



**A Report on an Archaeological Evaluation
at
Junction, St Mark Street, Lincoln**

NGR SK 9735 7085

Planning App. No.:

Produced by

John Samuels Archaeological Consultants

for

**Bournston Developments
Huntingdon House
278-290 Huntingdon Street
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JSAC 943/02/02
Site code: JSM02

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A report on an Archaeological Evaluation
At
Junction, St Mark Street, Lincoln

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Summary

An application for Planning Permission has been made and granted. The application proposes the creation of the construction of a new university student accommodation block and retail units.

The works are subject to archaeological evaluation, and this document forms the report on the evaluation excavation associated with the proposed scheme. It has been produced by John Samuels Archaeological Consultants on behalf of Bournston Developemnts.

The evaluation concluded that the depth to which ground disturbance has taken place remains virtually constant in both areas of the site, which were subjected to evaluation. However it is expected that towards the centre of the site disturbance will be to a greater depth, due to the insertion of inspection pits and diesel fuel tanks.

In order to increase our knowledge of the site and to assist in producing an archaeological mitigation strategy, the remaining geo-technical test pits should be archaeologically monitored.

1.0 Introduction

1.1 Site Location and Description

1.1.1 The site is centred on NGR SK 9735 7085 and occupies land towards the south part of Lincoln City Centre at the corner of St Mark Street and Brayford Wharf East. The site has most recently been used as a bus repair depot and associated offices (figure 1).

1.1.2 The site is bounded by St Mark Street to the south, by Brayford Wharf East to the west, by a newspaper headquarters building to the north. A narrow lane separates the eastern extent of the site from St Mark's Church Hall, a Grade II Listed Building.

1.2 Planning Background

1.2.1 An application for Planning Permission has been made and granted. The application proposes the creation of:

*the construction of a new university student accommodation block

*retail units.

1.3 Archaeological Background

1.3.1 No archaeological investigations had been undertaken on the site prior to the current planning application. However, archaeological investigations in the vicinity have revealed finds dating from the Iron Age, Roman, Saxon and Medieval periods. The site lies just to the north of a Carmelite friary founded in the mid 13th century AD.

1.3.2 A Desk Based Assessment was produced, by L-P Archaeology, ref: LP/06/75. On the basis of this assessment, suggest that if archaeological deposits survive within the site they are most likely to date from the Roman period onwards consisting of advancing north-south lines of waterfronts and dump deposits of Roman to Medieval periods and 18th & 19th century structures. There could also be buried peats from possibly as early as the Mesolithic period.

1.4 Project Objectives

1.4.1 The objectives of the evaluation have been set in the brief produced by the City Archaeologist:-

I to provide information on the depth and quality, below the modern surface, of surviving archaeological deposits which would be affected by any proposed groundwork.

- II to provide information which will allow for the assessment of the possible scale of development impact on any remains and other information which might influence development design so that such impact can be avoided or minimised.
 - III to provide information that will allow the local planning authority to reconcile development proposals with their policy of preserving archaeological remains and make an informed and reasoned decision on a planning application
 - IV to provide site-specific archaeological information which (if necessary) would allow for the design and integration of timing and funding of any further archaeological work (or other mitigation strategy) which may be required in advance of, or during, any subsequent development programme.
 - V to produce a project archive for deposition with the City and County Museum
 - VI to provide information for accession to the County Site & Monuments Record and the Lincoln Urban Archaeological Database.
- 1.5 This document forms the report on the findings of the trial excavation which was undertaken in accordance with a written scheme of investigation (*JSAC 943/02/01*) approved by the City Archaeologist on behalf of the local planning authority. It has been written By J F Marsden BA, MA, Cert.Arch. AIFA, in consultation with John Samuels BA, PhD, FSA, MIFA, MLI. It conforms to the requirements of 'Archaeology and Planning' *Planning Policy Guidance Note 16* (DoE 1990). It has been designed in accordance with current best archaeological practice and the appropriate national and regional standards and guidelines including :

Management of Archaeological Projects (English Heritage, 1991);

Code of Conduct (Institute of Field Archaeologists, 1994 rev 1997);

