

95/15

CITY OF
◇ LINCOLN ARCHAEOLOGY ◇
U N I T



**FORMER ST MARKS STATION SITE,
LINCOLN. SEWER INSTALLATION**

ARCHAEOLOGICAL WATCHING BRIEF

By K Wragg

95/15

CLAU ARCHAEOLOGICAL REPORT NO: 218



Lincolnshire County Council
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Report to
Simons Construction Ltd

December 1995

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ST. MARKS STATION, LINCOLN - SEWER INSTALLATION

ARCHAEOLOGICAL WATCHING BRIEF

Non-Technical Summary

Although virtually no dating evidence was recovered from the site, it seems likely that the majority of the features discovered relate to the medieval Carmelite friary and its burial ground. Based upon the results as outlined in the main report the following stratigraphic sequence can be put forward (from earliest to latest).

- 1) Undisturbed sands and gravels occurring (adjacent to the High Street and probably across most of the site) at levels of approximately 3.70-3.80m O.D.
- 2) Dump or "garden" deposits of indeterminate date appearing to pre-date the presumed medieval friary occupation (without further investigation, and reliable dating evidence, it is impossible to determine whether these levels represent Roman, Saxon, early medieval or even natural deposits).
- 3) Occupation of the site during the medieval period (possibly beginning in the late 11th and early 12th centuries), with revealed walls, robber trenches and burials probably connected with the Carmelite friary. Again without the benefit of accurate dating it is not possible to determine when this occupation started, or equally, when it came to an end.
- 4) Development of the site in connection with the arrival in the City of the railway in the mid 19th century. From the evidence of other archaeological investigations that have taken place across the site, it would appear that this development resulted in the truncation of the majority of the Post-medieval deposits. This phase of the development of the site continued until the closure of the station in the mid 1980's.

Whilst the exact nature and origins of some of the features revealed by this project remain unknown, it is clear that substantial archaeological remains are still well preserved to the north of the station buildings. Any further work proposed for the site in general, and in this area in particular, should pay specific attention to the identification and preservation, either physically or by detailed archaeological record, of these remains.

FORMER ST. MARKS STATION SITE, LINCOLN - SEWER INSTALLATION 1994

ARCHAEOLOGICAL WATCHING BRIEF

1.0 Introduction

This project resulted from plans by Simons Construction Ltd. to install a new sewer connection between the existing main sewer beneath the High Street and the retail development (St.Mark's West) to the west of the River Witham.

It was originally proposed that a "gravity" system would be used, necessitating the excavation of a deep trench (probably in excess of 2.0m) for the entire length of the required trench. Following consultation with the Lincoln City Council Department of Planning, and the City of Lincoln Archaeology Unit (C.L.A.U.), it was decided to modify plans in order to minimise the impact on any surviving archaeological deposits. Ultimately a "pumped" system, involving much shallower excavation, was chosen.

The finalised design comprised a machine excavated trench nominally 750mm deep for the majority of the proposed route, with a sloping section at the east end to allow connection to the existing sewer pipe (located approximately 3.5m below the modern ground level). The routing of the new sewer was complicated by the presence of an existing watermain, and by the proximity of the former station building. The latter is a Listed structure, and elements of the portico (entrance) are in a bad state of repair, giving rise to concern about their stability if trenching was to be carried out too close to the foundations.

Given the likelihood of disturbing important archaeological remains, including burials associated with the medieval Carmelite friary known to have previously occupied the site, the C.L.A.U. was commissioned to provide an intensive watching brief on the necessary groundworks, which was undertaken in November 1994.

The information in this document is presented with the proviso that further data may yet emerge. The Unit, its members and employees cannot, therefore, be held responsible for any loss, delay or damage, material or otherwise, arising out of this report. The document has been prepared in accordance with the terms of the Unit's Articles of Association, the Code of Conduct of the Institute of Field Archaeologists, and *The Management of Archaeological Projects 2* (English Heritage, 1991).

2.0 Historical and Archaeological Background

The site is located at NGR SK 9735/7075 in the "Wigford" area of Lincoln and is bounded by the High Street to the east, the River Witham to the west, St.Mark's Street to the north and Firth Road to the south.

The name of "Wigford" applies to that part of the City between the River Witham to the north and the St.Catherines area and South Common to the south. It is derived from that of the medieval suburb covering the same area (Cameron 1985, 45-6). It appears to mean "the ford by the wic, or vicus" (street or quarter).

In conjunction with developments in the area over the last ten years, several elements of the St.Mark's station site have been archaeologically investigated. A trial trench in 1985 located in a siding to the north-west of the area of the station, was followed up by an area excavation in 1986 which investigated the area between the two station platforms in order to locate the Carmelite friary. In 1987 a further trench was excavated, parallel to the High Street at the east end of the station platforms, to record the stratigraphic sequence through late Saxon, Norman

and Roman occupation down to what was probably natural sand. Also in 1987 a watching brief was carried out during groundworks for a new watermain to the north of the station buildings.

No further archaeological work was carried out on the site of the station complex until 1994 when a further watching brief was undertaken, again to the north of the station buildings. This project took place in conjunction with geotechnical work to assess the condition of the foundations along the north side of the station buildings.

The results from the archaeological investigations indicate that the former railway station site was originally part of a Roman commercial suburb lying to the south of the main defensive area, with properties fronting on to Ermine Street (High Street).

The earliest features recorded during excavations between the station platforms comprised a 2nd-century timber structure, lying to the west of the Roman Ermine Street and sited on the northern edge of an east-west drain. This building was possibly used for smithing and subsequently underwent several phases of modification. In the mid 3rd-century the drain was backfilled and stone-founded "strip" buildings were constructed, again fronting Ermine Street with their rear yards backing on to the river. One strip building contained within its back room a series of hearths, and occupation appeared to continue into the mid-to-late 4th century. In the late 4th century the walls were robbed to their foundations and massive dumps raised the ground level by c.1m; the area of dumping appears to have extended c.20m beyond the backs of the strip buildings.

The river was a continuous presence to the west of the site throughout the Roman period. Early on there were large Roman drains associated with surfaces which were being used at between 4.52m OD and 4.86m OD. Dumps backfilled the drain for the construction of later structures with ground level later rising to 5.25m O.D. Piling as part of the construction of some features suggests an awareness of the instability of underlying sands. After buildings went out of use the ground level was raised above the level of river flooding to between 5.68 and 5.77m OD, presumably to combat the rise in water level.

Lying above the Roman remains were pits and postholes indicating occupation from the early 10th century. Between the mid-10th and mid-11th centuries massive dumps extended west towards the river as far as the limits of excavation. There were also traces of timber buildings on both sides of the High Street.

By the end of the 12th century a lane had extended to the west from the High Street, situated roughly between the present station platforms. This gave access to buildings which had been erected on either side of the lane. By the 13th century (c.1269) the land to the north of the lane became part of the Carmelite Friary and a large stone building was erected, with the south side of the principal friary building undergoing two phases of construction. Between the 14th to 15th centuries the lane went out of use; to the north it was sealed by a graveyard and to the south was a series of rooms, sharing a north wall but each with a large hearth. To the north of the site the Friary graveyard was truncated in the 15th century by two substantial stone-buttressed structures.

