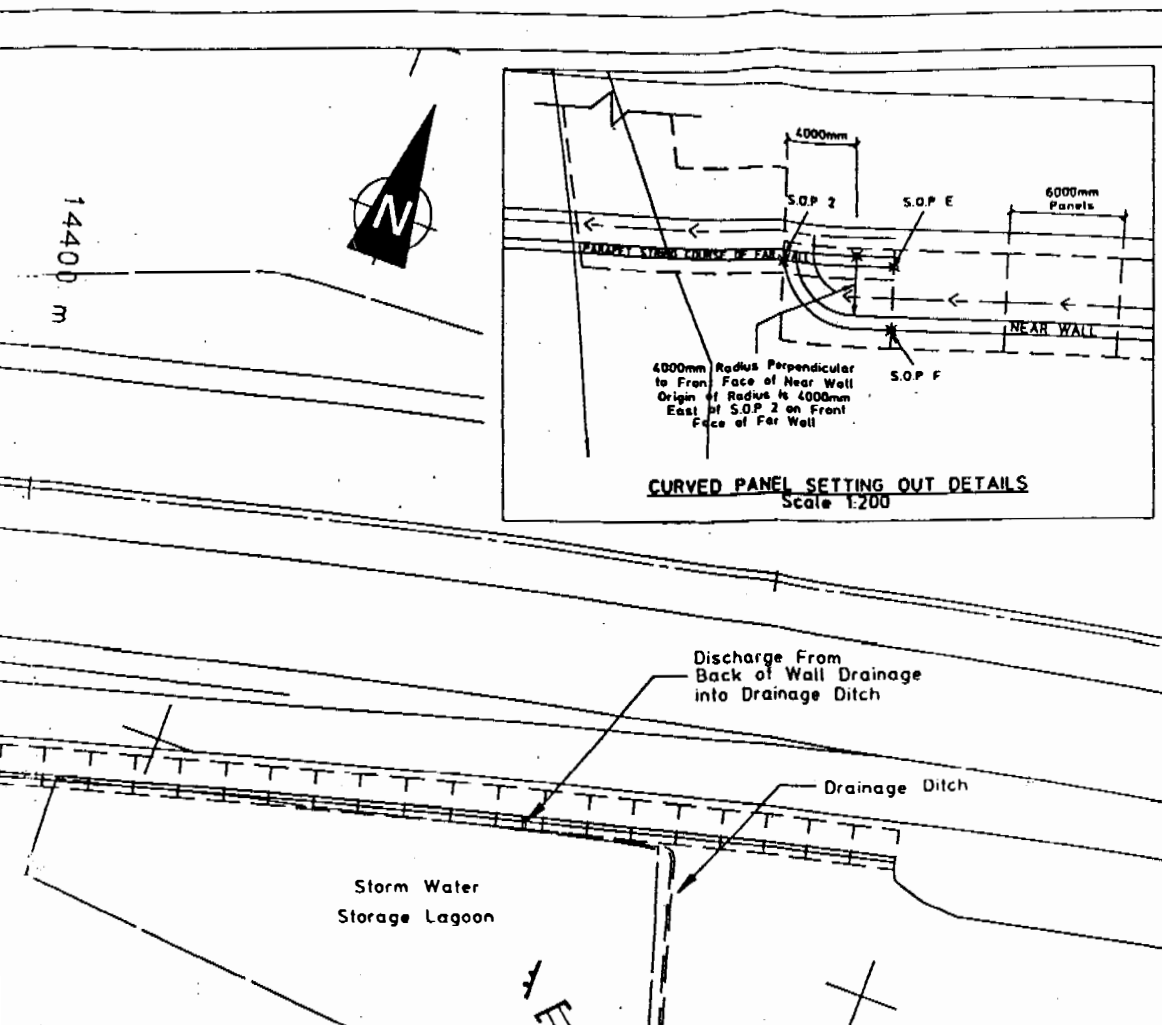


PART ELEVATION OF RETAINING WALL  
Scale 1:250




PART ELEVATION OF RETAINING WALL  
Scale 1:250

Rev	Amendment	Drawn by	Checked by	Approved by	Date	NOTES
D	Tender Issue	MRF	A.S.	<i>[Signature]</i>	3/93	1. This drawing shall be read in conjunction with drawing No. 200/91 to 220/90 inclusive.
E	Contract Drawing		A.S.	BSP	1/94	2. Materials of Construction Structural Concrete - Class 80/20 Reinforcing Concrete - Standard Mix BT1 Reinforcement - High Yield Steel to BS 4449
						3. Finishes All formed surfaces - F2 Patterned Profiled Formwork Type PPS All unformed surfaces - F1 All unformed surfaces - UP Surf of concrete - U1 Top surface of walls/perpet - U2
						4. Exposed concrete surfaces listed in Appendix 17/2 shall be impregnated in accordance with the instructions of BS 455.
						5. All buried surfaces accessible after casting shall be two coats of bitumen up to 150mm below ground level.