Standard and Guidance for Archaeological Evaluations (Institute of Field Archaeologists, 1994 rev 1999); and

Archaeological Handbook (Lincolnshire County Council 1998)

2.0 Methodology

- 2.1 The fieldwork was undertaken in accordance with the approved specification (*JSAC 943/02/01*).
- 2.2 Each trench was located by hand using a level and steel tape, and machined using JCB. Following the breaking of the concrete floor surface, initial machining was undertaken with a toothed bucket to remove the subsurface to the concrete floor and rubble makeup layers. Subsequent machining was undertaken using a toothless bucket.
- 2.3 Each trench was recorded prior to back filling. The recording comprised 35mm photography, scale drawing and descriptions of the soil horizons on pro-forma context sheets.
- 2.3 The datum line for each section drawing was related to Ordnance datum.

3.0 Excavation

The fieldwork was undertaken on the 29th and 30th May 2002.

3.1 Trench 1

3.1.1 Located within the visitor's car park close to the St Mark's Street boundary (see figure 2) it measured 1.75 x 3.30 metres and was initially excavated using a toothed bucket, following the removal of the car park surface excavation continued to a depth of 2.01 metres below the modern ground surface.

3.1.2 The majority of excavation was through post medieval layers containing brick fragments, 102,103,104, 105 & 106.

3.1.3 Sealed by (106) is a black, fine silt (107), of firm compaction, the upper layer of which contained an oyster shell, a fragment of an early 19th century clay pipe stem and 2 sherds of 18th century salt glazed pottery.

3.1.4 A sondage, 0.50m in depth, excavated in the northeastern corner of the trench confirmed that the black, fine silt (107) was still present; there was no evidence of snails, molluscs or organic matter.

Summary of Context sheets Trench 1

Context	Description	Pot	Bone	Other	Comment
101	Tarmac over road stone				Car Park surface
102	Dark Brown /black sandy loam			brick	Dumped layer
103	Light Brown/orange sandy silt			brick	Dumped layer
104	Light Brown/orange sandy silt			brick	Dumped layer
105	Dark brown sandy silt			brick	Dumped layer
106	Orange sandy clay			brick	Dumped layer
107	Black fine silt	18 th c			Clay pipe stem & oyster shell

3.2 Trench 2

- 3.2.1 Located within the existing Road Car Bus Depot close to the Brayford Wharf East boundary (see figure 2) trench 2 measured 1.85 x 2.80 metres and was initially excavated using a concrete breaker and toothed bucket, following the removal of the concrete surface (201) excavation continued to a depth of 2.04 metres below the modern ground surface.
- 3.2.2 The floor surface 201 was supported on a sub base of orange / red brick rubble in a concrete matrix, reinforced with steel mesh (202). This in turn overlay a make up layer 0.78m in thickness of brick rubble and concrete blocks, with dimensions of up to 0.50 x 0.30m x 0.30m, contained in a dark black / brown loamy matrix (203).
- 3.2.3 At a depth of 1.00m a brick wall was exposed (206), running north – south parallel to, but 0.30m from the western face of the excavation. This comprised bricks, 0.23m in length, laid in header bond and stamped with the name “CINAHILLS”. Parallel to this wall, but 0.35m to the east a similar wall was exposed.
- 3.2.4 Behind (206) a further wall (207) was revealed, 11.5 cms. thick and laid in a random bond. The gap between 206 and 207 was filled with a sandy orange mortar (204). The surface of the bricks (207) was coated with a soot like substance.
- 3.2.5 206 and 207 were supported on a substantial foundation (210) which extended into the base of the excavation, comprising an orange / red mortar containing brick and tile fragments.
- 3.2.6 To the rear of wall 207, a layer of firm black silty material, containing ash, slag, modern glass and pockets of burnt clay were exposed (208), this layer sealed layers (209 & 211).
- 3.2.7 In the north western corner of the excavation sealed by 208 and cut into 209 was a lense of loose sandy, white / yellow mortar (211) 0.08 m.in thickness.
- 3.2.8 Layer (209) was a firm green / brown clayey silt, which became more plastic with depth. Within the upper levels fragments of tile were present.

