



HISTORIC BUILDING
RECORDING

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ONE STRAWBERRY PLACE
NEWCASTLE UPON TYNE

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Grid Ref NZ 2450 6450

ONE STRAWBERRY PLACE, NEWCASTLE

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Summary

Northern Archaeological Associates (NAA) was commissioned by BAM to undertake a Historic England Level 2 historic building survey at One Strawberry Place, Newcastle upon Tyne (NZ 2450 6450). This work was performed prior to the planned development of office buildings on the site.

Settlement in Newcastle is known from the Roman period with further development in the medieval period. Growth continued throughout the post-medieval period, extending outside the medieval city walls in what is now the St James area. However, more major, industrial development occurred in the area during the 19th and 20th centuries, when the St James area saw the construction of tenement houses and industrial sites.

The area that now forms the Strawberry Place Car Park was first the site of gardens to the rear of Strawberry Place and Gallowgate. By the mid-19th century, the area was occupied by a sawmill with shop fronts to the west and houses to the north. The sawmill was later developed into a large furniture depot with attached stables. To the south-west were the Newcastle City Corporation public baths and wash houses. All these buildings were demolished in the late 20th century to accommodate the car park currently in use.

A Historic England Level 2 historic building survey was carried out on 5th August 2020 on the retaining walls around the car park. A photogrammetric survey was conducted to create a measured photomontage from Structure from Motion (SfM) software. Each distinctive feature of the walls was then assigned an individual number (context number).

The walls of the car park at Strawberry Place are vestiges of the buildings that formerly occupied the site and can be matched to the changing layout of the area depicted in historic maps. The walls show numerous scars, indicating several episodes of alteration including the dismantling of internal walls. The wall that supported the building plots to the north remains along with those of the buildings to the west. Only the east wall of the furniture depot remains intact, also showing alteration and the dismantling of internal walls. Only the north-east corner of the baths and wash houses remains, with the internal glazed bricks still visible.

The layout and fabric of the walls have low-moderate significance in their capacity to demonstrate the evolution in layout and use of the area. However, there are questions that cannot be answered by the building survey alone and it is recommended that an archaeological watching brief be held during the intended conversion of the site.

1.0 INTRODUCTION

- 1.1 Northern Archaeological Associates was commissioned by BAM to undertake a historic building recording of an area of land proposed for a new office development at Strawberry Place, Newcastle upon Tyne, centred on NZ 2450 6450 (Fig. 1).
- 1.2 The proposed development area (hereafter PDA) is composed of land currently used as a car park. The car park is divided into four areas bounded by retaining walls Fig. 2. The walls originally formed parts of structures formerly located in the PDA and constitute the primary focus of this survey.
- 1.3 A Historic England Level 2 measured and photographic survey was carried out in accordance with the relevant standards and guidance published by Historic England (2016) and the Chartered Institute for Archaeologists (CIfA 2014).

Scope

- 1.4 The survey included rectified photography of all retaining walls around the car park along with orthogonal photographs of any relevant features. A measured site photomontage of the walls was produced from Structure from Motion (SfM) software. The photomontage, combined with annotations made during the site visit, produced complete, accurate illustration of the walls.
- 1.5 The site archive will contain all the data collected during the investigative work, including site records and a full photographic catalogue. The archiving of any digital data arising from the project will be undertaken in a manner consistent with professional standards and guidance (Archaeology Data Service/Digital Antiquity 2013; Historic England 2015).
- 1.6 In accordance with the North-East Regional Research Framework for the Historic Environment, the work has the potential to provide a greater understanding of industry and transport (Petts and Gerrard 2006, 223).

Aims and objectives

- 1.7 The primary aim of the Historic Building Survey was to provide a 'descriptive record' (Level 2) of the retaining walls and prepare an assessment of significance to inform the proposed conversion works. The ensuing report and archive should be suitable to

mitigate against any subsequent loss of heritage significance resulting from the conversion of the walls.

1.8 The objectives of the survey were:

- production of an annotated rectified photography plan depicting the form and location of any structural features;
- a written and photographic record of the wall and their structural features, providing details of their form, function, date and significance, as well as key structural features;
- preparation of an illustrated report on the results of the investigations to be deposited with the Tyne and Wear Historic Environment Record (HER); and
- preparation of a labelled and catalogued digital photographic archive to be included as part of the Tyne and Wear HER.

2.0 BACKGROUND INFORMATION

Location

2.1 The PDA is located to the north-west of Newcastle city centre, just beyond the limit of the walled medieval and early post-medieval town. The PDA, which is roughly rectangular measuring approximately 65m by 46m, is bounded by Strawberry Place to the north-west and Strawberry Lane to the south-west. To the south-east the PDA is bordered by a walkway/access road, which is believed to overlie the course of the underground railway line from Monument to St James Metro Station a short distance to the west of the PDA. The north-eastern edge of the PDA borders a recent student-accommodation development. The Strawberry public house at the junction of Strawberry Place and Strawberry Lane is excluded from the PDA (Fig. 2).

2.2 The site is currently vacant land used for car parking. This is in four separate areas at varying levels, divided by retaining walls (Fig. 2). Immediately to the east of The Strawberry public house, and fronting onto Strawberry Place, is a plot (hereafter Plot A) at street level measuring 24m from south-west to north-east and 18m wide. To the south-east of (behind) the public house, a second car parking plot (Plot B), measuring approximately 18m by 12m, fronts Strawberry Lane. Immediately to the south-east of Plot B, an access ramp leads down from Strawberry Lane to a third car park, Plot C, which occupies much of the PDA, measuring up to 47m from north-west to south-east

and 36m wide. A fourth small area of car parking (Plot D) occupies the south-western corner of the PDA, to the south-east of the access ramp. The land slopes generally from north to south from c.56m above Ordnance Datum (aOD) on Strawberry Place to c.51m aOD on Gallowgate.

Topography

- 2.3 Plot A, the highest part of the PDA, is approximately level with Strawberry Place to the north. Behind this street frontage, Strawberry Lane slopes generally down to the south-east, and Plots B and D follow the slope of this roadway. Plot C lies at a lower level, the height difference between the plots being accommodated by retaining walls around the south-western and north-western sides of the plot. These walls are largely formed from parts of the buildings that formerly occupied the site, with some sections of modern in-fill. To the north-east of Plot C, the new student accommodation has been constructed from a lower level, with the north-eastern edge of the car park retained by an old brick wall. The walkway/service road bordering the south-eastern edge of the PDA slopes down to the south-east, below the level of the car park, and this side of Plot C is retained by a mixture of old and new walling and, at the south-eastern corner, by old walling supported by an earth bank.

Geology

- 2.4 The site overlies mudstones, siltstones, and sandstone of the Pennine Middle Coal Measures Formation. This is overlain by Devensian Diamicton Till (BGS 2020) which, during previous archaeological interventions nearby, has been seen to consist of yellow boulder clay.

Previous work

- 2.5 The PDA was the subject of a desk-based assessment (hereafter DBA) conducted by NAA in 2019. This building survey is a direct result of findings within the DBA and much of the ancillary information in this report is derived from the earlier work.

3.0 METHODOLOGY

Documentary evidence

- 3.1 The NAA DBA (2019) was the primary source of historical information for this report. Other readily available online sources were also consulted along with historic Ordnance Survey mapping.

3.2 The following sources were used to inform this report:

- Tyne and Wear Historic Environment Record
- Google Earth satellite imagery
- Historic Ordnance Survey (OS) maps

Building recording

3.3 Fieldwork was carried out on 5th August 2020. At the time of survey, the site had most recently been in use as a car park. Access was from the west along Strawberry Lane with access to a smaller section of the car park to the north from Strawberry Place.

3.4 A photogrammetric survey was conducted using a combination of Structure-from-Motion (SfM) photogrammetry and hand-drawn annotations of key features. All walls were photographed as parallel as possible in order to avoid distortion. General shots were taken of the PDA to inform its general area and character, as well as detailed photographs of any features of note.

3.5 Each retaining wall within the PDA was given a unique identification number (context number) based on which plot they retained. Where more than one wall was found within a plot, they were assigned numbers (100+) with individual phases within a wall given a unique number (10+). A complete list of all archaeological features can be found in the gazetteer (Appendix A).

4.0 HISTORICAL BACKGROUND

4.1 Evidence for prehistoric activity in Newcastle has been located through occasional finds dating to the period. However, any significant evidence of habitation begins in the Roman period with the construction of the fort *Pons Aelius*. The PDA lies approximately 500m north of Hadrian's Wall, but there is no evidence of any Roman constructions in the immediate environs of the site.

4.2 In the medieval period, settlement in Newcastle grew after the foundation of a castle in 1080. Although this was the first period of significant urban development in Newcastle, the area around the PDA remained outside urban medieval Newcastle, located c.120m beyond the city walls.

4.3 By the early 17th century, population growth in Newcastle required the construction of accommodation outside the city walls. Gallowgate, the street just south of the PDA is

first depicted in Speeds Map of 1611 (Fig. 3). The layout remained largely the same as construction continued in the area to the north-west of the city walls into the 18th and 19th centuries. The area of the PDA consisted of gardens for the buildings along Gallowgate throughout much of the 18th century (Figs 4–5).

- 4.4 The back gardens of the houses on Gallowgate were later encircled by developments along Strawberry Place in the early 19th century. Oliver's map of 1830 (Fig. 6) shows significant development in the area, including several buildings along the north of the PDA. Two of these later became the The Strawberry West Inn in 1859, which remains in use as a pub, having acquired the adjacent property sometime before 1940. The other buildings on site were likely to have been tenement houses for the poorer workers of local industries. One such local industry was a sawmill and timber yard that was built within the PDA by the time of the First Edition 25-inch Ordnance Survey (OS) map of 1860 (Fig. 7).
- 4.5 In the late 19th century, the Goad Insurance map of 1896 indicates that the timber yard and sawmill had been built over to become a furniture warehouse and depot. Stables for the warehouse were located to the rear, to the north, abutting the residences fronting Strawberry Place (Fig 8). What was once known as Leazes Lane on the west boundary of the PDA changed to Strawberry Lane, lined with shops and the Newcastle City Corporation public baths and wash houses. A wheelwright's and blacksmith shop were located immediately to the east of the baths with access via a small lane that opened onto Gallowgate.
- 4.6 The layout of the PDA remained largely the same into the 20th century. Major development around the PDA came with the construction of a drug packing and manufactory by 1930. This factory was located immediately east of the PDA across Dalrymple Court, the narrow passage that ran along the eastern boundary of the PDA. The Goad Insurance map of 1930 (Fig. 9) shows a change of use from shops to stables for the buildings along Strawberry Lane north of the baths and wash houses. The house in the north-east corner of the PDA, along Strawberry Place had also been demolished. By the time of the 25-inch OS map of 1940 (Fig. 10), the drug manufactory is referred to as a chemical works.
- 4.7 By the 1960s, the PDA was used as a depot, the baths and wash houses building had been converted to a TA centre, and the buildings beside the pub on Strawberry Place

had been demolished. By the 1980s, the PDA and nearby chemical works had been transformed into a car park. The site of the chemical works is now occupied by a student accommodation block.

