

**FORMER EASTGATE MOTORS CAR PARK,
WRAGBY ROAD, LINCOLN**

**ARCHAEOLOGICAL
TRIAL EXCAVATION REPORT**

Site code	EMCP 03
NGR:	SK 98270 71981
Planning Ref:	2003/403
LCCM Acc No:	2003.307

Report prepared for
Cherry Tree Developments (UK) Ltd

by

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PRN-70343 - undated limestone quarry.

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Summary

- An archaeological evaluation was undertaken at the former Eastgate Motors Car Park, Wragby Road, Lincoln. These works were required due to the location of the site within a designated conservation area, Conservation Area No.3; to assess the archaeological potential of the site, and to establish the effects on archaeological resources of redeveloping the area.
- One trench was investigated, located towards the centre of the site. This exposed evidence relating to modern activities, and one side of a medieval limestone quarry.
- It is concluded that redevelopment of the area will not adversely affect significant archaeological remains.



Fig. 1: Site location at scale 1:25,000. The site is highlighted in red and the area shown in fig. 2 is outlined in blue.
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1.0 Introduction

Pre-Construct Archaeology (Lincoln) was commissioned by Cherry Tree Developments (UK) Ltd to carry out an archaeological evaluation on the site of the former Eastgate Motors car park. The results of this evaluation will be used to assess the overall archaeological potential of the site, the potential threat of redeveloping the area, and if necessary, to advise a mitigation strategy that will seek to safeguard the interests of buried archaeological remains, and a residential development proposed by Cherry Tree Developments (UK) Ltd.

The archaeological programme was undertaken to fulfil the objectives of a formal project brief issued by the Lincoln City Archaeologist, and a project specification prepared by Pre-Construct Archaeology (Lincoln). This approach is consistent with the recommendations of *Archaeology & Planning: Planning Policy Guidance Note 16* (Department of the Environment, 1990); *Management of Archaeological Projects* (English Heritage, 1991), *Standards and guidance for archaeological watching briefs* (IFA, 1999), and the Lincolnshire County Council document, *Lincolnshire Archaeological Handbook: a manual of archaeological practice* (LCC, 1998).

2.0 Site location and description (Figs. 1 & 2)

The site is situated on the east side of the city centre, towards the crest of the limestone ridge (NGR: SK 98270 71981).

It comprises a triangular unit, of approximately 700m², situated on the east side of Wragby Road, approximately 50m north of the junction at Greetwell Gate, where the modern ground surface is approximately 60m OD. Although the ground surface here is level, the site is contained within a modern concrete revetment, surmounted by wooden lap fencing, where the ground outside of the area drops steeply away to the east.

It is located within Conservation Area No. 3 (Lindum and Arboretum).

3.0 Planning Background

Previous planning applications (refused) were submitted in December 2001 and May 2002. An appeal in January 2003 was also dismissed. A further application for the erection of three detached dwellings was granted in July 2003 (application no. 2003/403). As a condition of the planning permission, the City Archaeologist recommended the undertaking of an archaeological evaluation of the area. The results of this evaluation will be used to determine the archaeological potential of the site, the potential threat of future development and the formulation of a mitigation strategy to safeguard the interests of buried archaeological remains.