Summary of Context sheets Trench 2

Context	Description	Pot	Bone	Other	Comment
201	Concrete				
202	Concrete, Orange/ Red				
203	Brown/Black loam			brick	
204	Orange mortar				
205	Orange Mortar				
206	Brickwork				
207	Brickwork				
208	Black silt				Contains modern glass, slate and burnt clay
209	Brown/green clayey silt				Contains fragments of tile
210	Orange/ red mortar				Contains fragments of brick
211	White/yellow sandy mortar				Possible fragment of a floor surface

4.0 Results

Trench 1

- 4.1 Initial excavation was through the car park surface and its associated make up layers and post medieval demolition layers. At a depth of 1.68m below current ground level, a black, fine silt, of firm compaction was exposed. Contained within the upper surface of which was an oyster shell, a fragment of an early 19th century clay pipe stem and 2 sherds of 18th century salt glazed pottery.
- 4.2 The silt is believed to be water lain, although in the 0.90m that was investigated there was no evidence of snails, shells or organic matter. The upper surface of the silt was at a height of 5.18 m OD.

Trench 2

- 4.3 Located within the existing Road Car Bus Depot close to the Brayford Wharf East boundary. Following the removal of the concrete surface excavation continued to a depth of 2.04 metres below the modern ground surface. The upper levels comprised makeup layers for the concrete flooring; dumped demolition rubble cut thorough by the remains of the foundation levels of brick walls and associated foundations.
- 4.4 At a depth of 1.62m below existing ground level a firm green / brown clayey silt was encountered, which became more plastic with depth. Within the upper levels fragments of tile and brick were present. It is not clear whether these fragments had migrated downwards from the material, which sealed this deposit. The upper surface of the clay was at a height of 4.72m OD.

Conclusions

1 The lack of water borne organisms within the natural silts in trench 1 suggests that they may have been truncated. There are certainly sealed by 1.68m of dumped material.

2 The results from evaluation trench 2 suggest that the western end of the site has been heavily disturbed by episodes of modern building and demolition to a depth of at least 1.62m, to the extent that ant riverine deposits only survive as isolated pockets.

3 The depth to which ground disturbance has taken place remains virtually constant in both trenches, although it is expected that towards the centre of the site disturbance will be to a greater depth, due to the insertion of inspection pits and diesel fuel tanks.

4 The proposed ring beam and pile cap foundation and service trench design, although not finalised, would indicate that the maximum depth of excavation would be in the region of 1.80 metres below existing ground level. Should this prove to be the case there is slight possibility that the upper most archaeological deposits (if they survive) may be encountered at this depth. In order to increase our knowledge and assist in producing an archaeological mitigation strategy, the remaining geo-technical test pits should be archaeologically monitored.

6.0 Figures

Figure 1. Site Location.

Figure 2. Trench Location.

