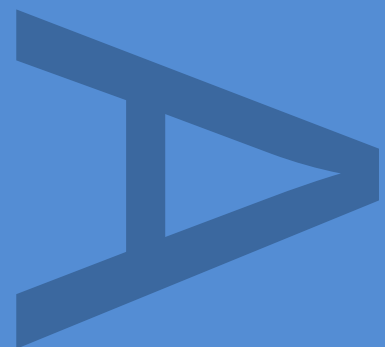


**PHOTOGRAPHIC SURVEY
OF STEWART'S GARAGES,
72 FARM LANE, FULHAM,
LONDON BOROUGH OF
HAMMERSMITH AND
FULHAM, SW6 1QA**

PCA REPORT NO. R11379

FEBRUARY 2013



PRE-CONSTRUCT ARCHAEOLOGY

Photographic Survey of Stewart's Garages, 72 Farm Lane, Fulham, London Borough of Hammersmith and Fulham, SW6 1QA

Central NGR: TQ 25433 77531

Planning Reference: 2011/03974/FUL

Commissioning Agent: CgMs Consulting

Compiled by: Kari Bower

Project Manager: Charlotte Matthews

Contractor: Pre-Construct Archaeology Limited
Unit 54 Brockley Cross Business Centre
96 Endwell Road
Brockley
London SE4 2PD

Tel: 020 7732 3925
Fax: 020 7732 7896
E-mail: cmatthews@pre-construct.com
Web: www.pre-construct.com

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February 2013

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DOCUMENT VERIFICATION

Site Name

Stewart's Garages, 72 Farm Lane, Fulham, London Borough of Hammersmith and Fulham, SW6 1QA

Type of project

Photographic Survey

Quality Control

Pre-Construct Archaeology Limited Project Code			K3017
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	Name & Title	Signature	Date
Text Prepared by:	Kari Bower		08.02.13
Graphics Prepared by:	Mark Roughley		08.02.13
Graphics Checked by:	Josephine Brown	<i>J. Brown</i>	08.02.13
Project Manager Sign-off:	Charlotte Matthews	<i>Charlotte Matthews</i>	08.02.13

Revision No.	Date	Checked	Approved

Pre-Construct Archaeology Ltd
Unit 54
Brockley Cross Business Centre
96 Endwell Road
London
SE4 2PD

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1 NON-TECHNICAL SUMMARY

- 1.1 Pre-Construct Archaeology Limited was commissioned by CgMs Consulting on behalf of their Client to carry out a photographic survey of Stewarts Garages, 72 Farm Lane, Fulham, London Borough of Hammersmith and Fulham, SW6 1QA, centred on OS NGR TQ 25433 77531 (hereafter referred to as the Site). The photographic survey of the building was required by the Local Planning Authority in order to fulfil a condition of planning consent for its demolition and the redevelopment of the Site.
- 1.2 The building on the Site (currently known as Stewart's Garages) is not listed nor does it lie within a Conservation Area, although it lies adjacent to Walham Grove Conservation Area to the west and Sedlescombe Road Conservation Area to the north.
- 1.3 The Site is located in the London Borough of Hammersmith and Fulham to the north of Fulham Broadway. It is bounded by Farm Lane to the west, by property boundaries to the north, by Seagrave Road to the east and by the London Oratory School to the south. The photographic survey of the Site was carried out on 28th and 29th January 2013 with high quality digital images and 35mm black and white film.
- 1.4 An archaeological desk-based assessment (CgMs Consulting 2011) shows that the site was occupied by market gardens in 1869-74. In 1880, the extant building (currently known as Stewart's Garages) was built by the London Road Car Company as their depot. The London Road Car Company was one of the largest proprietors of horse-drawn buses in London and their building was the largest of the horse stables in Farm Lane. The building was also one of the largest and finest in London and contained two-storey stables that were ranged around a quadrangle, where 700 horses lived in about 1890. The London Road Car Company occupied this extensive stable building for horse drawn cabs and omnibuses until 1905.
- 1.5 The London General Omnibus Company (LGOC) took over the premises in 1909, and left in 1923. The LGOC was the principal bus operator in London between 1855 and 1933. It was founded in 1855 to amalgamate and regulate the many independent horse-drawn omnibus services then operating in London. The LGOC soon became the largest omnibus operator in London. It bought out hundreds of independently-owned buses and established a consistent level of service for its fleet. The LGOC began using motor omnibuses in 1902, and the last LGOC horse-drawn bus ran on 25 October 1911. In 1908 the LGOC bought the Road Car Company, the Vanguard Company, and all its other main rivals, achieving an almost complete monopoly in London and owning a total of 994 motor buses.
- 1.6 The 1946 Bomb Damage Map shows the building on the Site suffered minor blast damage, probably as a result of a V1 Flying Bomb which landed to the east of the Site during 1944. The 1971 Ordnance Survey map shows the building was used as a 'Catering Depot'. At the time of the photographic survey the building was being used for storage, workshops and a garage.