5.0 BUILDING SURVEY

Layout and form

- 5.1 The four plots of the car park (Fig 11) are divided from each other by retaining walls, discussed below according to the plot they retain. Plot A is accessed from the north, along Strawberry Place and is separated by a tall retaining wall on its south side, divorcing it from the remainder of the car park (Plot C). A small wall lies on its northern boundary. Plot B is a terrace that now serves as the beer garden for The Strawberry. It is separated from the main car park (Plot C) by a wall that looks east. To the south-west of the main car park, Plot D is also terraced, held up by a short retaining wall.
- 5.2 The main car park, Plot C is accessed via a ramp from Strawberry Lane. It lies several metres below the retaining walls of the other plots. Along its eastern boundary, a wall separates it from the student accommodation to the east. At the time of survey, the south corner of the car park was blocked by a wooden barrier. Behind it lay a walkway between the PDA and the office building to its south under which is the metro-line. This walkway descends to the west and is separated from the PDA by a large mound of earth that culminates in a building foundation in the far south-east corner of the PDA (Fig 11).

Plot A

South Wall (A100)

- 5.3 Plot A is bounded by a tall (5.11m) brick wall **A100** showing several phases of repairs and interventions (Figs 12–13). The wall is not one continuous construction, but rather the older, south elevation, **A110**, is of older brick in English Garden Wall bond and shows several phases of repair. It spans a length of 29.5m and is abutted to the east by wall **A120**, a modern addition in stretcher bond that was constructed concurrently with the adjacent student accommodation block. To the west, the wall abuts wall **B100**, indicating wall **A100** may be a later construction.
- 5.4 On the south elevation, a horizontal building scar **A111** descends wall **A110** 5.36m east of its west end. This scar is likely to have been caused by a repair to the wall, with

the later addition of new bricks. However, the bond and brick types are similar on both sides of the scar indicating it may be an early intervention, similar in construction to the remainder of the wall. Another, more obvious building scar **A112** can be seen running down the middle of elevation **A110** (Plate 1). This scar protrudes 0.25m from the wall and is the remains of a wall that extended southward into the former depot in Plot C. Further east along the wall is the blocked opening **A113**, which has been filled with modern brick in stretcher bond, evidence of a more recent repair. Another, more complex set of building scars **A114** show two phases of repair. The lower section is of common bond. It is found between both building scars and then devolves into stretcher bond near the top of the wall, indicating a second, more uneven phase of repair.



*Plate 1: Wall **A110** looking north. The wall remains **A112** are visible on the left with the blocked door **A113** in the centre and the muddled scar **A114** on the right.*

- 5.5 The joint **A116** between walls **A110** and **A120** has a slight offset; elevation **A110** is set 0.43m out from wall **A120** (Plate 2). The bricks of wall **A110** are rounded-off near the joint indicating that the wall would have culminated naturally prior to the addition of wall **A120**. However, the presence of a few bricks at the base of this joint indicate a wall footing of another addition prior to the modern wall **A120**. However, the footings abut wall **A110**, demonstrating that whatever wall these bricks belonged to, it post-dated wall **A110**.
- 5.6 The north elevation **A130** of the wall is 0.89m high and is alike the south elevation **A110** showing bricks in English bond, with occasional repairs of modern brick to match those of elevation **A110**.



Plate 2: Wall joint A116 looking north-west.

North Wall (A200)

- 5.7 On the north boundary of Plot A is a short section of brick wall **A200** that separates the car park from the pavement (Plate 3). This wall is constructed of similar brick and bond to wall **A120** and is likely to date from creation of the car park.

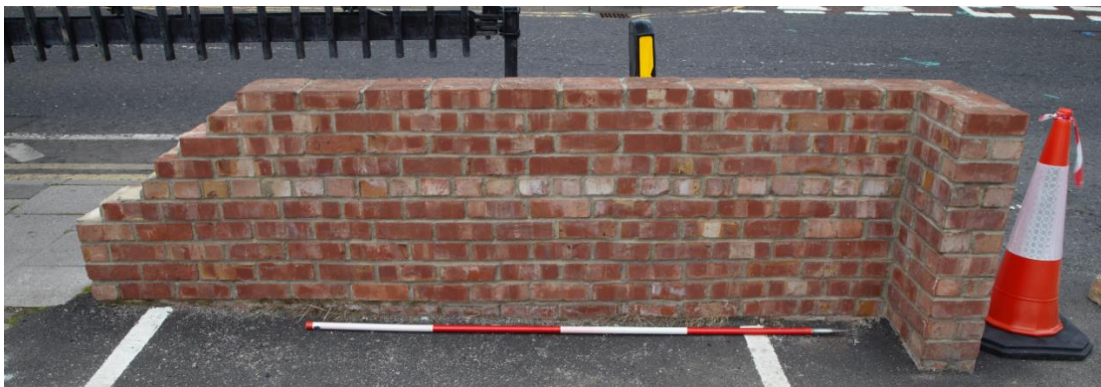


Plate 3: Modern wall A200 on north end of Plot A looking north.

Plot B

- 5.8 Plot B is currently used as the beer garden for the The Strawberry pub, although Google Earth satellite imagery shows its use as a car park until 2020. It is bounded by three walls; the longest of these walls is the west wall **B100**, which faces east and overlooks Plot C. A short section of wall **B200** then turns to the west, facing south toward the

access ramp to Plot C. An even shorter wall **B300** looks east toward Plot C and incorporates the modern stairs and pedestrian access to Plot B (Fig. 11).

North-west wall (B100)

- 5.9 Wall **B100** is 4.6m high and 19.3m long, consisting of several different constructions divided into three sections (Figs 14–15). A lower level of brick in English Bond is found on both the north **B110** and south **B140** ends of the east elevation. This brick construction rises approximately 1.9m and 1.3m respectively above current ground levels.
- 5.10 On the south side of **B140**, this brick wall gives way to stonework **B150** at a very uneven boundary. Small amounts of slate are present at the boundary as possible means of damp-proofing. On the north section, **B120** a similar boundary would have been found, but most of the stonework has been replaced with brick repairs **B121** in stretcher bond. A single metal rod or hook **B141** (Plate 4) is now bent back into the south brick wall. This hook was most likely used to tie items to the outside of the wall.



*Plate 4: uneven boundary between **B140** and **B150** with the rod **B141**.*

- 5.11 A large modern section of elevation **B130** divides the north and south (**B110**, **B120**, **B140**, **B150**) sections of wall. This wall is 7.05m wide in stretcher bond and appears to

be a recent construction. It extends c.10m out from the wall **B100**, shoring up a possibly unstable construction behind it.

- 5.12 The west elevation of wall **B100** is similar to the east elevation (Fig 16). The north-most section **B160** is largely covered in concrete with only a few bricks visible. The middle section **B170** is 1.2m tall and is of brick in stretcher bond, although the bricks are older than the reverse elevation **B130**. On the south side of this section are the remains of a wall, now extending 0.35m west and rising 0.5m above the floor of Plot B (Plate 5). The south section **B180** is of stone, like the reverse elevation **B150**. On either side of this section are large iron beams **B181** protruding from the asphalt c.0.3m (Plate 5). They are likely to have been related to the internal use of the building.



*Plate 5: Wall elevation **B180** looking east. The wall plinth **B171** is visible on the left along with the iron beams, **B181**, on either side of wall **B180**.*

South wall (B200)

- 5.13 The south wall is a short (1.9m) section of wall forming the south-east corner of Plot B (Figs 14–15). Like most of Plot B, the south elevation is composed of a brick base **B210** rising 1.6m high to then be succeeded by stone **B220**. Unlike walls **B110** and **B120**, the boundary between the brick and stone here is even with some evidence of slate at the boundary. The south-east corner of the wall **B220** where it joins wall **B150** contains ashlar quoins, a marked contrast in building styles from the chaotic construction of the remainder of the wall. The rear, north elevation of the wall, **B230** is only 1.17m high and of similar construction (Fig.16).

South-west wall (B300)

- 5.14 The south-west wall **B300** is very short, approximately 2m wide (Figs 14–15). It too has an east elevation composed of a brick base **B310** 0.85m high in English bond with stone **B320** above. At the boundary between brick **B310** and stone **B320** is a blocked metal drain **B311** 0.2m in diameter set in a 0.23m square hole. The south end of the wall has been truncated by the modern addition of concrete steps. The rear, north elevation **B330** of the wall is 1.3m high and of similar construction (Fig. 16).

Plot C

- 5.15 Plot C is bounded by a long wall **C100** that runs 48.95m along the entire eastern boundary of the site. This wall is linked with wall **A100** through a modern dog-legged brick wall **C130** that forms the north-east corner of the PDA. At the very south end of wall **C100** is perpendicular wall **C200** forming the corner of a small structure that then disappears under the earth mound that forms the south boundary of the car park.

East wall (C100)

- 5.16 The west elevation **C110** is of brick in English Garden Wall bond (Figs 17–18). It has two visible scars **C111**, each 0.64m wide and spaced 5m apart (Plate 6). These scars were probably caused by previous walls that abutted **C100** and were later removed. Most of this wall appears to be from a single phase of construction with few discernible archaeological features.



Plate 6: the two wall scars C111 looking west.