Figure 3. Trench 1 North-South Section. Scale 1:20

Figure 4. Trench 1 East-West Section. Scale 1:20

Figure 5. Trench 2 North-South Section. Scale 1:20

Figure 6. Trench 2 East-West Section. Scale 1:20



Figure 1 : Site Location

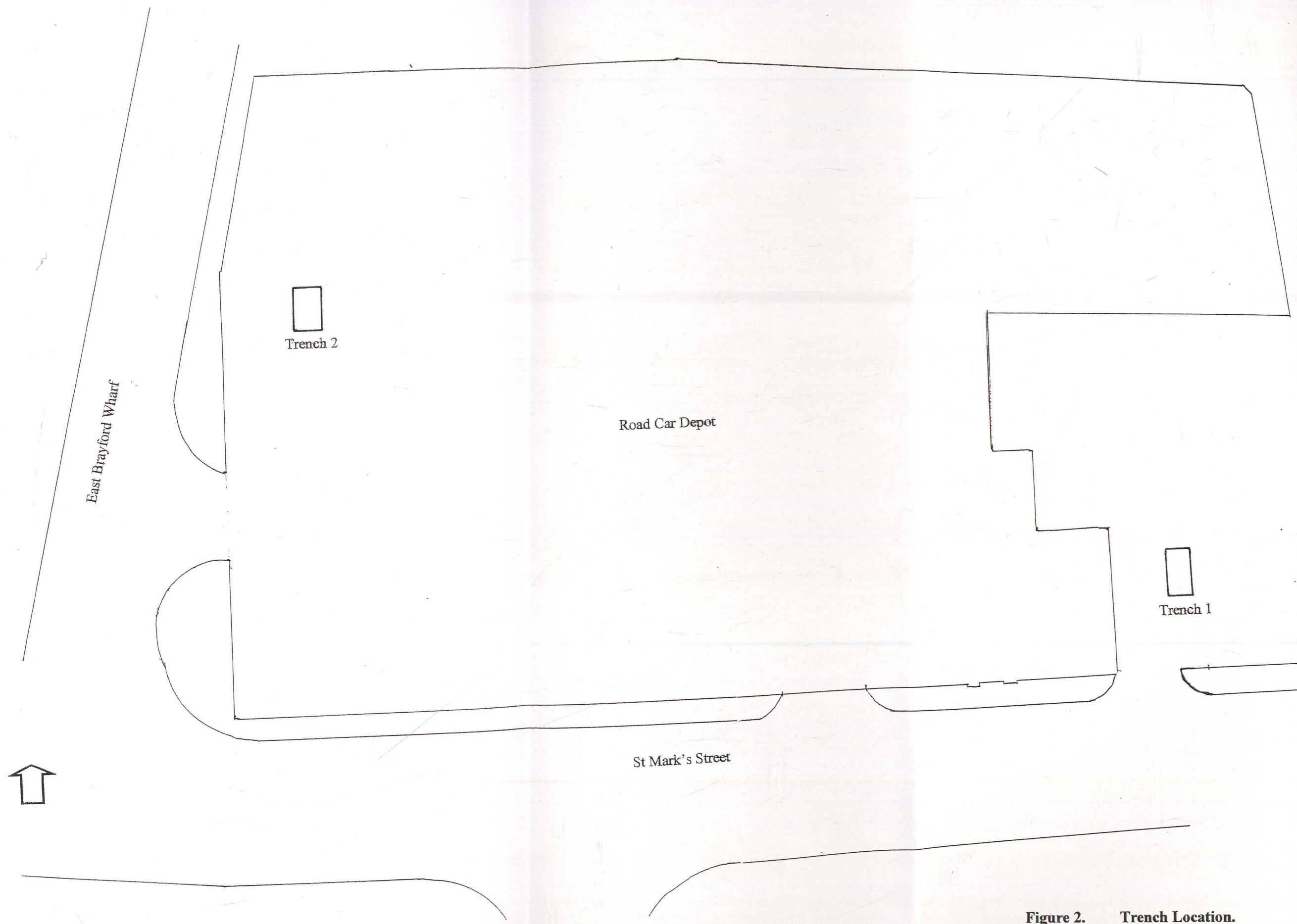


Figure 2. Trench Location.