2 INTRODUCTION

2.1 Background

2.1.1 Pre-Construct Archaeology Limited was commissioned by CgMs Consulting on behalf of their Client to carry out a photographic survey of Stewarts Garages, 72 Farm Lane, Fulham, London Borough of Hammersmith and Fulham, SW6 1QA, centred on Ordnance Survey National Grid Reference TQ 25433 77531 (hereafter referred to as the Site; **Figure 1**). The photographic survey was required by the Local Planning Authority as part of a condition of planning consent for the redevelopment of the Site.

2.1.2 The building on the Site (currently known as Stewart's Garages) is not listed nor does it lie within a Conservation Area, although it lies adjacent to Walham Grove Conservation Area to the west and Sedlescombe Road Conservation Area to the north.

2.1.3 Planning permission (Application Number 2011/03974/FUL) has been granted for the 'demolition of the existing building and redevelopment of the site by the erection of a part 2, part 3, part 4, part 5, and part 6 storey, plus lower ground floor and two storey basement building, comprising of 107 self-contained apartments, together with associated on-site gym facilities at lower ground floor level, 122 car parking spaces at basement levels (accessed off Seagrave Road) and landscaping'. Condition 5 of the consent states:

'No demolition or development shall take place until a photographic record has been made of all the existing buildings on the site, and the record has been submitted to and approved in writing by the council. The photographic record should cover the interior and exterior of the buildings and relate the photographs to floors plans of each level, and a copy of the approved photographic record shall be lodged with the Borough archive.

To ensure that a proper record is made of the buildings prior to their demolition and so that this information is made available to the appropriate statutory bodies, in accordance with Policy EN2B and EN8 of the Unitary Development Plan and 2011, and Policy BE1 of the Core Strategy 2011.'

2.1.4 This document forms the photographic survey as required by Condition 5 (above). The purpose of the photographic survey was to photographically record the building prior to demolition.

2.2 Site Location

2.2.1 The Site is located in the London Borough of Hammersmith and Fulham to the north of Fulham Broadway. It is bounded by Farm Lane to the west, by property boundaries to the north, by Seagrave Road to the east and by the London Oratory School to the south (**Figure 2**).

3 METHODOLOGY

3.1 Aims and Objectives

- 3.1.1 The aim of the photographic survey was to ensure that a record was made of the building prior to its demolition and to make this information available to the appropriate statutory bodies.

3.2 On-Site Photographic Survey

- 3.2.1 The photographic survey was carried out on 28th and 29th January 2013. Digital and black and white film photographs (35mm) were taken of the building's exteriors and interiors, where access was obtained. A selection of the photographs has been included in this report (**Plates 1 to 43**).

4 HISTORICAL BACKGROUND

4.1 Introduction

- 4.1.1 The following historical background is summarised/taken directly from the archaeological desk-based assessment for the site (CgMs Consulting 2011) and from the Built Heritage Assessment of a nearby site (KMHeritage 2011).

4.2 The development of the area

- 4.2.1 Until the mid-1850s, this part of London was largely fields and market gardens. The settlement at Walham Green just to the south (now known as Fulham Broadway) developed at a major road junction. The building of St John's Church in 1828 and the associated schools in 1836 were the start of development at Walham Green, and by the end of the nineteenth century it was a crowded working class area. A new station opened at Walham Green in 1880 and further development quickly followed. Farm Lane became a centre of the emerging horse bus industry, with at least five sites occupied by stables by 1900. As an area that was still relatively 'rural' in the 1870s, it became an ideal location for the construction of the large stables needed to house the many horses and omnibuses of the horse-bus companies.

4.3 History of the Site

- 4.3.1 The Site was occupied by market gardens in 1869-74. In 1880, the extant building (currently known as Stewart's Garages) was built by the London Road Car Company as their depot. The London Road Car Company was one of the largest proprietors of horse-drawn buses in London and their building was the largest of the horse stables in Farm Lane. The building was also one of the largest and finest in London and contained two-storey stables that were ranged around a quadrangle, where 700 horses lived in about 1890. The London Road Car Company occupied this extensive stable building for horse drawn cabs and omnibuses until 1905.
- 4.3.2 The London General Omnibus Company (LGOC) took over the premises in 1909, and left in 1923. The LGOC was the principal bus operator in London between 1855 and 1933. It was founded in 1855 to amalgamate and regulate the many independent horse-drawn omnibus services then operating in London. The LGOC soon became the largest omnibus operator in London. It bought out hundreds of independently-owned buses and established a consistent level of service for its fleet. The LGOC began using motor omnibuses in 1902, and the last LGOC horse-drawn bus ran on 25 October 1911. In 1908 the LGOC bought the Road Car Company, the Vanguard Company, and all its other main rivals, achieving an almost complete monopoly in London and owning a total of 994 motor buses.
- 4.3.3 The 1946 Bomb Damage Map shows the building on the Site suffered minor blast damage, probably as a result of a V1 Flying Bomb which landed to the east of the Site during 1944. The 1971 Ordnance Survey map shows the building was used as a 'Catering Depot'. At the time of the photographic survey the building was being used for storage, workshops and a garage.