- 5.17 A row of four bolts **C112** are spaced approximately 2.35m apart along the wall. These protrude c.0.3m from the wall on the east side. Bolts **B113** are spaced at a similar interval, although only small parts of the bolts remain and they are not visible on the

east elevation. Other holes **C114** may have previously contained bolts but none are now present.

- 5.18 The rear elevation of wall **C100** was not surveyed with photogrammetric survey due to access. Photographs of the wall were taken as near to the wall as possible (Figs 19–20). On the east elevation **C140** are two long grooves **C141** c.0.15m wide and deep, running the entire length of the wall. The brick edges around the top of the grooves are uneven (Plate 7), indicating some damage caused as a result of their creation, and that they are a later addition. They were possibly used to accommodate drainpipes.



Plate 7: The rear of wall C100. The scar C151 is visible extending down into the foliage. It is located just south of the groove C141.

- 5.19 Near the south end of wall **C110** is a separate section of wall **C120** (Plate 8). This section is 2.9m long and is also of English Garden Wall bond, but its courses are offset to those of **C110**. The rear of this section **C150** is slightly thinner (0.3m) than wall **C110** (0.43m). It also contains a vertical scar **C151**, which descends the length of the wall but stops a few courses before the top due to what is likely a more modern repair (Plate 7).

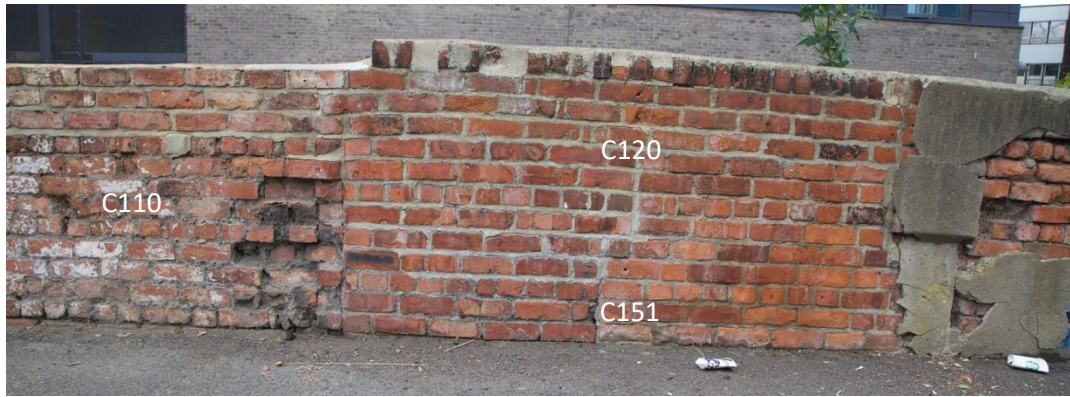


Plate 8: the south end of elevation C110 looking east. The section C120 is visible along with a short section of the wall scar C151, which is more visible on the rear elevation.

South wall (C200)

- 5.20 In the very south-east corner of the PDA is a short section of wall c. 2.5m high constructed in brick in a variant of English Garden Wall bond (Plate 9). At the time of survey, the wall C200 was largely covered in vegetation, but photographs from the DBA (NAA 2019) indicate the wall extends c.3.75m to the west before disappearing under the earth mound that supports the south end of Plot C. A clear repair is evident in the DBA photographs showing a scar that runs down the central section of the wall.



Plate 9: The corner of walls C100 and C200 looking north-west.

Plot D

- 5.21 Plot D is bounded by two walls, one on the north **D100** and east **D200** (Fig. 20). These walls used to belong to the Corporation baths and wash houses but have now been mostly razed and capped in concrete

North wall (D100)

- 5.22 The south elevation **D110** is low at 0.4m high and contains only five courses of both yellow glazed bricks and red bricks, the latter placed as a result of a later repair. The wall is constructed in Flemish Garden Wall bond with two full courses of yellow brick below interspersed courses of red and yellow brick (Plate 10). The final course is all unglazed red brick in stretcher bond.



Plate 10: The south, internal elevation D110 of the baths and wash houses looking north.

- 5.23 The north elevation **D120** is taller, 0.91m high, of red unglazed brick in stretcher bond. This elevation blends seamlessly into the modern brick wall that supports the ramp to the west and is likely a later alteration.

East wall (D200)

- 5.24 Little is left of the west elevation **D210**, it being only 0.2m high (Plate 11). Only one course of glazed yellow brick can be seen, occasionally stepped back to follow the slope of the car park. The edge of each stepped-back course has been finished with a red brick, which is probably a modern addition to accommodate the change of use toward a retaining wall.



Plate 11: The west, internal elevation 210 of the baths and wash houses looking east.

5.25 The east elevation rises 0.95m high above Plot C. The lower courses **D220** are composed of older red brick in a variant of common bond. Upper courses **D230** contain more recent brickwork in stretcher bond.

6.0 PHASING AND DISCUSSION

Plot A

6.1 Wall **A100** is first visible on the 1830 map (Fig. 6) running behind the properties fronting Strawberry Place. It separated the alleyway and house garden from the main gardens in the rear (now Plot C). This map shows three properties in what is now Plot A. Of these, the two plots to the west, adjacent to the pub, stretched north–south the whole length of Plot A. These buildings were reached by a small alley that ran along the rear to the central building in Plot A. The garden wall **A100** separated this snicket from the garden to the south. Another alley ran perpendicular to wall **A100**, right through the centre of what is now Plot A. East of this alley, another building lay perpendicular to it with a garden in the rear. This garden was separated from the larger garden to the south by a garden wall **A100**. The map does show a small gap in the length of the wall, but the scar of such a gap is not present on the current wall and may instead have been a door. Considering the whole of wall **A100** was most likely present by 1830, the repair scar **A111** must therefore pre-date this period. There is no documentary evidence for this repair.

6.2 There is no evidence on the 1830 map of wall **B100**. However, the join at this wall (Plates 12 and 13) indicates that wall **A100** was built against wall **B100**. A small section of wall **B100** extends north. There is no indication of the presence of a wall oriented north–south across the alley immediately north of the garden wall **A100** on the 1830 or any subsequent map. It is possible that this section of wall only appeared during the later repairs to the wall evident in the stretcher bond of **B121**.



*Plate 12: (left) Join of wall **A100** and **B100** looking east; Plate 13: (right) join looking north*

6.3 By the time of the First Edition 25-inch OS map in 1860 (Fig. 7), the layout of the buildings remained the same except for the gardens to the rear. These had been replaced with a series of walls creating yards behind the buildings fronting Leazes Lane. The eastern-most of these walls joined with wall **A100**. This wall is likely to be the source of the wall remains **A112**. The use of this wall in 1860 is uncertain, but on the Goad Insurance map of 1896 (Fig. 8) it appears as an internal division within the furniture depot. To the left of this wall, the small alley running to Strawberry Place visible in 1830 is still in place. This alley may have culminated in an access door, now blocked **A113**, connecting the furniture depot with Strawberry Place.

6.4 To the east of the alley, stables were built in the former garden of the east property in Plot A. The stables opened into the furniture depot and were sectioned off from the depot by two partition walls. The Insurance maps of 1896 and 1930 (Figs 8–9) show that the stables were accessed from a gap in wall **A100** west of the alley and another just west of the east end of Plot A. The building scars **A114** likely indicate the placement of this west access. The east access is still visible in the rounded bricks **A116** that abut the modern wall. The vent **A115** would have provided further ventilation for the stables.

- 6.5 The OS 25-inch 1940 map shows the demolition of the stable. It is likely that the repairs in the wall at **A114** occurred at this time. The east access would also have been blocked up, but that section of the wall was later destroyed and rebuilt with the modern construction of the student accommodation block, as evidenced by the remaining footings **A116**. The 1940 map shows retention of the access to the building from the alley to Strawberry Place, so the blocking of door **A113** would have occurred after the demolition of the depot to make way for the car park. This is most likely the date for the demolition of the internal depot wall **A112**

Plot B

- 6.6 Wall **B100** first appears in documentary evidence in the First Edition 25-inch 1860 OS Map (Fig. 7) where the garden visible on the 1830 Oliver's map (Fig. 6) was replaced by two buildings fronting what was then Leazes Lane. Behind these buildings was a long shed of either wood or metal and a small yard. On the First Edition OS map, brick walls were denoted as red (e.g. **A100**), but the black colour of this boundary indicates it may have been a fence, over which a brick wall was built by the time of the Goad Insurance map of 1896 (Fig 8).
- 6.7 On the 1896 map (Fig 8), two buildings have been built around a central courtyard behind the frontages on Leazes Lane. These buildings extend further east than the adjacent building to the south. This offset is still visible in the layout of Plot B, with a corner between **B100** and **B200** at the southern corner of the longer, northern buildings (Fig. 8). Wall **B300** therefore represents the east wall of the adjacent, shorter, southern building.
- 6.8 These buildings were probably all built in the same period, since the base of all three walls are of the same brickwork. It is uncertain why, but they were later rebuilt in stone. Considering the haphazard construction, this was possibly done out of expedience with little attention to aesthetic.
- 6.9 The modern section of wall **B130** correlates with the wall that once closed off the east end of the courtyard between the buildings. It was perhaps of a weaker construction, used as a boundary rather than for structural support. Such a construction may explain the addition of a modern brick wall **B130** to support this old boundary wall now used to retain the new car park terrace.

- 6.10 The shorter building to the south was probably demolished at the time of the construction of the car park to accommodate the access ramp. Only a short section of the east wall of that building remains **B300**, now truncated by modern concrete steps.