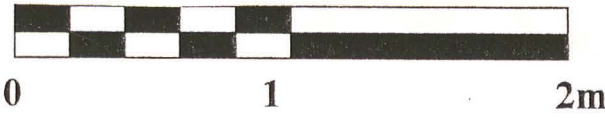
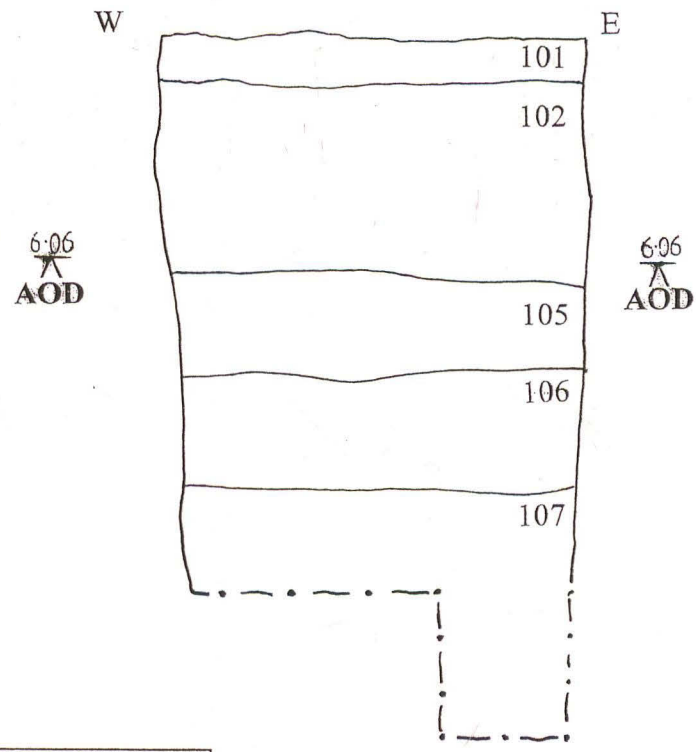
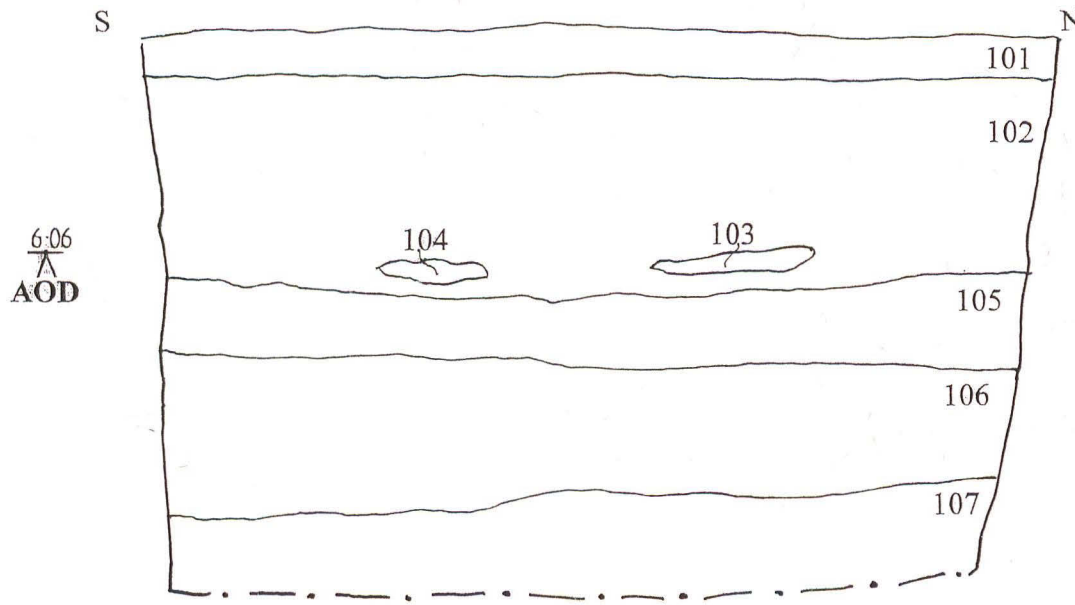