5 BIBLIOGRAPHY

Secondary Sources

KM Heritage 2011 *101 Farm Lane, Fulham, SW6 1QJ: Built Heritage Assessment*

Unpublished Report

CgMs Consulting 2012 *Updated Archaeological Desk Based Assessment of Land at*

Seagrave Road, Fulham, London Unpublished Report

Online Resources

The Victorian Dictionary of London online at: <http://www.victorianlondon.org/publications6/horse-01.htm>

6 ACKNOWLEDGEMENTS

- 6.1 Pre-Construct Archaeology Limited would like to thank CgMs Consulting on behalf of their Client for commissioning the Photographic Survey. Lorraine Mayo of CgMs Consulting is thanked for her help and assistance.
- 6.2 The project was managed for Pre-Construct Archaeology Limited by Charlotte Matthews. Kari Bower compiled this report and Mark Roughley prepared the illustrations.

7 OASIS FORM

OASIS ID: preconst1-143277

Project details

Project name	Stewart's Garages, 72 Farm Lane, Fulham Photographic Survey
Short description of the project	<p>Pre-Construct Archaeology Limited was commissioned by CgMs Consulting on behalf of their Client to carry out a photographic survey of Stewarts Garages, 72 Farm Lane, Fulham, London Borough of Hammersmith and Fulham, centred on OS NGR TQ 25433 77531. The photographic survey of the building was required by the Local Planning Authority to fulfil a condition of planning consent for its demolition and the redevelopment of the site. The building is not listed nor does it lie within a Conservation Area. The photographic survey was carried out in January 2013. The site was occupied by market gardens in 1869-74. The extant building was built in 1880 by the London Road Car Company as their depot. The Company was one of the largest proprietors of horse-drawn buses in London. The building was also one of the largest and finest horse stables in London and contained two-storey stables that were ranged around a quadrangle, where 700 horses lived in about 1890. The London General Omnibus Company (LGOC) took over the premises in 1909, and left in 1923. The 1946 Bomb Damage Map shows the building on the Site suffered minor blast damage, probably as a result of a V1 Flying Bomb which landed to the east of the site during 1944. The 1971 Ordnance Survey map shows the building was used as a 'Catering Depot'. At the time of the photographic survey the building was being used for storage, workshops and a garage.</p>
Project dates	Start: 28-01-2013 End: 29-01-2013
Previous/future work	No / No
Any associated project reference codes	K3017 - Sitecode
Type of project	Building Recording
Site status	None
Current Land use	Industry and Commerce 4 - Storage and warehousing
Monument type	MULTISTOREY STABLE Post Medieval

Significant Finds	NONE None
Methods & techniques	"Photographic Survey"
Prompt	Planning condition

Project location

Country	England
Site location	GREATER LONDON HAMMERSMITH AND FULHAM FULHAM Stewart's Garages
Postcode	SW6 1QA
Study area	0 Square metres
Site coordinates	TQ 25433 77531 51 0 51 28 56 N 000 11 36 W Point

Project creators

Name of Organisation	Pre-Construct Archaeology Limited
Project brief originator	Conservation Officer London Borough of Hammersmith and Fulham
Project design originator	Charlotte Matthews
Project director/manager	Charlotte Matthews
Project supervisor	Kari Bower
Type of sponsor/funding body	Developer
Name of sponsor/funding body	CgMs Consulting on behalf of their Client

Project archives

Physical Archive Exists?	No
Digital Archive recipient	Hammersmith and Fulham Archives and Local History Centre
Digital Archive ID	K3017
Digital Contents	"none"
Digital Media available	"Images raster / digital photography"
Paper Archive recipient	Hammersmith and Fulham Archives and Local History Centre
Paper Archive ID	K3017
Paper Contents	"none"
Paper Media available	"Photograph", "Report"