The friary walls were levelled and the foundations robbed before the mid-16th century, and several buildings were later constructed fronting on to High Street.

Later levels were truncated by the construction of the Midland Railway which opened in 1846, and from this date the majority of the occupation on the site was mainly concerned with railway expansion and operation.

A more detailed description of the archaeological and historical background can be found in the desk-top assessment produced, in December 1994, in advance of plans for redevelopment of the station site itself (C.L.A.U. Report no.142). Large-scale excavations north of the station are in progress at the time of the compilation of the final version of this report (November 1995)

3.0 Results

The archaeological remains uncovered during this project can be broadly grouped into four periods, as follows:

Period 0 - The natural geological deposits, mainly sands and gravels.

Period 1 - Roman, Saxon or early medieval occupation (the lack of dating evidence making individual identification of deposits impossible)

Period 2 - Medieval occupation including burials, probably associated with the Carmelite friary.

Period 3 - Modern occupation, largely connected with the Railway usage of the site.

Period 0 was represented by a mottled orange/grey-brown sand [110], which was only revealed at the extreme eastern end of the trench, adjacent to the High Street, owing to the increased depth of the trenching.

This deposit was overlain by a layer of moderate-well compacted mid-dark grey-brown clayey earth [115], containing only very occasional small pieces of limestone, which appears to represent the earliest surviving occupation deposit in this area of the site (Period 1). [115] was present throughout the majority of the length of the trench and was sealed by two further deposits. The first of these, [106], occurred at the eastern end of the trench and comprised a dull orange-brown sand containing occasional small pebble inclusions, while the second comprised a thin (c.50mm) lense of crushed limestone pieces [119], seen adjacent to the west end of the station buildings.

Both [106] and [119] were then sealed by a 300mm thick layer of grey-brown sandy earth [105]/[118], containing limestone flecks, rounded pebbles/gravel, and brick/tile flecks. This layer was present throughout the trench and was cut by three groups of later intrusive features, including graves, wall construction cuts, and robber trenches, which have been allocated to Period 2 (see Fig.4), together with a modern brick feature [128] situated opposite the eastern end of the station portico.

From east to west, the Period 2 elements were as follows: The first group appeared at the extreme eastern end of the trench (revealed in the large connection trench adjacent to the High Street), where three burials were revealed, together with an area of burning, the function of which could not be interpreted. Of the burials only one (fill [102]/remains [103]/ cut [104]) was contained within an identifiable grave cut, with the other two simply comprising partial human remains ([107] & [108]) with no obvious graves. All appeared to be oriented E-W, and all seemed to be articulated, although they were only partially revealed by the machining. The burnt feature [109] comprised a c.300mm thick layer of multi-coloured sand and clay, ranging in colour from bright orange, through red to dark grey/black, and was only revealed in the south facing section, so accordingly its full extent is not known, although it was seen to extend approximately 2.0m E-W. [109], together with the three burials, was then overlain by a very mixed/disturbed dump layer [101] approximately 800mm thick, which comprised a multi-banded sand/sandy earth containing pebbles and several modern service pipes, and lay immediately below the modern road surface and bedding, [100].

The second group of features was recorded adjacent to the eastern end of the main station building, and appeared to represent the remains of one or more structures. The remains took the form of three robber trenches (from west to east: [130]/[134]/[136]), all seemingly oriented N-S and revealed approximately 400-500mm below the modern ground surface. The three trenches were all filled with a similar material, a mid yellow-brown, coarse-grained, sandy mortar containing frequent small limestone pieces and occasional small flecks of brick or tile (from west to east: [129]/[133]/[135]). The westernmost robber trench fill, [129], was sealed by a thin secondary fill [132], which comprised a dull orange-brown sand containing no inclusions.

The third, and final, group of intrusive features associated with Period 2, was revealed adjacent to the western end of the station portico and comprised the remains of a limestone wall [120], with its associated construction cut [137], and a layer of mid-light yellow-brown sandy mortar [126] containing small tile and limestone fragments, which appeared to be a layer of bedding for the wall. The wall was oriented approximately E-W and comprised

only 1-2 roughly constructed courses of irregularly shaped limestone blocks, packed with small pieces of limestone and grey-brown clayey earth, and probably represents the remains of foundations. A layer of re-used broken roof tiles [124], forming a roughly horizontal layer or surface, then sealed the wall remains [120], possibly representing demolition debris or a rough floor level from a later structure.

Any later (i.e. probably post friary) occupation deposits overlying these three groups of features appear to have been truncated, probably during the construction of the railway in the mid 19th century, with the result that only archaeological deposits relating to the modern period (Period 3 - see above) survive above this level, at least in the area revealed by this project.

The previously mentioned tile layer/surface [124] was cut by a modern trench, [138], containing a 25mm diameter lead water pipe [125], and a mid grey-brown slightly clayey earth fill [139], which was in turn sealed by a single course of modern red bricks [123], showing no sign of bonding and appearing to form a possible surface. A further modern brick surface/feature [128], in this case constructed from a combination of blue "engineering" and red "common" bricks, was revealed opposite the eastern end of the station portico, and this latter feature was cut by a watermain trench [127] lying to the south. Both brick features were overlain by a thin layer (c.50mm - 100mm) of off-white/yellow-brown coarse grained sand or mortar [117], which contained moderately frequent amounts of small, irregularly shaped, limestone pieces. This deposit was present for at least 50m E-W and closely resembled modern "Type 1" hardcore.

To the east of [117], a similar layer [131] was recorded, sealing the three previously mentioned robber trenches. It also comprised a "hardcore-like" material, but appeared to contain more frequent brick/tile inclusions, and although it is probably the same deposit as [117], no direct relationship between the two was identified.

The deposits overlying the [117]/[131] horizon, can be grouped into two distinct elements, reflecting the different types of terrain currently present across the site, namely metalled surfaces of various types to the north and east of the area under consideration, and soil overburden and scrubland towards the river to the west.

The latter elements comprised a layer of moderately compacted mid orange sand [113], which was seen to directly overlay layer [117], and was itself sealed by two further deposits, [112] and [116]. Layer [116] consisted of a very mixed mid brown, coarse grained, sandy earth containing gravel, sand and limestone patches, modern concrete, granite cobbles/setts and plant remains, which formed the surface deposit on the periphery of the metalling, adjacent to the western end of the station buildings. [112] comprised a thin (c.125mm) layer of dark grey (possibly oil-stained) clayey earth and gravel, which gradually disappeared towards the west and was sealed by [114], a mid brown sandy earth. Finally [114] was itself sealed by a layer of fine mid brown sandy earth mixed with mid grey stone pieces (10% earth/90% stone), which appeared to be re-used light grade railway ballast. All of these layers seem to represent various phases of tipping and reclamation between the station buildings and the River Witham.