Plot C

- 6.11 Wall **C100** is first visible in Oliver's 1830 map (Fig. 6) and once separated Dalrymple Court from the gardens to the west. However, the First Edition OS map (Fig. 7) seems to imply that only a section of this wall, the eastern boundary of the sawmill, was made of brick. It is therefore uncertain when the wall was constructed. Yet, most of wall **C100** appears of a similar construction and likely dates from a single phase. The adjacent wall to the south, **C200** was potentially built at a similar time. It too appears on both the 1830 and 1860 map, although it is specifically drawn as brick on the 1860 OS map (Fig. 7).
- 6.12 By the end of the 19th century, the whole area of Plot C was occupied by a furniture depot with wall **C100** forming its eastern wall. The two scars **C111** were perhaps left from internal walls, possibly associated with the two lines that are shown crossing the glass roof of the eastern section of the building (Fig. 8).
- 6.13 Visible to the south of the depot on the 1896 and 1930 Insurance map is a small building marked 'sawdust store,' separated from the depot by a narrow gap (Figs 8–9). This gap may have had a door or opening to provide access from Dalrymple Court to the yard behind. It was subsequently blocked up **C120**, but at a different thickness than the remainder of the wall.
- 6.14 Wall **C200** served many purposes. It is first depicted in 1830 as separating the gardens of Plot C from the back yards of houses fronting Gallowgate (Fig. 6). It remains in 1860 (Fig. 7) as the division between the sawmill yard and the domestic yard of Gallowgate houses. It then seems to have been severely reduced by 1896 as it forms a short division within the van yard south of the furniture depot. By 1930, it separates the warehouse to the south from a midden to its north.
- 6.15 It is uncertain what purpose the bolts **C112** served but since they protrude into what was historically Dalrymple Court, they would be associated with an external feature in the lane.

Plot D

- 6.16 The walls of the south and west elevations of Plot D have glazed brick. These would have been used internally in what was once the Corporation baths and wash houses. The baths building first appeared on the Goad Insurance map in 1896 (Fig. 8), prior to which the area was part of a yard for buildings fronting Leazes Lane (Fig. 7). The presence of glazed brick matches the use of the building as baths. The later addition of red bricks occurred in the demolition of the building and the capping of walls to create the car park.

7.0 STATEMENT OF SIGNIFICANCE

- 7.1 Any heritage asset has a distinct cultural significance derived from a wide range of varying values and perspectives, encompassing not just the physical fabric of the building but also its setting, use, history, traditions and local distinctiveness.
- 7.2 The following section discusses the significance of the Strawberry Place Car Park walls based on the three high-level themes – archaeological interest, historical interest and architectural and artistic interest – specified in Planning Note 2: Managing Significance in Decision-Taking in the Historic Environment (Historic England 2015) and Historic England Advice Note 12 (Historic England 2019). Table 1 summarises the walls' overall significance.

Archaeological Interest

- 7.3 The archaeological interest of the remains at Strawberry Place is low–moderate. The walls have the potential to explain some of the uses of the buildings they supported and what building phases and changes in use occurred over the last 200 years. In particular, the remains of the baths building have the potential for providing information on the use and layout of Victorian city corporation public baths and wash houses.

Historical Interest

- 7.4 The walls have a low–moderate historic value. The layout and phasing of the walls around the car park support the documentary evidence provided by historic maps. These can tell us about the changes in population, industry, and urban fabric that took place in the area 19th and 20th centuries,

Architectural and artistic interest

7.5 The architectural and artistic interest of the walls is low. The walls were constructed for practical uses and hold no evidence of artistic or architectural flourishes.

Table 1: summary of values contributing to the significance of Strawberry Place.

Assets	Assessment	Ranking	Overall
Evidential	The walls have the potential to show the historic use of the buildings in the area.	Low–moderate	Low–moderate
	The walls have the potential to show changes in building use over time.	Low–moderate	
	The walls of Plot D have the potential to inform on the layout and design of Victorian public baths and wash houses.	Moderate–low	
Historic	The walls provide support for the layout depicted on historic maps.	Low–moderate	Low–moderate
	The walls show the historic layout of the area during a key part of Newcastle’s growth.	Moderate–low	
Aesthetic	Walls were built to be practical and have little artistic design.	Low	Low
	Walls in plot B show evidence of hasty repairs with a general disregard for cohesion.	Low	
	The glazed bricks of Plot D show an aesthetic effort toward decorating the interior of the baths building.	Low–moderate	

8.0 CONCLUSIONS AND RECOMMENDATIONS

8.1 This report has examined the history, fabric, and change of use of the retaining walls of the Strawberry Place Car Park to understand more about the development and function of the structures previously on the site. It is considered that this report and accompanying archive provide a comprehensive record of the standing structures suitable to mitigate appropriately against any potential loss of heritage significance arising from the intended building works.

8.2 However, there remain several unanswered questions regarding the phasing of the walls. An interpretation of phasing has been offered in this report, but further significant evidence may be uncovered during any sub-surface intervention or intrusions into the standing fabric (e.g. removal or part-removal of walling) In order to mitigate against any loss of evidential value, archaeological monitoring (a watching brief) is required during building works as indicated by the DBA (NAA 2019).

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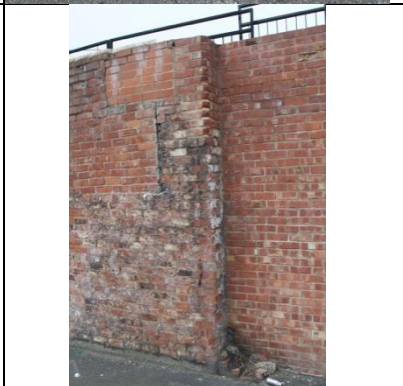
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APPENDIX A: GAZETTEER




Plot	Number	Name	Description	Phase	Photo
A	100	North car park wall	South retaining wall of Plot A. The wall is composed of brick with several phases of alteration and repair.	Pre-1830 with subsequent alteration	See Fig. 12
A	110	South elevation	South elevation standing 5.11m above Plot C and is 29.5m long. This elevation is largely in English Garden Wall bond, with building scars and filled in holes.	Pre-1830 with subsequent alteration	See Fig. 12
A	111	Vertical scar	A vertical building scar that descends most of the length of the wall. It is located 5.36m from the west end of the wall. The bond and brick type are similar on both sides.	Pre-1830	





A	112	Wall remains	Scar from wall now removed. May have started as a yard wall but later became an internal wall of depot. The remains are 0.68m wide and protrude 0.25m from wall A110.	Pre-1860 with subsequent alteration	
A	113	Blocked doorway	Blocked doorway once providing access between depot and alley to Strawberry Place. It is 1.22m wide and 2.9m high.	Pre-1860 with subsequent alteration	

A	114	Wall repair scar	Wall scars on east side of A110 showing the possible west stable access. At the base of this wall the width of the scar is 1.76m. The opening was later blocked and repaired in two subsequent phases of intervention	Pre-1896 with subsequent alteration	
A	115	Wall vent	Vent in wall is 0.43x0.33m. The vent probably dates to the walls use as a stable.	Pre-1896 with subsequent alteration	
A	116	Wall joint	Wall joint and footings. Joint between modern wall and east end of wall A110. A line of historic rounded brick protrudes 0.03m from the modern abutting wall. At the foot of this joint are a couple of wall footings that post-date wall A110, but pre-date the modern brick wall A120 that abuts it.	Pre-1896 with evidence of mid-20th century alteration	




A	117	Blocked opening	A small opening in the top of wall A110 has been blocked with modern brick. It is 1.25x0.83m and may be a blocked window from the mid-20th century.	Mid-20th century with modern alteration	
A	118	Blocked openings	A series of three blocked openings near the top of the wall. These were probably used to accommodate wall furniture and later blocked. The holes average 0.74m wide and 0.54m high.	Unknown	
A	120	North elevation	North elevation of the south wall. It is similar in construction to the south elevation showing the same alterations.	Pre-1830 with subsequent alteration	See Fig. 12
A	130	Modern extension	Modern brick wall extending to the east as part of the construction of the student accommodation.	Modern	



A	200	North wall	Modern wall built concurrently with the car park.	Modern	
B	100	West car park wall	West wall of the car park serving as a retaining wall for the terrace south of The Strawberry pub. The wall has a brick base (c.1.6m) below a stone construction. A middle section (B130) is modern.	Pre-1860 with subsequent alteration	See Fig. 13
B	110	North brick base of east elevation	Brick base of the north section of wall B100. This wall rises 1.9m, stretching 6.05m wide. It is of brick in English Garden Wall bond.	Pre-1860 with subsequent alteration	
B	120	Upper north section of east elevation	This section of wall was originally built in stone only parts of which remain. It is 2.55m tall.	Post-1860 with subsequent alteration	
B	121	Brick repair to upper section	Most of the stone has been replaced with brick in a stretcher bond.	Unknown	
B	130	Middle of east elevation	A section of modern brick wall in stretcher bond measuring 7.05x4.1m. It stands out of the wall 0.16m and was probably built to shore up a weak wall to the rear, some of which is still visible.	Pre-1860 with modern alterations	




B	140	South brick base of east elevation	The brick base of the south section of wall B100 rises 1.3m, stretching 6.2m across. This wall is of brick in English Garden Wall bond with an uneven transition into the stone (B140) above.	Pre-1860	
B	141	Metal rod	A metal rod embedded in the brick base of wall B100. The rod (c.0.2m long) has been bent over and may have been used for tying or holding items to the wall.	Unknown	
B	150	Upper south section of east elevation	The south stone section of wall B100 rising 2.3mm above the brick boundary. The stonework is uneven as is the boundary with the brick below. Ashlar quoins line the corner of the stonework.	Post-1860 with subsequent alteration	See B140
B	160	North section of west elevation	West elevation of wall 120. The wall is 1.2m tall and has been almost entirely rendered in concrete, but a small amount of visible brickwork can be seen on the south edge showing largely modern brickwork. It is capped in concrete.	Post-1860 with subsequent alteration	

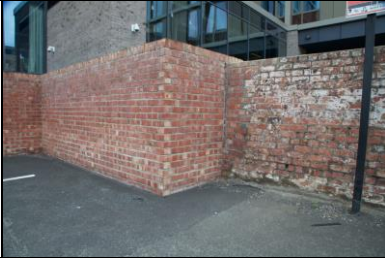


B	170	Middle of west elevation.	Section of wall to the rear of B130 showing brickwork in stretcher bond rising 1.21m above Plot B. A visible joint separates this wall from B160 showing a different construction. It is capped in concrete.	Post-1860 with subsequent alteration	
B	171	Small E-W wall section	Short section of wall 0.35m thick and extending 0.5m west. This wall is in similar construction to that of B170 and was probably of the same construction. It is capped in concrete.	Pre-1896 with subsequent alteration	
B	B180	South section of west elevation	Rear of wall B150 rising 1.35m above Plot B. It is of stone, in a similar construction to B150 and is capped in concrete	Post-1860 with subsequent alteration	
B	B182	Iron beams	Two steel beams that extend approximately 0.3m above current floor levels. They are located at the far north and south ends of wall B180 and were related to an unknown use of the building.	Pre-1896	