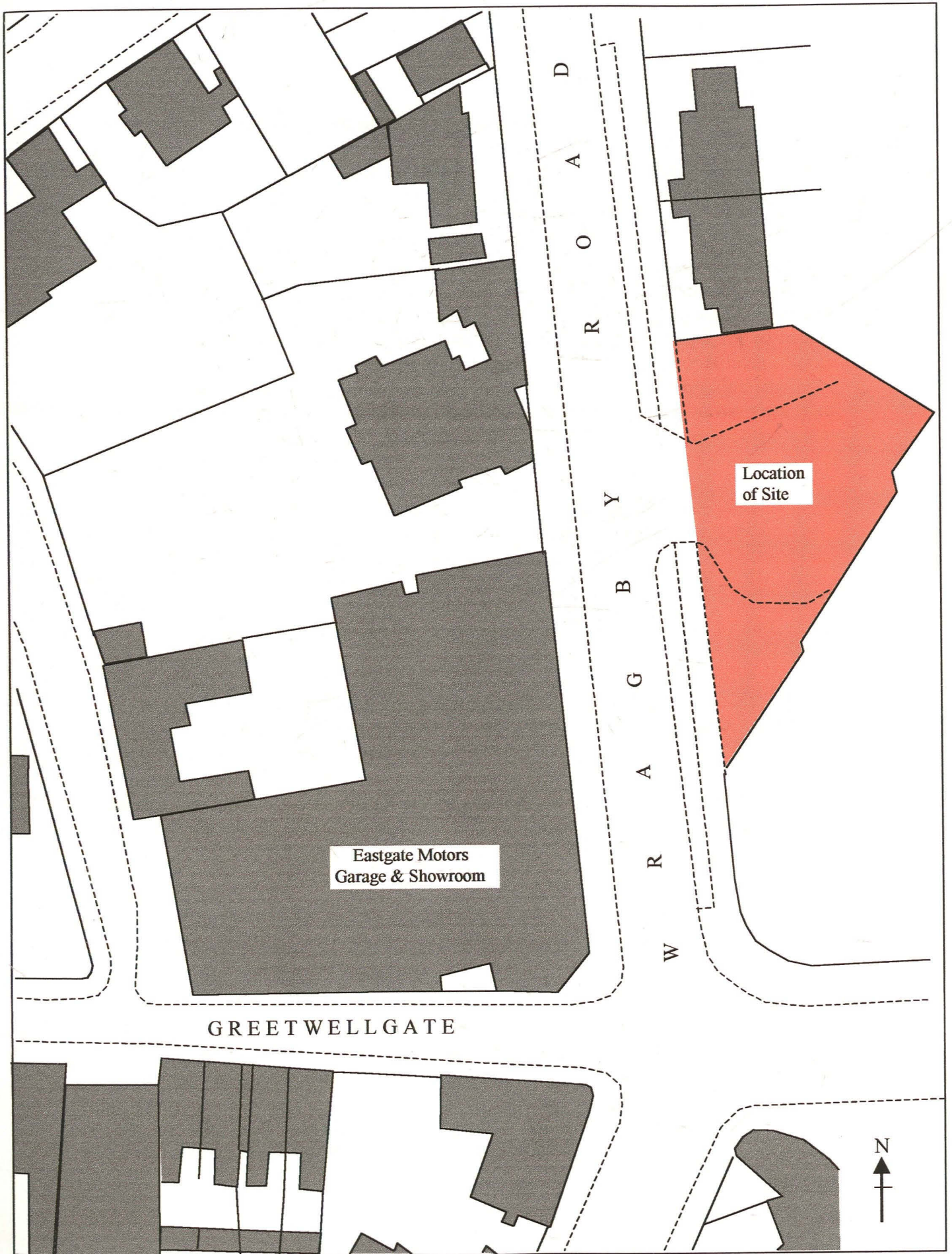


Fig. 2: Location of site. Scale 1:500

4.0 Archaeological and historical background

Records held as part of the Lincoln Urban Archaeological Database (draft) at Lincoln City Council indicate that the site is located within an area of historical and archaeological interest.

During the prehistoric periods the site was part of the limestone uplands. Prehistoric remains have not been recorded in the immediate vicinity of the proposed redevelopment, although it is known that, prior to the arrival of *Legio IX* in the mid-1st century AD, Lincoln was a major sub-tribal centre of the *Corieltauvi*, the so-named tribal unit that occupied much of the East Midlands. Despite this, pre-Roman activity in the vicinity of the later Roman city has proved elusive to detection, due largely to the great depth of overlying deposits. Remains have been recorded in the vicinity of Brayford Pool, and possibly at The Lawn, just beyond the west gate of the Roman legionary fortress (Jones 2002).

Following the arrival of the Roman army, c.AD 60, a legionary fortress was established at the top of the limestone scarp; the east gate of which was approximately 400m to the west of the current site. It appears that a military training and recreational complex was established beyond the East Gate. Later, the Roman town spread beyond the town walls, and although few investigations have been carried out in this area, it has been suggested that these suburbs and cemeteries lie close to, or in the area, of the redevelopment area.

