

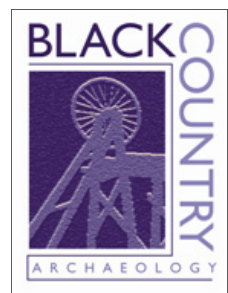
A Landscape Character Framework for the Black Country Regeneration Corridors

Based on the Black Country Historic Landscape Characterisation



**Black Country
Archaeology Service**

November 2009



Acknowledgements

This investigation would not have been possible without the financial and other support of the regional office of English Heritage. I am grateful too for the support of Mike Shaw and Sue Whitehouse, together with other officers from the planning policy and historic environment teams of the four participating local authorities. Initial ideas for the project owe a debt to previous landscape character frameworks, such as that carried out for West Berkshire Council. Notwithstanding these contributions, any errors are entirely my own.

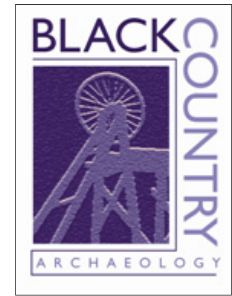
Paul Quigley
Landscape Archaeologist

Contents

	Page
Executive Summary	5
1. Introduction	7
1.1 A Guide to Landscape Character	7
1.2 The Black Country Historic Landscape Characterisation	8
1.3 The Rationale for a Typology of the Landscape	9
2. Analysis of the Historic Landscape Characterisation	11
2.1 The Landscape of the Regeneration Corridors	11
2.2 The Creation of a Typology of Landscape Elements	12
2.3 Sampling of Elements for Site Visits	16
3. A Typology of the Landscape	17
3.1 Canalside Industry After 1938	18
3.2 Canalside Industry Before 1939	20
3.3 Other Industry After 1938	22
3.4 Other Industry Before 1939	24
3.5 Passenger Railway Stations and Metro Stops	26
3.6 Canalside Open Land	28
3.7 19 th Century & Edwardian Terraced Housing	30
3.8 Post-war High Density Housing	32
3.9 Mixed 20 th Century Semi-detached & Terraced Housing	34
3.10 Mixed Inter-war Semi-detached & Terraced Housing	36
3.11 Late 20 th Century Semi-detached Housing	38
3.12 Inter-war Semi-detached & Detached Housing	40
Appendices	
Appendix 1: Detailed HLCs inside the Regeneration Corridors	43
Appendix 2: The Regeneration Corridors and the BCHLC data	44
Appendix 3: Site Visits - Industry and Communications	46
Appendix 4: Site Visits - Housing	47
Appendix 5: Data items collected during fieldwork	48
Appendix 6: Historic Centres in Existence Before 1750	49

A Landscape Character Framework for the Black Country Regeneration Corridors

Executive Summary



Landscape character and distinctiveness are ideas which are increasingly seen as important in the protection, management and regeneration of the environment.

This investigation sets out to produce a generalised, structured description of the character of the landscape of the Regeneration Corridors. These areas, representing a quarter of the area of the Black Country local authorities, have been identified for major change in the period up to 2026.

The investigation summarised in this report has produced a typology of the elements within the landscape based on cartographic sources for the period up to 2000 as well as selected site visits in 2008.

The principle tool used in the investigation has been the database assembled as part of the Black Country Historic Landscape Characterisation (BCHLC). The BCHLC is a comprehensive record of modern land use, period of origin, and previous land use. The information on the historical development of the landscape it holds allows the descriptions within our typology to pay attention to the past origins of each identified 'type'.

Initial analysis quantifies the distinctive nature of the Corridors relative to the rest of the Black Country, and shows that the prevalence of industrial sites and communications infrastructure (such as canals and railways) distinguish them from the remainder of the area. Victorian and Edwardian terraced housing is also more common in the Corridors than outside them.

For the purposes of this investigation, the landscape is divided into 12 simplified 'types' of landscape which together account for about two-thirds of the area of the Regeneration Corridors.

The analysis produces evidence of the extent of the change already underway in the Regeneration Corridors, including the loss of some of their oldest and least common industrial structures. Although they are not designated, many of these may represent important assets in the context of the historic environment of the Black Country.

The report makes recommendations for future research and identifies issues to be taken into account in consideration of the protection and enhancement of the historic environment.

Section 1: Introduction

1.1 A Guide to Landscape Character

Landscape character and distinctiveness are ideas which are increasingly seen as important in the protection, management and regeneration of the environment.

This report is intended to be used as a guide to the character of selected parts of the Black Country landscape. It is worth clarifying here that the term 'landscape' is not confined in this report to references to open, rural environments, but instead is taken to encompass both rural and urban settings (including what others might refer to as 'cityscape' or 'townscape'). The location of the Black Country is illustrated in Figure 1.

Specifically, the report focuses on those areas which have been particularly identified for change in the period up to 2026 by the Joint Core Strategy of the four Black Country local authorities of Dudley, Sandwell, Walsall and Wolverhampton. These areas of change are referred to as the Regeneration Corridors¹.

In general, this report confines itself to considering these Corridors, the extent of which is illustrated in Figure 2.

The report describes a range of different features of what is a largely urban environment, including the character of industry and housing, as well as some

aspects of canals and railways for example.

However, it should be emphasised that the remit of the report is primarily *descriptive*, rather than to assess the relative merit of different landscape elements. It follows in particular that just because a type of landscape which exists within the

Below: In this report, the term 'landscape' is taken to refer to urban as well as rural environments



¹More details of the Black Country Joint Core Strategy and the Regeneration Corridors are available from: <http://blackcountrycorestrategy.dudley.gov.uk>

Regeneration Corridors is described in the pages of this report, it should not be inferred that this type of landscape is necessarily in more need of preservation than any other.

In this respect, this guide to the character of the defined Regeneration Corridors perhaps offers a challenge to preconceived ideas about what is important in a landscape. Whereas other guides to architecture and landscape might describe relatively rare public or religious architecture, for example, this investigation focuses on more numerous but more ordinary structures which we are more likely to encounter. For this reason historic settlement centres, for example, are also not described directly (see Appendix 6).

In providing a structured understanding of the existing landscape, it is hoped that the report will provide a resource for both the Black Country Joint Core Strategy and the Environmental Infrastructure Strategy.

1.2 The Black Country Historic Landscape Characterisation

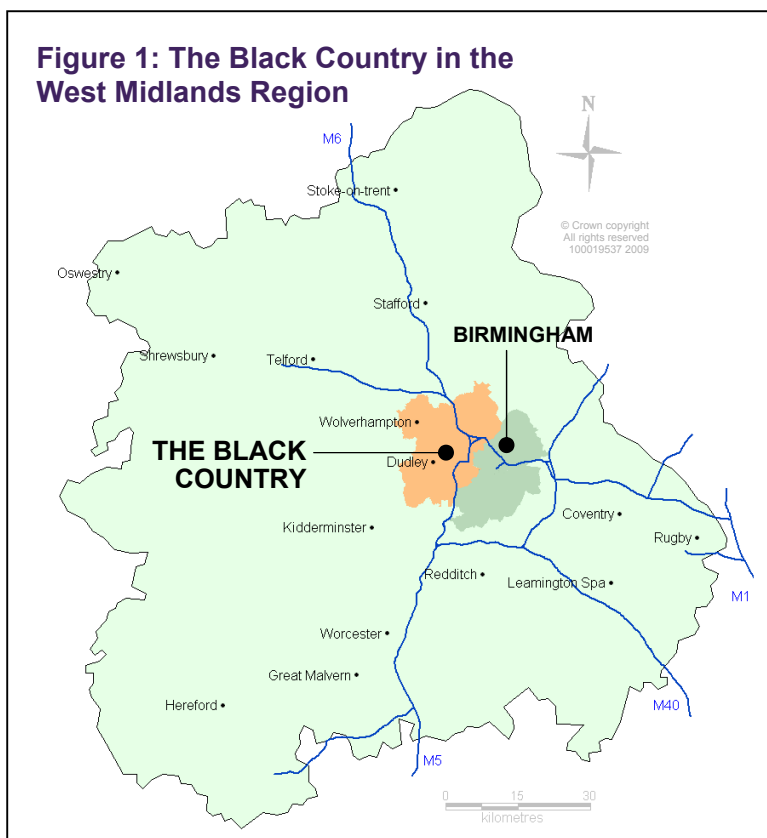
During the production of this guide, an invaluable tool has been the data compiled by the Black Country Historic Landscape Characterisation (abbreviated throughout as BCHLC), and the descriptions of landscape character here are based primarily on it.

Started in 2005, the BCHLC has been supported by English Heritage and carried out by the Black Country Archaeology Service on behalf of the four local authorities. The first report of the project was completed in 2008 and is a record of the modern character, period of origin, and previous use of more than 12,000 parcels of land, comprising the entire area of the four Black Country districts. With its completion, it has brought the opportunity to inform planned change in the modern landscape in a way which

is more securely based on evidence of how the whole landscape has developed.

In so far as this Character Framework relies on the work of the Black Country Historic Landscape Characterisation, it is only one possible interpretation of the BCHLC data. Others might use the information within its database in different ways and for different purposes, and might therefore come to different understandings of the landscape and the way it has changed.

Figure 1: The Black Country in the West Midlands Region



1.3 The Rationale for a Typology of the Landscape

Ideally, our guide to the character of the landscape would be based on much more detailed fieldwork and research than has been possible. Unfortunately, both the time and expense that would have been involved in new fieldwork of this type meant that a different approach had to be found.

Furthermore, while some important and very useful work has already been carried out in particular local areas of the Corridors, the map in Appendix 1 illustrates that the geographic coverage of these studies only accounts for a small proportion of the wider landscape considered in this report.

In the absence of previous work of the required scope, and facing the necessity of producing a way of understanding the landscape in a

relatively short amount of time, a sample-based typology of landscape elements was viewed as the most appropriate way of producing a character framework.

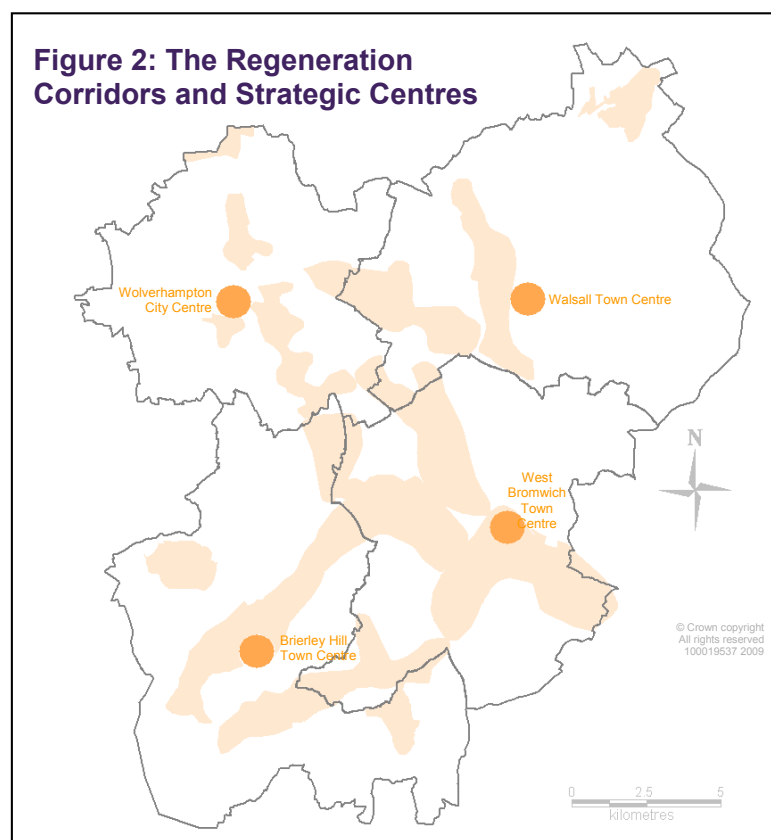
Put another way, it was proposed to create a generalised description of the landscape based on relatively restricted direct observations. This was made possible by the work already carried out in creating the BCHLC, and its existence ensured that the fieldwork which was conducted as part of this study was targeted to have the most useful influence on our wider understanding.

So, the full descriptions of our typology which follow in Section 3 are an outcome of analysis of the content of the BCHLC as well as of the data collected from fieldwork. But the fieldwork itself was also directed by our knowledge of the structure of the landscape acquired from the investigation which the BCHLC represents.

The process which the investigation follows consists broadly of three stages:

- (i) to analyse the BCHLC data to select those types of Landscape Element which particularly represent the landscape of the Corridors;
- (ii) to make a small selection of examples from each type and to use this selection as the basis of observation in the field;
- (iii) to bring the desk-based analysis and field observation together in a typology of the landscape.

The following sections describe this process in more detail.



Section 2: Analysis of the Historic Landscape Characterisation

2.1 The Landscape of the Regeneration Corridors

The area within the boundaries of the four Black Country local authorities amounts to 356 square kilometres (137 square miles). Within this, the Regeneration Corridors (hereafter referred to as the Corridors) represent a quarter of the landscape, a total of 89 square kilometres.

In order to be able to analyse the landscape of the Corridors in a quantitative way it has been necessary to identify all the individual landscape elements which fall within their boundaries. Using the BCHLC data to do this was inevitably rather imprecise, largely because the loosely drawn boundaries of the Corridors do not necessarily match those of the parcels of land used to build up the earlier characterisation.

Below: Industrial and transport infrastructure are two key features of the landscape of the Regeneration Corridors. Here, factories are sited next to Tittford Canal in Langley (Sandwell).



Table 1: Comparison of Land Use

Modern Land Use (in descending order of area in the Black Country)	% of Regeneration Corridors	% of Black Country
Settlement	27%	47%
Industrial	35%	11%
Field System (farm land)	-	10%
Recreational	7%	10%
Open Land	8%	6%
Public Services	3%	5%
Communications ¹	8%	3%
Other	8%	7%
Total	100%	100%

¹Including all canal and railway infrastructure, car parks, major road roundabouts and bus and coach stations.

Source: BCHLC, 2000

However, as Appendix 2 describes, for the purposes of this report an area of land was defined which roughly corresponds to that within the Corridor boundaries and which, at the same time, is made up of whole 'polygons' of BCHLC data (the latter being the smallest units used to record landscape character by the BCHLC). The area defined by Appendix 2 (very slightly smaller than the area of the original Corridors) has been used in all of the calculations of area which appear within this report.

The representation of the Corridors in this way allows us to compare, for example, the land use within their boundaries to that in the wider Black Country. This is presented in Table 1. One key difference illustrated by this comparison is the predominance of industry and communications (i.e. canals and railways) compared to the Black Country as a whole and, within this report, the description of landscape character draws heavily on this result.

2.2 The Creation of a Typology of Landscape Elements

This investigation depended on the creation of a system of classifying landscape elements as they were recorded in the BCHLC. The original design of the project envisaged that this system would be simple enough so that it would necessitate the definition and description of no more than fifteen 'types'.

The selection of the types which would ultimately be described within the report (as opposed to those which would not be) was influenced primarily by land use (both modern and historical) but also, in the case of canalside features, the potential for development. In respect of area of land use, it considers those elements which are both relatively common within the corridors but also, in general, distinctive to the corridors when compared to the Black Country as a whole.



Areas of change: a reused mid 19th century Methodist Chapel in Smethwick (Sandwell) belies the neighbourhood's evolution in character from residential to industrial.

Table 2: Black Country Industrial Sites

Type of Industry	% of Industry in Corridors	% of Industry in rest of Black Country
Canalside, after 1938 (Type 1 in Section 3)	31%	20%
Canalside, before 1939 (Type 2 in Section 3)	6%	5%
Other, after 1938 (Type 3 in Section 3)	53%	57%
Other, before 1939 (Type 4 in Section 3)	10%	18%
Total	100%	100%

Source: BCHLC, 2000

The following is a brief discussion of the process of classifying the landscape, starting with the most common use of land within the Corridors.

Industry

In terms of area, industry is the most common feature of the landscape of the Corridors. Furthermore, the Corridors themselves represent the core of Black Country industry: more than three quarters (78%) is within their boundaries.

For the purpose of this investigation, classification of this industry primarily took into account its period of origin. The vast majority of surviving industrial sites originate after the 4th edition Ordnance Survey map was surveyed (which, for most of the Black Country, was around 1938). In saying they originate from this time, we mean that these sites are not believed to contain buildings from an earlier period.

Put more precisely, four fifths (83%) of industry recorded by the BCHLC is from the period after 1938, while a much smaller proportion, just less than a fifth, has its roots in the early 20th century or before. Within the Corridors these proportions are very similar and, this division, essentially between pre-war and post-war industry, forms a key part of our typology of industry within the Corridors.

However, in addition to period of origin, the classification also takes into account the proximity of each industrial site to a navigable canal. While this criterion may ultimately something say about the origins of the site as an industrial location (this was not clear at the start of the investigation), it was thought to be of interest in terms of each site's potential for development.

While a quarter of Black Country industrial land outside the Corridors is adjacent to a canal, this proportion rises to more than a third when we consider only the industrial land within the boundaries of the Corridors.