The elements making up the various metalled surfaces were mainly encountered at the eastern end of the trench and comprised the previously mentioned tarmac road surface [100], seen adjacent to the High Street, and [121], a layer of roughly square granite setts, together with the associated sand and gravel bedding [122], present opposite the station portico. The surface made up of granite setts would appear to be the remnant of an earlier station forecourt, and was seen to be sealed by the modern tarmac [100] along its eastern edge.

4.0 Results and Conclusions

Although, as previously mentioned, virtually no dating evidence was recovered from the site, it seems likely that the majority of the features relate to the medieval Carmelite friary and its burial ground. Based upon the results as outlined above the following stratigraphic sequence can be put forward (from earliest to latest).

1) Undisturbed sands and gravels occurring (adjacent to the High Street and probably across most of the site) at levels of approximately 3.70-3.80m O.D.

2) Dump deposits of indeterminate date appearing to pre-date the presumed medieval friary occupation (without further investigation, and reliable dating evidence, it is impossible to determine whether these levels represent Roman, Saxon, early medieval or even natural deposits).

3) Occupation of the site during the medieval period (possibly beginning in the late 11th or early 12th centuries), with revealed walls, robber trenches and burials probably connected with the Carmelite friary. Again without the benefit of accurate dating it is not possible to determine when this occupation started, or equally, when it came to an end.

4) Development of the site in connection with the arrival in the City of the railway in the mid 19th century. From the evidence of other archaeological investigations that have taken place across the site, it would appear that this development resulted in the truncation of the majority of the Post-medieval remains. This phase of the development of the site continued until the mid 1980's.

Whilst the exact nature and origins of some of the features revealed by this project remain unknown, it is clear that substantial amounts of archaeological remains are still well preserved to the north of the station buildings. Any further work proposed for the site in general, and in this area in particular, should pay specific attention to the identification and preservation, either physically or by detailed archaeological record, of these remains.

5.0 Acknowledgements

The City of Lincoln Archaeology Unit would like to thank the following for their assistance during this project: Messrs. P.Vine and M.Tharret (Simons Construction Ltd); staff of the groundwork contractor UCS Plant Ltd., Wragby, Lincoln.

APPENDIX A

APPENDIX A

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APPENDIX B

APPENDIX B

Archive Deposition

The archive consists of:

No.	Description
1	Site diary
1	Report
39	Context records
9	Scale drawings
1 set	Photographic records - Colour slides
1	Stratigraphic matrix

The primary archive material, as detailed above, is currently held by:

The City of Lincoln Archaeology Unit,
Charlotte House,
The Lawn,
Union Road,
Lincoln,
Lincolnshire,
LN1 3BL.

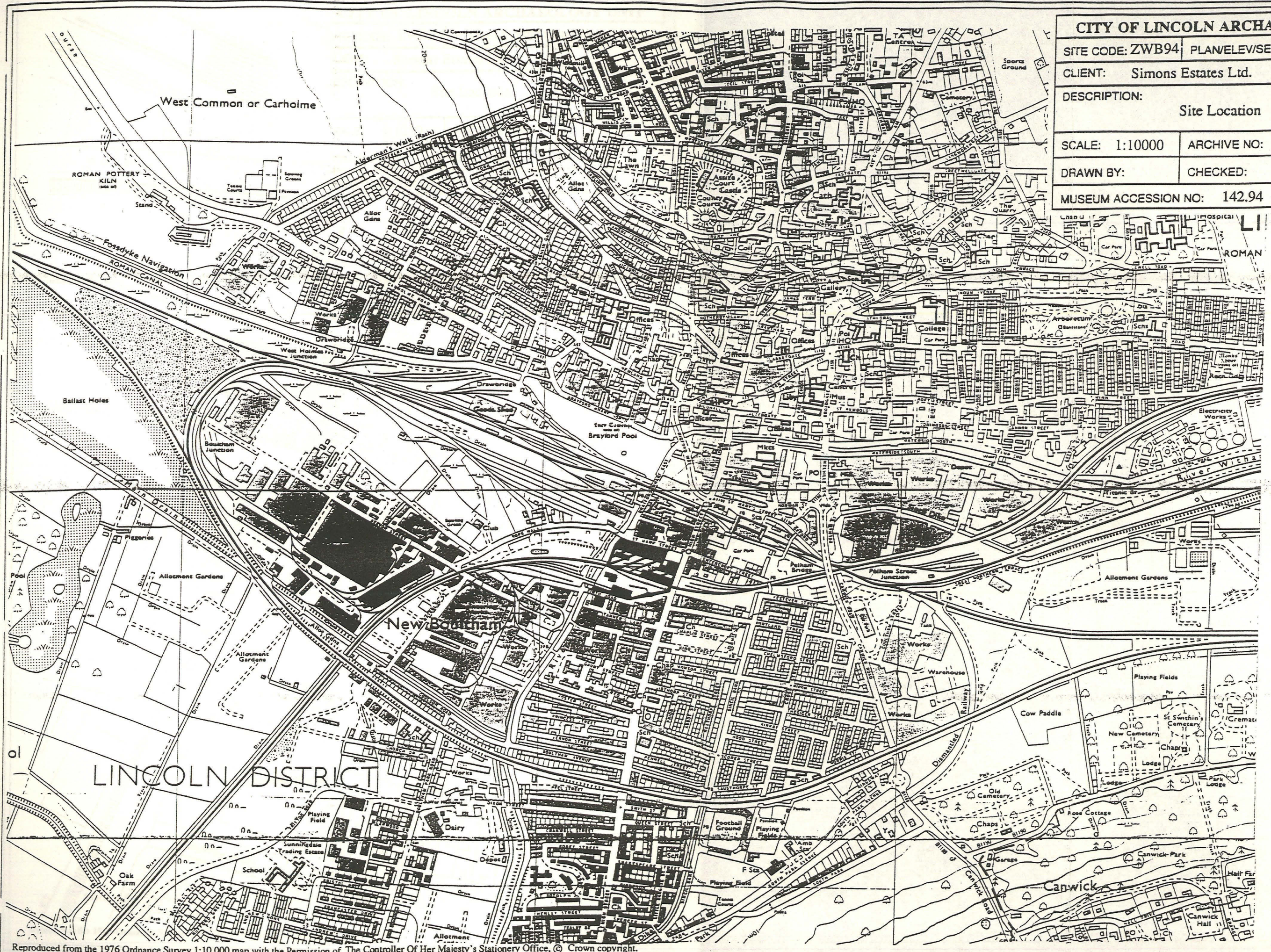
It is intended that transfer to the City and County Museum, Friars Lane, Lincoln, in accordance with current published requirements, under Museum Accession Number 142.94, will be undertaken following completion of this project.

