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LINDSEY ARCHAEOLOGICAL SERVICES

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Lincoln, Glebe Park Mains Replacement Scheme

NGR: SK 985 720 (centre)
Site Code: LGWR 02
LCNCC Museum Accn No.: 2002.234

Archaeological Monitoring

Report prepared for
Anglian Water Services Ltd

by G. Tann

LAS Report No. 608
September 2002

heritage



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F.N.FIELD B.A. (Hons), MIFA

Event L13473
Source L18179
L18180

negative

[V. Tentative info for
metalling in Eastgate]

Contents

List of Figures and Plates

Summary	1
Introduction	1
Archaeological Background	1
The Watching Brief	1
Conclusion	3
Acknowledgements	3
References	3
Archive Summary	4
The Figures	
The Plates	

List of Figures and Plates

Figures

- Fig. 1** Location of Greetwell Road (C based on the 1976 Ordnance Survey 1:10,000 map; © Crown Copyright, reproduced with the permission of the Controller of HMSO. LAS Licence No. AL 100002165).
- Fig. 2** Approximate position of the monitored trenches, with observation numbers for locations described in the report. Based on an Anglian Water Services 1:2,500 plan. © Crown Copyright, reproduced with the permission of the Controller of HMSO. LAS Licence No. AL 100002165).

Plates

- Pl. 1** Trench across the northern side of Eastgate, east of St. Peter's Church, (Observation 1).
- Pl. 2** Compact layers of soil, gravel and stone chips below the centre of the modern road and its limestone rubble bedding layer, at Eastgate (1) looking south.
- Pl. 3** Location of the Anglian Water trench (2) at the junction of Greetwellgate and Wragby Road (looking east).
- Pl. 4** Compacted gravel deposits cut by later service pipe trenches at the junction of Greetwellgate and Wragby Road (2) looking NE.
- Pl. 5** Band of dark loam, possibly a buried ground surface, below the modern Greetwell Road, at the junction with St. Annes Road (5) looking south.

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Summary

Trenches excavated during directional drilling of a replacement water main identified no features or finds of archaeological significance. With few exceptions, the visible deposits were backfill from earlier service trenches. During the monitoring, compacted road metalling was seen in Eastgate, probably forming part of the known Roman road.

Introduction

Lindsey Archaeological Services (LAS) was commissioned by Anglian Water Services Ltd in March 2002 to conduct a watching brief during directional drilling for a replacement water main along Greetwell Road, Lincoln, and in neighbouring roads (Fig. 1). Archaeological monitoring had been requested by Lincolnshire County Council Conservation Section and the Lincoln City Archaeologist, in case the works disturbed archaeological remains.

Fourteen intermittent monitoring visits were made by Geoff Tann and Dave Marshall between 10th April and 8th July 2002.

Archaeological Background

The works for the new main affected areas close to several known archaeological sites. Eastgate/Wragby Road follows the line of a Roman road (Margary ref. 27) towards Horncastle and the Roman port of Burgh-le-Marsh (Margary 1973, 238-40). An extramural Roman cemetery is known to flank the southern side of this road in the area of Queensway, Gerald's Close and Curle Avenue, and there are suspicions that it may extend to Greetwell Road and Monks Manor Drive. There is evidence of Roman activity at the western end of Greetwell Road, and there are stone quarries of Roman and later date in that area. A medieval hospital, St. Giles's Hospital, is known to have stood on the north side of Wragby Road close to the Curle Avenue junction.

The Watching Brief

The new main was directionally drilled where this was possible. Machine-excavated trenches were inspected, and assigned numbers by LAS for recording purposes where observations were noted. The approximate positions of these trenches are marked on Fig. 2.

Eastgate

1. No Anglian Water works were monitored in Eastgate, but monitoring of works in Greetwellgate coincided with backfilling of a trench across the northern side of Eastgate by other contractors (Pl. 1). With their consent, this trench was inspected. Only the upper 0.4m of this trench

remained visible, with the modern tarmac surface overlying a layer of stone rubble. Beneath this was a very compact mix of stone chips and gravel (Pl. 2). Suspicions that this was the Roman road from Eastgate were supported by the contractors' claim that this had been 'the most solid ground to dig in Lincoln'. Immediately to the north of the road, the front entrances of houses are set below street level, possibly within the flanking ditch. No trenches were seen in Langworthgate, the assumed path of the Roman road.