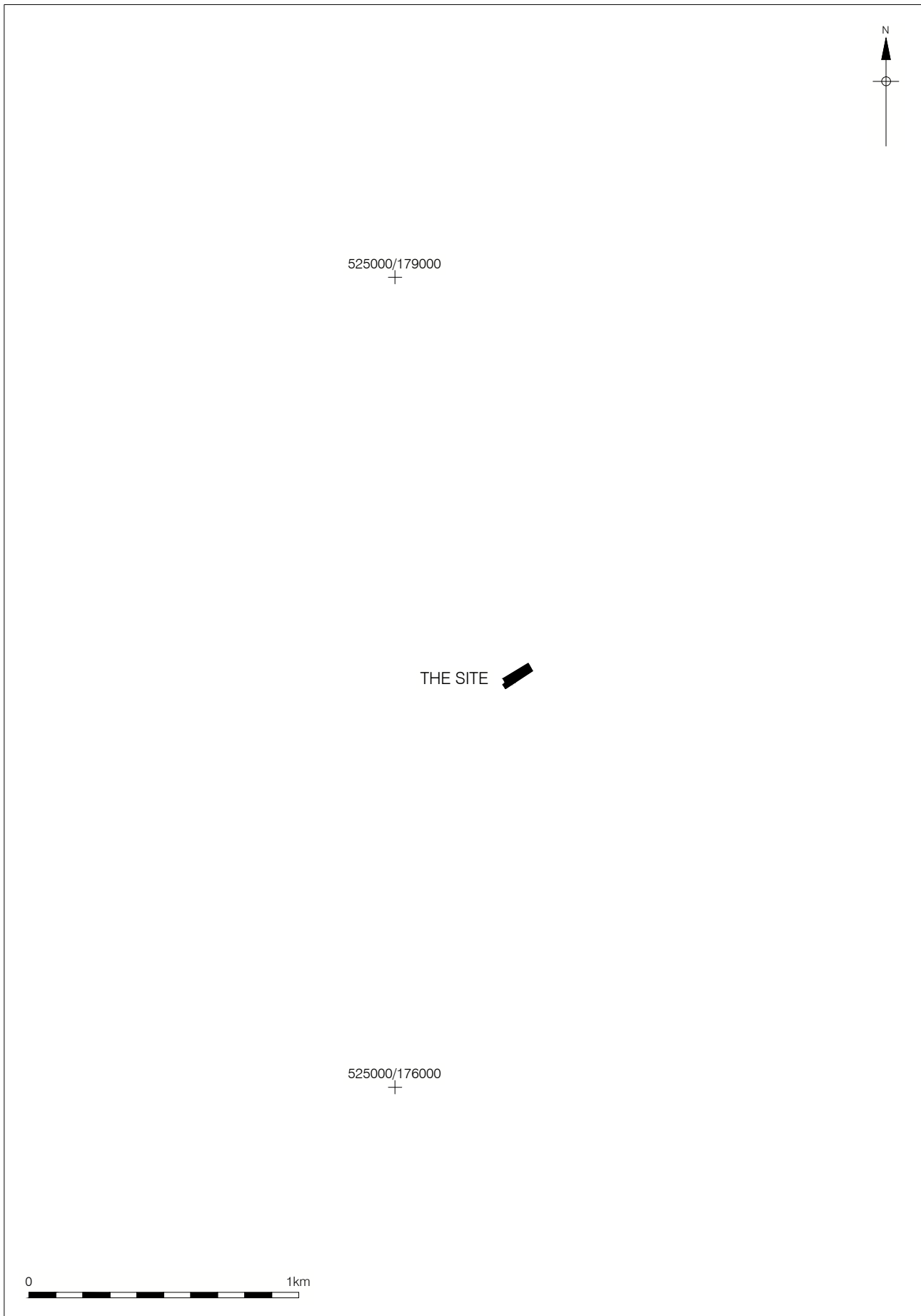
Project

bibliography 1

Publication type	Grey literature (unpublished document/manuscript)
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06/02/13 MR

Figure 1
Site Location
1:20,000 at A4





Plates



Plate 1: South-west facade with entrance from Farm Lane, looking north-east (72FmLa D100 001)



Plate 2: South-west facade with entrance (left) from Farm Lane, looking south-east (72FmLa D100 002)



Plate 3: South-west facade with entrance from Farm Lane, looking north (72FmLa D100 003)



Plate 4: Terracotta embossed plaque with fruit design on south-west facade, looking north-east (72FmLa D100 030)



Plate 5: Long north-west elevation (right) and adjacent alleyway, looking north-east (72FmLa D100 04)



Plate 6: Long north-west elevation (left) and adjacent alleyway, looking south-west (72FmLa D100 05)



Plate 7: Central part of long north-west elevation, looking south (72FmLa D100 06)



Plate 8: North-east end of long north-west elevation opposite private rear gardens, looking east
(72FmLa D100 07a)



Plate 9: North-west (right) and north-east (left) elevations and Seagrave Road, looking south-west (72FmLa D100 008a)



Plate 10: North-east elevation and Seagrave Road, looking west (72FmLa D100 009)



Plate 11: Inserted entrance in north-east elevation from Seagrave Road, looking south-west (72FmLa D100 010)



Plate 12: Internal view of the entrance from Seagrave Road in the north-east elevation, looking north-east (72FmLa D100 012)



Plate 13: Internal view of blocked transom windows in the north-east elevation, looking north-east (72FmLa D100 017)