APPENDIX C

APPENDIX C - TABLE SUMMARISING FEATURES

Context no.	Description	Interpretation
[100]	Road Surface	Modern Tarmac Surface
[103]	Human Remains	Burial No.1
[104]	Cut Feature	Grave Cut for Burial No.1
[107]	Human Remains	Burial No.2
[108]	Human Remains	Burial No.3
[120]	Limestone Feature	Possible Wall
[121]	Cobbled Surface	19th/20th Century Road Surface
[123]	Brick Feature	Modern Surface
[128]	Brick Feature	Modern Surface
[130]	Linear Cut Feature	Robber Trench
[134]	Linear Cut Feature	Robber Trench
[136]	Linear Cut Feature	Robber Trench
[137]	Linear Cut Feature	Construction Cut for Wall [120]
[138]	Linear Cut Feature	Modern Pipe Trench

CITY OF LINCOLN ARCHAEOLOGY UNIT		
SITE CODE: ZWB94		PLAN/ELEV/SECTION NO:
CLIENT: Simons Estates Ltd.		
DESCRIPTION:		Site Location
SCALE: 1:10000	ARCHIVE NO:	
DRAWN BY:	CHECKED:	DATE:
MUSEUM ACCESSION NO: 142.94		

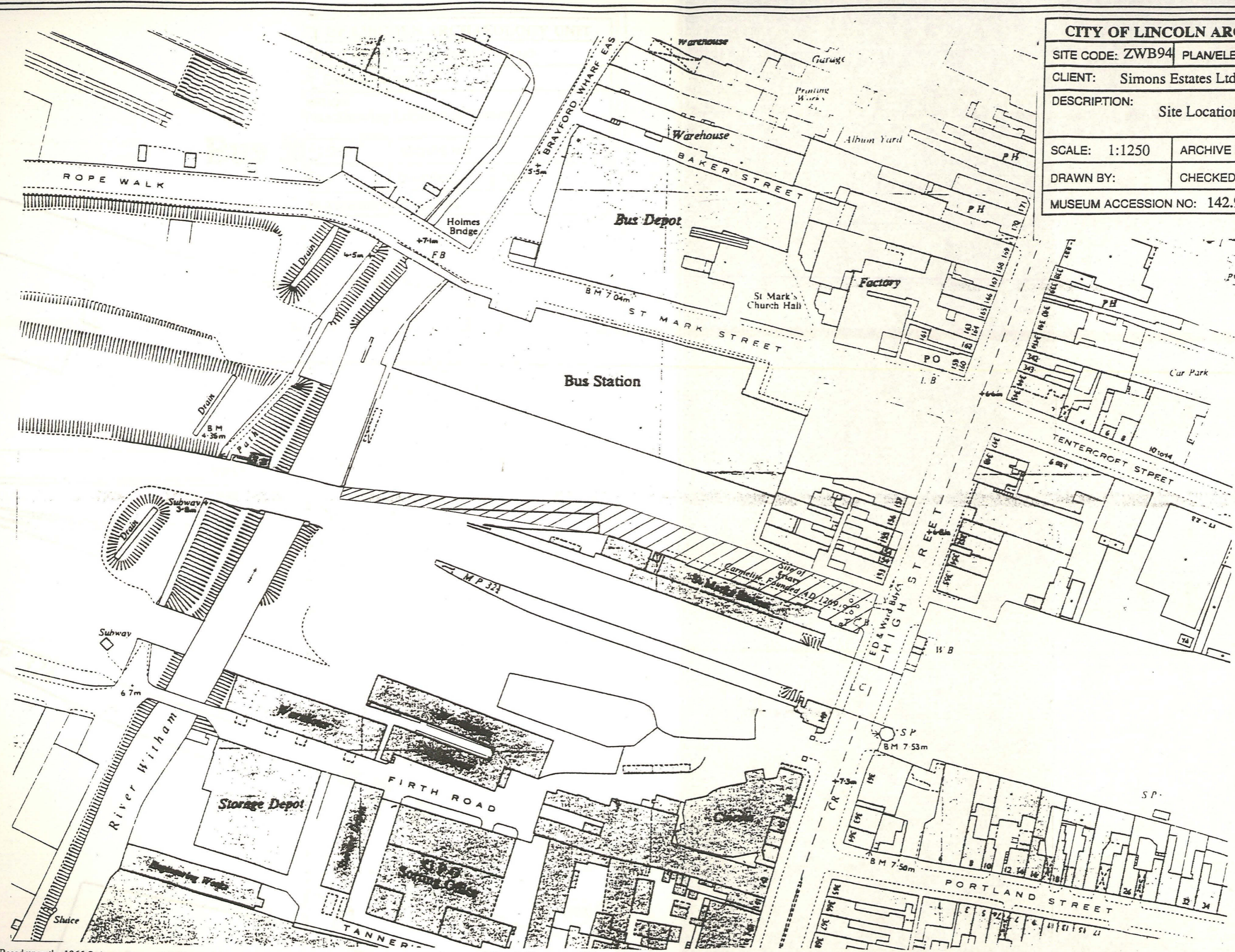


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Fig.1

CITY OF LINCOLN ARCHAEOLOGY UNIT

SITE CODE: ZWB94		PLANE/LEV/SECTION NO:	
CLIENT: Simons Estates Ltd.			
DESCRIPTION: Site Location			
SCALE: 1:1250		ARCHIVE NO:	
DRAWN BY:		CHECKED:	DATE:
MUSEUM ACCESSION NO: 142.94			







Based upon the 1966 Ordnance Survey 1:1250 map with the Permission of The Controller Of Her Majesty's Stationery Office. © Crown copyright.