Combining period of origin and proximity to a canal produces a distribution of industry between four types, as shown in Table 2.

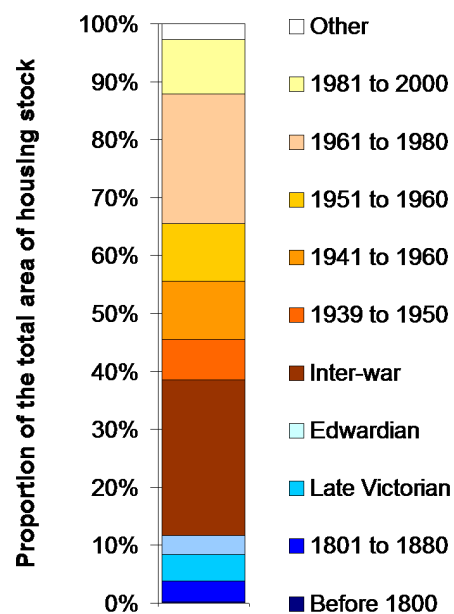
Housing

Housing accounts for almost half of the land in the Black Country. Each of the almost 7,500 records of housing in the BCHLC contains a one-line description of its type as well as an assessment of its period of origin.

Period of origin of Black Country housing is illustrated in Figure 3.

These results reflect the fact that housing built between the First World War and the 1960s, often in large estates, remains an important part of the stock in the Black Country as a whole, as well as in the Corridors themselves, as we shall see.

Figure 3: Black Country Housing Stock by Period of Origin
(as it stood in 2000)



Source: BCHLC, 2000



At the turn of the 21st century, housing from the period 1920 to 1938 was still the most common type in the Black Country. These semis are on William Road, Smethwick (Sandwell).

Within this mid-20th century stock, 'inter-war' housing (for our purposes, defined as being that built between the 3rd and 4th edition Ordnance Survey Maps, i.e. roughly in the period 1920 to 1938) is the single most common period origin.

These two pieces of information - on type and period - make it possible to classify housing into some relatively simple groups. For the Black Country as a whole, ten types of this kind were created. This relatively simple categorisation, shown in Table 3 over the page, accounted for 95% of the land given to settlement.

Of the housing types shown in Table 3, six were selected as being most representative of the residential areas of the Corridors (these are shown with an asterisk in Table 3). Table 4 summarises these types and the following pages describe them in more detail.

The term 'mixed' in the titles of these housing types reflects the fact that developments of this period often mixed semi-detached homes within a system of short terraces.

Table 3: Black Country Housing

Classification by form and period, in rough chronological order

Type of Housing	% of Housing in Corridors	% of Hsg. in rest of Black Country
Older low density	2%	4%
c19/Edwardian terraces*	14%	6%
Mixed 'inter-war' semis/	13%	9%
'Inter-war' semis/	12%	17%
Small mid c20 semis	5%	9%
Mid c20 semis/detached	4%	10%
Mixed c20 semis/	14%	10%
Post-war high density*	15%	9%
Late c20 semis*	11%	12%
Late c20 detached	4%	10%
Other	5%	4%
Total	100%	100%

Source: BCHLC, 2000

Table 4: Common Housing Types in the Black Country Regeneration Corridors

(in rough chronological order)

Type of Housing	Definition
c19 & Edwardian terraces (Type 7 in Section 3)	All areas of housing originating between 1801 and 1919 which include terraced properties.
Mixed 'inter-war' semis & terraces (Type 10)	All areas of housing originating between 1920 and 1938 which are described as being 'mixed semis and terraced'.
'Inter-war' semis & detached (Type 12)	All areas of housing originating between 1920 and 1938 which do not include terraced properties.
Mixed c20 semis & terraces (Type 9)	All areas of housing from the period after 1938 which are described as being 'mixed semis and terraced'.
Post-war high density (Type 8)	All areas of housing from the period after 1940 which are described as consisting only of either terraced properties or flats.
Late c20 semis (Type 11)	All areas of housing from the period after 1960 which include semi-detached but not terraced properties.

Source: BCHLC, 2000

Transport Infrastructure

We have already seen that, after industry and housing, communications infrastructure (in the form of canals and railways) is a distinctive part of the Regeneration Corridor landscape.

However, in using BCHLC data to categorise canals and railways our investigation was restricted in that the extent of the information held in the BCHLC on these linear features was relatively limited, and not enough to be able to distinguish between their various characteristics. This was compounded by the relatively inaccessible nature of railways placing limitations on our ability to assess them during fieldwork.

As a result, our typology was restricted to taking account of canalside locations (as mentioned above in respect of industrial sites) rather the canals themselves. This limitation of the BCHLC in describing canals will be addressed by a forthcoming report by the Black Country Archaeology Service.



A recently derelict canalside industrial site on Brettel Lane (Dudley).

Table 5: Black Country Open Land

Type of Open Land	% of Open Land in Corridors	% of Open Land in rest of Black Country
Canalside (Type 6 in Section 3)	63%	21%
Other	37%	79%
Total	100%	100%

Source: BCHLC, 2000

In respect of railway infrastructure, we have included passenger rail stations and Metro stops as a category of landscape element within our typology (described as type 5 in Section 3).

Open Land

Lastly, we have chosen to consider open land, and in particular canalside open land. Table 1 showed that open land is particularly common in the Regeneration Corridors, and we can also see from Table 5 that this concentration is emphasised even further when we consider open land situated next to a canal.

2.3 Sampling of Landscape Elements

For each type of landscape element, a small number of locations were selected in order to collect additional information on the type through site visits. Sites were selected randomly by unit area (in other words, larger sites were more likely to be selected to be visited) and the categories of information collected are detailed in Appendix 5.

As the sample was taken from the BCHLC, and this was recorded using 2000 data, checks of the current status of these sites were carried out using more recent aerial photography².

For the majority of types, checks showed sites to be generally unchanged, with some demolition

and redevelopment of sites selected from canalside industry (types 1 and 2 - see Appendix 4). As might have been expected, some areas of open land had since been reused.

However, for one type in particular, type 4 (industry not located next to a canal but originating before 1939), the change in the 21st century appeared to have been more substantial. Based on the selection of sites which were checked, we can say with some confidence that at least a third, and probably somewhere in the region of a half of all the 139 sites in this category experienced total or partial demolition in the first few years of the current decade.

Table 6: Landscape Element Types and Their Distribution

Type	Category of Land Use	Number of Sites in Corridors	'Representative' 1	'Distinctive' 2
1. Canalside Industry after 1938	Industry	243	11%	84%
2. Canalside Industry before 1939	Industry	66	2%	81%
3. Other industry after 1938	Industry	596	19%	77%
4. Other industry before 1939	Industry	139	4%	66%
5. Passenger Rail/Metro Stations ³	Communications	(26)	-	(70%)
6. Canalside Open Land	Open Land	-	4%	43%
7. c19 & Edwardian terraces	Settlement	225	4%	30%
8. Post-war high density housing	Settlement	300	5%	25%
9. Mixed c20 semis & terraces	Settlement	96	4%	21%
10. Mixed inter-war semis & terraces	Settlement	76	4%	22%
11. Late c20 semis	Settlement	147	4%	16%
12. Inter-war semis and detached	Settlement	199	4%	12%

Notes: ¹The percentage of the area of the Corridors accounted for by this type.

²The percentage of this type located inside the Corridors.

³Stations are not currently identified separately in BCHLC Records. However, 70% of all Black Country passenger rail stations and Metro stops are in a Regeneration Corridor.

Source: BCHLC, 2000

²Accessed on <http://maps.google.com> on 15 February 2008.

Section 3: A Typology of the Landscape

Based on analysis of the Black Country Historic Landscape Characterisation and direct observation of sample sites, the following pages describe 12 types of landscape element within the Black Country Regeneration Corridors.

Table 6 opposite lists these types by their general category of land use, and indicates the level to which each is representative of the Corridors (i.e. is common within them) as well as distinctive to them (i.e. is, in Black Country terms, contained within the Corridors).

When assessing the latter measure, it is worth keeping in mind that the Corridors themselves represent a quarter of the area of the Black Country, so types which show a figure of more than 25% in the final column of Table 3 are to some extent disproportionately located in the Corridors, and therefore distinctive to them.

The descriptions of each type are intended to summarise information relating to the buildings present, as well as other features such as street layout and previous land use. However, the descriptions are also illustrated by photographs taken as part of the investigation (in the period March to July 2008). Unintentionally, these photos serve to emphasise the standing buildings, perhaps at the expense of other features (including the below-ground remains of previous phases, for example). However, their inclusion should not imply that the presence of these buildings is more significant than other characteristics of the landscape type.

A note on housing density

In the case of the descriptions of housing (i.e. types 7-12), a reference is included to the density of homes per hectare (summarised in Table 7 below). For the purposes of calculating net dwelling density, planning policy³ usually defines housing land as including, in addition to houses and gardens, access roads, car parking, incidental open space and play areas. The numbers of homes per hectare detailed in this report are calculated to be roughly equivalent to this definition.

Table 7: Density of Housing

By character type, in descending order

Type of Housing	Homes per hectare
Post-war high density (type 8)	55
c19/Edwardian terraces (type 7)	45
Mixed inter-war semis/terraces (type 10)	33
Mixed c20 semis/terraces (type 9)	28
Late c20 semis (type 11)	28
Inter-war semis/detached (type 12)	23
Average	33

³Planning Policy Statement 3, Annex B: Definitions. Available from www.communities.gov.uk/publications/planningandbuilding/pps3housing.

Type 1: Canalside Industry Originating After 1938

► **Geographic Distribution:** Partly a result of the common occurrence of canals inside the Regeneration Corridors, canalside industry is much more typical of their landscape than of other parts of the Black Country. The vast majority of this type (i.e. 84%) is within the Corridors, making up 11% of their total area.

► **Industrial Sector:** Unlike the older industrial sites, a substantial proportion - in this case a third - are described as *industrial estates* (although almost half are described in the BCHLC only as a works).

► **Origins & Recent Change:** Having recorded that they are post 1938, little differentiation is made in the BCHLC between the period of origin of these sites, although it is clear that the vast majority of buildings date from the period after WW2. Areas selected for fieldwork indicate that most of this type in the Regeneration Corridors in 2000 still remains, although there have been some notable site clearances (see, for example, the photo of the site at *Reservoir Place* below).

► **Previous Land Use:** Post 1938 industrial development took advantage of the availability of land from disused mining and quarrying sites which had not been available in earlier phases. In addition, the proximity to canals also means this type is more likely to have been built on land which had already been used for industrial purposes. As a result, more than a third of the previous land uses were industrial, and a further quarter represented reclamation of extractive sites.

► **Proximity to Historic Settlements:** Sites of this type are situated within 250m of the location of historic settlements at Ettingshall, Oldbury and Tipton Green.

► **Building Form:** Individual records of industry within the BCHLC may contain several different building types, many of which may not be contemporary with their particular site. Nevertheless, buildings from the post-war period are less likely to include brick built elevations and instead favour part brick, part steel clad walls. Roofs are shallow pitched, and are often covered by steel or asbestos-concrete.

► **Site Structure:** The type is often made up of relatively small units, either populating a purpose-built industrial estate or re-using a building previously occupied by a single concern.

► **Relationship with canal:** A large number of both these sites and those of industry from earlier periods (see type 2) are built on the banks of early, 18th century 'contour' canals. These are generally narrow, with one tow path, and on a similar level to the land fronting the canal.

Symbolic of the change in forms of communication used by local industry, this disused canal basin on the Walsall canal (1799) is now overlooked by an elevated section of the M6. The basin served a colliery and later a brickworks, but was incorporated into a 20th century industrial facility. As is visible in the background of this image, this site on *Reservoir Place* (Walsall) has itself recently been demolished (HBL5632).



Above and below: industrial estates are common within this type. This particular purpose built estate at Crystal Drive, Smethwick (Sandwell) occupies half of the site of the massive Chance glassworks which ceased production at the start of the 1980s. (HBL11412).



Above: the *Albion Works* at Moor Street, Brierley Hill (Dudley). The site is dominated by a large six-bay building now apparently occupied by a dozen small enterprises. (HBL 7884).





In a previous, 19th century, phase of industrial use, this site on Cornwall Road, Smethwick (Sandwell) was home to the *Woodford Iron Works* which used a basin cut into the site in order to access the old line of the Birmingham Canal. In the 20th century it appears to have been substantially rebuilt, and has been home to *Richmond Motors Group*, *Incanite Foundries Ltd*, *Wellman Process Engineering Ltd* and a number of other enterprises (HBL12153).

Where to see them in the Regeneration Corridors

► **East of Oldbury, B69** (25 BCHLC polygons)

Around the junction of the old and new Birmingham canals, including a broad swathe of land either side of the West Coast railway line, east of *Sandwell & Dudley Station*. Also visible from the elevated section of the M5 motorway north of Junction 2.

► **Great Bridge, B70** (11 BCHLC polygons)

The land surrounding the Ryder's Green Junction of the Walsall Canal, accessible from *Ryder's Green Lane*, *Charles Street* and *Great Bridge Street*.

► **Dudley Port, DY4** (9 BCHLC polygons)

The area next to the old and new Birmingham canals (and largely between them) at Dudley Port. Accessible from *Sedgley Road East* and *Dudley Road West*.

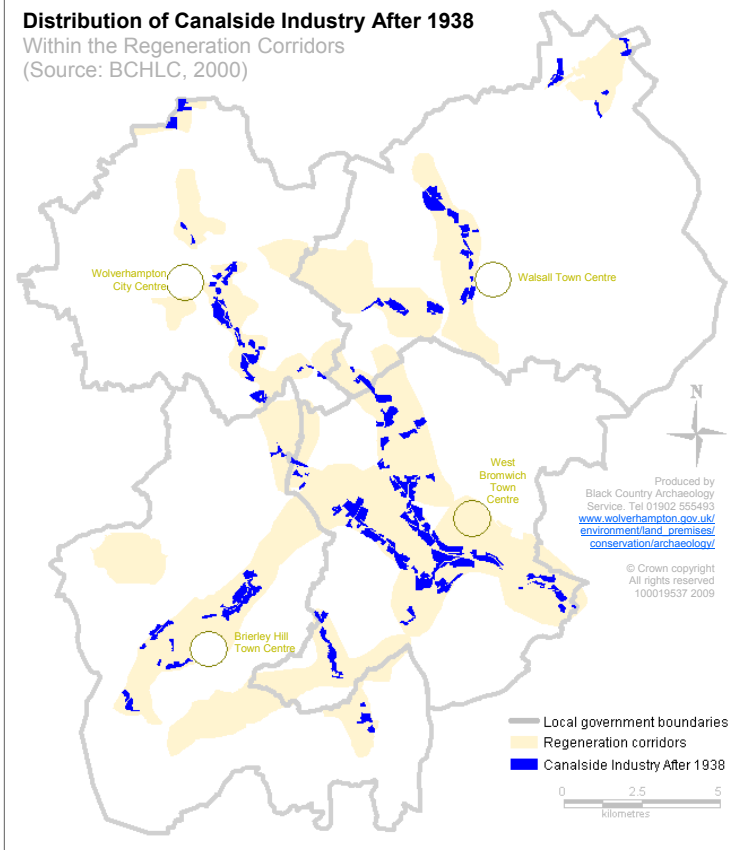
► **Peartree La., Dudley, DY2** (8 BCHLC polygons)

The area around Park Head Junction of the Dudley Canal, accessible from *Peartree Lane*.

► **Bloxwich, WS2** (5 BCHLC polygons)

Either side of the Wyrley & Essington Canal, accessible from *Leamore Lane* and *Fryer's Road*.

Distribution of Canalside Industry After 1938
Within the Regeneration Corridors
(Source: BCHLC, 2000)



Hainge Road, Tividale, appears to have been a planned industrial area laid out in the mid/late 20th century next to the Birmingham canal. Like much of the Black Country industry built after 1938, this site used land previously taken up by coal extraction.



One of about forty firms on Hainge Road, Tividale (Sandwell), the route itself was built in the mid 20th century and it forms the spine of a large industrial area (see plan above) (HBL10943).

Type 2: Canalside Industry Originating Before 1939

► **Geographic Distribution:** Partly a result of the common occurrence of canals inside the Regeneration Corridors, canalside industry is much more typical of their landscape than of other parts of the Black Country. The vast majority of this type (i.e. 81%) is within the Corridors, albeit only making up 2% of their total area.

► **Industrial Sector:** Much more than other types, these sites are described in the BCHLC as *metal works*, echoing the early role of the canals in linking the Black Country ironworks. More than half of them are recorded in this way.

► **Origins & Recent Change:** Industrial sites of this type are generally older than their equivalents situated away from the canals. About a third of this type originates between the wars, but the remainder has Victorian or Edwardian origins. With relatively long histories, many of these sites have been redeveloped in several phases, leaving buildings originating in several different periods. Areas selected for fieldwork indicate that most of this type in the Regeneration Corridors in 2000 still remains, although there have been some notable redevelopments.