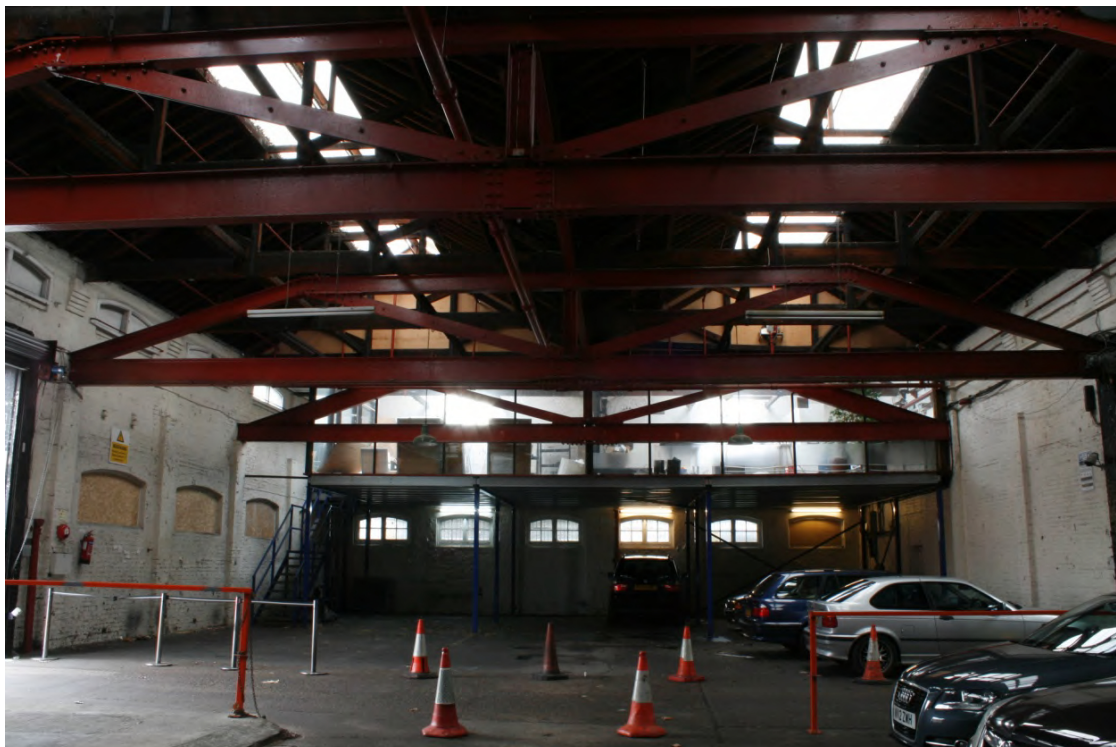


Plate 14: Internal view of north-east elevation (left) and south-east elevation (centre) and mezzanine, looking south-east (72FmLa D100 014)