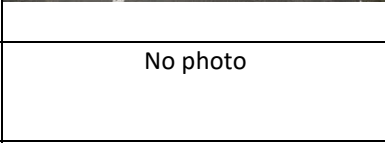

One Strawberry Place, Newcastle Upon Tyne: Historic Building Survey



B	200	South elevation	A short (1.9m) section of wall oriented east–west, perpendicular to wall B100. It is of similar construction to the south section of B100.	Pre-1896 with subsequent alteration	
B	210	Brick base of south elevation	The brick base of wall B200 is of English bond and rises 1.6m. It has an even boundary with the stone above (B220).	Pre-1896	
B	220	Upper section of south elevation	The upper section of wall B220 is of stone in an uneven construction with corner quoins to the east. It is 2.8m high.	Pre-1896	
B	230	North elevation	This wall rises 1.17m above Plot B and is of the same construction as B220. The west corner has been rendered in concrete.		
B	300	South-west wall of Plot-B	A short (2.1m) section of wall oriented north–south perpendicular to wall B200. It is truncated on the south by modern concrete stairs providing pedestrian access between Plots B and C.	Pre-1896 with subsequent alteration	
B	310	Brick base of east elevation	The lower section of wall B300 is of brick 0.85m high in an uneven bond. It has an uneven boundary with the stone (B320) above.	Pre-1896	

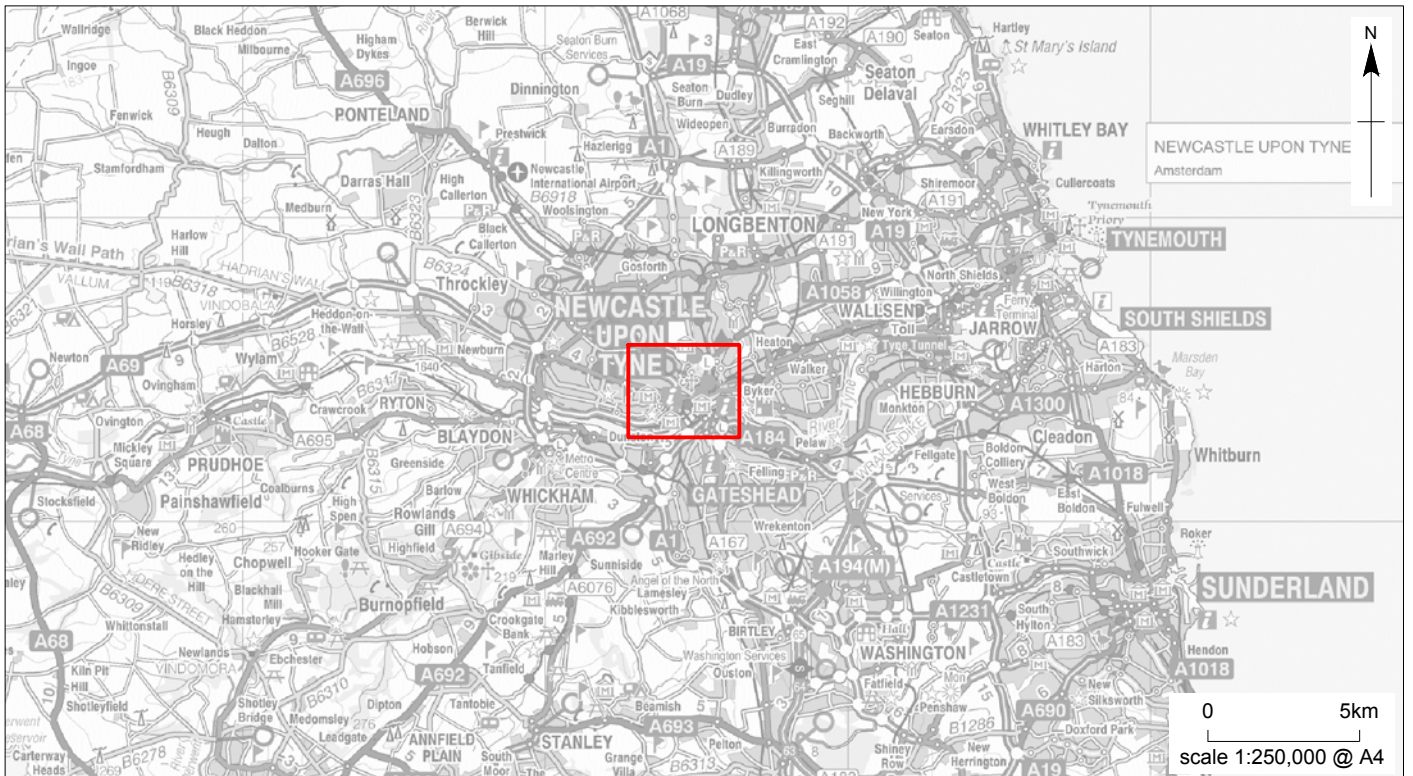
B	311	Drain	Square hole 0.23m wide in the brick wall B310 containing a round metal drain 0.2m in diameter.	Unknown	
B	320	Upper section of east elevation	The upper section of wall B300 is of stone, rising 2.9m, in a similar construction to B220 and B150. It has no quoins on the south side indicating it was probably cut short by the modern stairs.	Pre-1896	See B300
B	330	West elevation	A short section of wall 1.3m high of similar construction to B320.	Pre-1896 with subsequent alteration	
C	100	East wall of car park	Brick wall extending 48.95m along east edge of Plot C. The wall rises 3.6m above the car park level.	Pre-1860 with subsequent alteration	See Fig. 13
C	110	West elevation	West, older section of brick wall in English Garden Wall bond.	Pre-1860 with subsequent alteration	See Fig. 13

C	111	Vertical building scars	Two scars 5m apart, each 0.64m wide These are likely to be the vestiges of internal division walls in the depot.	Pre-1896 with subsequent alteration	
C	112	Bolts	A series of 4 bolts each spaced 2.3m apart. They are roughly 1-inch diameter bolts that extend out c.0.2m from the east elevation.	Unknown	
C	113	Bolt remains	Four small holes in the wall have remains of bolts, but these are far less complete than C112, only retaining some bits of metal.	Unknown	No photo
C	114	Bolt holes	Three holes between bolt remains B113 indicating the possible location of more bolts.	Unknown	No photo
C	120	Wall alteration	Section of later intervention 2.9m wide. Both 120 and 110 are in English Garden Wall bond but are offset from each other, demonstrating different phases of work. Another episode of repair has altered the upper three courses in stretcher bond.	Post-1896	

C	130	Modern extension	Modern addition that extends to the north of the wall. It is modern in stretcher bond.	Modern	
C	140	East elevation	The east, older section of the wall. Due to access, this elevation could not be recorded in detail.	Pre-1896	
C	141	Grooves	Two long grooves extend down the length of the wall. These are c.0.15m deep and 0.15m wide. They may have been used to hold a gutter.	Unknown	
C	150	East elevation of wall section	Short section of wall, thinner (0.3m) than those beside it (0.43m).	Post-1896 with subsequent alteration	

C	151	Wall scar	Faint wall scar between two sections of English Garden Wall bond, 1.5m from north end of wall section. The scar is more prominent on the east elevation.	Post-1896	
C	200	South wall of car park	A short c.3.75m section of wall standing c.2.5m high of brick in English Garden Wall bond showing several repairs of unknown date.	Pre-1830 with subsequent alteration	
D	100	North wall of Plot D	North wall of plot D.	Pre-1896 with subsequent alteration	
D	110	South elevation	South elevation of Plot D. The wall rises only 0.4m high, and only five courses of brick. The lower two courses are of yellow glazed brick devolving into unglazed red brick above.	Pre-1896 with subsequent alteration	
D	120	North elevation	North wall of Plot D looking onto the access ramp for Plot C. This wall is 0.91m high of red brick in stretcher bond and fades seamlessly into the modern wall to the west. It is most likely a later alteration.	Pre-1896 with subsequent alteration	
D	130	Modern extension	Modern wall located at west end of north wall D100. It is made of bricks placed on end. It acts as the retaining wall for the access ramp and is modern.	Modern	No photo
D	200	East wall of Plot D	East wall of Plot D. Wall is sloping, 0.95m high at the north end and 0.3m at south end.	Pre-1896 with subsequent alteration	

D	210	West elevation	West elevation. This wall contains only two courses of glazed yellow brick, now stepped down to accommodate the slant in the car park. At the end of each stepped course is a red brick addition.	Pre-1896 with subsequent alteration	
D	220	East elevation	The lower courses are composed of older red brick in a variant of common bond. Upper courses contain more recent brickwork in stretcher bond.	Pre-1896 with subsequent alteration	
D	230	Modern alteration	Modern bricks above historic brick of D220. The bricks are in stretcher bond.	Modern	