Greetwell Gate

Access trenches between Eastgate and the junction with St Leonard's Lane revealed no undisturbed deposits. Trenches in the road outside the Peacock Inn cut through previous trench fill. The southern pavement overlay dark brown clay loam which did not appear to have been disturbed.

2. In the centre of the Wragby Road/Greetwell Road/Greetwell Gate road junction, the 0.6m thick modern road surface and bedding layer sealed 0.2m of yellow sand. Beneath the sand was compacted gravel, which extended below the trench base. In this location, the gravel has been introduced, and this would seem to mark a former road surface. The compaction was initially taken to indicate that this road may be Roman, but as no similar deposits were observed further to the east, this is unlikely. It is more probable that the section of Wragby Road southwards to Lindum Road was particularly well constructed in the post-medieval period.

Winnowsty Lane

3. A trench close to No. 8 Winnowsty Lane revealed only material backfilling existing trenches.

Greetwell Road

All the trenches inspected along Greetwell Road were placed over the existing main. The existing trench, and other adjacent services, had removed all early deposits in most locations.

4. Outside No. 5 Greetwell Road, some undisturbed yellow clay was seen in the trench face.

5. At the junction of Greetwell Road, St. Annes Road, and Monks Manor Drive, the 0.4m thick tarmac and asphalt surface had a 0.2m thick bedding layer of clay with limestone chips (Pl. 5). Beneath this was 0.1m of dark brown loam, with what seemed to be undisturbed weathered limestone below. The dark brown loam layer may have been a turf or subsoil layer, onto which the medieval or later road was laid. The underlying layer, although not bedrock, contained patches of orange clay loam which probably denotes root disturbance.

6. Apparently undisturbed clay and limestone brash was also exposed at the junction of Greetwell Road with Curle Avenue.

Sewell Road

The gardens of properties beside Sewell Road are at considerably lower levels than the road, reflecting the numerous limestone quarries formerly in this area at the top of Lindum Road.

7. Beside St. James' Houses, grey/brown clay and limestone brash was seen in the side of a disturbed trench.

8. At Homewood, the bedding for the road covered a 0.5m thick layer of dark brown loam, brick fragments and stone rubble. The underlying orange/brown loam may also have been quarry waste.

Other Roads

A trench in the centre of Wragby Road at the junction with Curle Avenue exposed only disturbed deposits from previous utility trenches. No significant observations were made in Monks Manor Drive, Greetwell Close, Queensway, St. Annes's Road, or Lindum Terrace.

Conclusion

The watching brief was unable to contribute any positive archaeological information despite the extensive groundworks in this area of high potential. The directional drilling techniques used, and the reuse of the existing main, mean that virtually all ground disturbance is within the existing backfilled trench. The intermediate trenches excavated between moling points are invariably positioned to locate other services, and here additional prior disturbance is present.

The replacement of mains in this scheme was achieved with no identified damage to significant archaeological remains.

Acknowledgements

LAS is grateful to Anglian Water Services and their site contractors ALHCo Ltd and SubTerra for their co-operation. Naomi Field and Jane Frost prepared illustrations and produced the report.

Geoff Tann
Lindsey Archaeological Services
17th September 2002

References and Background Sources

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Margary, I.D. 1973 *Roman Roads in Britain*. Third Edition.
Vince, A., and Jones, M.J. 1990 *Lincoln's Buried Heritage*. City of Lincoln Archaeology Unit, November 1990.

Archive Summary

Anglian Water Services plans

Annotated plans and field notes

Photographs: colour prints, LAS film nos. 02/45/32-3; 02/57/15-20; 02/81/3; (including those used in this report).