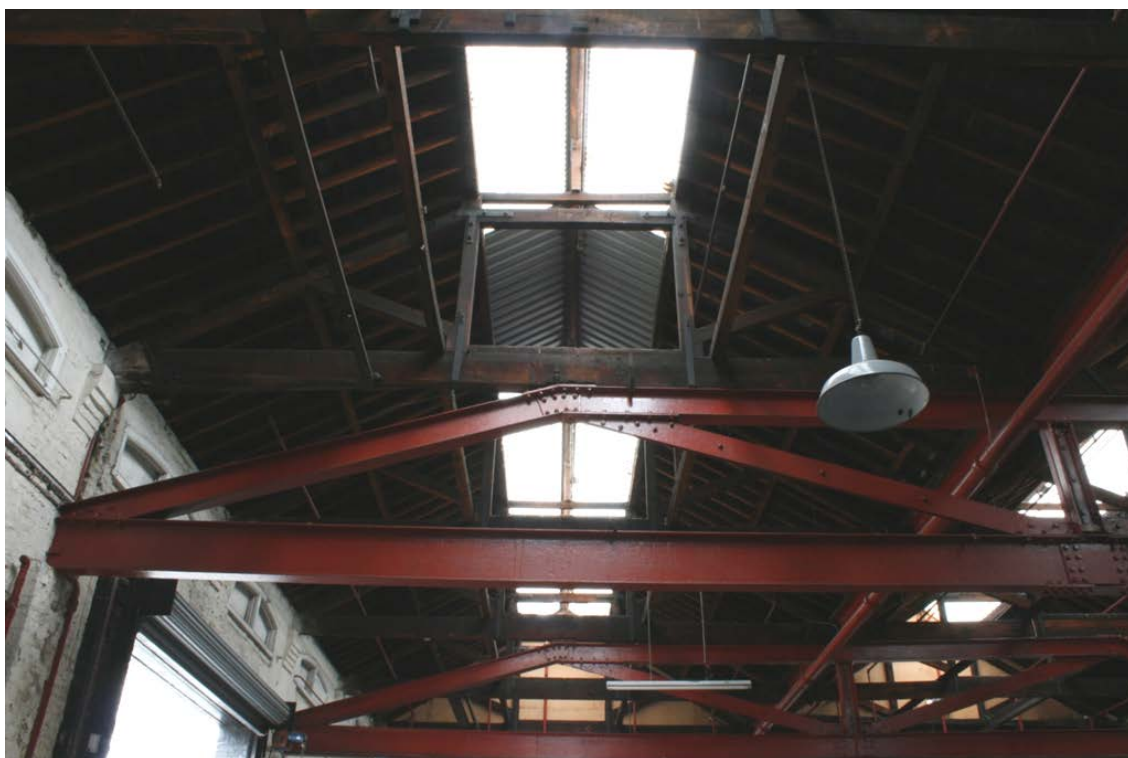


Plate 15: Internal view of north-east elevation (left) and roof trusses, looking south-east and up (72FmLa D100 016)

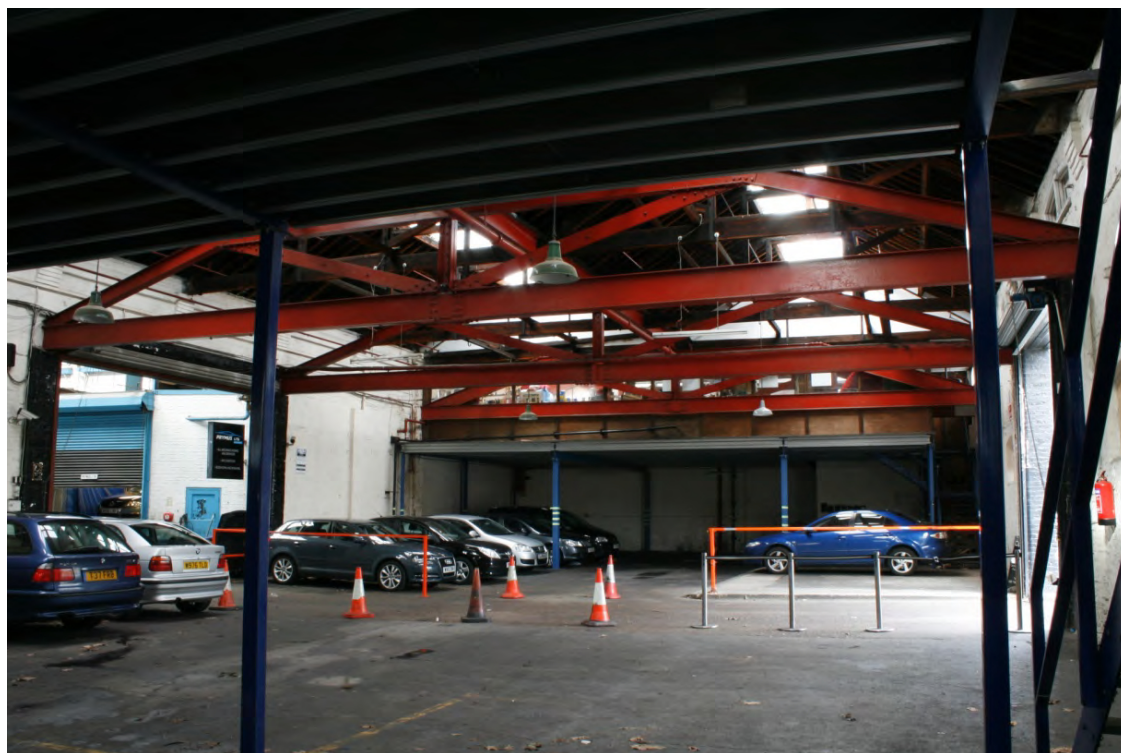


Plate 16: Internal view of the parking area and mezzanine at the north-east end of the building, looking west (72FmLa D100 013)



Plate 17: Entrance into main workshop area of garage from the north-east end of the building, looking south-west (72FmLa D100 020)



Plate 18: Main workshop area of the garage, looking south-west (72FmLa D100 021)



Plate 19: Main workshop area of the garage and Seagrave Road entrance (beyond), looking north-east (72FmLa D100 025)



Plate 20: Skylights in the main workshop area of the garage, looking north-west (72FmLa D100 026)



Plate 21: Passageway entrance from the main workshop area of the garage to the parking area beyond, looking south-west (72FmLa D100 023)



Plate 22: Passageway from the main workshop area of the garage to the parking area beyond showing blocked windows, overhead bridge and winching mechanisms, looking west (72FmLa D100 052)



Plate 23: Overhead bridge and winching mechanisms within passageway from the main workshop area of the garage to the parking area beyond, looking south-west (72FmLa D100 053)



Plate 24: Overhead bridge within passageway, looking north (72FmLa D100 051)



Plate 25: Passageway entrance leading from the parking area at the south-west end of the building to the main workshop area beyond, looking north (72FmLa D100 055)