Following the collapse of the Roman administration, c.410 AD, the city fell into decline, although areas within the *colonia* and suburbs continued to be inhabited, and the Roman road system remained in use.

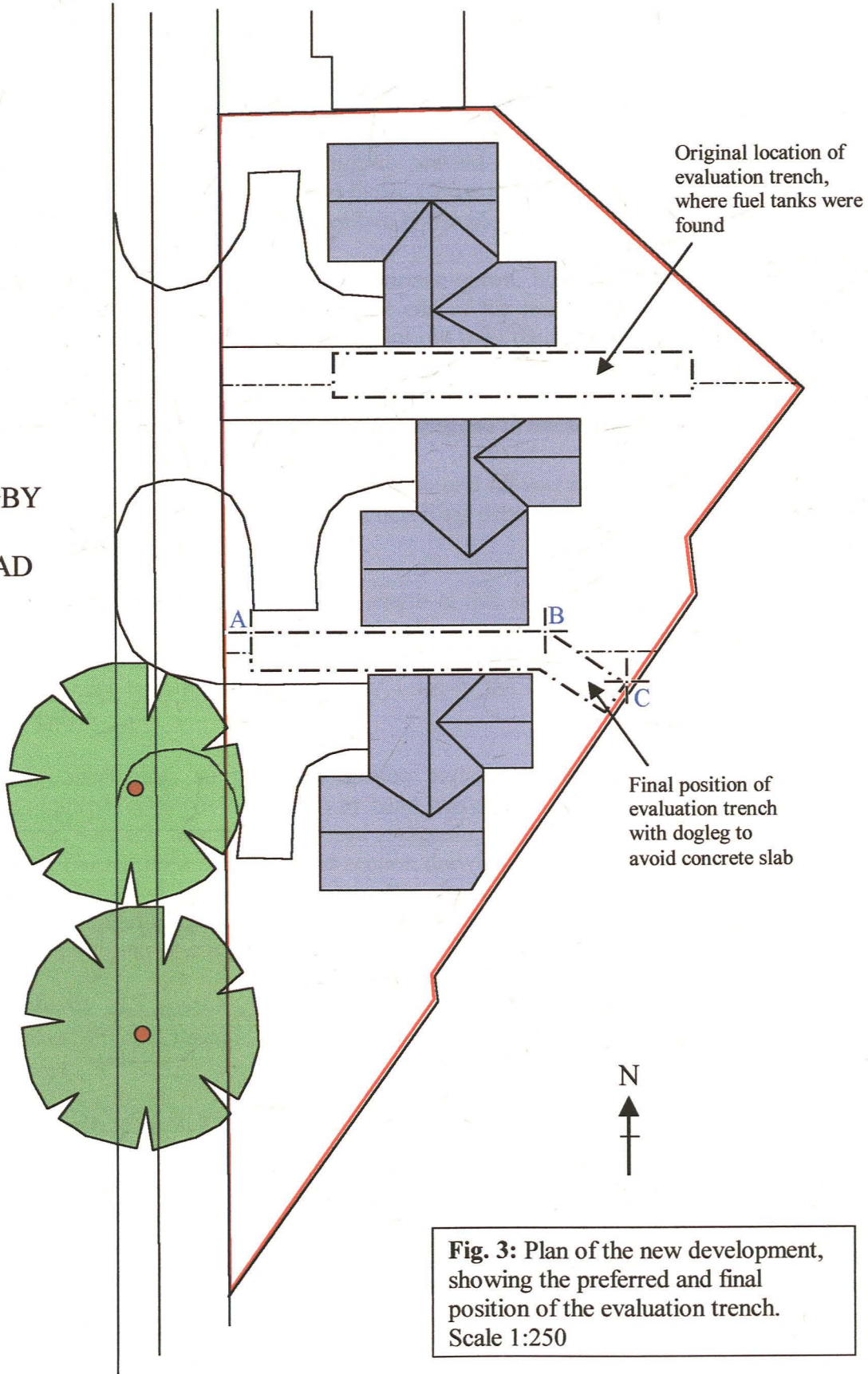
During the high medieval era (9th – 14th century AD), the site lay within the city's arable fields; an area that was also the subject of quarrying throughout the medieval and in to the industrial period (extensive medieval quarries have been recorded to the south of Wragby Road).