Correspondence

THE FIGURES

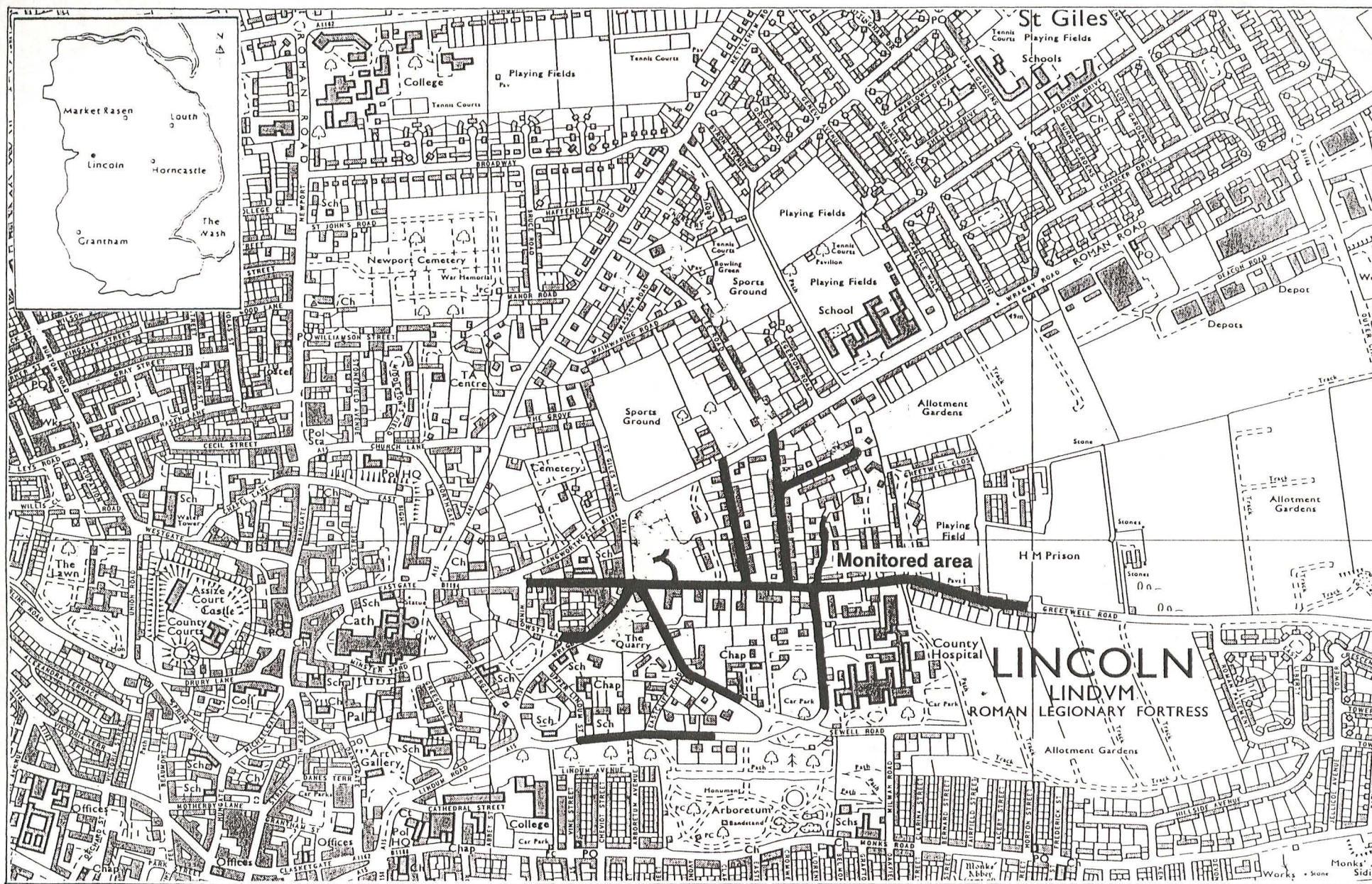


Fig. 1 Location of Greetwell Road, showing the extent of archaeological monitoring (based on the 1976 Ordnance Survey 1:10,000 map; © Crown Copyright, reproduced with the permission of the Controller of HMSO. LAS Licence No. AL 100002165).