Plate 26: Passageway entrance from the parking area at the south-west end of the building to the main workshop area beyond, looking south-east (72FmLa D100 054)



Plate 27: Parking area at the south-west end of the building, looking north-east (72FmLa D100 031a)



Plate 28: Original window opening (right) and later inserted door (centre) and ground floor window (left) on the north side of building, looking north (72FmLa D100 032)



Plate 29: Inserted ground floor windows and blocked double doorway on the north side of building, looking north-west (72FmLa D100 033)



Plate 30: Inserted ground floor windows, original doorway (since infilled with brickwork), inserted double doorways (one blocked) on the north side of the building, looking north-west (72FmLa D100 034)



Plate 31: New workshops on the south side of the building, looking east (72FmLa D100 037)



Plate 32: New workshops on the south side of the building, looking south-west (72FmLa D100 038)



Plate 33: Internal view of the passageway entrance from Farm Lane into the building, looking south-west (72FmLa D100 041)



ROAD CAR COMPANY: FARM LANE YARD

Plate 33a: Late 19th century view (the same view as Plate 33) of the London Road Car Company depot showing the passageway entrance from Farm Lane into the building, looking south-west (<http://www.victorianlondon.org/publications6/horse-01.htm>)



Plate 34: Internal view of the passageway entrance from Farm Lane into the building, looking south-west (72FmLa D100 057)



Plate 35: Internal view of the passageway entrance from Farm Lane into the building, looking north-west (72FmLa D100 056)



Plate 36: Double door with cast iron strap hinges stamped Charles Collinge of Lambeth, looking south (72FmLa D100 043)



Plate 37: Ground floor access corridor within storage area on the north side of the building, looking south-west (72FmLa D100 044)



Plate 38: Ground floor access corridor within the storage area on the north side of the building, looking north-east (72FmLa D100 049)



Plate 39: Ground floor access corridor within the storage area on the north side of building showing added late 20th century plywood partition walls and doorway, looking north-east (72FmLa D100 045)



Plate 40: Ground floor access corridor within the storage area on the north side of building showing added late 20th century plywood partition walls and two doorways, looking east (72FmLa D100 046)