► **Previous Land Use:** In general the construction of these sites represented either the renewal of industry or the expansion of industry on to agricultural land. More than two fifths of the most recent previous land uses were industrial, while a smaller proportion were field systems.

► **Proximity to Historic Settlements:** A site of this type is situated within 250m of the location of historic settlement of Ettingshall.

► **Building Form:** Individual records of industry within the BCHLC may contain several different building types, many of which may not be contemporary with their particular site. Nevertheless, brick is much more common among building materials of this period. The form of industrial buildings from the pre-war period and earlier pays due regard to the need for natural light, often in the roof profile or in the material used for roofs.

► **Site Structure:** The type is generally characterised by much larger individual sites than the others described here and, unlike the later sites adjacent to canals, none are recorded as being *industrial estates*.

► **Relationship with canal:** A large number of these sites are built on the banks of early, 18th century 'contour' canals. These are generally narrow, with one tow path, and on a similar level to the land fronting the canal.



The former premises of the metal processing company *Frost & Sons (Moxley) Ltd* next to the Walsall Canal, established in the 19th century (HBL5148). This view shows iron slag used in the construction of the building. While this part of the five hectare site now appears to be disused, the remainder is still an active location for the steel manufacturer *Wesson W Glynwed Steels Ltd*.



The buildings of the now disused brickworks next to the Dudley Canal in Brierley Hill (Dudley) (HBL7892). Bought by *Harris & Pearson* in the 1850s and expanded to its current size of two hectares, it ceased activity in the late 20th century.



This two hectare canalside site at Greet's Green (Sandwell) (HBL12510) was previously an ironworks. Since the early 20th century it has been re-used by the chemical industry, currently as a formaldehyde storage facility.



Above: This 5 hectare site with its long canal frontage at Bridge Street North, Smethwick (Sandwell) was, in the 19th century, part of the *Birmingham Plate Glass Works*. By 1900 it had become the *Kingston Metal Works*. In the 21st century, part of the site has now been rebuilt as an industrial estate (HBL12129).



The listed buildings of Langley Maltings on Titford Canal (Sandwell) were built in 1870. The 1.7 hectare site was last used by *Wolverhampton & Dudley Breweries PLC*, but is now disused (HBL11371).

Where to see them in the Regeneration Corridors

► **Soho, Smethwick, B66** (3 BCHLC polygons)

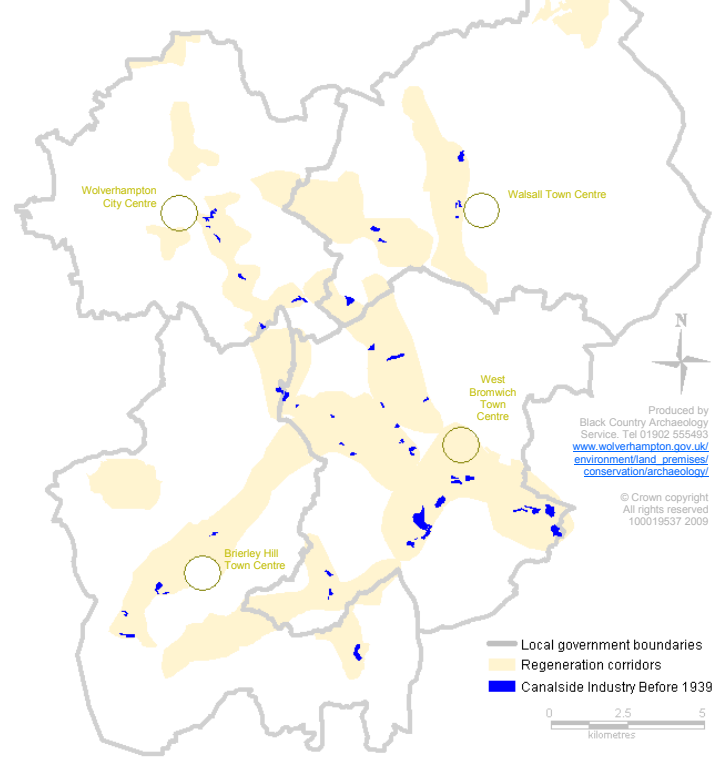
On two sides of the Birmingham canal in Smethwick, including the site of the Soho Foundry on *Foundry Lane* and the former premises of *GKN* on *Heath Street*.

► **Langley, Oldbury, B69** (3 BCHLC polygons)

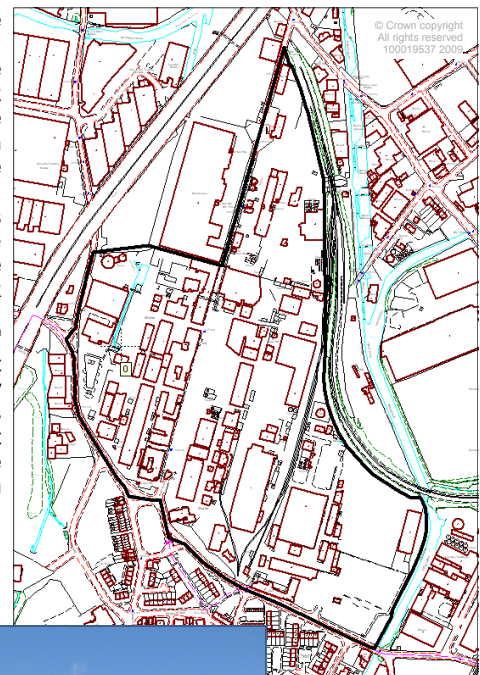
On the banks of the Titford Canal, including the extensive site occupied by the multinational chemical manufacturer *Rhodia* and Langley Maltings on *Western Road*.

Distribution of Canalside Industry Before 1939

Within the Regeneration Corridors
(Source: BCHLC, 2000)



Examples of large manufacturing sites owned by a single employer are now rare in the Black Country. The 20 hectare site now occupied in Langley (Sandwell) by the multinational chemical manufacturer *Rhodia* is possibly the largest single manufacturing site remaining in the Black Country. It was established in the middle of the 19th century and for a large part of its history was owned by *Albright & Wilson*. This is the plan of the site as it stood in 1999, showing the boundary of the site within the BCHLC (Source: BCHLC/*Landline*, 2000).



Left: the main entrance to the Oldbury premises of the chemical manufacturer *Rhodia* (Sandwell) (HBL11365).

Type 3: Other Industry Originating After 1938

► **Geographic Distribution:** This type of industry is much more typical of the landscape of the Regeneration Corridors than other parts of the Black Country: the vast majority (77%) of it is within the Corridor boundaries, making up almost a fifth (19%) of their total area. This is the largest proportion of the Corridors covered by any of our types.

► **Industrial Sector:** Unlike the older industrial sites, many of these - in this case a third - are described as *industrial estates* (although almost half are described in the BCHLC only as *works*). 'Depots' and 'warehouses' also account for a small share.

► **Origins & Recent Change:** Little differentiation is made in the BCHLC between the period of origin of these sites, although it is clear that the vast majority of buildings date from the period after WW2. Areas selected for fieldwork indicate that most of this type in the Regeneration Corridors in 2000 still remains, although there have been some notable redevelopments.

► **Previous Land Use:** Among the previous uses of the land taken by this type, industrial, extractive and agricultural purposes all feature prominently. Each of these three categories contributed around a quarter of the land on which these sites were constructed. .

► **Proximity to Historic Settlements:** Scattered across a wide area, sites of this type are situated within 250m of the location of eleven historic settlements in the Black Country.

► **Building Form:** Individual records of industry within the BCHLC may contain several different building types, many of which may not be contemporary with their particular site. Nevertheless, buildings from the post-war period are less likely to include brick built elevations and instead favour part brick, part steel clad walls. Roofs are shallow pitched roofs, often of steel or asbestos-concrete.

► **Site Structure:** The type is often made up of relatively small units (although not, on average, as small as type 1).



Although its origins are in the mid 20th century, Pensnett Industrial Estate (Dudley) is still seeing construction of new industrial units (HBL8210).



A firm manufacturing aluminium extrusions on Ann Street, Willenhall (Walsall). The steel clad building on the left is a 21st century construction. The brick fronted building on the right is from the second half of the 20th century and replaced a Victorian infant school (HBL5062).

Both Hall Street and Bond Street near the centre of West Bromwich (Sandwell) are locations for a small industrial units. Perhaps the largest, *Sections and Tubes Ltd* manufacture a number of cold rolled metal products. The site replaced an area of pre-1880 terraced housing fronting onto Lyng Lane (HBL12466)



A post-war 20th century industrial building used here as a wholesale warehouse, Pensnett Industrial Estate (Dudley) (HBL8210).



A late 20th century industrial estate built on fields in Wednesfield (Wolverhampton), such as those which are common within this type. Accessed from a new road (off Planetary Road), the estate consists of more than 50 units constructed with steel frames and part brick, part steel clad-elevations. Shallow pitched roofs incorporate translucent roof lighting. Each unit has car parking at the front (HBL3222).

Where to see them in the Regeneration Corridors

► **Kenrick Way/Telford Way, West Bromwich, B71**

(17 BCHLC polygons)

Centred around the junction of these two dual carriageways, and close to Junction 1 of the M5.

► **Pensnett**

(8 BCHLC polygons)

Accessible from *Stallings Lane* and, to the south, *Dudley Road*, this area is dominated by Pensnett Industrial Estate.

► **Wednesfield**

(19 BCHLC polygons)

Between *Wednesfield Way* in the north and the railway bridge on *Neachells Lane* in the south.

► **Darlaston/Wednesbury**

(8 BCHLC polygons)

A large area straddling the boundary between Sandwell and Walsall, accessible from the *Black Country New Road* and *Woden Road West*.

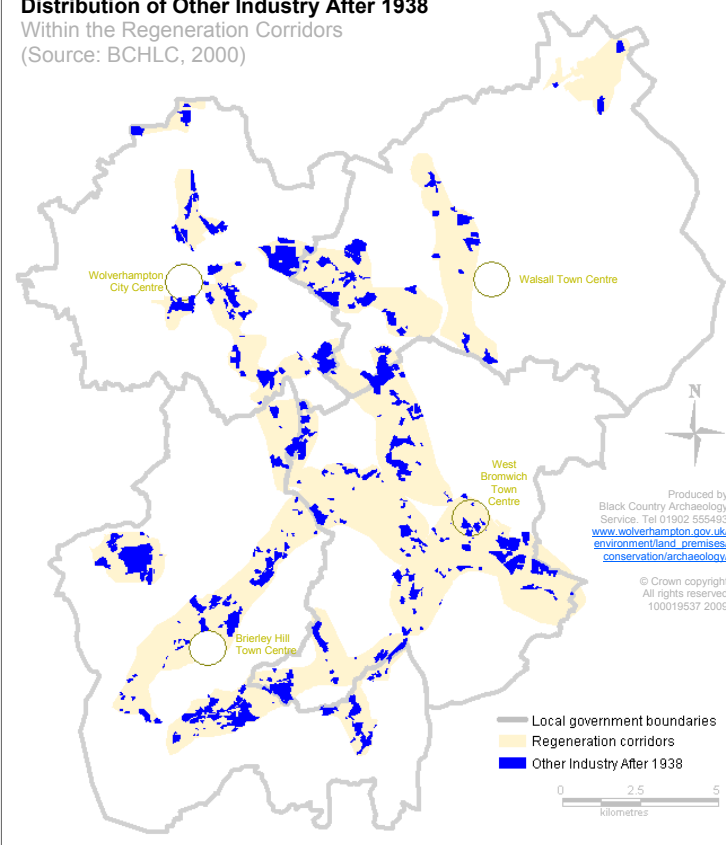
► **Bilston, WV14**

(8 BCHLC polygons)

Centred on the junction of *Vulcan Road* & *Oxford Street* in Loxdale.

Distribution of Other Industry After 1938

Within the Regeneration Corridors
(Source: BCHLC, 2000)



A plan of the vast Pensnett Industrial Estate (1948) in Dudley, which was laid out around a series of straight avenues on former colliery land.

Right: on Portway Road, Oldbury (Sandwell), this area was largely undeveloped agricultural land until a group of industrial units were built on it in the second half of the 20th century. One of the units, *GSB Logistics* (shown here), is involved in packaging and distribution of clothing (HBL11316).



Type 4: Other Industry Originating Before 1939

► **Geographic Distribution:** Much more typical of the Regeneration Corridors than other parts of the Black Country, two thirds of this type within the Black Country is located inside the Corridors, making up 4% of their total area.

► **Industrial Sector:** About a third of the area covered by this type of industry is described in the BCHLC only as 'metal works', and a further quarter simply as 'works'. Chemical and engineering works are also significant. Unlike the later industrial sites adjacent to canals, only a very small proportion are recorded as industrial estates.

► **Origins & Recent Change:** Generally more recent than the remaining industry adjacent to the canals (see type 2), more than two-thirds of this type originates after 1920. The surviving buildings and structures are from a range of periods. Of all types, this appears to have undergone most change in the 21st century: based on a small selection, possibly more than half have undergone demolition in the last eight years.

► **Previous Land Use:** As might be expected for older industrial sites which may have been built some distance from a canal, their construction often represented the expansion of industry over agricultural land. More than two fifths of the most recent previous land uses were as part of field systems.

► **Proximity to Historic Settlements:** Sites of this type are situated within 250m of the location of seven historic settlements in the Black Country.

► **Building Form:** Individual records of industry within the BCHLC may contain several different building types, many of which may not be contemporary with their particular site. Nevertheless, it appears that this type is more likely to be structured around building forms which use natural light to some degree. These include north-lit sheds, either built to house large production facilities, such as in Blakenhall (see image opposite, top), or as small units such as the buildings of the scrap yard on Webb Street, Coseley (see image). Brick is common in elevations, sometimes part steel clad, while asbestos-concrete is common as a roofing material.

► **Site Structure:** The type is often made up of relatively small units (although not, on average, as small as type 1).



Next to the line of a disused railway, these buildings on Kendricks Road, Darlaston (Walsall) are now used by a precision tool manufacturer (HBL6302).



The Jones & Attwood factory (Titan Works) on Old Wharf Road, Stourbridge (Dudley). Built in the 19th century, it is used today by a number of companies (HBL7782).

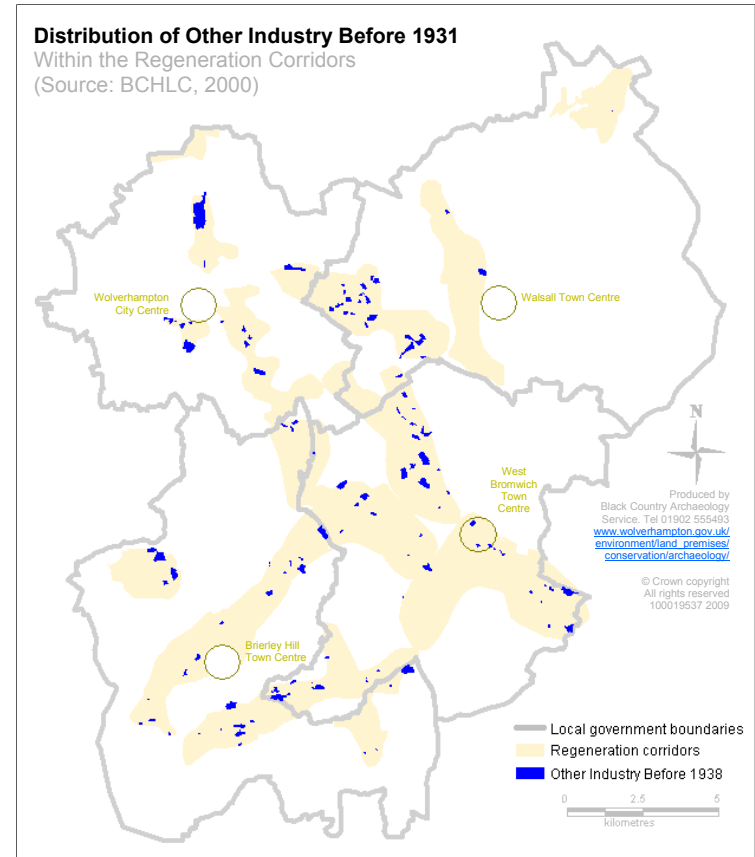
Immediately below: an inter-war building on Bilston Lane, Willenhall (Walsall) (HBL5071).



Right: early 20th century industrial buildings on Sunbeam Street, Wolverhampton (HBL680).

Bottom centre: The premises of Steelway, on the Bilston Road in Wolverhampton (HBL2509). Bottom right: The small north-lit shed of a scrap-yard in Coseley, the steel cladding was presumably added later (HBL7193).





Top left: north-lit sheds, characteristic of the motor industry on Marston Road in Blakenhall (Wolverhampton) (HBL680).

Left: The exposed brick work at the rear of the site at Temple Bar, Willenhall (Walsall), reveals several phases of use (HBL5047).