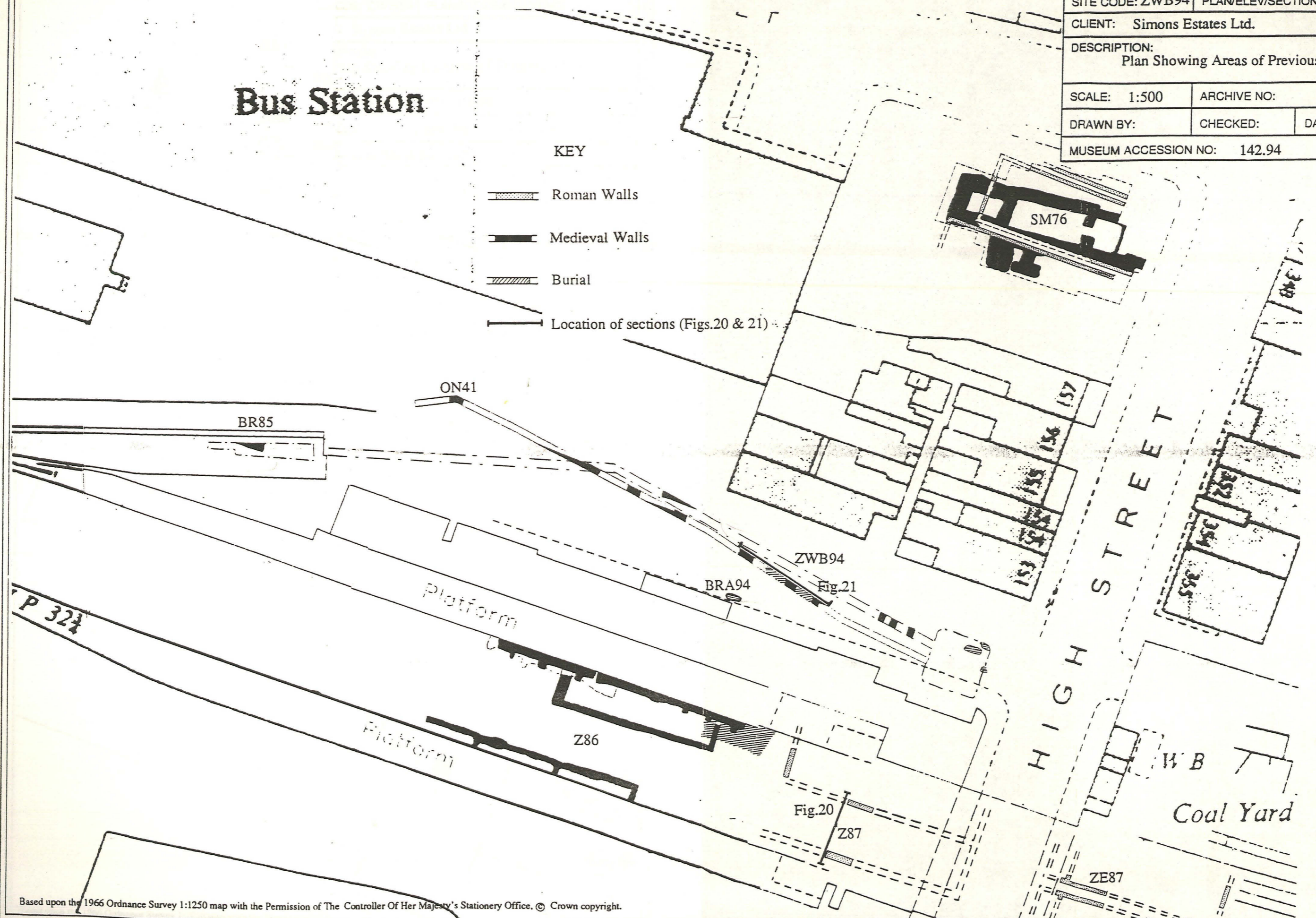
Fig.2

CITY OF LINCOLN ARCHAEOLOGY UNIT		
SITE CODE: ZWB94	PLANE/ELEV/SECTION NO:	
CLIENT: Simons Estates Ltd.		
DESCRIPTION: Plan Showing Areas of Previous Investigation		
SCALE: 1:500	ARCHIVE NO:	
DRAWN BY:	CHECKED:	DATE:
MUSEUM ACCESSION NO: 142.94		

Bus Station

KEY

-  Roman Walls
-  Medieval Walls
-  Burial
-  Location of sections (Figs.20 & 21)



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Fig.3

CITY OF LINCOLN ARCHAEOLOGY UNIT

SITE CODE: ZWB94 | PLANELEV/SECTION NO:

CLIENT: Simons Estates Ltd

DESCRIPTION:
Plan Showing Location of Features

SCALE: 1:200

ARCHIVE NO:





DRAWN BY:

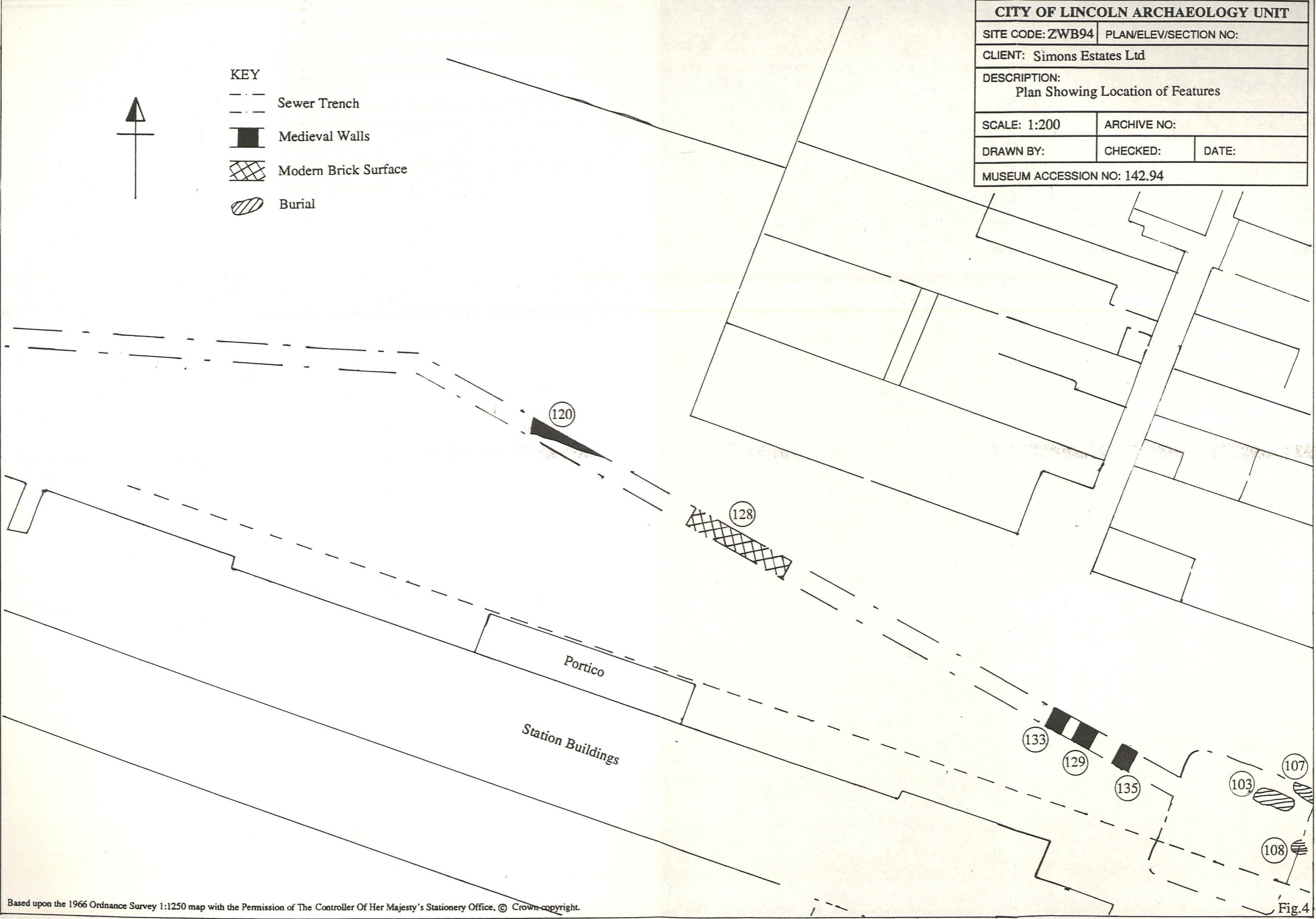
CHECKED:

DATE:

MUSEUM ACCESSION NO: 142.94

KEY

-  Sewer Trench
-  Medieval Walls
-  Modern Brick Surface
-  Burial



Based upon the 1966 Ordnance Survey 1:1250 map with the Permission of The Controller Of Her Majesty's Stationery Office. © Crown copyright.