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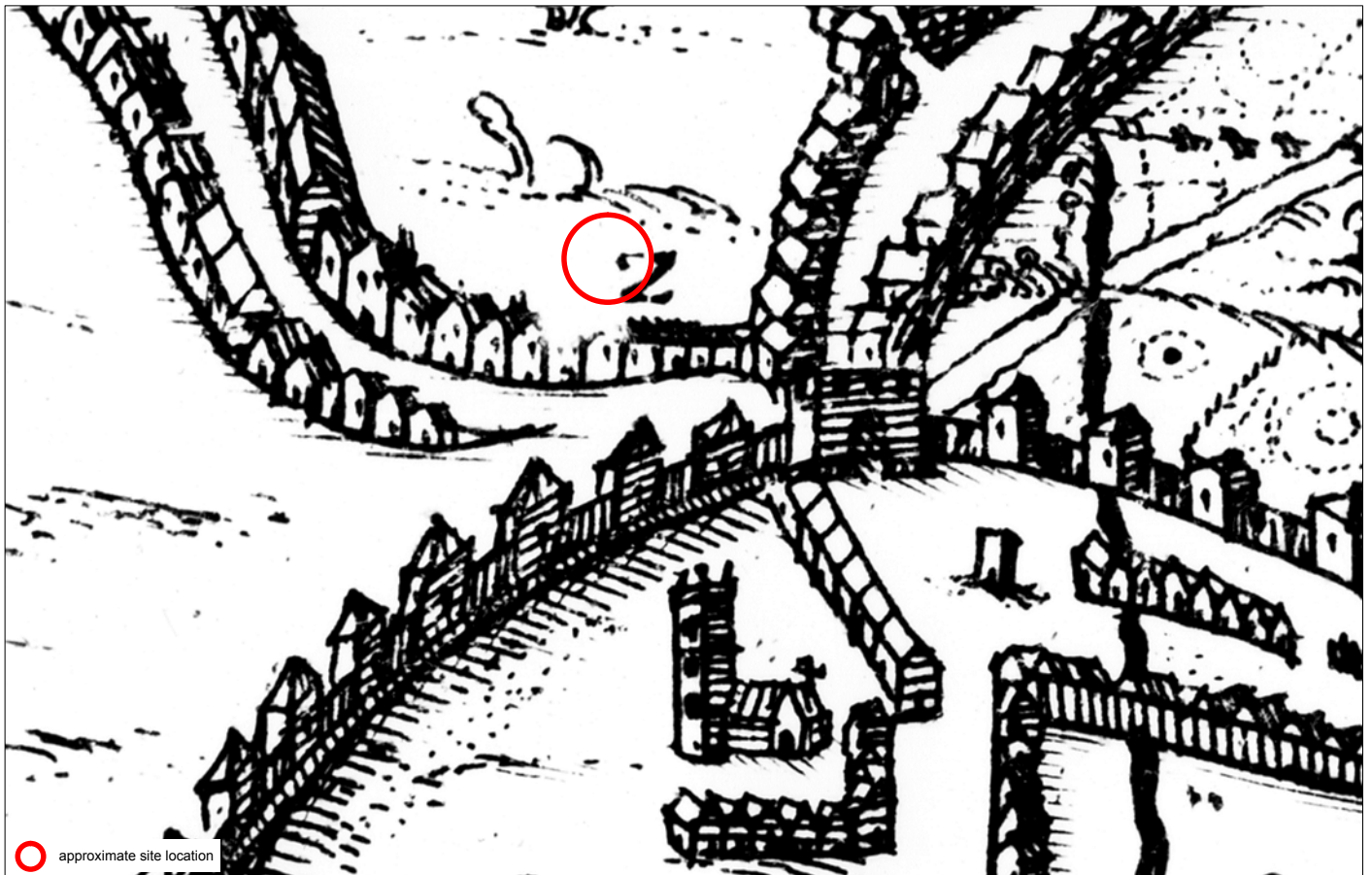
One Strawberry Place, Newcastle: site location


Figure 1



One Strawberry Place, Newcastle: PDA layout

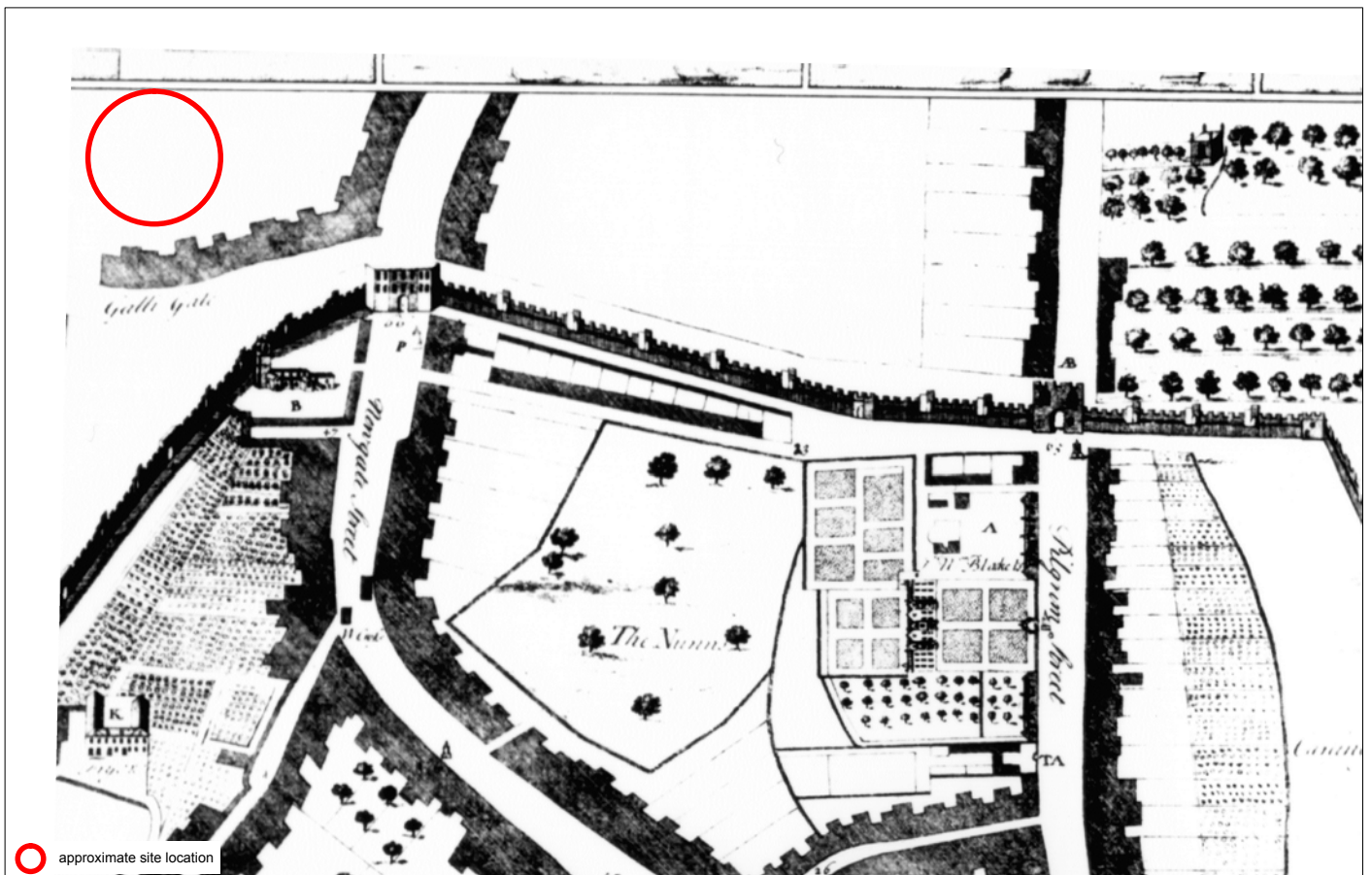
Figure 2




 approximate site location
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One Strawberry Place, Newcastle: extract from Speed's map of 1611

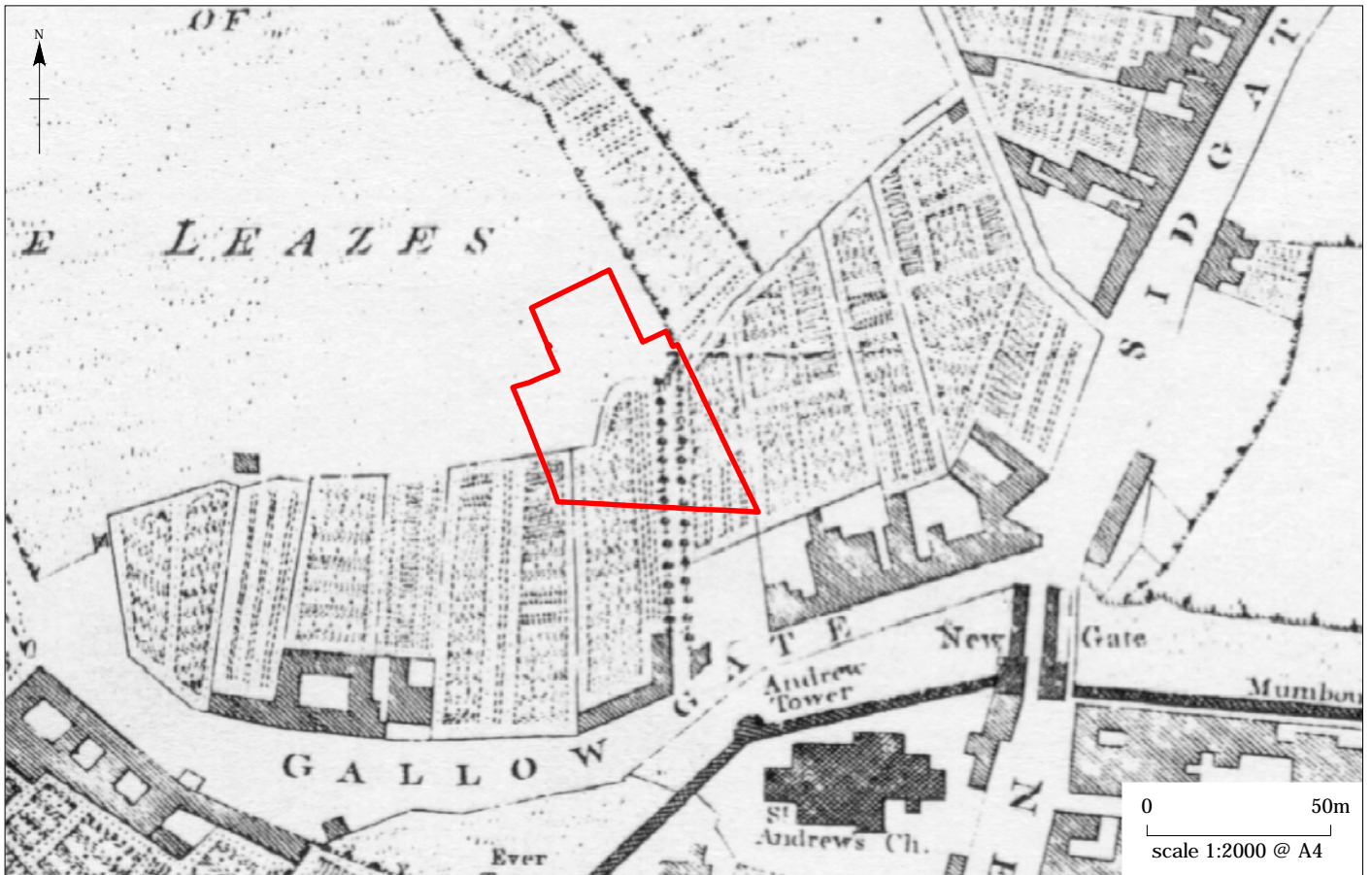
Figure 3



 approximate site location
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One Strawberry Place, Newcastle: extract from Corbridge's map of 1723