Right: A rare example of continuity from the 19th to the 21st centuries. Established on this site more than 200 years

ago, this brick and tile works in Pensnett (Dudley) is possibly the oldest surviving active manufacturing facility in the Black Country (HBL8509).

Where to see them in the Regeneration Corridors

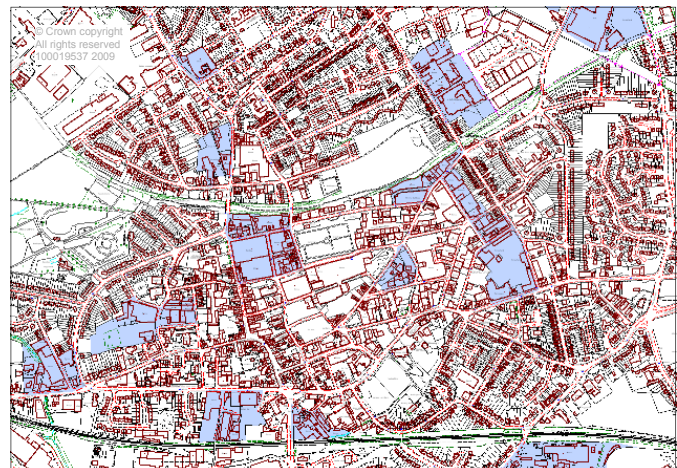
► **Pensnett, Dudley** (4 BCHLC polygons)

Travelling from *Dudley Road* on to *Dreadnought Road*, *Tansey Green Road* & *Oak Lane* you pass a number of these sites.

► **Great Bridge** (5 BCHLC polygons) On *Bagnall Street* and *Harvills Hawthorn*.

► **Willenhall** (17 BCHLC polygons) A number of sites dispersed around the town, all within a radius of a kilometre.

► **Blakenhall** (1 BCHLC polygon) Several industrial estates off *Upper Villiers Street*.



Above: the unusual pattern of surviving pre-1939 industrial sites (shown with blue tint) scattered around the centre of Willenhall, mixed with commercial and residential areas of the town.

Type 5: Passenger Rail/Metro Stations

► **Geographic Distribution:** Rail and metro lines and associated stations are a particular feature of the Corridors. Of the 37 stations and metro stops in the Black Country, 70% are located within the Corridor boundaries, and most of the remainder are within a short distance.

► **Type:** Equally divided between rail stations and metro stops (there are thirteen of each within the Corridors), the stations and metro stops themselves occupy relatively small sites. However, associated infrastructure such as car parks encompass a much larger area. Most stations and metro stops now have car parks associated with them.

► **Period of Origin:** Although a majority of rail stations originated in the 19th century, Metro stops are modern, being part of the re-use of the old rail route in 1999. Only two Metro stops within the Regeneration Corridors are on the site of former rail stations. Most car parks were added in the mid/late 20th century, although several have been laid out since 2000.

► **Previous Land Use:** Typically, land used for car parks had been previously occupied by railway sidings which became disused with the shift of freight to the roads. In addition, demolition of housing created space for the expansion of car parking space.

► **Proximity to Historic Settlements:** No rail or Metro stations are within 250m of historic settlements.

► **Building Form:** Metro stops generally only include steel and glass shelters on the platforms, although larger structures have been built to provide lift and stair access to them. Rail stations include the remains of both of larger brick built Victorian buildings, as well as lower quality single-storey flat-roofed prefabricated units.

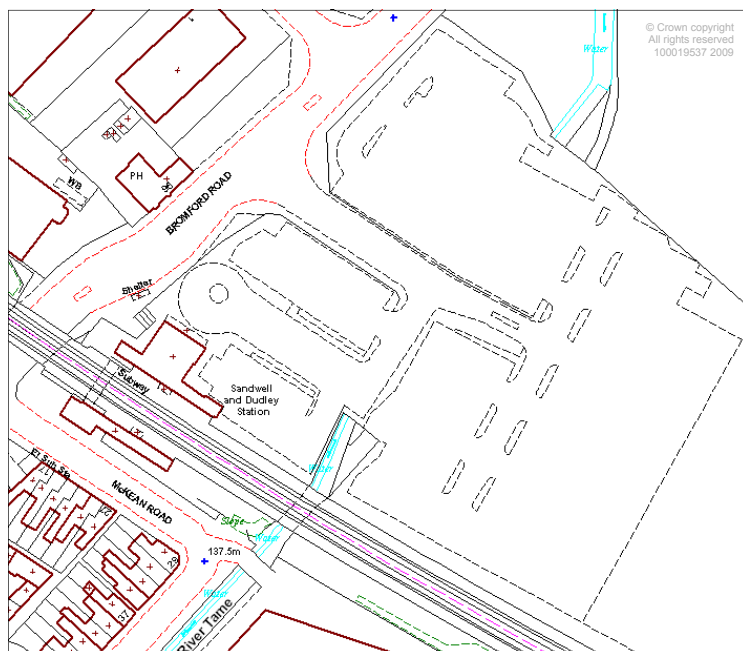
► **Line and Platforms:** With the exception of *Smethwick Galton Bridge* and *Hawthorns*, stations serve one line with two platforms. Only one of the rail stations has platforms of more than 250m in length.



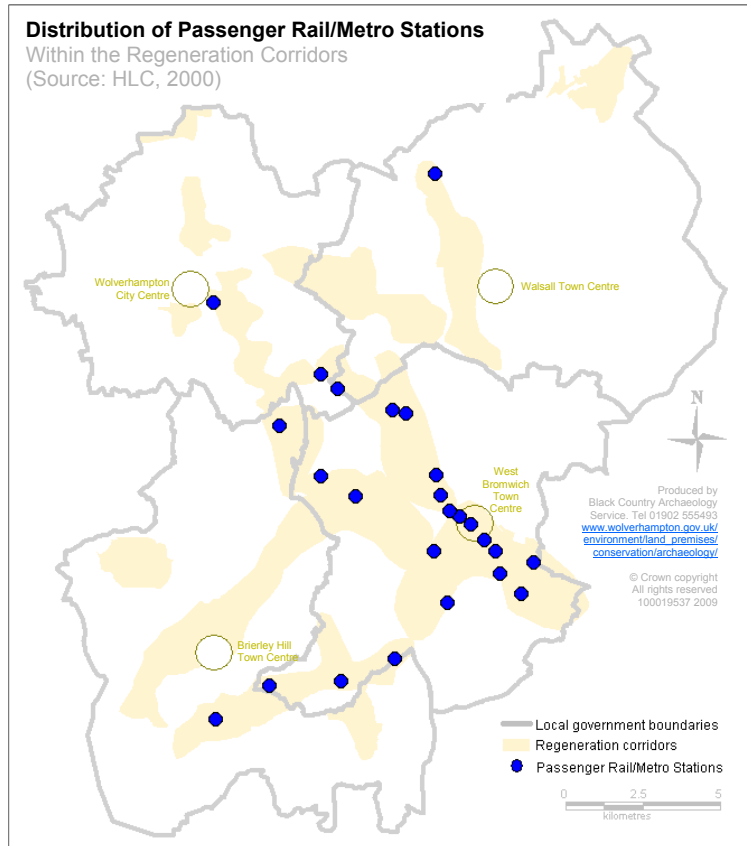
Late 20th century lift towers are distinctive features of stations at *Hawthorns* (above) and *Smethwick Galton Bridge* (below) (both in Sandwell). Opened in the 1990s, both were the first stations to be built on their respective sites (although the latter replaced Smethwick West station) and they perform important functions in linking intersecting routes.



Metro stops near the centre of Wednesbury and West Bromwich are the only two which are built on the sites of former, 19th century rail stations. However, few if any of the earlier features remain.



A plan of Sandwell and Dudley Station in Oldbury at the end of the 20th century. The large car park, built over the River Tame and on a neighbouring railway goods yard, opened the station up to 'park and ride' use to travel to centres such as Wolverhampton, Birmingham, and other cities served by the West Coast Mainline (Source: BCHLC/Landline 2000).



Top left: the car park of Old Hill Station (Sandwell), opened in 1866. Now providing access to the 'Jewellery Line', the station was previously at the junction of three lines. The car park was built on the former site of *Victoria Leather Works*.

Left: Dartmouth Street Metro stop in West Bromwich (Sandwell).



Smethwick Rolfe Street Station (top right) and *Dudley Port Station* (bottom right) (both in Sandwell), both opened in 1852. Rolfe Street has retained its original red brick ticket hall while Dudley Port, a remnant of a larger pair of linked stations, currently relies on a Portakabin to house its ticket office.



Where to see them in the Regeneration Corridors

► **East of Oldbury, B69**

(25 HLC polygons)

Around the junction of the old and new Birmingham canals, including a broad swathe of land either side of the West Coast railway line, east of *Sandwell & Dudley Station*. Also visible from the elevated section of the M5 motorway north of Junction 2.

► **Great Bridge, B70**

(11 HLC polygons)

The land surrounding the Ryder's Green Junction of the Walsall Canal, accessible from *Ryder's Green Lane*, *Charles Street* and *Great Bridge Street*.

Type 6: Canalside Open Land

► **Geographic Distribution:** Partly a result of the common occurrence of canals inside the Regeneration Corridors, canalside open land is more typical of the Regeneration Corridors than of other parts of the Black Country. More than two fifths (43%) of what exists within the Black Country is located within the Corridors, making up 4% of their area.

► **Type:** Half of the area of canalside open land is recorded as being derelict, and the remainder is described as rough grassland or scrub. In this instance, derelict is generally taken to refer to land known to have had buildings on it or where aerial photography shows no vegetation. On average, derelict sites are half as large as those of rough grassland and scrub.

► **Period of Origin:** The land which is derelict generally became so in the post-war 20th century, with only about a fifth having been derelict for a longer period - i.e. from the 1930s or earlier. Rough grassland and scrub has an older profile, with about a two thirds originating before WW2.

► **Recent Change:** open land, particularly that described as derelict, is in a sense land which is awaiting re-use. Not surprising then that it appears that a proportion of the land which was derelict in 2000 has since been built on. In the meantime, it is also the case that new land has entered this category in the 21st century as buildings are subjected to demolition and redevelopment.

► **Previous Land Use:** The previous uses of this land differ depending on whether it is recorded as derelict or rough grassland/scrub. Most derelict land (i.e. 54%) was previously used for industrial purposes, while rough grassland and scrub is largely (i.e. 57%) former agricultural land. Land previously used for mining and quarrying is split between both sub-categories.

► **Proximity to Historic Settlements:** A site of this type is situated within 250m of the location of historic settlement of Ettingshall.



A large area of canalside derelict land on Ward Street in Ettingshall (Wolverhampton). Created in the mid/late 20th century when a 19th century works was demolished, the site sits between the canal and railway and previously had access to both via local sidings and canal basin (HBL2165).



Right: Rough grassland and scrub next to Ladymoor Road in Bilston (Wolverhampton). Located on the banks of the now bypassed Bradley loop of the Birmingham canal, this land was previously in an area of coal mining (HBL12).



Right: Rough grassland and scrub (seen here on the right of the picture) next to the Wyrley and Essington canal in Brownhills (Walsall). This was previously agricultural land (HBL1570).

Where to see it in the Regeneration Corridors

► **Bilston, WV14**

(5 HLC polygons)

The area next to the Birmingham Canal and the remains of the Bradley canal loop, accessible from *Ladymoor/Anchor Road*.

► **Oldbury, B69**

(4 HLC polygons)

South of the junction of the Birmingham Canal and Gower Branch Canal, a large area of open land encompasses both banks of the River Tame (Oldbury Arm). The land is accessible from *Dudley Road East*.

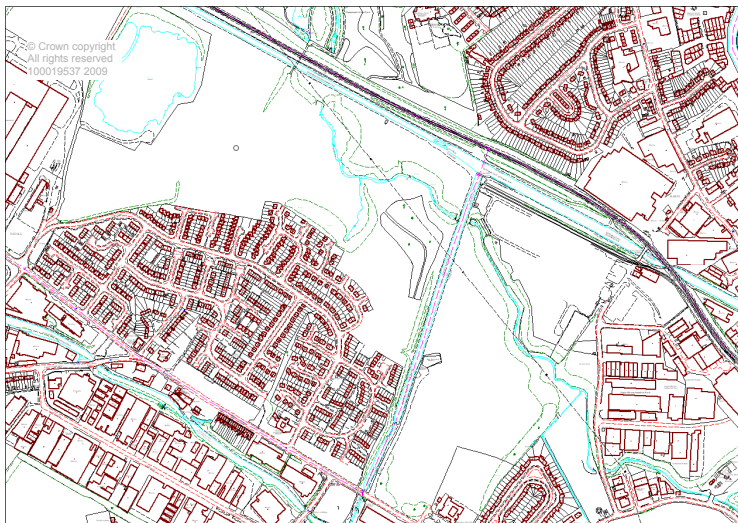
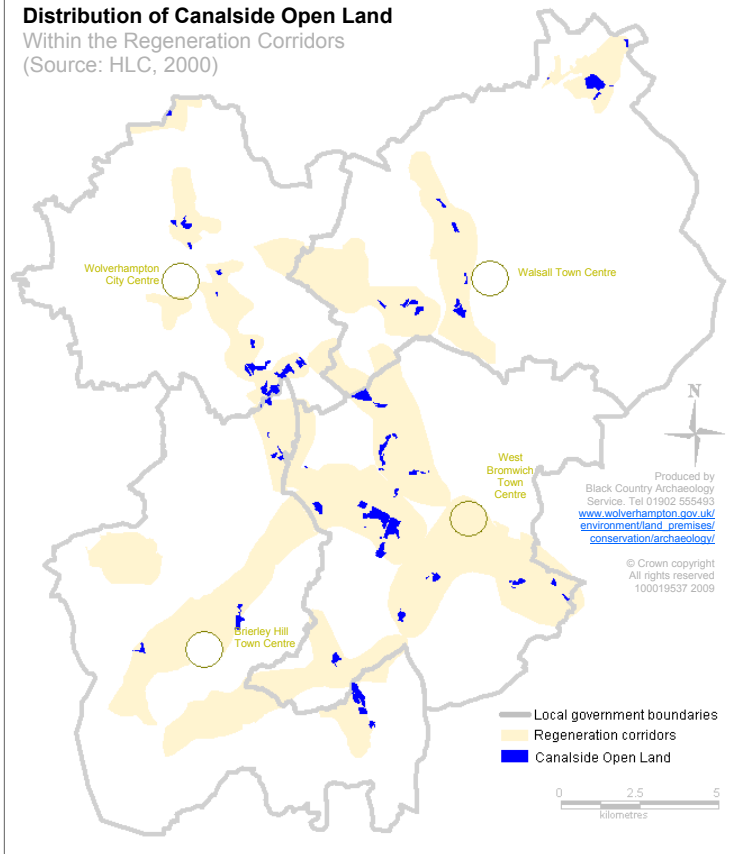
► **Brownhills, WS8**

(3 HLC polygons)

Large areas of open land next to the Wyrley and Essington canal, opposite Brownhills town centre. Accessible by footbridge from the town, or from *Clayhanger Road/Bridge Street*.

Distribution of Canalside Open Land

Within the Regeneration Corridors
(Source: HLC, 2000)



Left: the largest concentration of canalside open land in the Regeneration Corridors is this 60 hectare area of rough grassland and scrub in Tividale (Sandwell). Previously colliery land, the area is crossed by the Birmingham canal and the River Tame. The plan shows the area as it was in 2000 (Source: BCHLC/Landline, 2000).



Of the open land recorded adjacent to canals in 2000, a substantial amount has been put to new uses in the intervening years. These include industry, housing and religious buildings. Some examples of 21st re-use of open land next to the Birmingham Mainline Canal in Sandwell include a scrap yard on Rabone Lane (above, right), housing on Doulton Drive (below, right) and a Hindu Temple on Dudley Road East (left).



Type 7: Nineteenth Century & Edwardian Terraced Housing

► **Geographic Distribution:** This type of housing is more typical of the Regeneration Corridors than other parts of the Black Country. Around a third of this type of housing in the Black Country is located in the corridors, making up 4% of their total area.

► **Housing Type:** Most of the area covered by this type are in fact occupied by properties described as *small* terraced houses, i.e. each occupying a footprint of 50sqm or less. At least half of this is 'tunnel-back' housing.

► **Origins & Recent Change:** A substantial majority are late Victorian (1881-1900) and another sizeable proportion (one in five) Edwardian (1901-1910). Areas sampled showed that most housing of this type identified in the regeneration corridors in 2000 remains. Little demolition appears to have taken place, and some small 21st century infill had been built.

► **Previous Land Use:** In general these streets represent the expansion of housing into agricultural areas. More than 70% of most recent previous land uses were field systems, with 11% representing the reclamation of extractive sites. Only 6% of the area is the result of housing replacement.

► **Proximity to Historic Settlements:** Sites of this type are situated within 250m of the location of eight historic settlements in the Black Country.