Fig.4

CITY OF LINCOLN ARCHAEOLOGY UNIT

SITE CODE: ZWB94 PLANE/SECTION NO: 2

CLIENT: Simons Estates Ltd

DESCRIPTION:
S-Facing Section 2, 20m East of River Witham

SCALE: 1:10

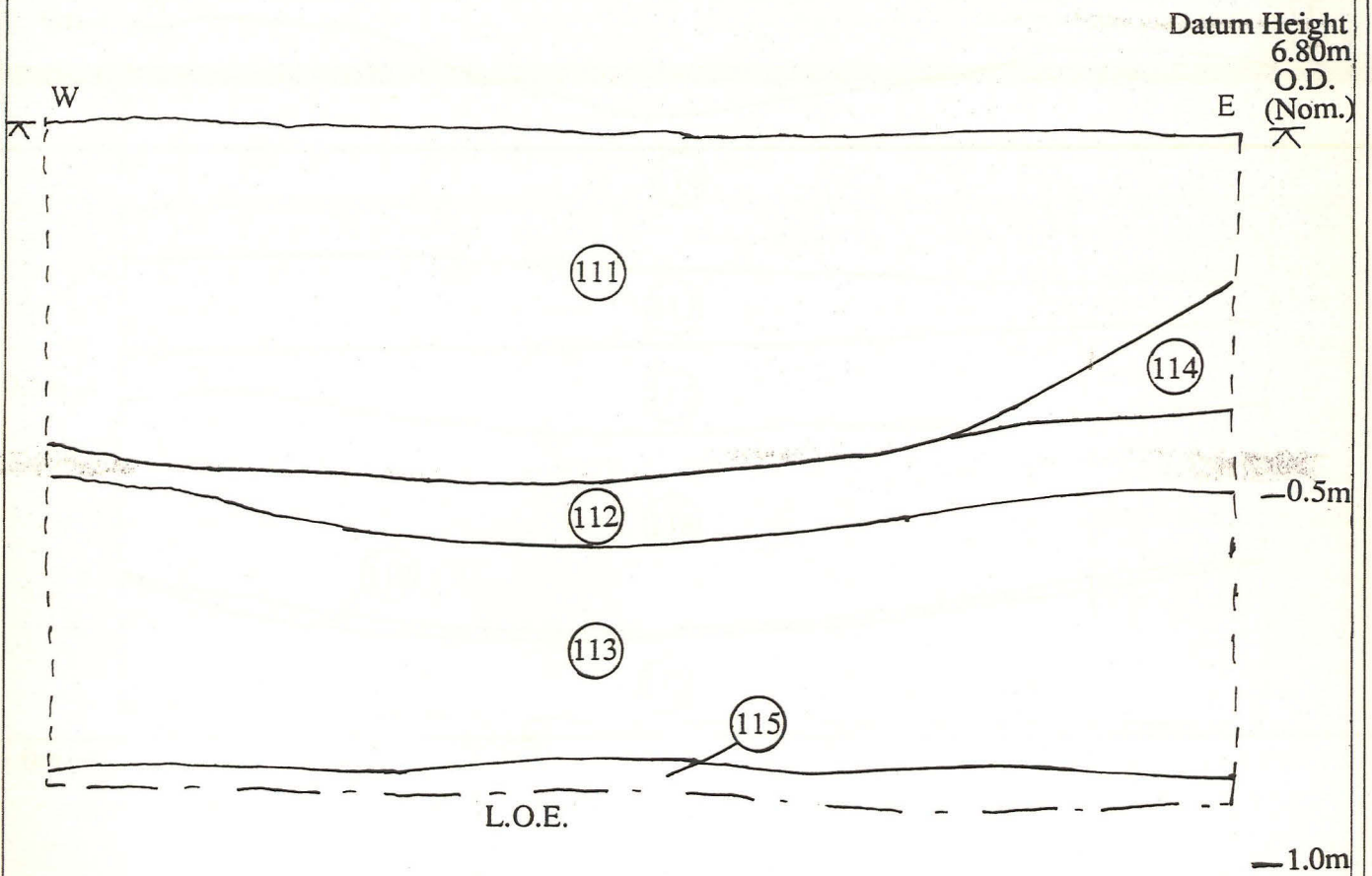
ARCHIVE NO:

DRAWN BY: K.W.

CHECKED:

DATE: 15/11/94

MUSEUM ACCESSION NO: 142.94



CITY OF LINCOLN ARCHAEOLOGY UNIT

SITE CODE: ZWB94

PLANE LEVEL SECTION NO: 3

CLIENT: Simons Estates Ltd

DESCRIPTION:

S-Facing Section 3, West End of Station Buildings

SCALE: 1:10

ARCHIVE NO:

DRAWN BY: K.W.

CHECKED:

DATE: 16/11/94

MUSEUM ACCESSION NO: 142.94

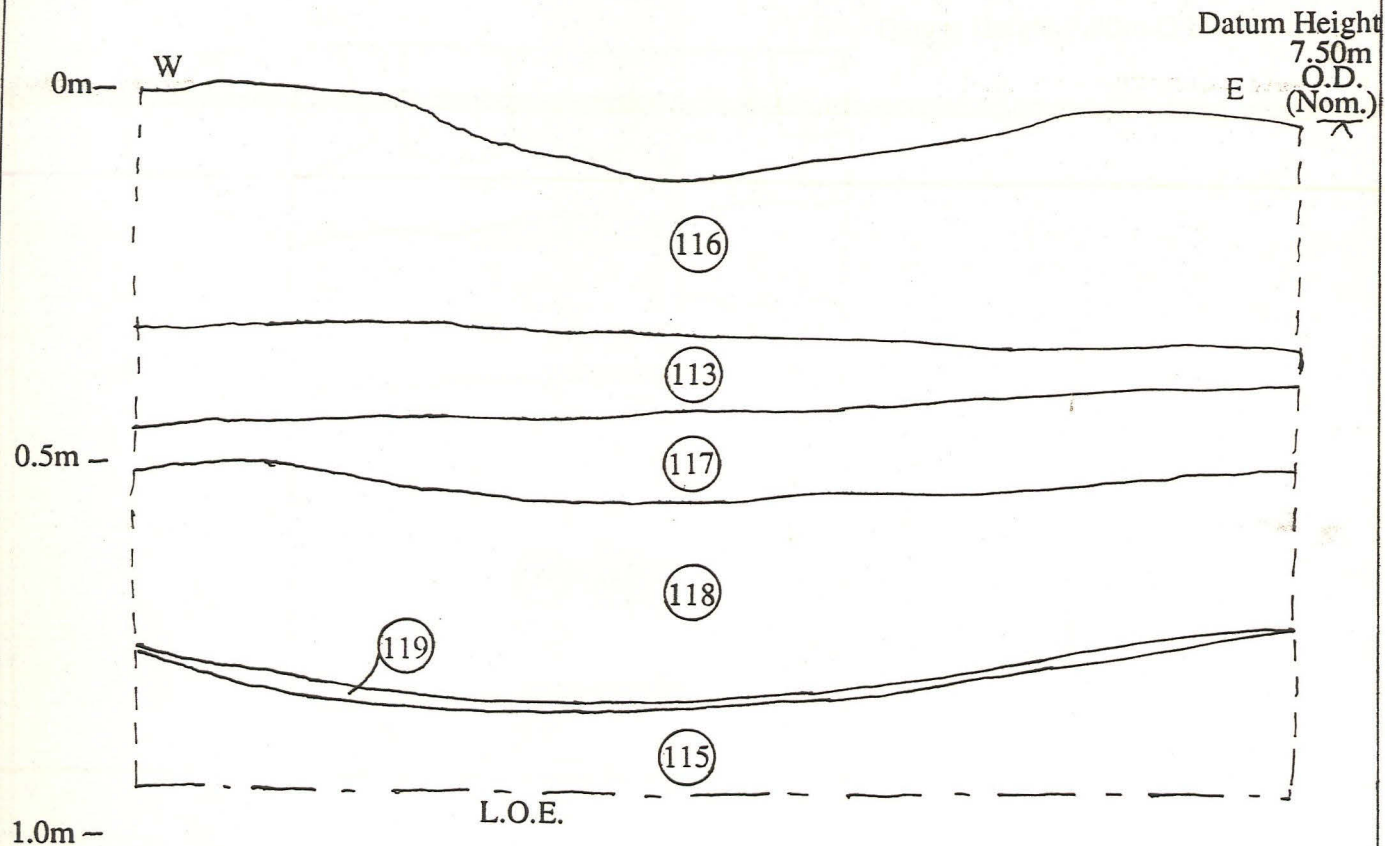


Fig.6

CITY OF LINCOLN ARCHAEOLOGY UNIT		
SITE CODE: ZWB94	PLANE/ELEV/SECTION NO: 4	
CLIENT: Simons Estates Ltd		
DESCRIPTION: S-Facing Section 4, Showing Possible Wall 120		
SCALE: 1:10	ARCHIVE NO:	
DRAWN BY: K.W.	CHECKED:	DATE: 16/11/94
MUSEUM ACCESSION NO: 142.94		

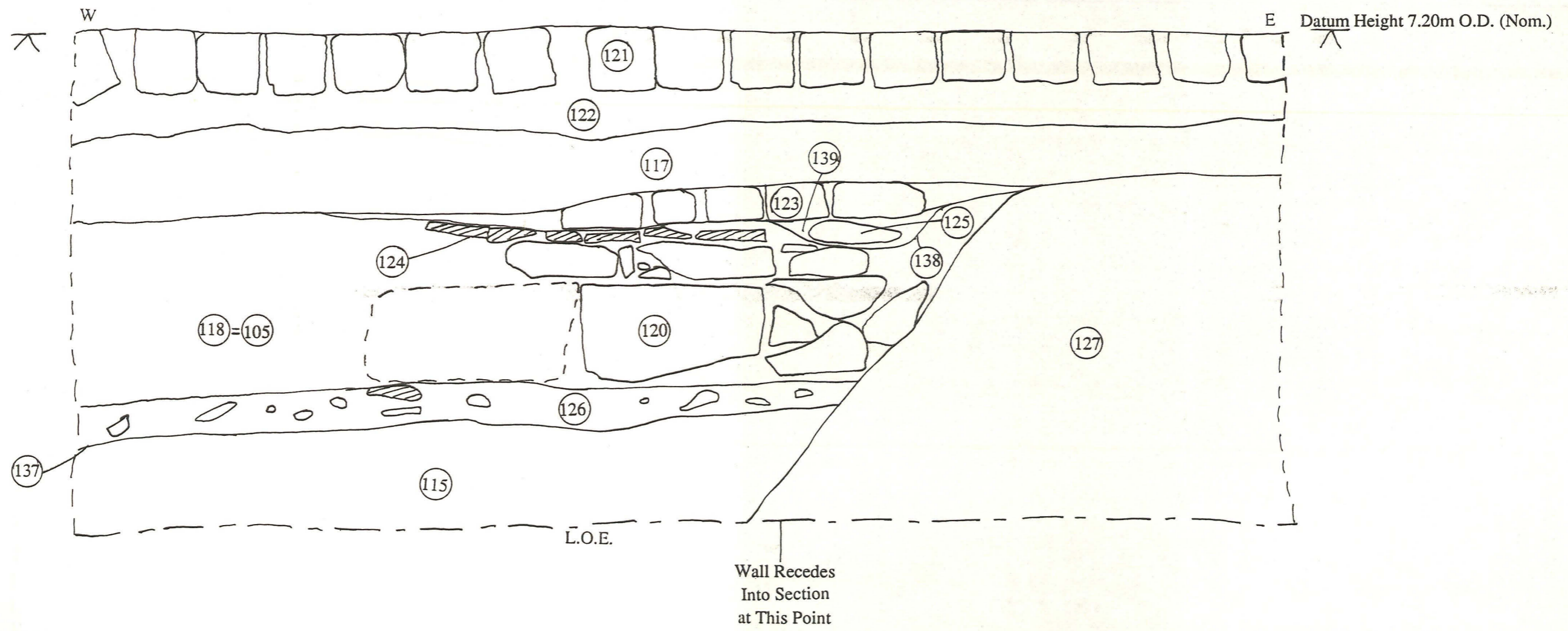


Fig.7

CITY OF LINCOLN ARCHAEOLOGY UNIT		
SITE CODE: ZWB94		PLANELEV/SECTION NO: 5
CLIENT: Simons Estates Ltd		
DESCRIPTION: S-Facing Section 5, Showing Possible Robber Trenches 129 / 130		
SCALE: 1:10	ARCHIVE NO:	
DRAWN BY: K.W.	CHECKED:	DATE: 16/11/94
MUSEUM ACCESSION NO: 142.94		

Datum Height
7.20m O.D.
(Nom.)