The section of Wragby Road adjacent to the site is depicted on 1817/1848 Marrat maps as Wainwell Gate/Wanwell Styth, meaning street/spring of the wagon-well. The road is likely to be of medieval origin.

The Victorian period saw the construction of middle and upper class housing in this area, and in the 1880's the site was within the landscaped garden of Limefield House to the east, suggesting that it is possible it escaped the quarrying activity of the area.

The site was most recently used as a car park for the Eastgate Motors Garage and Showroom opposite, but was formerly a filling station.

WRAGBY
ROAD



Original location of
evaluation trench,
where fuel tanks were
found

Final position of
evaluation trench
with dogleg to
avoid concrete slab

N
↑

Fig. 3: Plan of the new development, showing the preferred and final position of the evaluation trench. Scale 1:250

5.0 Methodology

The specification of the project required the excavation of one trench, 15m x 1.6m, within the former Eastgate Motors car park, set perpendicular to Wragby Road (fig. 3). The positioning of this trench proved problematic, as the favoured location (between the northern two house plots, to prevent undermining future foundations) exposed a series of large, modern fuel tanks. Consequently, the trench was positioned 12m to the south, between the southern two house plots.

The majority of the area consisted of a tarmac apron, however a large, rectangular concrete slab was encountered at the east end of the trench. This formed part of a system to cantilever the pump attendant's hut out over the eastern retaining wall of the site, and it was decided to dig the trench along the southern side of this slab, rather than remove part of what may have been a delicately balanced structure. Consequently, the eastern 4m of the trench doglegged to the southeast.

The tarmac and underlying rolled 'type 1' structural fill was removed with a JCB 3CX excavator employing a toothed bucket; underlying deposits were then removed using a toothless ditching blade.

In order to gain the maximum possible length in this area, the evaluation trench was divided in two halves: the eastern half being excavated, recorded and backfilled before excavation of the western part. All deposits were removed in spits not exceeding 0.2m to a maximum depth of 1.4m in the eastern half and 2.4m in the western part.

Following machining, all plan and section surfaces were examined and cleaned. Where required, selective excavation by hand was carried out to establish the profile, orientation, date and function of exposed archaeological features. These features were accurately plotted on a site plan, and section drawings were made at a scale of 1:20. Context information was recorded on *Pro forma* context record sheets, and a transparency photographic record was maintained throughout the investigation; selected views from which have been reproduced in this report.

The fieldwork was carried out over a period of one day, 7th August 2003, under the supervision of Simon Savage, assisted by Suzie Matthewson.

6.0 Results (Figs 3, 4)

As noted above, the modern ground surface consisted of the tarmac apron of the former filling station – context 100. This layer completely covered the area to a depth of between 0.02 and 0.08m, and at the east end of the trench also covered a large concrete slab (101) along whose southern edge the trench was diverted.

These modern surfaces were bedded onto a 0.15m thick layer of compacted 'Type 1' structural fill material, which in turn had been placed over a layer of 4-inch limestone fill material. This activity is associated with the surfacing of the filling station apron and was given the context number 102. The overall thickness of this material was generally 0.25 – 0.3m, however at the east and west ends of the trench it was