► **Building Form & Materials:** By definition terraced (although sometimes short), and generally 2-storey. Unusually, a third storey is sometimes achieved using dormer windows and, where these are original, the roof pitch is steeper to accommodate the extra storey. Chimney stacks largely remain (generally front and back, and shared between adjoining properties). Brick, slate and timber were among original materials, frequently now replaced with tiles and UPVC, or covered with render or cladding. In places, original detailed ornamentation survives.

► **Site Structure & Boundary Treatment:** With an average of 45 homes per hectare, this is very high density housing. Setback is smaller than for any other type (between 2.5 and 3.5m). Properties are often without a front garden and, where a garden or yard exists, there is little if any room for mature vegetation. Front gardens are commonly bounded by a red or blue brick wall. Plots usually include a rear garden or yard across their width.

► **Street Structure:** Terraces are generally contemporary with streets, although some terraces are situated alongside older, more long-distance routes. The majority of streets laid out at the time the houses were constructed do not necessarily follow the line of earlier (agricultural) landscape patterns. Where space allowed, the streets were laid out in straight, grid-iron patterns (with typically 60-100m between them). They are generally narrow, allowing normally 10-13m between property boundaries on the street front, and many retain the relative uniformity of house design along the streets. Parking is generally on street.



Prince Street, Pleck (Walsall). This narrow late Victorian street has only a two-metre setback. One consequence of so little parking space together with low car ownership is that pedestrian street life is more lively than in other more modern neighbourhoods.



Late Victorian terraces Trinity Street, West Bromwich (Sandwell). The street linked Trinity church with West Bromwich High Street. The other side of the street was replaced in the second half of the 20th century.



UPVC replacement door and window frames, seen here together with Victorian ornamentation on houses on Campbell Street, Brierley Hill (Dudley).



These terraces on Durban Road, Cape Hill (Sandwell) are unusual within this type in having sufficient front garden to convert into hard-standing to accommodate cars.

Where to see them in the Regeneration Corridors

► **Birchills, Walsall WS2** (6 HLC polygons)

To the north-west of Walsall Strategic centre and located between two branches of the Walsall canal.

► **Greets Green, West Bromwich B70** (7 HLC polygons)

A short distance to the west of West Bromwich Strategic Centre, this neighbourhood has been part of the Greets Green New Deal for Communities Area.

► **High Street (south-east), West Bromwich B70** (4 HLC polygons)

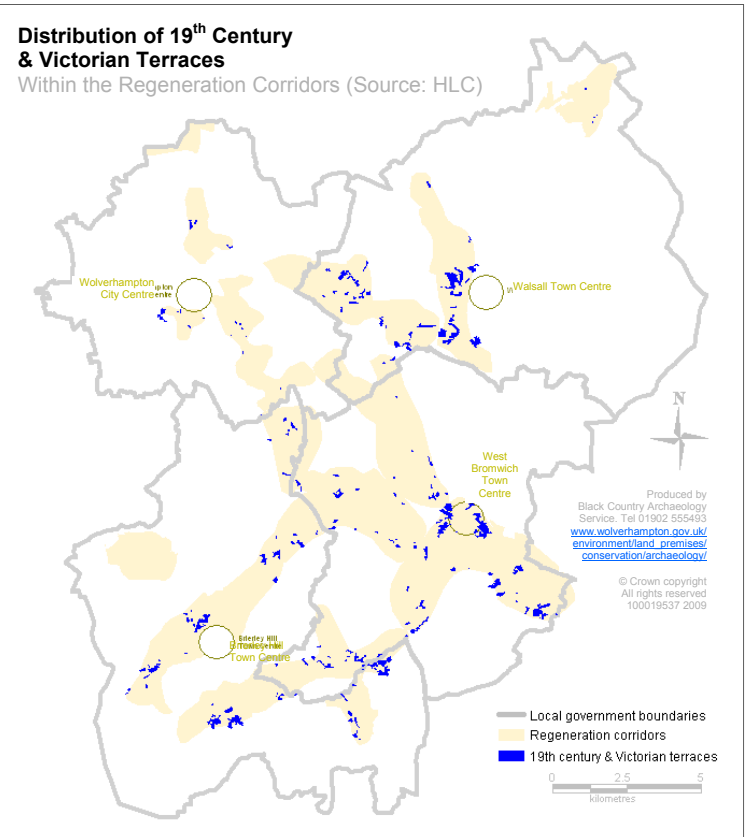
On the eastern edge of West Bromwich Strategic Centre and close to junction 1 of the M5.

► **Blackheath/Halesowen B62** (12 HLC polygons)

South of Blackheath High Street and the Jewellery line rail route. On the boundary of Sandwell and Dudley.

► **Lye DY9** (12 HLC polygons)

The area surrounding the centre of Lye: south of the Jewellery line rail route and the River Stour.



Above: small terraced houses (tunnel back) on Cape Hill, in the south-east of the area within the Regeneration Corridors. This grid of streets is contemporary with the houses, being laid out in the last decade of the 19th century (Source: BCHLC, 2000).



Right: Terraces with dormer windows on Windmill Lane, Smethwick (Sandwell)

Type 8: Post-war High Density Housing

► **Geographic Distribution:** This type of housing is more typical of the Regeneration Corridors than other parts of the Black Country. Around a quarter of this type of housing in the Black Country is located in the corridors, making up 5% of their total area.

► **Housing Type:** Two fifths of the area covered by this type are in fact occupied by properties described as *small* terraced houses (i.e. each occupying a footprint of 50sqm or less) while a similar area is covered by housing described as apartments or maisonettes. High rise flats (sometimes with terraces alongside) make up only 12% of the area, although possibly a higher proportion of the units.

► **Origins & Recent Change:** Almost two-thirds (by area) were built in the 1960s or 1970s, although a further fifth date from the 1980s or 1990s. Areas sampled showed that, in the period since 2000, this housing type has undergone the most demolition and redevelopment.

► **Previous Land Use:** In general these homes represented, at the time of their construction, a renewal of housing. Almost half (by area) were built on areas of previous housing, the largest proportion of any of the housing-based Landscape Elements profiled here. A fifth were built on former agricultural land, and a similar proportion on former industrial or open land.

► **Proximity to Historic Settlements:** Sites of this type are situated within 250m of the location of eight historic settlements in the Black Country.

► **Building Form:** Either 2-storey terraces or high rise flats, often mixed on the same streets (although, in cases, the high rise element has been demolished). This is the only housing type to create accommodation higher than 3 storeys. Roof pitch is often flat or very shallow and built without chimneys. Commonly brick (or part brick) elevations with tiles on pitched roofs where they exist. Little obvious ornamentation.

► **Site Structure & Boundary Treatment:** With an average near to 55 houses per hectare, this is the highest density housing of all the housing types described here. It is not generally possible to compare setback for this type, given that the accommodation is generally in high rise blocks, or fronting on to paths rather than the street itself. High rise flats are often surrounded by open lawns. Many low rise plots were designed without a front garden and instead houses open to common pedestrian access leading to the street. Where they exist gardens are either walled or fenced.

► **Street Structure:** Both low and high rise elements were usually built to regular designs within each development. Housing and street layout are generally contemporary, often diverging significantly from preceding patterns on the same site. Low rise housing often adopts the approach of fronting the properties on to pedestrian-only routes (illustrated in the plan opposite), while vehicular access is restricted to narrow lanes at the rear. Areas of car parking are generally built into the street design.



A development of the 1960s/70s on Copthall Street, Great Bridge (Sandwell). It includes four eight-storey blocks such as these.



Scrimshaw House on St Quentin Road, Pleck (Walsall). This development illustrates the combination of terraces and tower blocks common within this type. Since 2000, ... have been demolished.



The fronts of terraced housing off Beechcroft Road, Old Hill (Sandwell). Flat or shallow roofs are common on housing of this type.

Where to see them in the Regeneration Corridors

► **Smethwick, B66**

(28 HLC polygons)

The largest surviving single area is to the south of the A557 dual-carriageway (i.e. *Tollhouse Way - Soho Way - Grove Lane*). It is accessible from the roads that cross it, in particular *Crockett's Lane* and *Windmill Lane*. Several of the tower blocks which were previously part of this landscape have been demolished.

► **Lye, Stourbridge DY9**

(6 HLC polygons)

The area south-east of *Chapel Street* and *High Street*.

► **Willenhall, Walsall WV13**

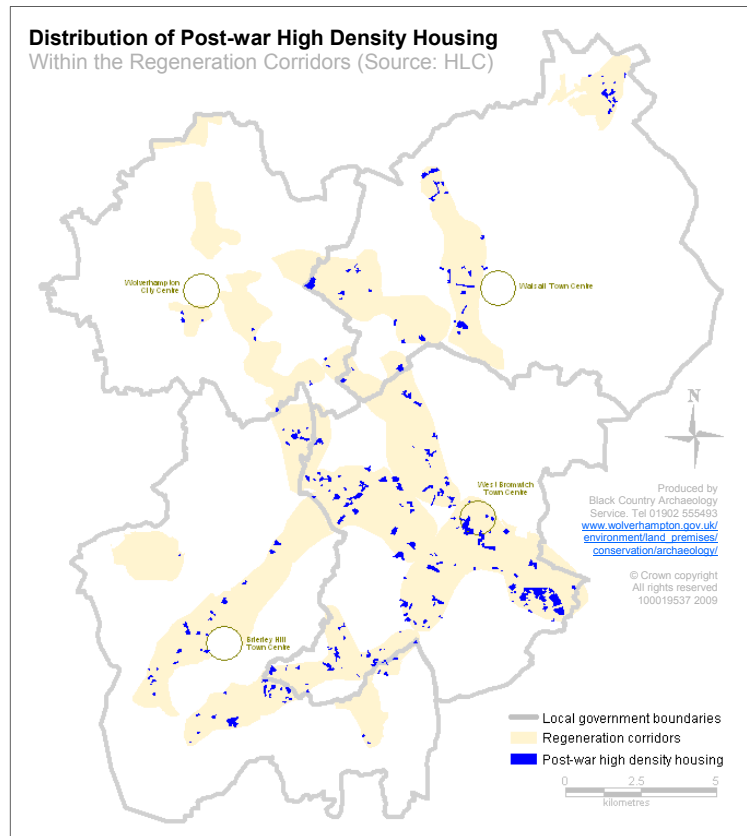
(1 HLC polygons)

Noose Crescent and associated cul-de-sacs (off *Noose Lane*).

► **The Lyng, West Bromwich B70** (9 HLC polygons)

In 2000, this area was one of the largest clusters of this type - especially in the area south of West Bromwich town centre, between *Bromford* and *Lyng Lanes*. However the majority has since been re-developed or has been demolished.

Distribution of Post-war High Density Housing Within the Regeneration Corridors (Source: HLC)



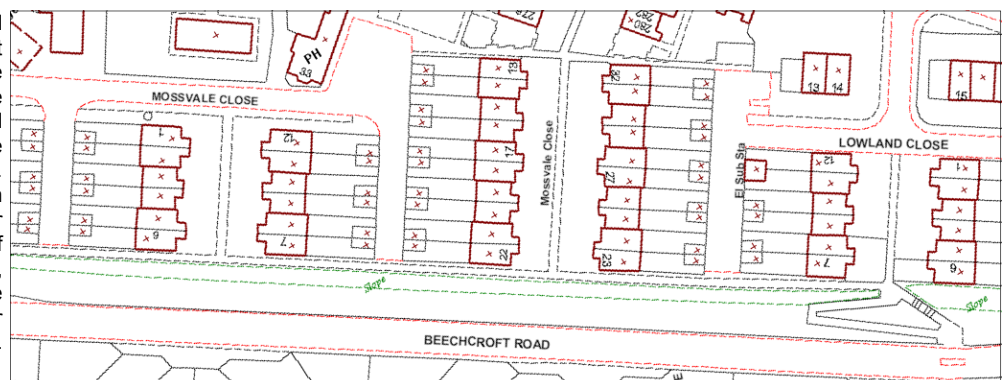
Right: part of a development of terraced housing on the slope rising from High Street, Lye (Dudley). It was built in the 1960s and 70s on an area of squatter settlement dating back at least 200 years.



Left: Flat roofed terraces on James Close, Smethwick (Sandwell). North Smethwick includes the largest concentration of post-war high density housing in the Regeneration Corridors.

Right: these terraced houses (see photo at the bottom of the opposing page) date to the 1960s/70s and were built on the site of a disused colliery.

The street pattern provides for vehicular access to the rear of the properties while, in the front, lawns are divided by paths for pedestrians.



Type 9: Mixed 20th Century Semi-detached and Terraced Housing

► **Geographic Distribution:** This type of housing is not particularly typical of the Regeneration Corridors (when compared to the wider Black Country), but is nevertheless a significant part of them. Around 21% of this type of housing in the Black Country is located in the corridors, making up 4% of their total area.

► **Housing Type:** Almost all of these neighbourhoods are in fact occupied by properties described as mixed *small* semis and terraced houses (i.e. each occupying a footprint of 50sqm or less).

► **Origins & Recent Change:** Although these homes originate from a long period starting in 1938, more than two thirds of them originate from the second half of the 20th century, in particular from the 1950s (almost 1 in 3 are from this decade). Areas sampled showed that little demolition appears of this type of housing has taken place, and some small 21st century infill had been built.

► **Previous Land Use:** These neighbourhoods were built on land with a variety of different uses, although the largest contributions were made by former agricultural land (38%), and land reclaimed from mining and quarries (20%).

► **Proximity to Historic Settlements:** Sites of this type are situated close to the location of historic settlements of Darlaston, Homer Hill and Tipton Green.

► **Building Form:** By definition terraced or semi-detached and generally 2-storey with moderate pitched tiled roofs, not generally hipped. Chimney stacks survive where built, but were not included on later examples. Brick built elevations and little obvious ornamentation.

► **Site Structure & Boundary Treatment:** At about 28 homes per hectare, density is below average for the Black Country. Setback is somewhere near average (at about 8 to 12m). Front gardens often consist of lawns (or hard standing) and are bounded by low walls or hedges, or with no barrier. Aside from boundary hedges and occasional small trees, front gardens contain little vegetation.

► **Street Structure:** Housing is generally contemporary with streets and is built with regular frontage designs. The majority of streets laid out at the time the houses were constructed do not necessarily follow the line of earlier landscape patterns. The shape of streets varies between sites, although they are usually narrow, and the use of cul-de-sacs in the design is common. Most commonly, streets are laid out without trees. Parking is generally on street, or on hard standing at the front of properties.



A typical example of this type is shown here in Dawes Avenue, West Bromwich (Sandwell). Of about 250 houses in this post-war development, about one in every five is built within a semi-detached pair, the remainder being terraced. The terraces are generally short, comprising 4, 5 or 6 properties.



Curtin Drive, Moxley (Walsall). In this large 1950s development of more than 400 properties, the norm is for houses to be organised in a terrace of four homes with access through a central passage, as shown here.



A smaller, more modern development of mixed semis and terraces at Pearson Street, Cradley Heath (Sandwell). Here, more space is made for cars.



Mid 20th century bungalows and semis on Huckler Road, Pleck (Walsall). Huckler Road and neighbouring streets were laid out on land which was previously allotments.

Where to see them in the Regeneration Corridors

► **Moxley, WV14** (2 HLC polygons)

The large estates both to the north and south of the *High Street*, near the boundary between Walsall and Wolverhampton.

► **South of Walsall, WS2** (2 HLC polygons)

Large estates off *Bescot Road* and *Darlaston Road*, just north of junction 9 of the M6 motorway.

► **Broadway West etc., Walsall, WS1** (4 HLC polygons)

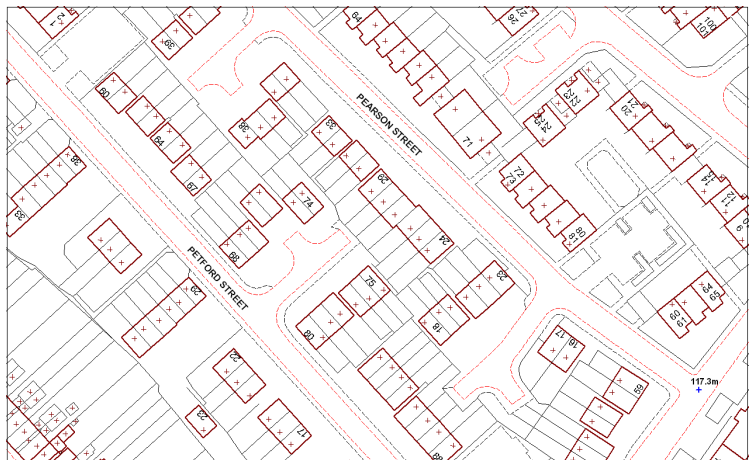
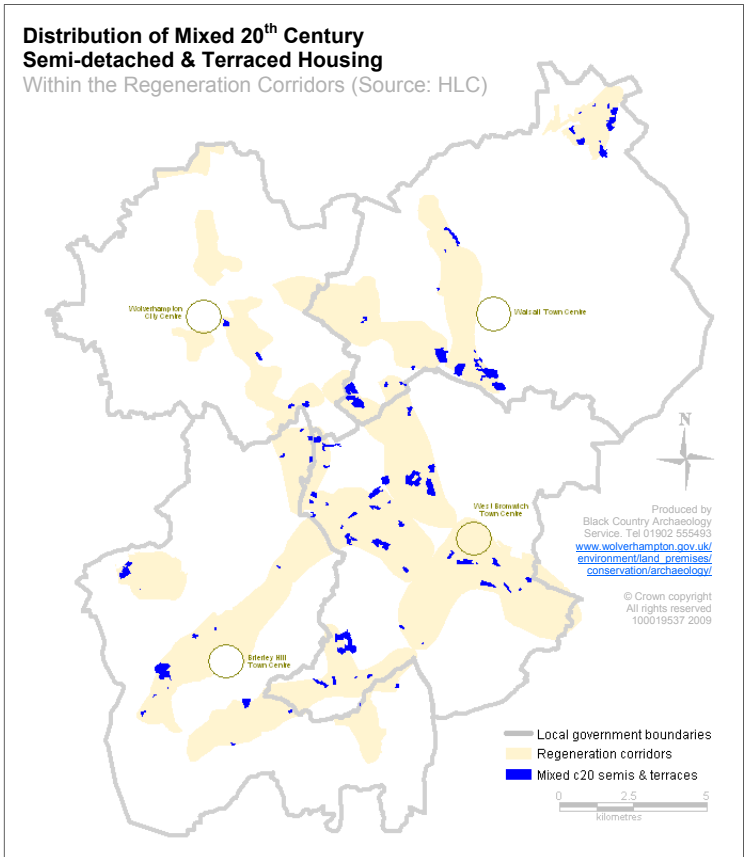
Estates bounded in the West by the A4031 *West Bromwich Road* and in the north by the A4148 *Broadway*.