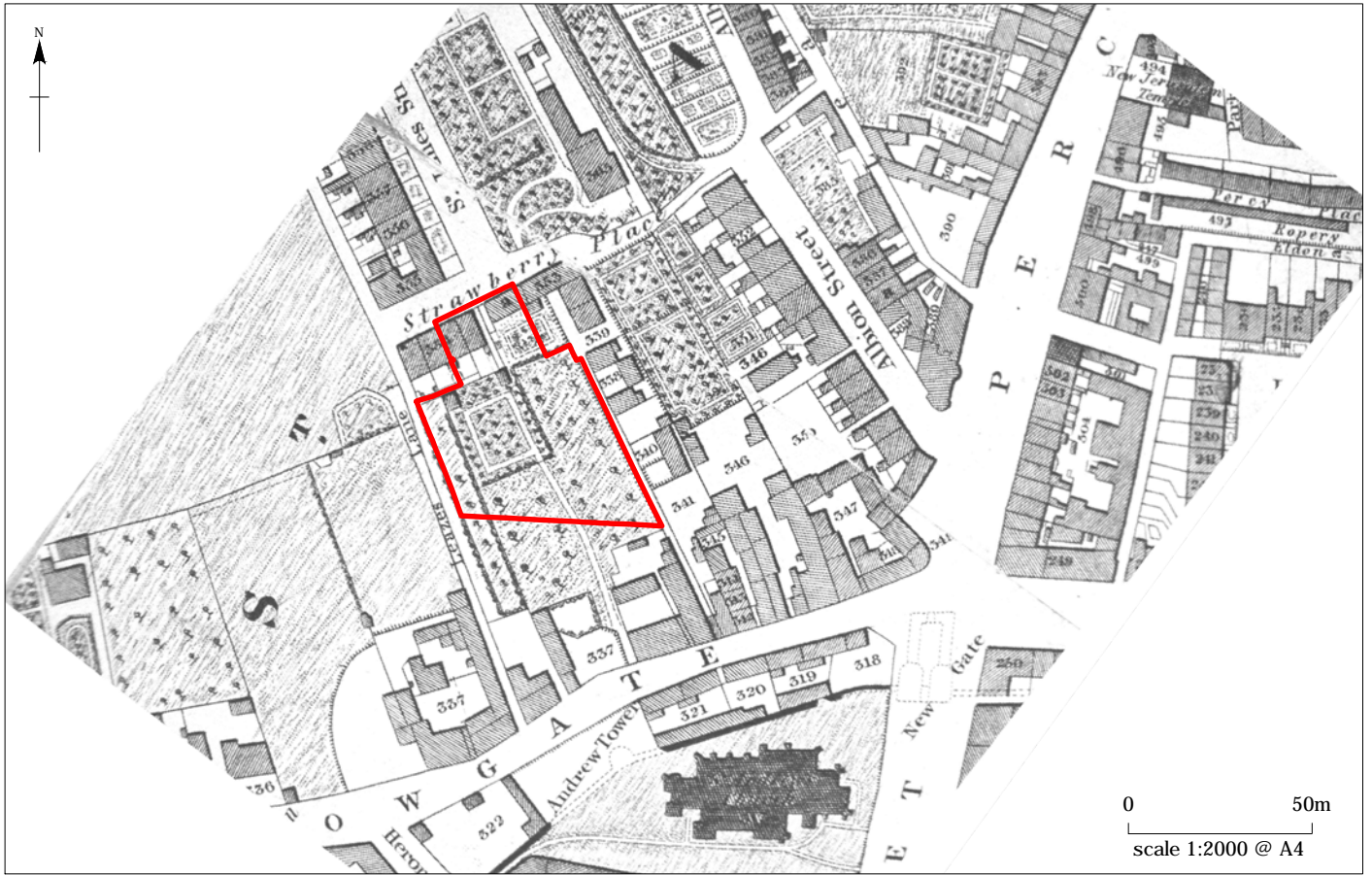
Figure 4



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One Strawberry Place, Newcastle: extract from Hutton's map of 1772

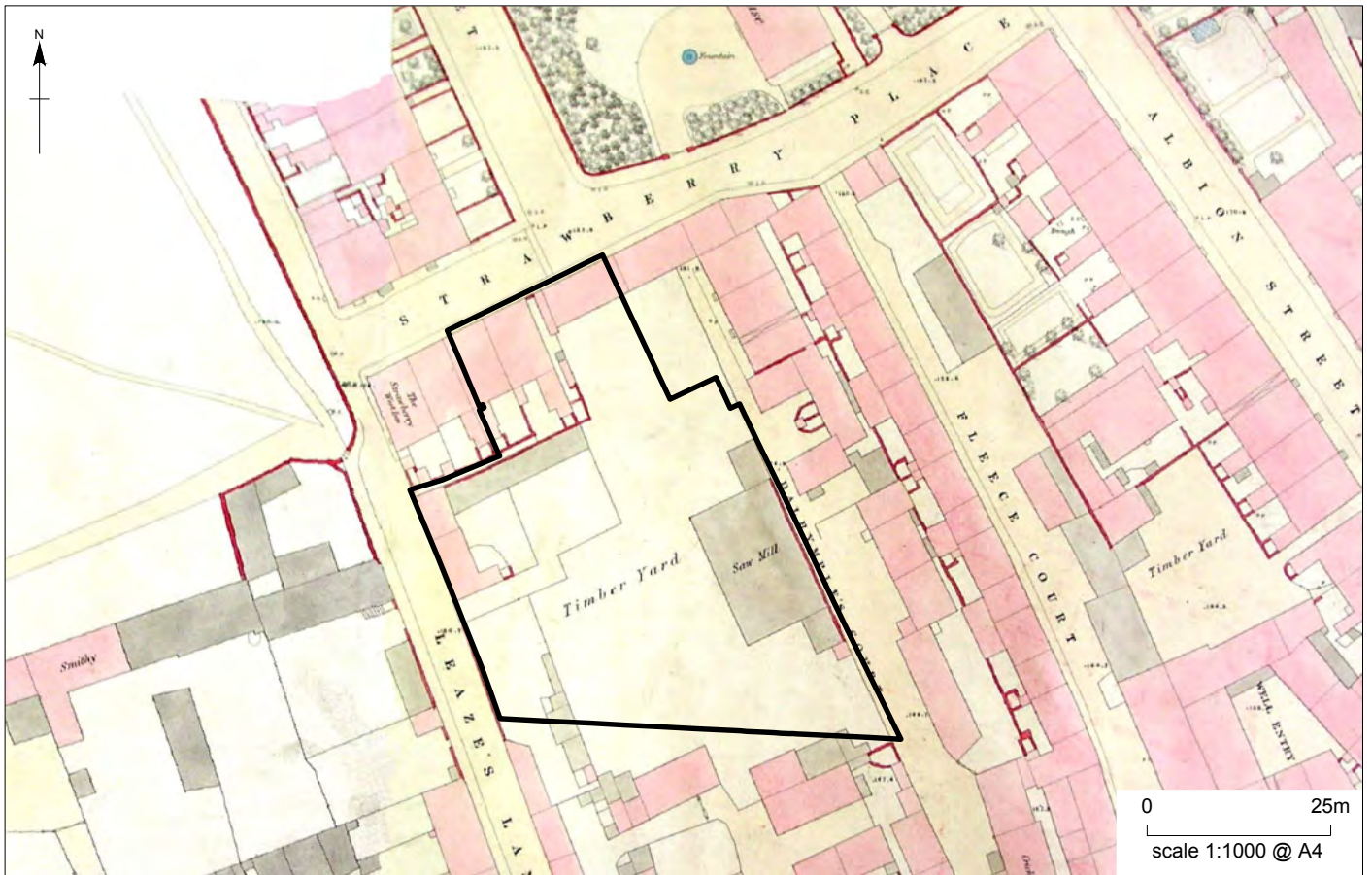
Figure 5



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One Strawberry Place, Newcastle: extract from Oliver's map of 1830