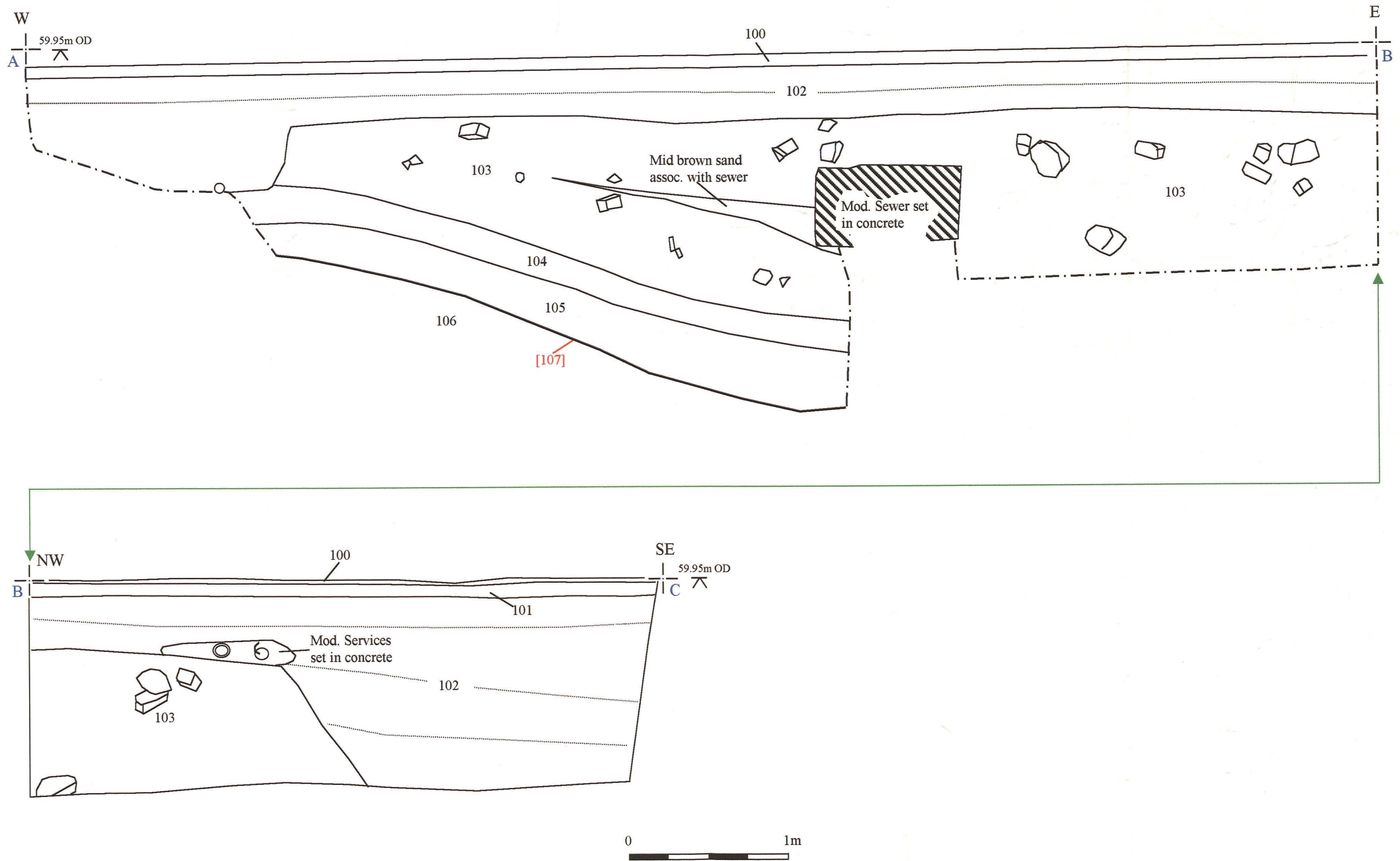


Fig. 4: South and south-west facing section of the evaluation trench. Scale 1:25

considerably thicker. At the west end of the trench 102 filled a trench associated with a live electricity service, which prevented further excavation in the immediate vicinity, and at the east end this material was deposited to a depth greater than 1.4m (the maximum excavated for safety reasons in the eastern half of the trench).

Sealed beneath 102, was a dark greyish brown mixed rubble deposit consisting of bricks of varying sizes, including masonry chunks and engineering brick, concrete and stone fragments, roof tile and cast-iron pipe fragments; set in a loose mortar matrix (103). This material was deposited to a depth greater than 1.4m in the eastern half of the trench.