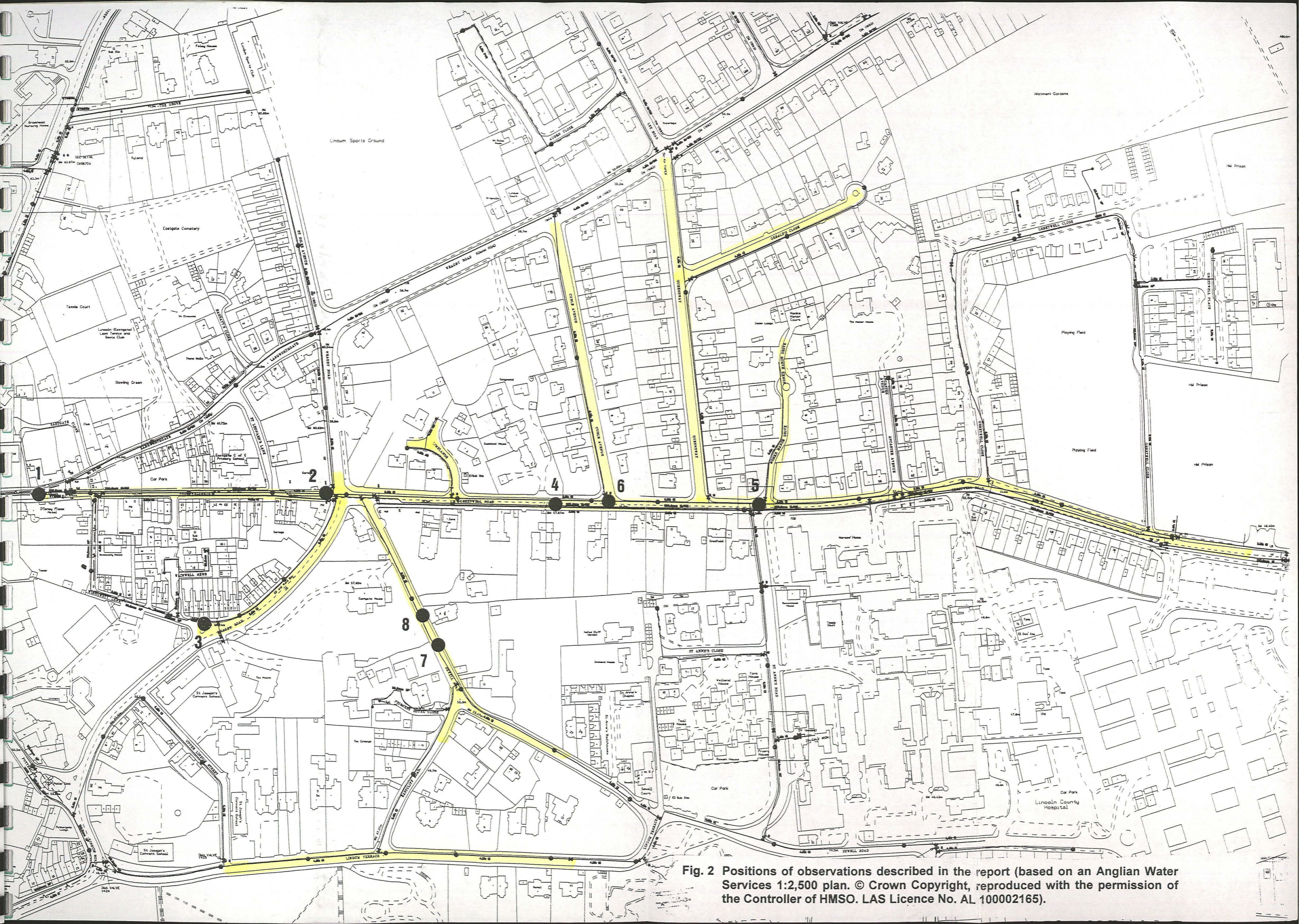


Fig. 2 Positions of observations described in the report (based on an Anglian Water Services 1:2,500 plan. © Crown Copyright, reproduced with the permission of the Controller of HMSO. LAS Licence No. AL 100002165).

THE PLATES



PI. 1 Trench across the northern side of Eastgate, east of St. Peter's Church, (Observation 1).

PI. 2 Compact layers of soil, gravel and stone chips below the centre of the modern road and its limestone rubble bedding layer, at Eastgate (1), looking south.





Pl. 3 Location of the Anglian Water trench (2) at the junction of Greetwellgate and Wragby Road (looking east).

Pl. 4 Compacted gravel deposits cut by later service pipe trenches at the junction of Greetwellgate and Wragby Road (2), looking NE.





Pl. 5 Band of dark loam, possibly a buried ground surface, below the modern Greetwell Road, at the junction with St. Annes Road (5), looking south.