Figure 3. Trench 1 North-South Section. Scale 1:20

Figure 4. Trench 1 East-West Section. Scale 1:20

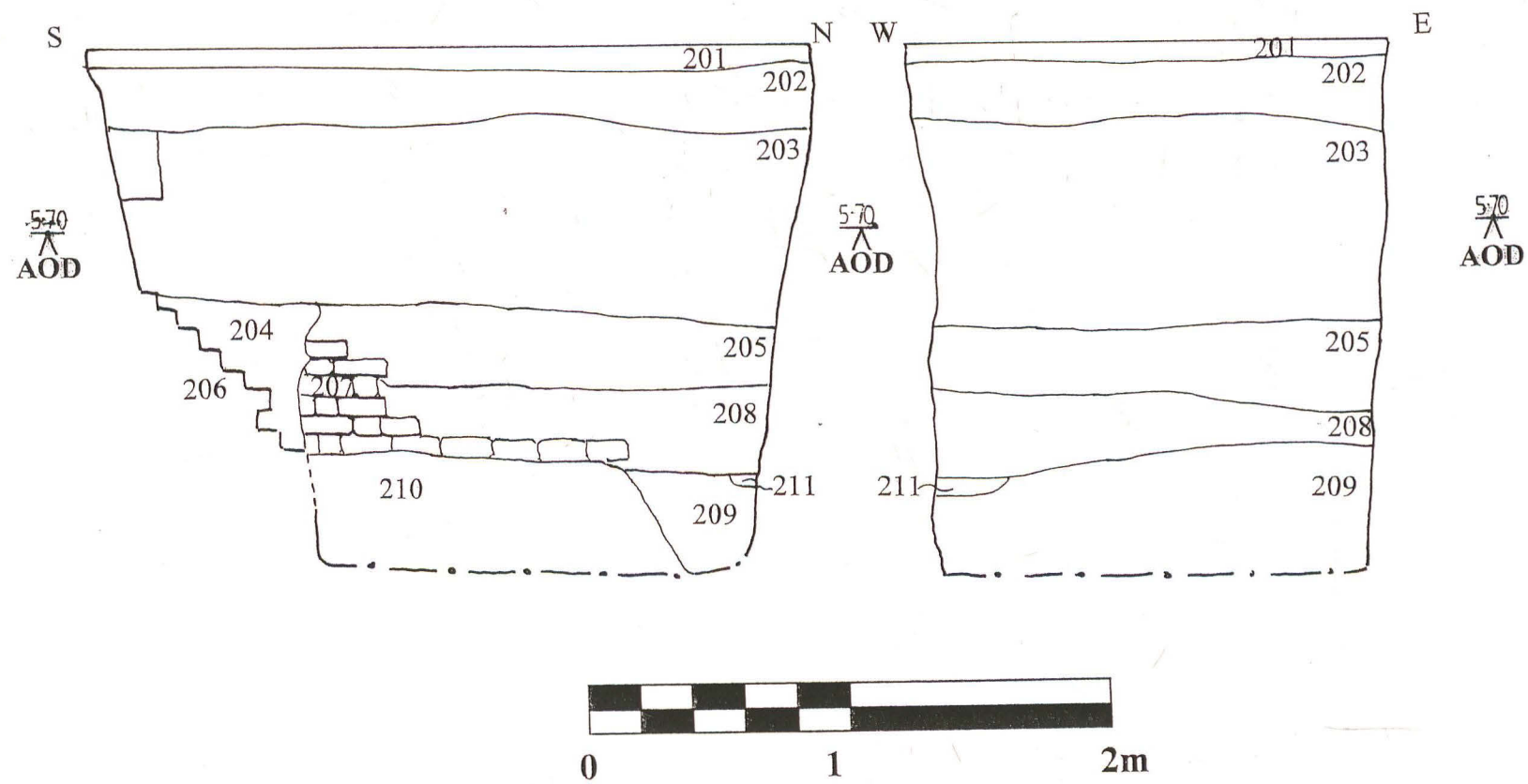


Figure 5. Trench 2 North-South Section. Scale 1:20

Figure 6. Trench 2 East-West Section. Scale 1:20

7.0 Photographs.

- Plate 1. Location of Trench 1 prior to excavation.
- Plate 2. Trench 1, West section face.
- Plate 3. Trench 1, North west faces.
- Plate 4. Location of Trench 2 prior to excavation.
- Plate 5. Trench 2, surface broken.
- Plate 6. Trench 2, West section face.
- Plate 7. Trench 2, southern end of trench showing parallel foundation walls.
- Plate 8. Trench 2, Detail of brickwork in western face.



Plate 1.

Location of Trench 1 prior to excavation.