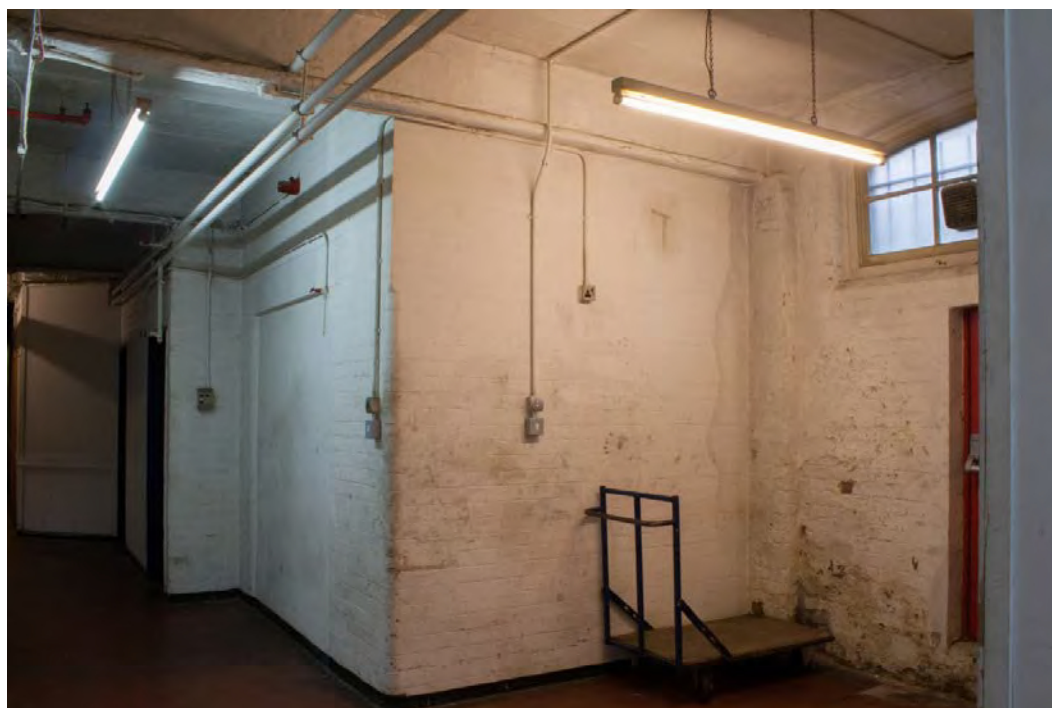


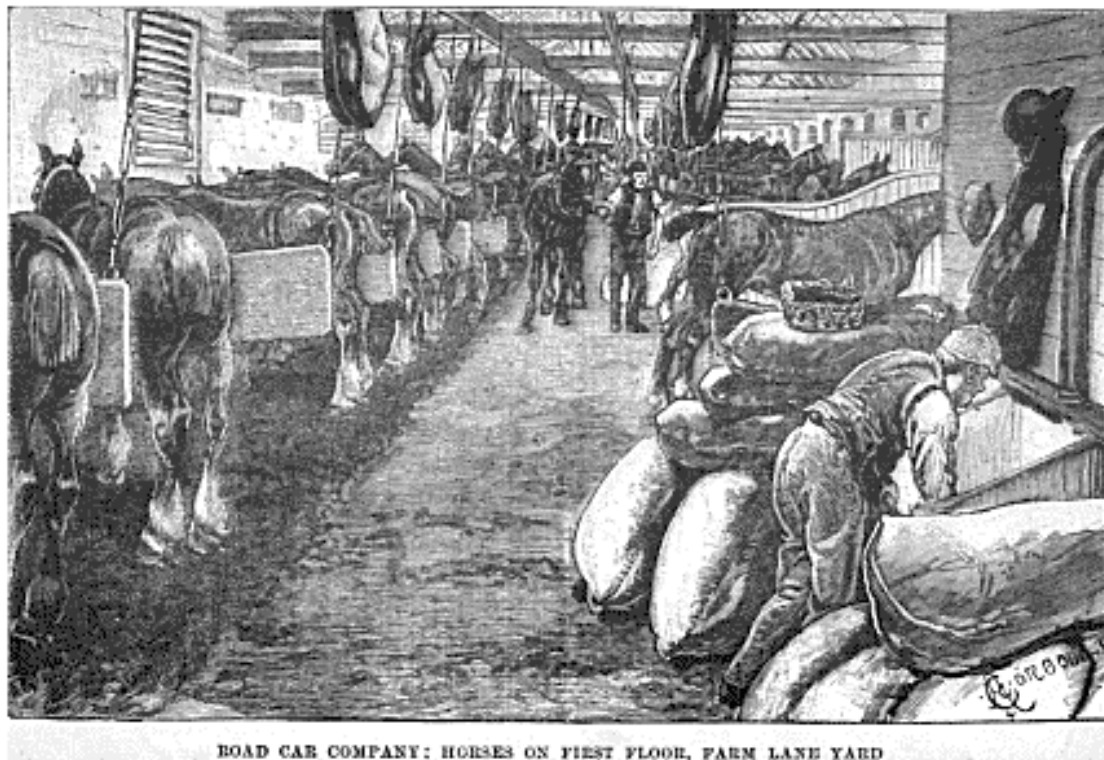
Plate 41: Ground floor access corridor within of the storage area on the north side of the building, showing 20th century brick partition walls and blocked double doorway, looking west (72FmLa D100 045)



Plate 42: Entrance into the first floor storage area on the north side of the building, showing roof structure and 20th century brick partition walls, looking south (72FmLa D100 058)



Plate 43: First floor access corridor along storage area on the north side of the building, showing original Queen truss, looking north-east (72FmLa D100 059)



ROAD CAR COMPANY: HORSES ON FIRST FLOOR, FARM LANE YARD

Plate 43a: Late 19th century view (similar to Plate 43) of the first floor stables of the London Road Car Company depot at Farm Lane, Fulham (<http://www.victorianlondon.org/publications6/horse-01.htm>)



Plate 44: Original Queen truss above the first floor storage area on the north side of the building, looking east (72FmLa D100 060)



Plate 45: First floor access corridor along the storage area on the north side of building with original timber roof trusses overhead, looking north-east (72FmLa D100 041)



Plate 46: Original cobbling within a small storage space area on the first floor, looking north (72FmLa D100 062a)

PCA

PCA SOUTH

UNIT 54
BROCKLEY CROSS BUSINESS CENTRE
96 ENDWELL ROAD
BROCKLEY
LONDON SE4 2PD
TEL: 020 7732 3925 / 020 7639 9091
FAX: 020 7639 9588
EMAIL: info@pre-construct.com

PCA NORTH

UNIT 19A
TURSDALE BUSINESS PARK
DURHAM DH6 5PG
TEL: 0191 377 1111
FAX: 0191 377 0101
EMAIL: info.north@pre-construct.com

PCA CENTRAL

7 GRANTA TERRACE
STAPLEFORD
CAMBRIDGESHIRE CB22 5DL
TEL: 01223 845 522
FAX: 01223 845 522
EMAIL: info.central@pre-construct.com

PCA WEST

BLOCK 4
CHILCOMB HOUSE
CHILCOMB LANE
WINCHESTER
HAMPSHIRE SO23 8RB
TEL: 01962 849 549
EMAIL: info.west@pre-construct.com

PCA MIDLANDS

17-19 KETTERING RD
LITTLE BOWDEN
MARKET HARBOROUGH
LEICESTERSHIRE LE16 8AN
TEL: 01858 468 333
EMAIL: info.midlands@pre-construct.com