						6. Pallets and chumbers (50mm x 20mm) shall be provided in excess of those shown.
						7. Safety Fences, lighting columns, drainage, kerbs and the above formation level are detailed and measured as shown.
						8. The required safe bearing pressure of foundation formation is 200kN/m <sup>2</sup> .



## CONTRACT DRAWING

**ORIGINAL AT A3**

Title <b>DEPARTMENT OF TRANSPORT</b>			
Scheme <b>M25 IMPROVEMENTS JUNCTIONS 7-8</b>			
Title <b>MARKEDGE LANE RETAINING WALLS GENERAL ARRANGEMENT-EAST WALL</b>			
Office of issue	Telephone no	Drawing no	Rev
MOLO	0352-750122	10204/08220/02	E

***PHOTOGRAPHS***



1



2

ORIGINAL IN  
COLOUR





3



4

ORIGINAL IN  
COLOUR





5



6

ORIGINAL IN  
COLOUR





ORIGINAL IN  
COLOUR

7



8





9

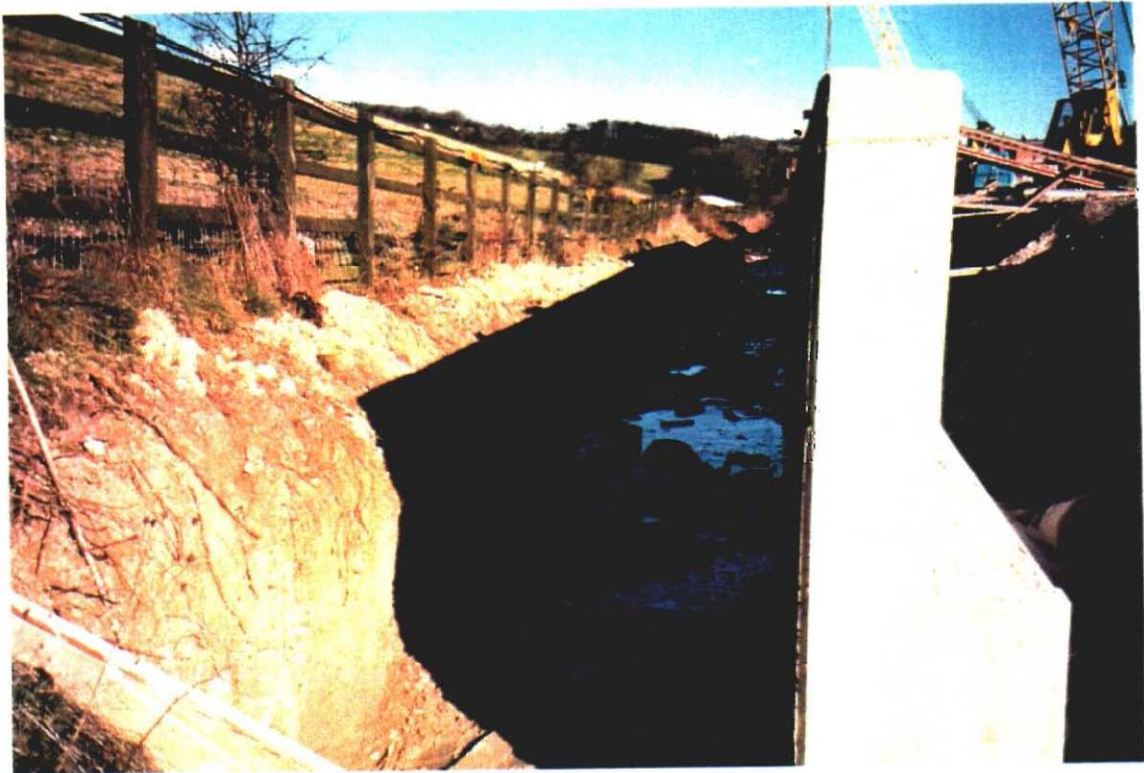


10

ORIGINAL IN  
COLOUR



11



12



ORIGINAL IN  
COLOUR

13



ORIGINAL IN  
COLOUR