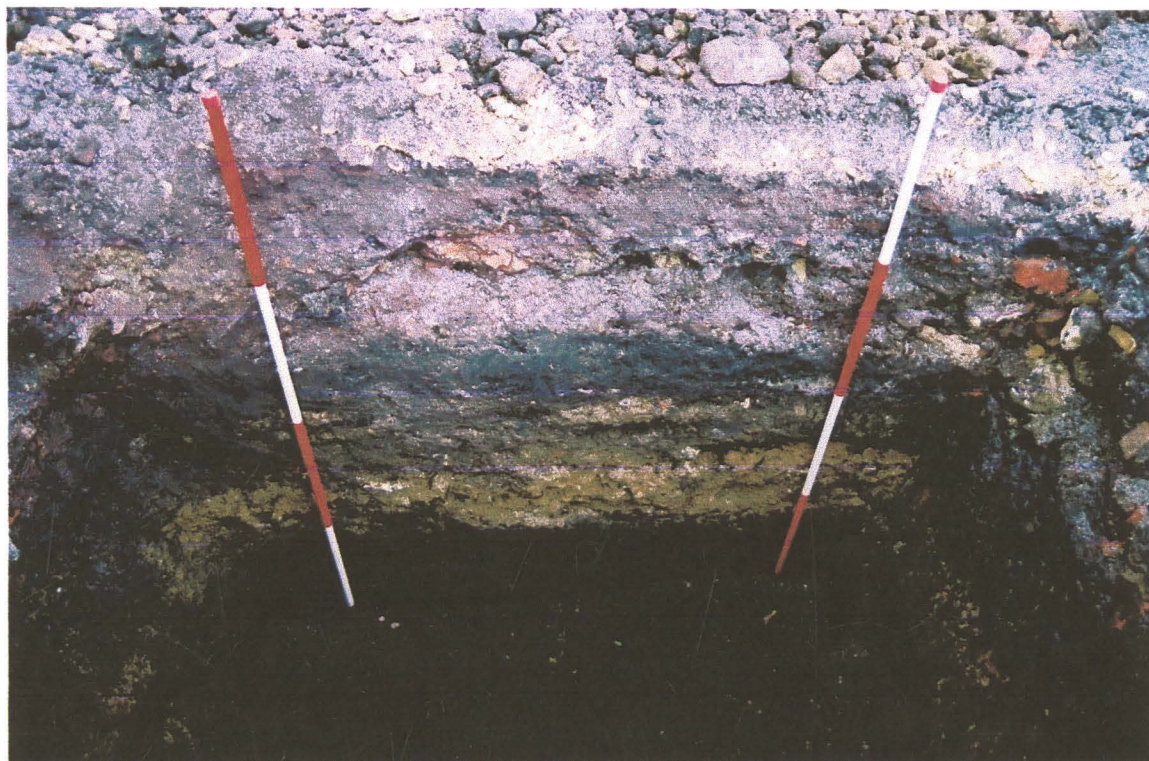


Plate 2.

Trench 1, West section face.