► **West Bromwich, B70** (3 HLC polygons)

Estates off *Dial Lane* and *Harvills Hawthorn/New Street*, north west of Black Lake Metro station.

► **The Brickhouse Estate, Rowley Regis, B65** (2 HLC polygons)

The large area of housing immediately to the east of the Dudley Canal, accessible from *Harvest Road*.



Post-war redevelopment of Victorian terraced neighbourhoods in some cases modified the street plan by introducing short cul-de-sacs to the simpler, pre-existing patterns. Here, in Cradley Heath (Sandwell), the parallel lines of Pearson and Petford Streets (which can be seen running top-left to bottom-right) were the basis of the preceding plan (HBL10696).



Post-war semi-detached houses on Swanfield Road, Stourbridge (Dudley).

Type 10: Mixed Inter-war Semi-detached & Terraced Housing

► **Geographic Distribution:** This type is no more typical of the Regeneration Corridors than it is of the wider Black Country, but is nevertheless a significant part of the corridors. Around 16% of this type in the Black Country is located in the corridors, making up 4% of their total area.

► **Housing Type:** Almost all of the areas within this type are in fact occupied by homes described as *small* semis and terraced houses, i.e. each occupying a footprint of 50sqm or less. The type differs from the other inter-war category described here (see type 12) in that it includes developments of short terraced houses, achieving a higher density. The surviving examples are also, in general, in estates twice the size of the other inter-war category.

► **Origins & Recent Change:** Houses of this type all date from the period 1920 to 1938. Those sampled showed that most housing of this type identified in the regeneration corridors in 2000 remains.

► **Previous Land Use:** While half of these properties were built on agricultural land, almost a third of this type of housing was built on land reclaimed from the Black Country's extractive industries, a higher proportion than for any of the other housing-based Landscape Elements profiled here.

► **Proximity to Historic Settlements:** Sites of this type are situated close to the location of historic settlements of Bloxwich, Cradley Heath and Tipton Green.

► **Building Form:** By definition semi-detached or terraced (although these are often short), and generally 2-storey. Slated roofs are a moderate pitch and, characteristic of this type and type 12, are hipped. Brick chimney stacks are shared between adjacent properties. Brick elevations are often rendered. Little obvious ornamentation.

► **Site Structure & Boundary Treatment:** At 33 homes per hectare, this type shows average density for the Regeneration Corridors. At about 8 to 12m, setback is also somewhere near average. Front gardens are often bounded by walls of pre-cast concrete blocks, although in places these have been removed to provide access for cars.

► **Street Structure:** Housing is generally contemporary with streets and is built with regular frontage designs. Most streets laid out at the time the houses were constructed do not generally follow the line of earlier landscape patterns. The shape of streets varies between sites, although they are usually narrow. Trees, now mature, were sometimes built into the design of the street (see photos of Bentley and Stourbridge). Parking is generally on street, or on converted garden space at the front of properties.



A third of this Type of housing was built on land previously used by extractive industries. In the case of these small inter-war semis and short terraces on Eastern Avenue, Brierley Hill (Dudley), homes were built on land previously given to coal pits. The houses have been rendered to conceal their brick frontages, and the unusual one-sided street was perhaps caused by the position of the houses backing onto a railway line.



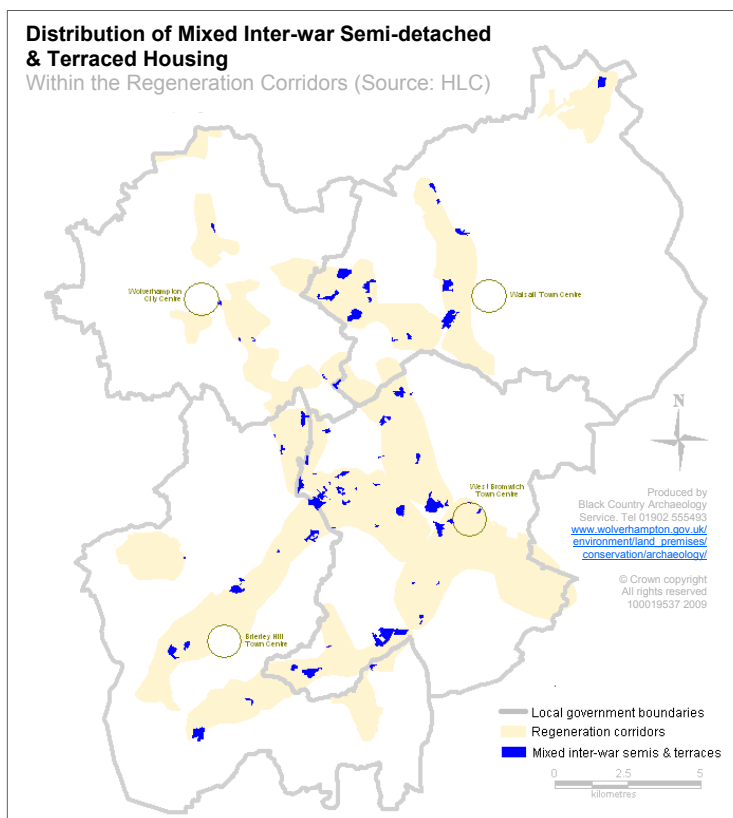
With a density typical of the type, the design of these streets, which contain terraces of four houses each, leave substantial open space at road junctions. The picture shows the corner of Belle Vue Road and Carlyle Road, Rowley Regis (Sandwell). Since the houses were built, a collective yard has been cut into the back gardens to provide garages space for residents.



A short, hipped-roof terrace on Frieze Road, Bentley (Walsall). This neighbourhood was built between the wars on land which was previously agricultural. The street design made room for trees which, in places, line the roads.



Short terraces on Webster Road, Willenhall (Walsall). This large estate (see street plan below) was built on land previously used by a colliery.



Where to see them in the Regeneration Corridors

► Willenhall, WV13

(5 HLC polygons)

Five areas of housing all within 1km of Willenhall Market Place: the estate around *Webster & Thorne Roads* (see photo above); that around *Aston & Pinson Roads*; off *Tyler Road*; on *Hobley* and *Marston Streets*; and *Gough* and *Slater Streets* and the connecting roads.

► Blackheath, Sandwell B65

(3 HLC polygons)

The housing either side of the north-south A4034 *Oldbury Road*, south of the route of *Pennycricket* and *Mincing Lanes*.

► The Hamblett's Estate, West Bromwich, B70

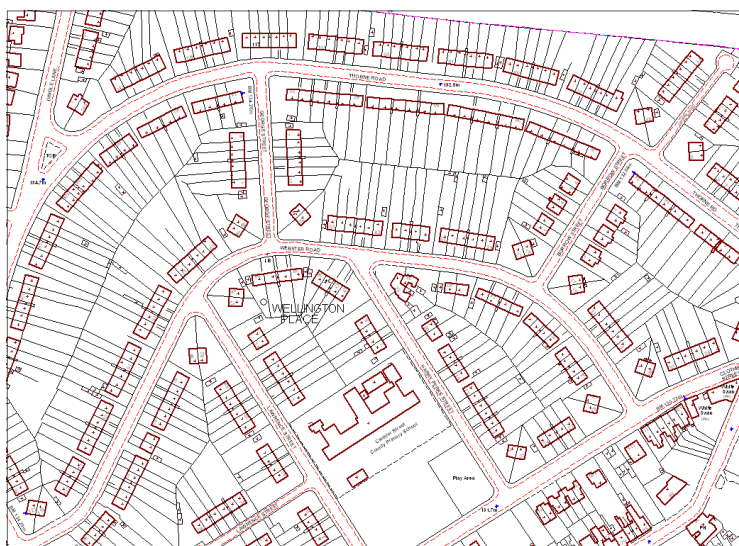
(2 HLC polygons)

The area north-east of the junction of *Claypit* and *Wood Lanes*.

► Tipton Green, Sandwell

DY4 (4 HLC polygons)

The areas either side of *Birmingham New Road* (east of the junction with the A4037) and, to the north, the housing either side of *Park Lane West*.



Above: the short terraces of a large inter-war estate on Thorne and Webster Roads, Willenhall (Walsall). Each terrace typically has four or six properties (HBL5037).

Right: a short, gabled terrace in Prescott Road, Stourbridge (Dudley).



Type 11: Late 20th Century Semi-detached Housing

► **Geographic Distribution:** This Type of housing is not particularly typical of the Regeneration Corridors (when compared to the wider Black Country), but is nevertheless a significant part of them. Around 16% of this type of housing in the Black Country is located in the corridors, making up 4% of their total area.

► **Housing Type:** The type consists mostly of *small* 20th century semis (i.e. each occupying a footprint of 50sqm or less) originating from the period after 1960.

► **Origins & Recent Change:** Two thirds of these type of homes were built in the 1960s and 1970s. Little or no demolition has taken place since 2000.

► **Previous Land Use:** These neighbourhoods were built on land with a variety of different uses - uses more diverse than the other housing-based Landscape Elements profiled and having the smallest contribution (30%) from former agricultural land. More than a quarter was built on land previously occupied by mining or quarrying and, more than other housing types, this represents the conversion of industrial land to residential uses.

► **Proximity to Historic Settlements:** Sites of this type are situated close to the location of historic settlements of Homer Hill and Lye.

► **Building Form:** By definition semi-detached (although some detached properties are included on the same streets), and generally 2-storey. Roof pitch tends to be shallow in the older examples and steeper in more recent development. External appearance of brick and tile is occasionally ornamented by different brick colours, and elevations clad on the first floor (see photos on opposite page). Later examples often include a garage within the structure of the house.

► **Site Structure & Boundary Treatment:** At an average of 28 properties per hectare this is relatively low density housing in the context of the regeneration corridors. Setback is somewhere near average (at about 8 to 12m), and many of the front gardens are open with, little in the way of barriers, providing access to integral garages. Later examples have variety of planting within the design, providing screening between plots.

► **Street Structure:** Houses are generally contemporary with streets and, in the case of the earlier examples within this type, were often built with regular frontage designs. The later developments tend to have mixed house designs. The majority of streets do not follow the line of earlier landscape patterns. Streets are generally laid out in curved pattern with adjoining cul-de-sacs and they are, in general, wider than other earlier designs. Houses are likely to have access to garages or other designed car parking rather than being limited to on street parking.



Later examples of the type include mixed semi-detached and detached homes built in private estates in the 1990s. Above is one such set of streets containing small semis and detached houses built a former factory site in Willenhall (Walsall); immediately below is an estate of slightly larger semis and detached properties built on former agricultural land as part of the residential expansion of Clayhanger near Brownhills (Walsall).



Below: examples of the type on Lancaster Road, Brierley Hill and (bottom) New Farm Road, Stourbridge (both in Dudley).

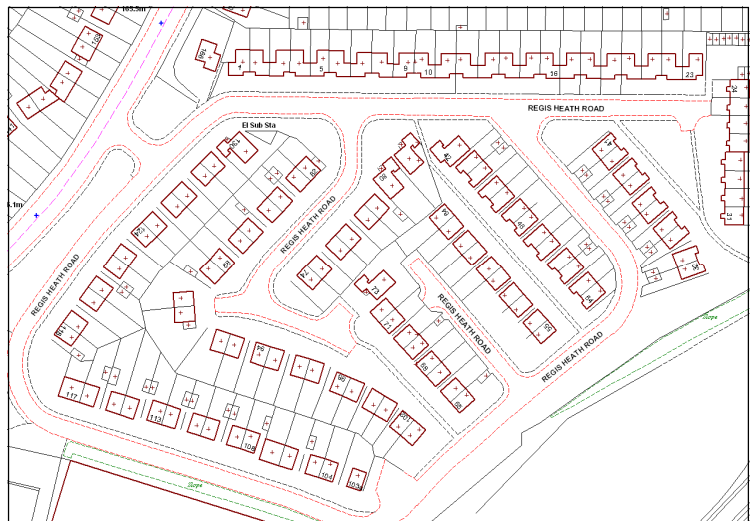
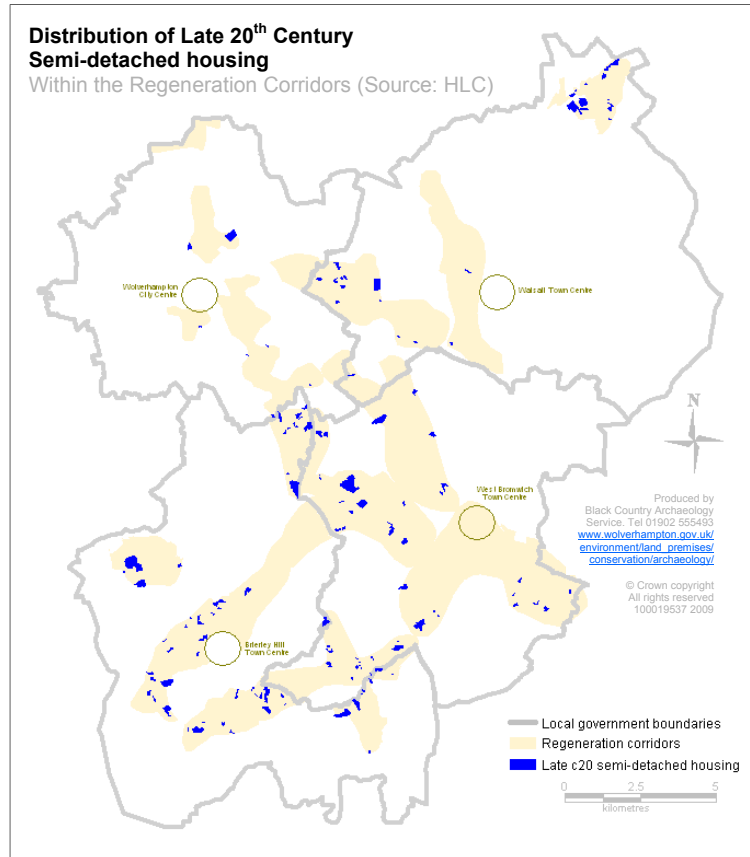




Small semis built in the '60s or '70s around a cul-de-sac which was laid out on a field and former clay pit in Blackheath (Sandwell).



Another cul-de-sac with small semis from the same period, this time built on former colliery land in Kingswinford (Dudley).



Above: small semi-detached housing in Regis Heath Road, Blackheath (HBL11089).

Where to see them in the Regeneration Corridors

► **Clayhanger Lane, WS8** (6 HLC polygons)

The new estates off Clayhanger Lane, south-west of the centre of Brownhills.

► **Dudley Port, Tipton, DY4** (3 HLC polygons)

In particular two estates: to the south-east of Dudley Port station on *Hellier Avenue* and streets off; and, to the north-west, *Kirkham Way* and streets off.

► **Kingswinford, DY6** (4 HLC polygons)

Two large areas: *Larch Road* and streets off; and *Balfour Road* and adjoining streets.

Right: A semi-detached property at Bredon Avenue, Stourbridge (Dudley).



Type 12: Inter-war Semi-detached and Detached Housing

► **Geographic Distribution:** This type of housing is not particularly typical of the Regeneration Corridors (when compared to the wider Black Country), but is nevertheless a significant part of them. Around 16% of this type of housing in the Black Country is located in the corridors, making up 4% of their total area.

► **Housing Type:** Two-thirds of the area covered by this type of housing are in fact occupied by properties described as *small* semi-detached houses, i.e. each occupying a footprint of 50sqm or less. About 6% of the areas within this type comprise only detached homes.