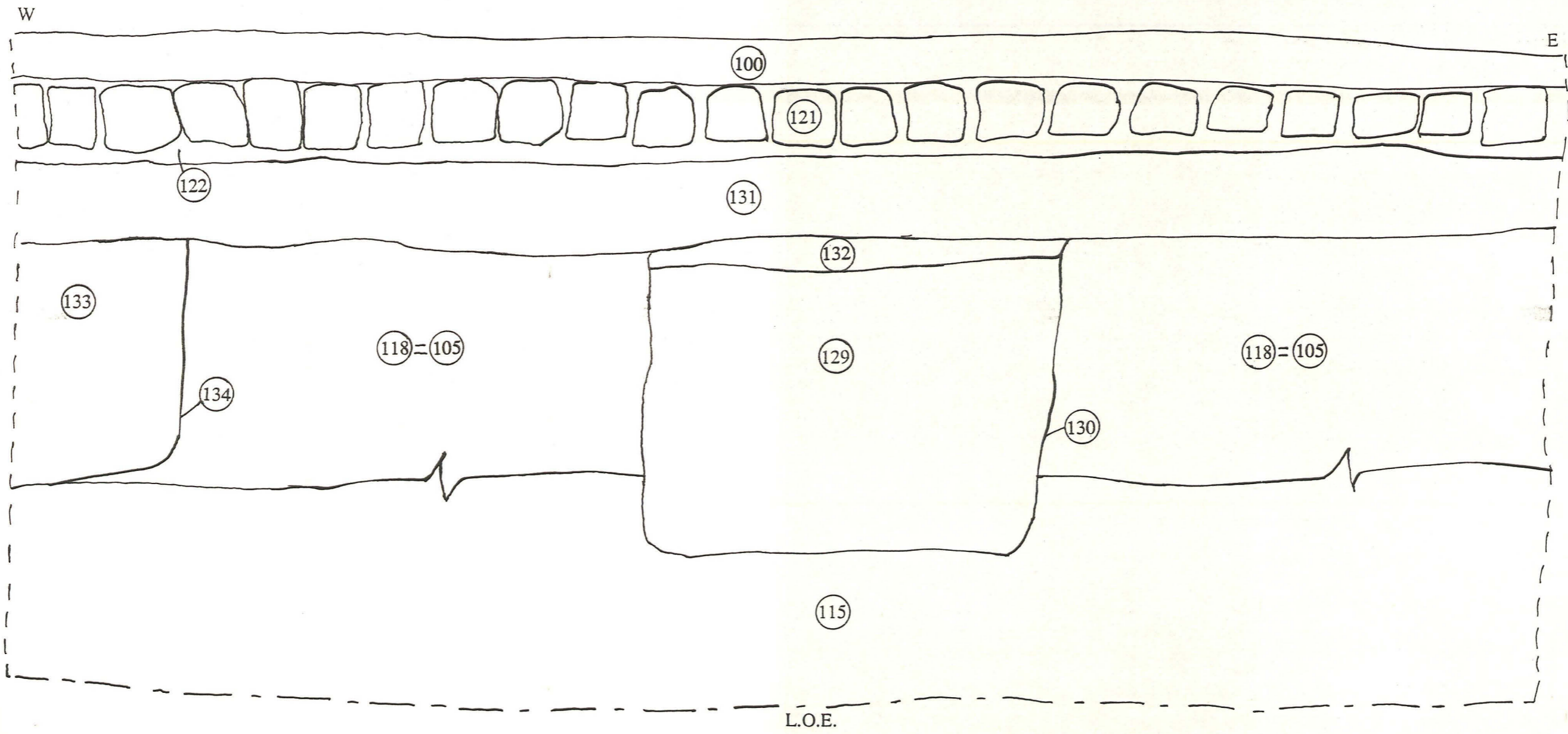



Fig.8

CITY OF LINCOLN ARCHAEOLOGY UNIT		
SITE CODE: ZWB94	PLAN/ELEV/SECTION NO: 1	
CLIENT: Simons Estates Ltd		
DESCRIPTION: S-Facing Section 1, East End of Site		
SCALE: 1:10	ARCHIVE NO:	
DRAWN BY: K.W.	CHECKED:	DATE: 03/10/94
MUSEUM ACCESSION NO: 142.94		

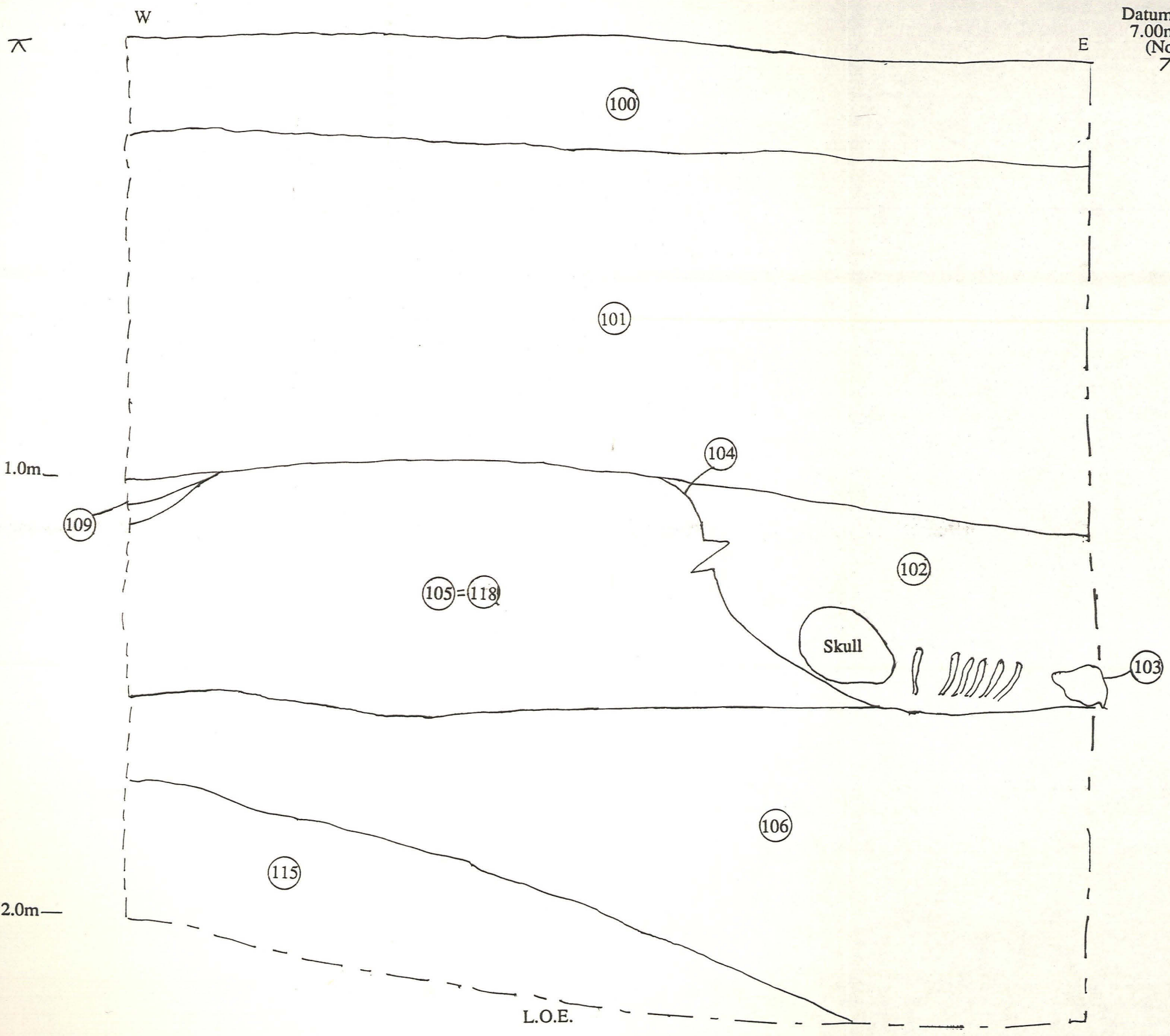


Fig.9