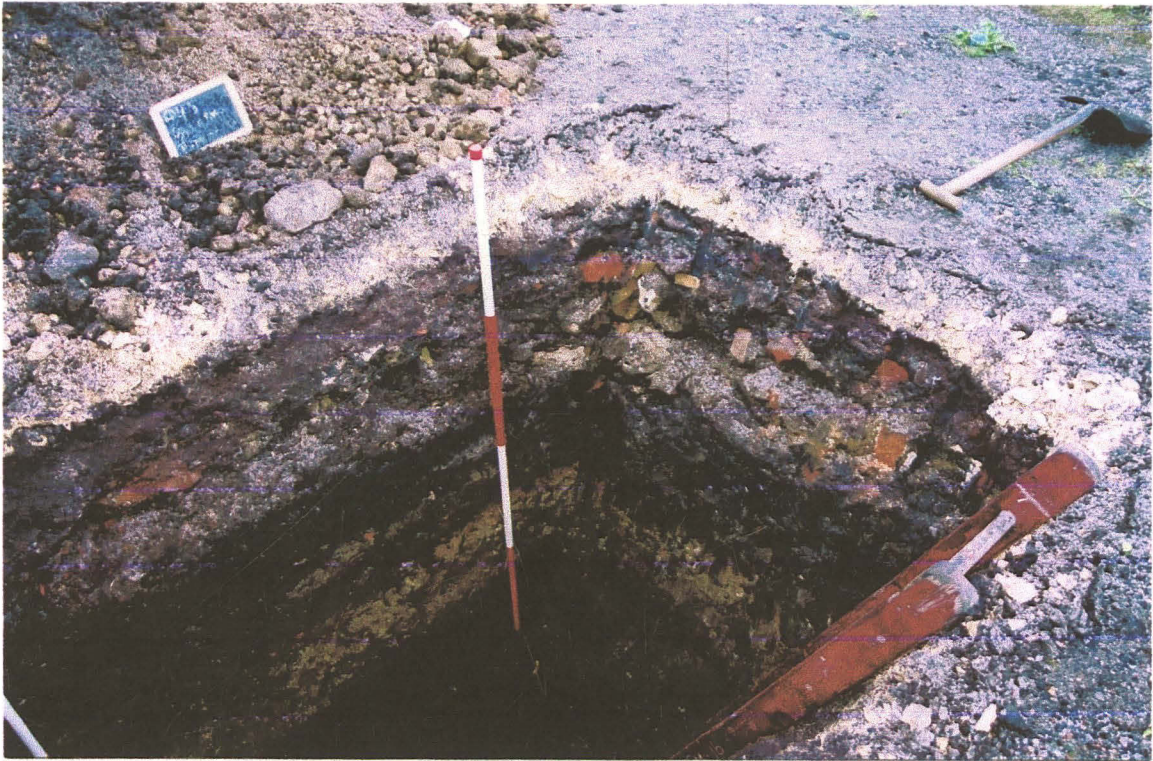


Plate 3. Trench 1, North and West faces.



Plate 4. Location of Trench 2, prior to excavation.



Plate 5.

Trench 2, surface broken.

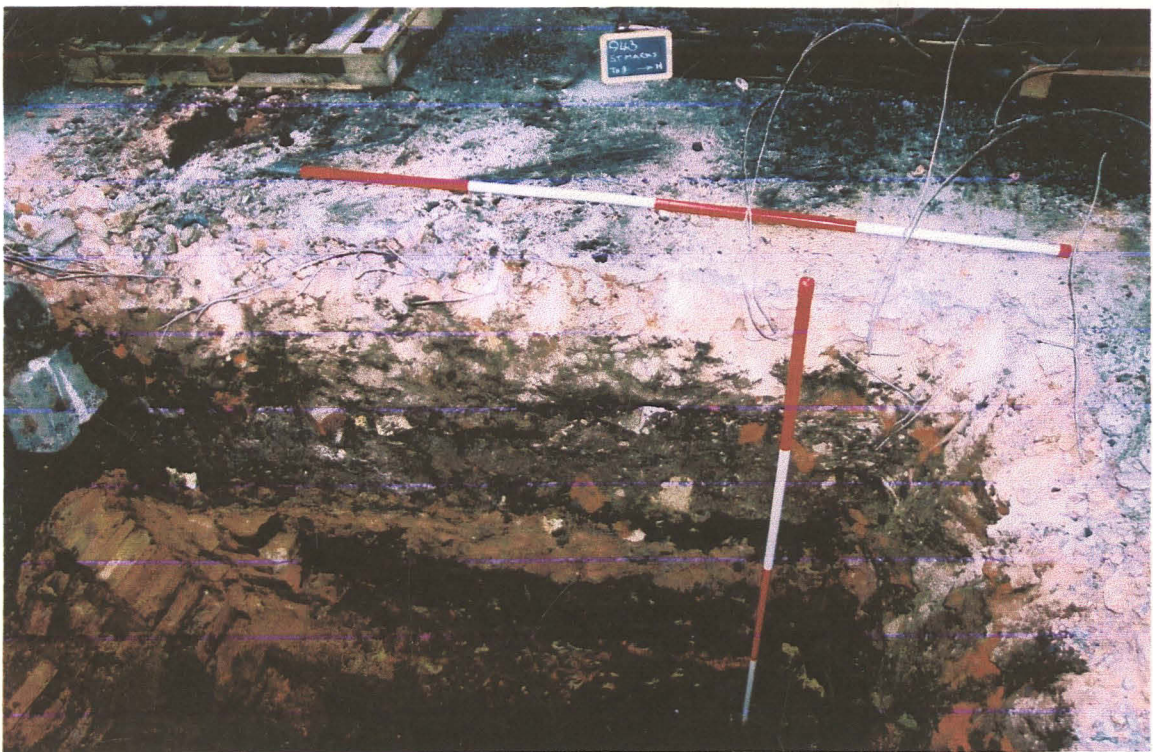


Plate 7.
Trench 2, southern end of trench showing parallel
foundation walls.



Plate 8