► **Origins & Recent Change:** Houses of this type all date from the period 1920 to 1938. Areas sampled showed that most housing of this type identified in the regeneration corridors in 2000 remains and little redevelopment appears to have taken place.

► **Previous Land Use:** Of all the housing built after 1919 which is represented in this report, this type was the one most commonly built on former agricultural land. More than half of this housing was built on field systems, while the re-use of former mining or quarrying sites and other open land also contributed a small proportion of the land on which it was built.

► **Proximity to Historic Settlements:** Sites of this type are situated close to the location of historic settlements of Darlaston, Dudley, Homer Hill, Tipton Green and Willenhall.

► **Building Form:** By definition semi-detached or detached, and generally 2-storey, often with a relatively steeply pitched hipped tiled roof. On semis, brick chimney stacks are shared between adjacent properties. Brick elevations, sometimes rendered, include characteristic front bay windows, sometimes on both storeys and including front gable. Ornamentation sometimes provided by patterned brickwork or mock timber framing.

► **Site Structure & Boundary Treatment:** At 23 homes per hectare, density is nearly two-thirds of the average for the housing types described here. Setback is relatively large, and generally above 10m. Original designs often included room for a drive and garage at the side of each house. Otherwise front gardens enclosed by fences or hedges, often with a low brick wall, are much more common than those bounded by pre-cast concrete. Front gardens are large enough to accommodate more mature vegetation.

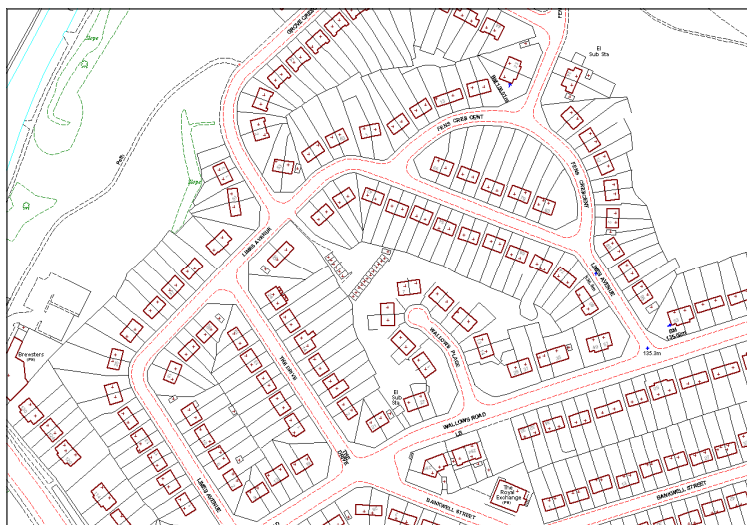
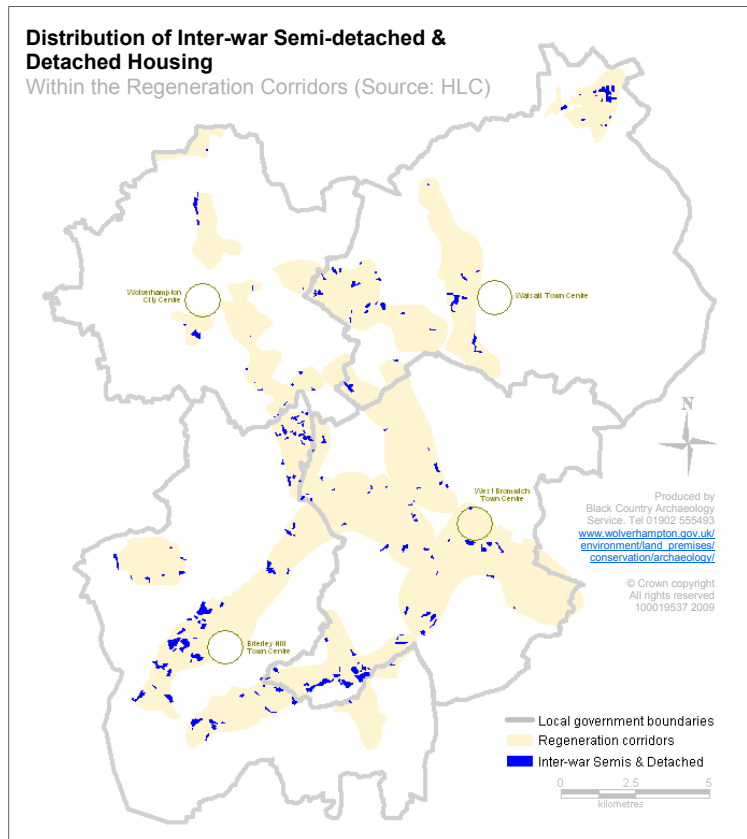
► **Street Structure:** Some of these houses are contemporary with their associated streets while others (perhaps a minority) are built alongside older, pre-existing routes. They often have varying frontage designs. Streets can be narrow, but houses are likely to have access to garages or other designed car parking rather than being limited to on street parking. Although many streets do not have much vegetation outside of the property boundaries, some of those on older routes are tree lined.





Above: a detached property on Wolverhampton Road, Kingswinford (Dudley). Detached houses are not only unusual within this type, but also within the regeneration corridors more generally.

Opposite: examples the semis more typical of this type. From top to bottom: Hodson Avenue, a short cul-de-sac in Willenhall (Walsall); Highfield Road, Rowley Regis (Sandwell), an inter-war extension to the earlier, Victorian street; the 1930s semis on Poplar Avenue, Brownhills (Walsall), land which was historically part of Cannock Chase.



Above: small semi-detached housing off Wallows Road, Brierley Hill (HBL8585).



Right: small semi-detached housing on Havacre Lane, Coseley (HBL7172).

Where to see them in the Regeneration Corridors

► **Pensnett Road, Brierley Hill, DY5** (6 HLC polygons)

In particular, to the north of Pensnett Road, including Wallows Road and the streets off.

► **South of Moor Street, Brierley Hill**

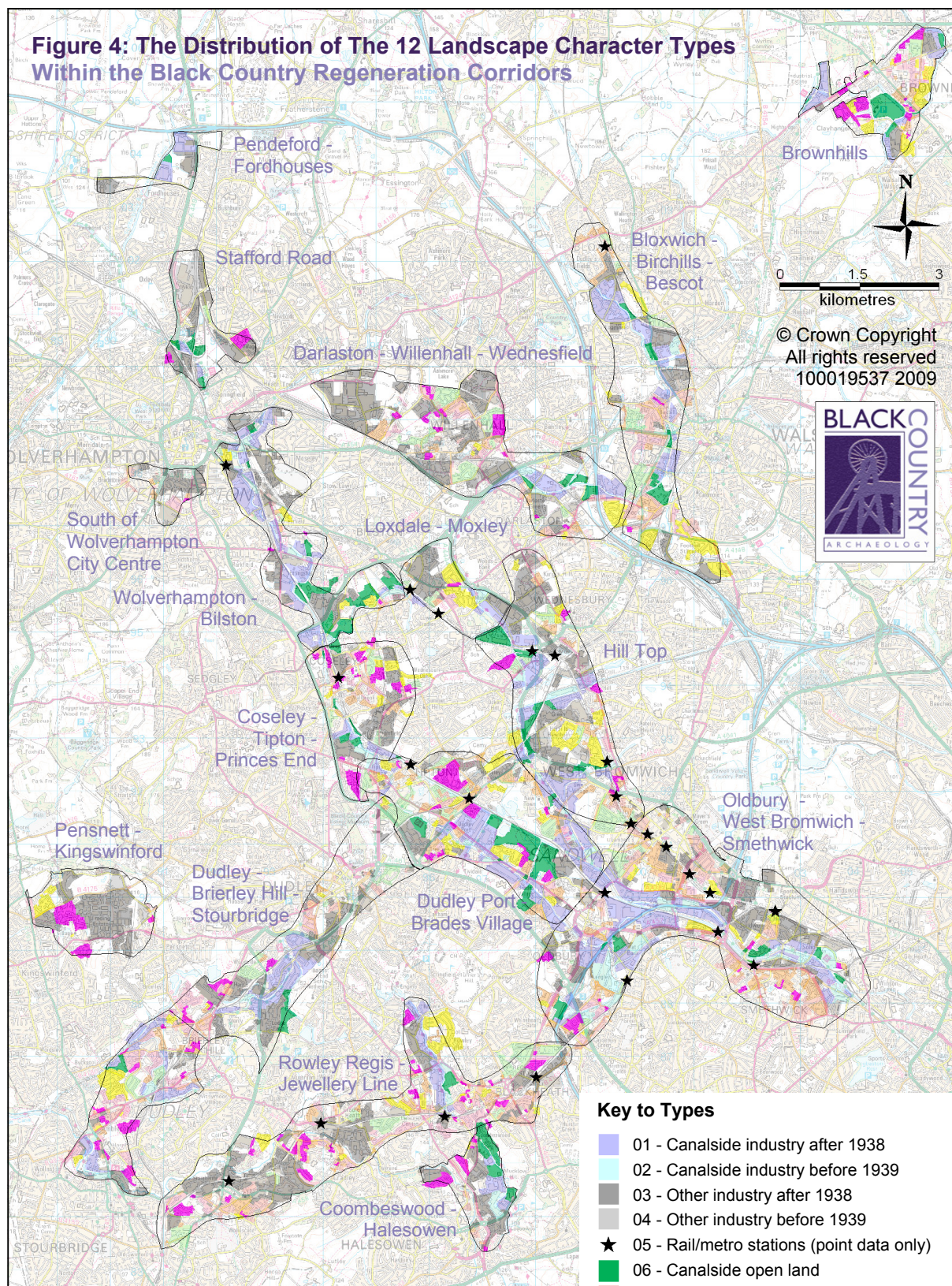
(4 HLC polygons)

In particular two large areas of housing between the Stourbridge and Dudley Canals.

► **Blackheath, Sandwell B65** (6 HLC polygons)

Housing either side of the railway, north-west of Old Hill station. Accessible from Waterfall Lane/ Perry Park Road and, to the south, Gorsty Hill Road and Station Road.

Figure 4: The Distribution of The 12 Landscape Character Types Within the Black Country Regeneration Corridors



Key to Types

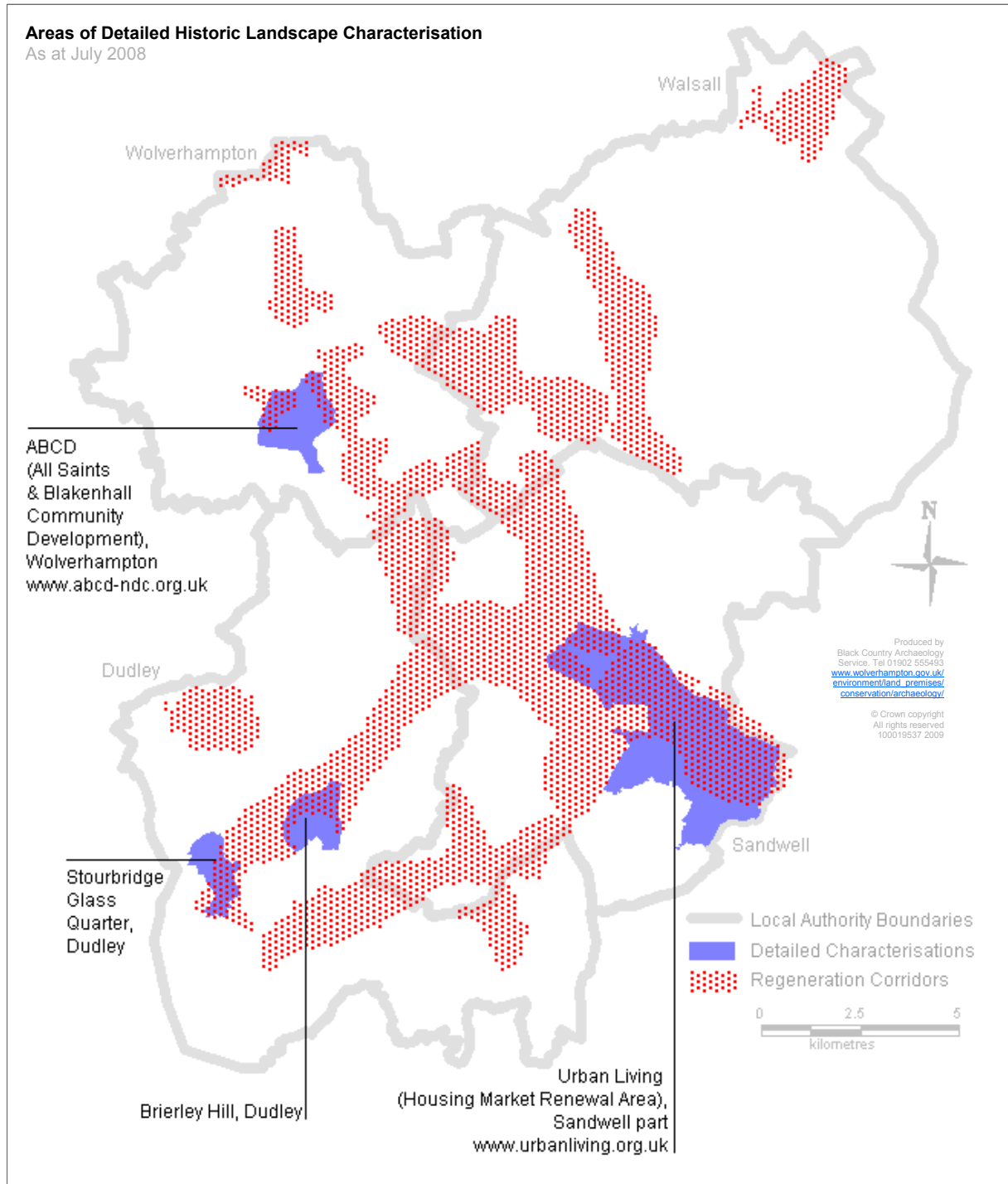
- 01 - Canalside industry after 1938
- 02 - Canalside industry before 1939
- 03 - Other industry after 1938
- 04 - Other industry before 1939
- ★ 05 - Rail/metro stations (point data only)
- 06 - Canalside open land
- 07 - c19 & Edwardian terraces
- 08 - Post-war high density housing
- 09 - Mixed c20 semis & terraces
- 10 - Mixed inter-war semis & terraces
- 11 - Late c20 semis
- 12 - Inter-war semis and detached

Source: Black Country Historic Landscape Characterisation, 2000

Note: Between them, the 12 landscape Character Types selected and described by this report cover 66% of the area of the Regeneration Corridors. It follows that a minority of the area of the Corridors is not shown as coloured on this map.

Appendix 1: Detailed Historic Landscape Characterisations inside the Regeneration Corridors

The map below shows the areas of the Black Country which have been the subject of a Detailed Historic Landscape Characterisation:



Appendix 2: The Regeneration Corridors and the BCHLC data

As was explained in Section 2.1, it was necessary to create a 'proxy' of the Corridors within the BCHLC data, in order to allow their landscape to be analysed using the BCHLC. That is to say, it was necessary to define an area of land which roughly corresponded to the corridors but which, at the same time, was made up of whole 'polygons' of BCHLC data (the latter being the smallest units used to record landscape character by the BCHLC).