It was decided to excavate the western half of the trench to the underlying natural surface (or archaeologically significant deposits, if present), and consequently the thickness of 103 was determined to be between 0.38m at the western end of the trench increasing to 1.25m to the east.

Sealed beneath 103 was a dark brown humic sandy clay layer, containing rare gravel inclusions – 104, which lay over a gritty mid grey sandy silt layer containing occasional small and medium limestone fragments – 105. Immediately beneath 105, the natural underlying limestone (106) was exposed, which here consisted of a weathered light brownish-yellow coarse brash. The exposed surface of the limestone bedrock was observed to be angled to an increasing degree, being at a depth from the ground surface of some 1.2m at the western edge of the trench, becoming 2.2m some 3.5m to the east, where the excavation of the western half of the trench stopped. This represents the edge of a large limestone quarry present to the east and north of the area, which explains the sloping nature of layers 104 and 105, which can be interpreted as an original topsoil (104) and subsoil (105), both of which had developed within the quarry void and were subsequently buried when the area was levelled for the filling station.

7.0 Discussion and conclusions

The discovery of the edge of a large limestone quarry (present to the east and north of the area) is significant. This Quarry appears to respect the line of Wragby Road, suggesting that the road was well established before the quarrying activity commenced. Despite rigorous cleaning, however, no datable finds were recovered during the evaluation, so it is not possible to determine the date of the quarry, nor to establish what may have been present in the area prior to its emergence.

The construction of the filling station has resulted in large scale localised filling of this area, up to the level of Wragby Road.

8.0 Effectiveness of methodology

The methodology employed at the former Eastgate Motors car park site has allowed a reasonably comprehensive understanding of the archaeological potential of the site, and the potential threat that redeveloping the area may have. It appears from the results of the evaluation that the archaeological resource will remain largely

unaffected by the construction of three houses and their associated infrastructure, as the edge of the quarry lies at depth and has already been filled to Wragby Road street level.

9.0 Acknowledgements

Pre-Construct Archaeology (Lincoln) would like to thank Cherry Tree Developments (UK) Ltd for commissioning the work and the assistance of their staff during the evaluation. Thanks are due to Suzie Matthewson for her hard work. Thanks go also to Mick Jones, the Lincoln City Archaeologist for his assistance during the course of the investigation.

10.0 References

Jones, MJ 2002 *Roman Lincoln: Conquest, Colony & Capital*, Tempus

Hill, F *Medieval Lincoln*, Cambridge University Press

Palmer-Brown, C, 2003, *Former Eastgate Motors, Wragby Road, Lincoln, Archaeological Evaluation Report*, Unpublished developer report, PCA (Lincoln)

11.0 Site archive

The documentary archive for the site is currently in the possession of Pre-Construct Archaeology (Lincoln). The paper and object archive will be deposited at Lincoln City and County Museum within six months. Access to the archive may be gained by quoting the global accession number, 2003.307.

Copies of this report have been deposited with the commissioning body, the City Archaeologist, the County Sites and Monuments Record for Lincolnshire, and the City and County Museum, Lincoln, along with an ordered project archive for long-term storage and curation.

Appendix 1: Colour Plates



Plate 1 (left):
General shot of the
development site,
looking south-east
across Wragby
Road.



Plate 2 (right):
General shot of the
development area
taken during the
machining of the
evaluation trench,
looking south-east.



Plate 3 (left):
General shot of the
development site,
looking north-east
across Wragby Road.



Plate 4 (left): General view of the trench excavated in the preferred area, looking south-west. The fuel tanks (covered with concrete) and associated pipework can be seen throughout.



Plate 5 (right): South and south-east facing section of the eastern half of the evaluation trench, looking north-west.



Plate 6 (left): South facing section of the western half of the evaluation trench, looking north-east.

Appendix 2: Context summary.

Context	Description
100	Tarmac apron of the former filling station
101	Large Concrete slab
102	Compacted 'Type 1' structural fill material
103	Mixed rubble deposit below 102
104	Dark brown humic sandy clay layer, below 103
105	Gritty mid grey sandy silt layer below 104
106	Natural Tabular Limestone
107	Quarry cut