Two options were considered, i.e. to use the combined area of the all the whole BCHLC polygons which *intersected* the Corridors or, alternatively, just those whole polygons which lay *within* the Corridors (in this latter case, a polygon was viewed as being within the Corridors if its centroid fell within their boundaries - it therefore follows that not all of these polygons necessarily falls entirely within the Corridors). In the event, the proxy area was taken to be the area of all those polygons which lay within the

BCHLC Polygons and the Regeneration Corridors

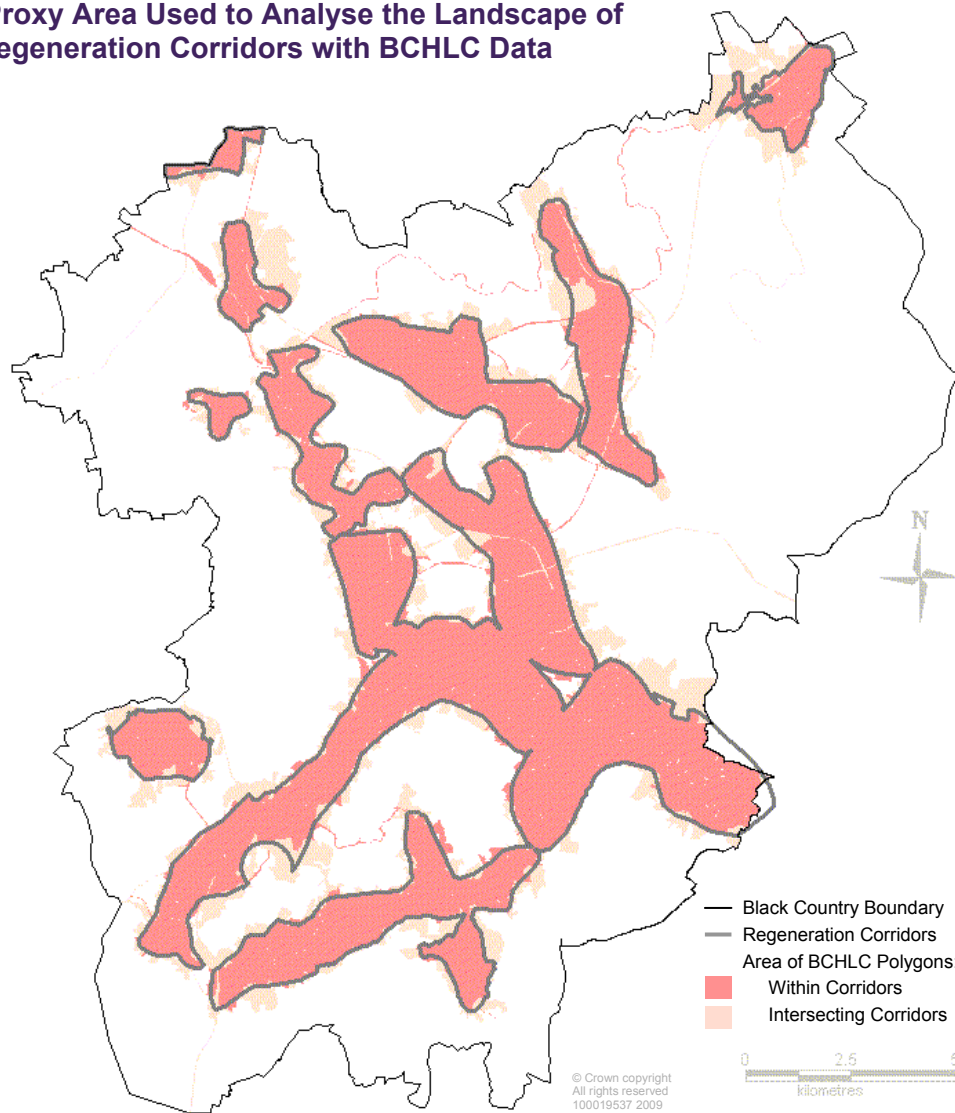
	Number of BCHLC Polygons	% of Polygons	Area in sq km	% of the area of the Black Country
All Black Country	12,664	100%	356	100%
Regeneration Corridors	-	-	89	25%
Polygons <i>intersecting</i> Corridors	4,179	33%	121	34%
Polygons <i>within</i> Corridors	3,501	28%	88	25%

Source: BCHLC, 2000

boundaries of the Regeneration Corridors. The reason for this choice was that this area most closely corresponded to the Corridor boundaries as drawn. As the Table on this page shows, this proxy area encompassed 88 km² compared to the area of the actual Corridors of 89 km².

The map opposite shows how this proxy area compared with the original boundaries of the Regeneration Corridors.

The Proxy Area Used to Analyse the Landscape of the Regeneration Corridors with BCHLC Data



Appendix 3: Site Visits - Industry & Communications Infrastructure

Type 1: Canalside Industry After 1938¹

1. Part of Works, Tarmac Rd, Millfields Rd, Bilston (HBL2268).
2. Various Works, Reservoir PI/ Woodward's Rd, Walsall WS2 (HBL5632).
3. *Albion Works*, Moor St, Brierley Hill (HBL7884).
4. Various Works, Hainge Road, Tividale (HBL10943).
5. Industrial Estate at Sandwell Business Park, Crystal Drive, Smethwick (HBL11412).
6. Several Works, Cornwall Rd/ Foundry La, Soho, Smethwick (HBL12153).

Type 4: Other Industry Before 1939

1. Scrap yard, Webb St, Coseley (HBL7193).
2. Metal works, Bilston La., Willenhall (HBL5071).
3. Blakenhall & Marston Industrial Estates & Imex Business Park, Wolverhampton (HBL680).
4. Brick works, Dreadnought Rd, Pensnett, Dudley (HBL8509).
5. Queensgate Works & Old Dominion Works, Bilston Rd, Wolverhampton (HBL2509).
6. Metal works, Temple Bar, Willenhall (HBL5047).
7. Engineering works, Heath Rd, Darlaston, Walsall (HBL6302).
8. Titan Works, Old Wharf Rd, Dudley (HBL7782).

Type 2: Canalside Industry Before 1939²

1. Metal works, Bull La, Darlaston (HBL5148).
2. Brick works, Bull Street, Brierley Hill (HBL7892).
3. Chemical works, Trinity St, Oldbury (HBL11365).
4. Brewery or cooperage, Langley Maltings, Western Rd, Langley, Oldbury (HBL11371).
5. Metal works, Bridge St Nth, Smethwick (HBL12129).
6. Industrial Works, Ryders Green Rd, Great Bridge (HBL12510).

Type 5: Passenger Rail/Metro Stations

1. Dudley Port Station, Tipton.
2. Smethwick Rolfe Street Station, Smethwick.
3. Old Hill Station, Cradley Heath.
4. Trinity Way Metro Stop, West Bromwich.
5. Lodge Road Metro Stop, West Bromwich.
6. Dartmouth Street Metro Stop, West Bromwich.

Type 3: Other Industry After 1938

1. Planetary Rd Industrial Estate, Wednesfield, Wolverhampton (HBL3222).
2. Industrial Works, Ann Street, Willenhall, Walsall (HBL5062).
3. Garage, Trindle Rd, Dudley (HBL7011).
4. Pensnett Industrial Estate, Kingswinford, Dudley (HBL8210).
5. Industrial Works, Portway Rd, Oldbury, Sandwell (HBL11316).
6. Industrial Works, Hall Street & Bond Street, West Bromwich, Sandwell (HBL12466).

¹These locations were selected from the HLC as recorded in 2000. A check of their status using aerial photography accessed on <http://maps.google.com> on 15 February 2008 showed all sites appeared to be generally unchanged, with the exception of: the buildings within HBL5632 which appeared to have been largely demolished;

²Following similar checks, the buildings within HBL12129 appeared to have been substantially redeveloped.

Appendix 4: Site Visits - Housing

Type 7: c19 Century & Victorian Terraced Housing

1. Small terraces (tunnel back), Salisbury Rd & Rosebery Rd, Cape Hill (HBL12060).
2. Small terraces (tunnel back), Burlington Rd, West Bromwich (HBL12553).
3. Small terraces (tunnel back), Birmingham St & Fisher St, Willenhall (HBL5014).
4. Small terraces (tunnel back), Prince St & Wednesbury Rd, Pleck (HBL5579).
5. Medium terraces, Birmingham Rd, Dudley (HBL7038).
6. Medium terraces Campbell St & Raglan St, Brierley Hill (HBL8605).

Type 10: Mixed Inter-war Semi-detached & Terraced Housing

1. Small semis & terraces, Grange Est., Lye (HBL1892).
2. Small semis & terraces, Thorne Rd, Willenhall (HBL5037).
3. Small semis & terraces, Lane Ave & Bentley New Dr., Walsall (HBL6210).
4. Small semis & terraces, Springfields Rd, Brierley Hill (HBL7883).
5. Small semis & terraces, Spring Ave & Belle Vue Rd, Rowley Regis (HBL11047).
6. Small semis & terraces, Clifford Rd, West Bromwich (HBL12481).

Type 8: Post-war High Density Housing

1. Small terrace housing at Beechcroft Rd, Old Hill (HBL10656).
2. High rise flats between Whitehall Rd & Cophall St, New Town, Great Bridge (HBL11544).
3. Small terrace housing in Queens Close & James Close (the 'Metric Estate'), Smethwick (HBL12171).
4. Apartments & maisonettes in Jervoise St, West Bromwich (HBL12529).
5. High rise flats & terraces in Oxford St & St Quentin St, Pleck, Walsall (HBL5586).
6. Small terraced housing on Love La & High St, Lye (HBL7634).

Type 11: Late c20 Semi-detached Housing

1. Mixed small semis & detached housing, Honeybourne Way & cul de sacs off, Willenhall (HBL5028).
2. Mixed medium semis & detached housing, Bell Heath Rd, Larkspur Way and roads off, Brownhills (HBL1581).
3. Small semis, Romsley Rd, Bredon & Malvern Aves, Lye (HBL7581).
4. Small semis, Larch Rd & Ash Crescent., Kingswinford (HBL8153).
5. Medium semi detached housing, Lancaster Rd, Brierley Hill (HBL8789).
6. Small semis, Regis Heath Rd, Blackheath (HBL11089).

Type 9: Mixed c20 Semi-detached and Terraced Housing

1. Small semis & terraces, Castle View Rd, Moxley (HBL5132).
2. Small semis & terraces, Gower St. etc., Pleck (HBL5588).
3. Small semis & terraces, Ridgeway Rd. etc., Wordsley (HBL7867).
4. Small semis & terraces, Pearson St. etc, Cradley Heath (HBL10696).
5. Small semis & terraces, Wolseley Rd etc, Harvills Hawthorn (HBL11487).
6. Small semis & terraces, Dawes Avenue, West Bromwich (HBL12432).

Type 12: Inter-war Semi-detached & Detached Housing

1. Small semis, Vernon, Poplar & School Avenues, Ogle Hay, Brownhills, Walsall (HBL1249).
2. Small semis, Hodson Ave, Willenhall, Walsall (HBL5068).
3. Small semis, Kenelm Rd, Coseley, Dudley (HBL7172).
4. Small semis, Apley Rd, Wollaston, Dudley (HBL7788).
5. Medium semis, Wolverhampton Rd, Kingswinford, Dudley (HBL8266).
6. Mixed small semis and detached housing, Perry Park & Highfield Roads, Rowley Regis, Sandwell (HBL11061).

Appendix 5: Data Items Collected During Fieldwork

Information collected on each landscape element

Settlement

Period & Predominance

- Recorded period of the polygon.
- Any apparent differences in period between buildings in the polygon

Building Form

- Terraced / Semi-detached / Detached
- Number of storeys (incl. presence of dormer windows)
- Pitch of roof
- Presence of chimneys
- Ornamentation
- Materials: bricks / tiles / stone / render etc.

Site Structure and Boundary Treatment

- Size of setback (between kerb line and building line)
- Plot width and length
- Presence of walls/fences/hedges and materials
- Presence, size and shape of garden
- Amount and maturity of vegetation
- Visibility of gardens from street

Street Structure

- Houses contemporary with streets or not
- Uniform / varying frontage designs
- Orientation of houses to each other
- Line of streets based on earlier pattern or not
- Width of streets
- Form of streets, straight, geometric, circular, looped, cul-de-sac
- Uniform / varying kerb alignment and carriageway
- Amount of vegetation and green space
- Maturity of vegetation
- On street parking / Garages (original or converted) / hard standing (original or added)

Detailing/Distinctiveness

- features (including above which are particularly distinctive)

Relationship to Historic Settlement

- Proximity to an historic settlement

Industry

Period & Predominance

- Recorded period of the polygon
- Any apparent differences in period between buildings in the polygon

Industrial Sector

- Current industrial sector

Building Form

- Number of storeys (incl. presence of dormer windows)
- Pitch of roof
- Presence of chimneys
- Ornamentation
- Materials: bricks / tiles / stone / render etc.

Site Structure and Boundary Treatment

- Plot size
- Presence of walls/fences/hedges and materials

Access

- Nature of road access
- Width of street
- Amount and type of parking

Detailing/Distinctiveness

- features (including above which are particularly distinctive)

Relationship to Historic Settlement

- Proximity to an historic settlement

Canals & Canalsides

Period

- Recorded period of the polygon
- Recorded date of opening from Dean's map
- Original canal company

Type of canal

- Level relative to ground
- Presence of aqueduct / embankment / cutting and size
- Number of towpaths

Site structure

- Proximity to road bridge
- Form of canalside
- Amount and maturity of vegetation

Railway Stations

Period & Predominance

- Recorded period of the polygon
- Any apparent differences in period between structures in the polygon
- Original railway company

Building Type

- Number of storeys
- Pitch of roof
- Materials: bricks / tiles / stone / render etc.
- Ornamentation

Line and Platforms

- Type of line (rail / metro)
- Number of platforms
- Size of platforms
- Materials

Site Structure

- Level of structures relative to street and to line
- Amount of vegetation and green space
- Maturity of vegetation
- Presence of walls/fences/hedges and materials
- Amount and type of Parking

Relationship to Historic Settlement

- Proximity to an historic settlement

Open Land

Period

- Recorded period of the polygon
- Any apparent differences in period within the polygon Plot size

Previous Use

- Recorded previous uses of the polygon
- Any apparent differences in previous use within the polygon

Site Structure and Boundary Treatment

- Plot size
- Presence of walls/fences/hedges and materials
- Presence and access to canals
- Presence and type of access to existing roads

Relationship to Historic Settlement

- Proximity to an historic settlement

Appendix 6: Historic Centres in Existence Before 1750

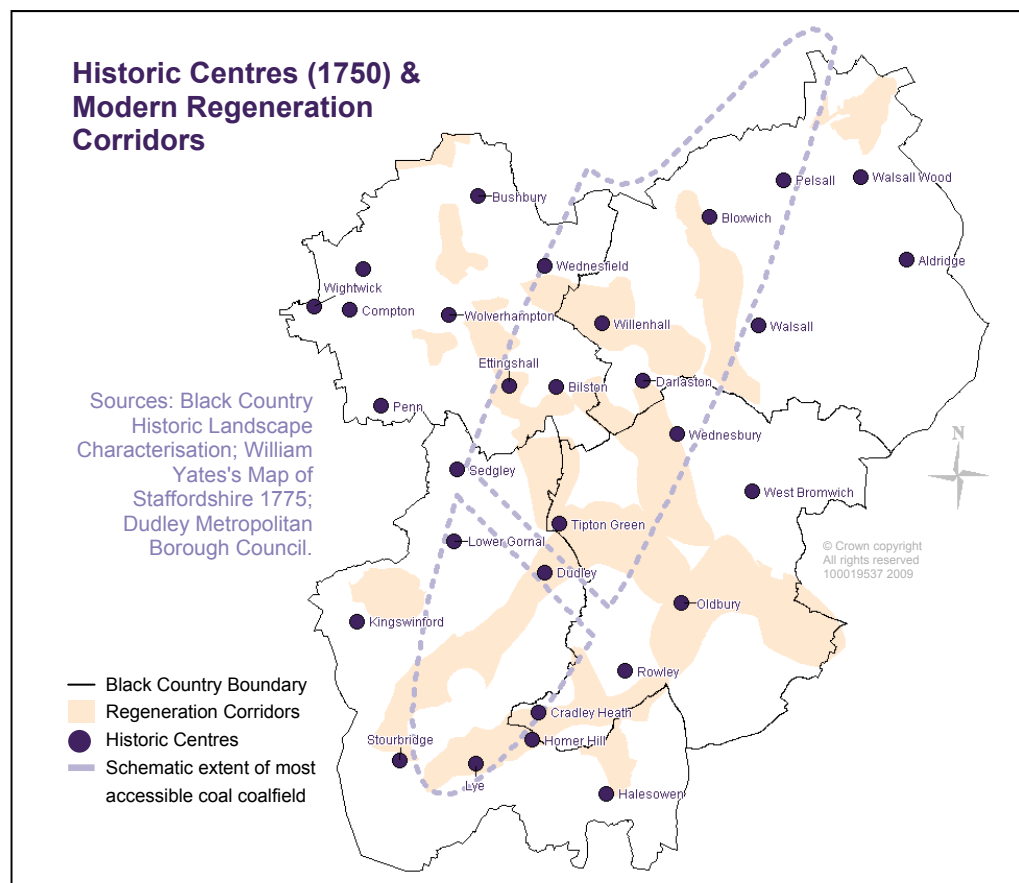
As the earlier sections of this report have explained, the 12 landscape types which are described on pages 18 to 41 have been defined by their modern use and period of origin, and then selected by their relative occurrence within the Regeneration Corridors.

The centres of historic settlements are important locations of heritage assets (both surviving and below-ground archaeology). However, they are not included among our 12 types because they do not represent either a single land use or period of origin, nor are they particularly

distinctive to, or representative of the Regeneration Corridors.

To illustrate this last point, the map below shows the locations of 30 centres which existed in 1750. Of these, 8 lie within the Regeneration Corridors (Ettingshall, Tipton Green, Dudley, Lye, Homer Hill, Oldbury, Cradley Heath, and Willenhall).

Despite the exclusion of historic centres as a type, the description of each of those types which were included contains details of how the associated sites are situated relative to the centres shown below.





Black Country Archaeology Service
Planning Policy & Urban Design
Regeneration & Environment
Wolverhampton City Council
Civic Centre
St Peter's Square
WOLVERHAMPTON
West Midlands
WV1 1RP

tel: 01902 555493
fax: 01902 555637
email: paul.quigley@wolverhampton.gov.uk

More details of the Black Country's programme to characterise the local historic landscape can be found at:
http://ads.ahds.ac.uk/catalogue/archive/blackcountry_hlc_2009