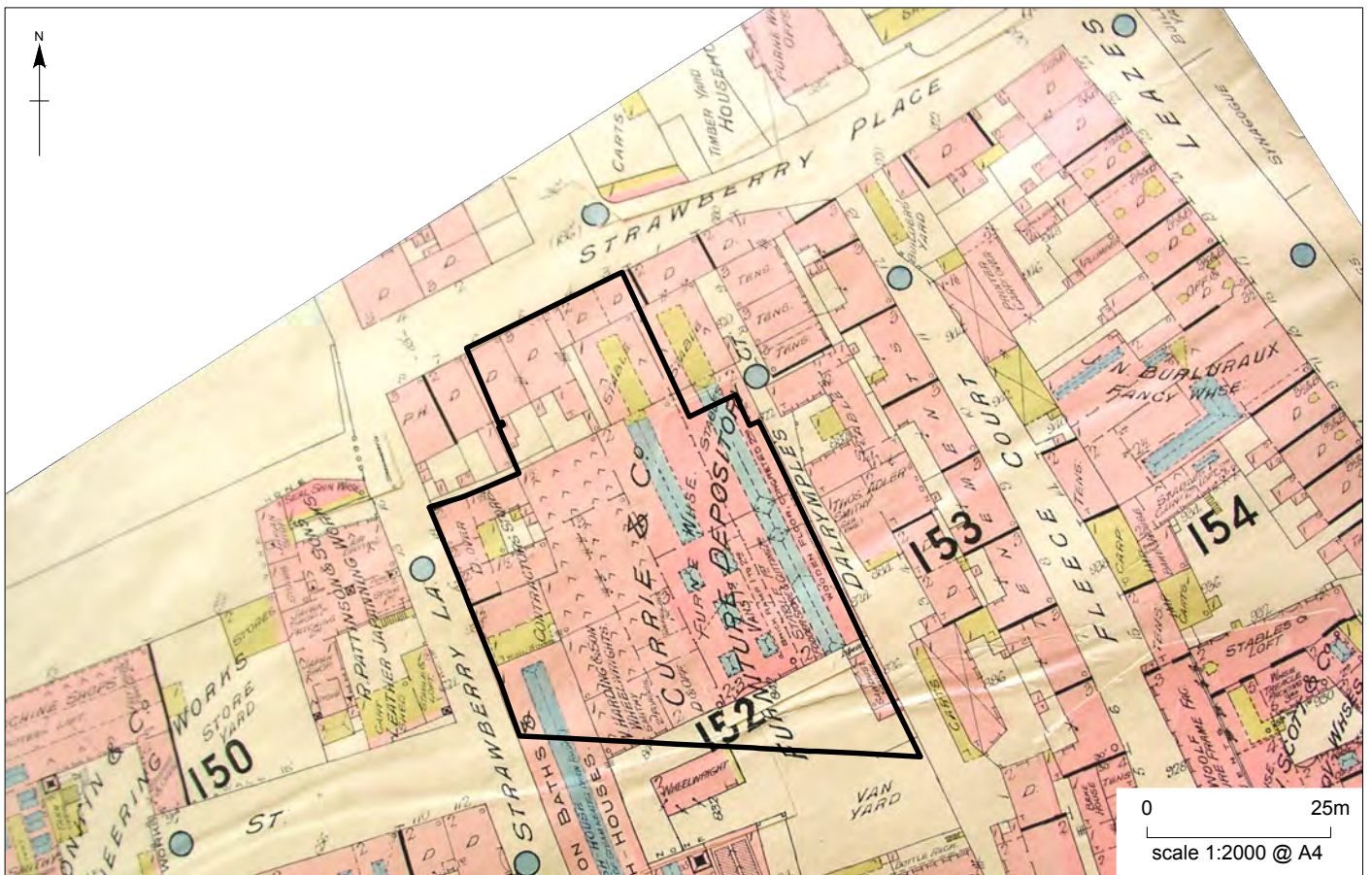
Figure 6



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One Strawberry Place, Newcastle: Ordnance Survey
First Edition map of 1860

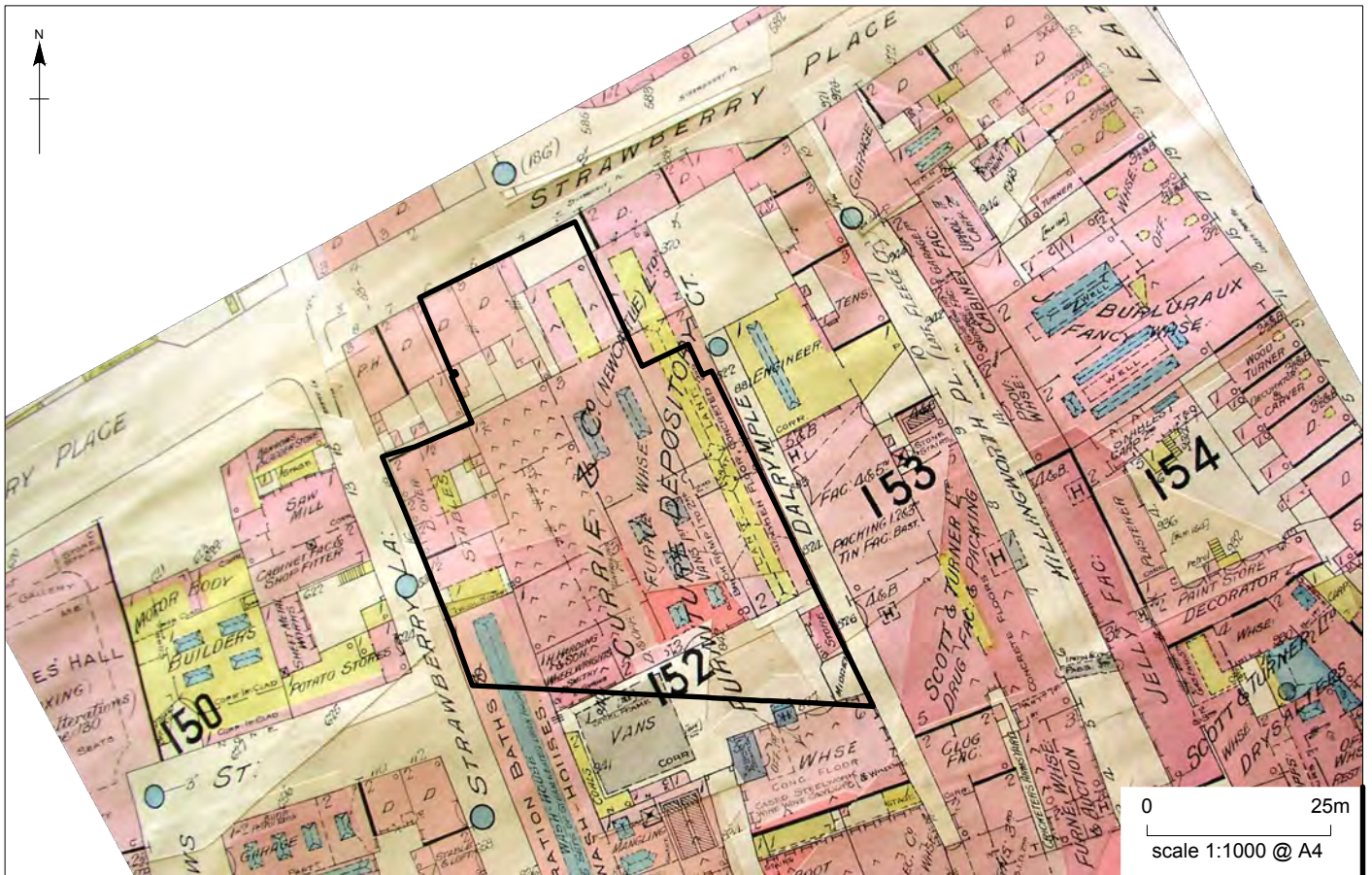
Figure 7



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One Strawberry Place, Newcastle: extract from the Goad Insurance
plan of 1896

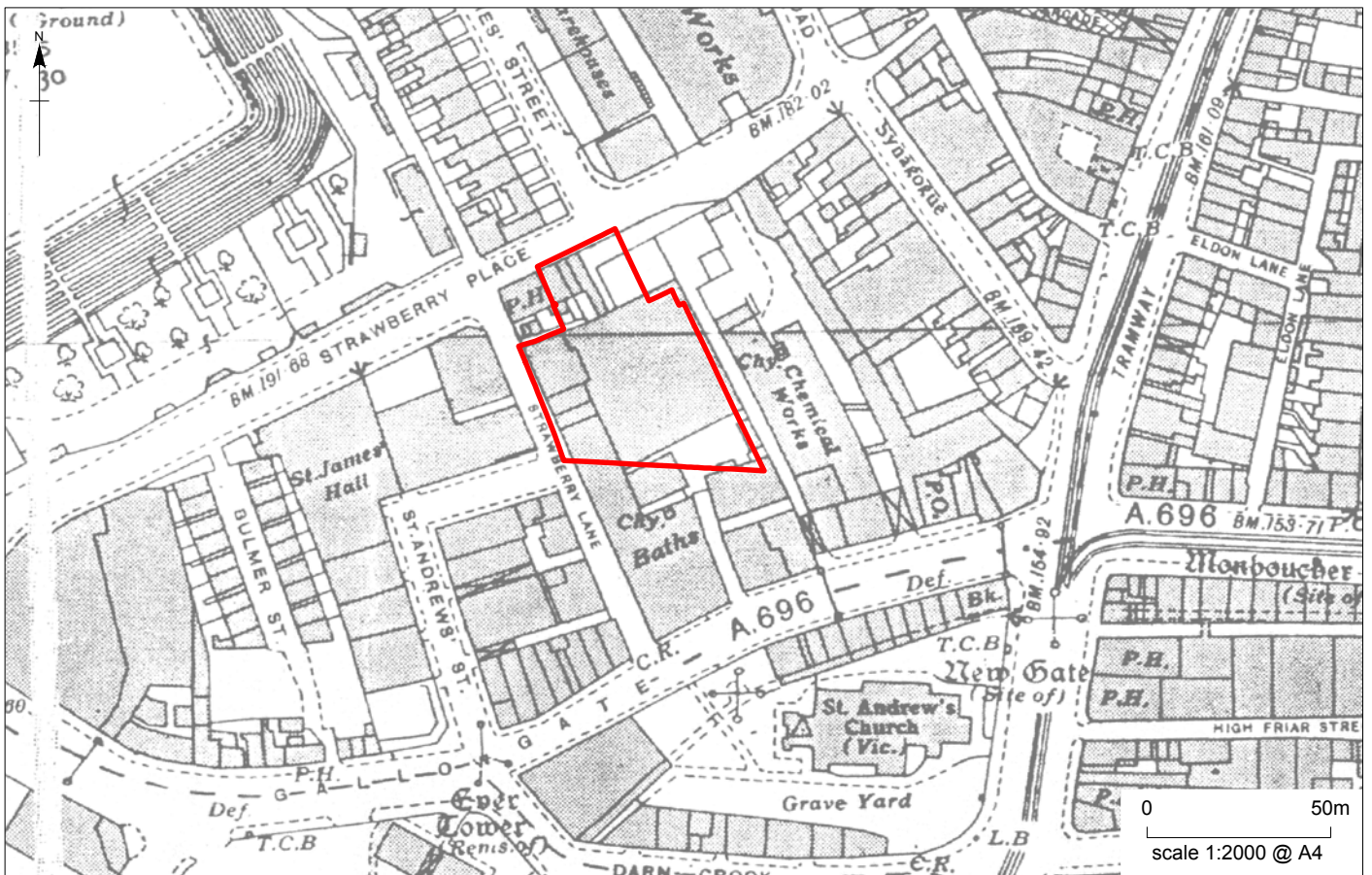
Figure 8



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One Strawberry Place, Newcastle: extract from the Goad Insurance plan of 1930

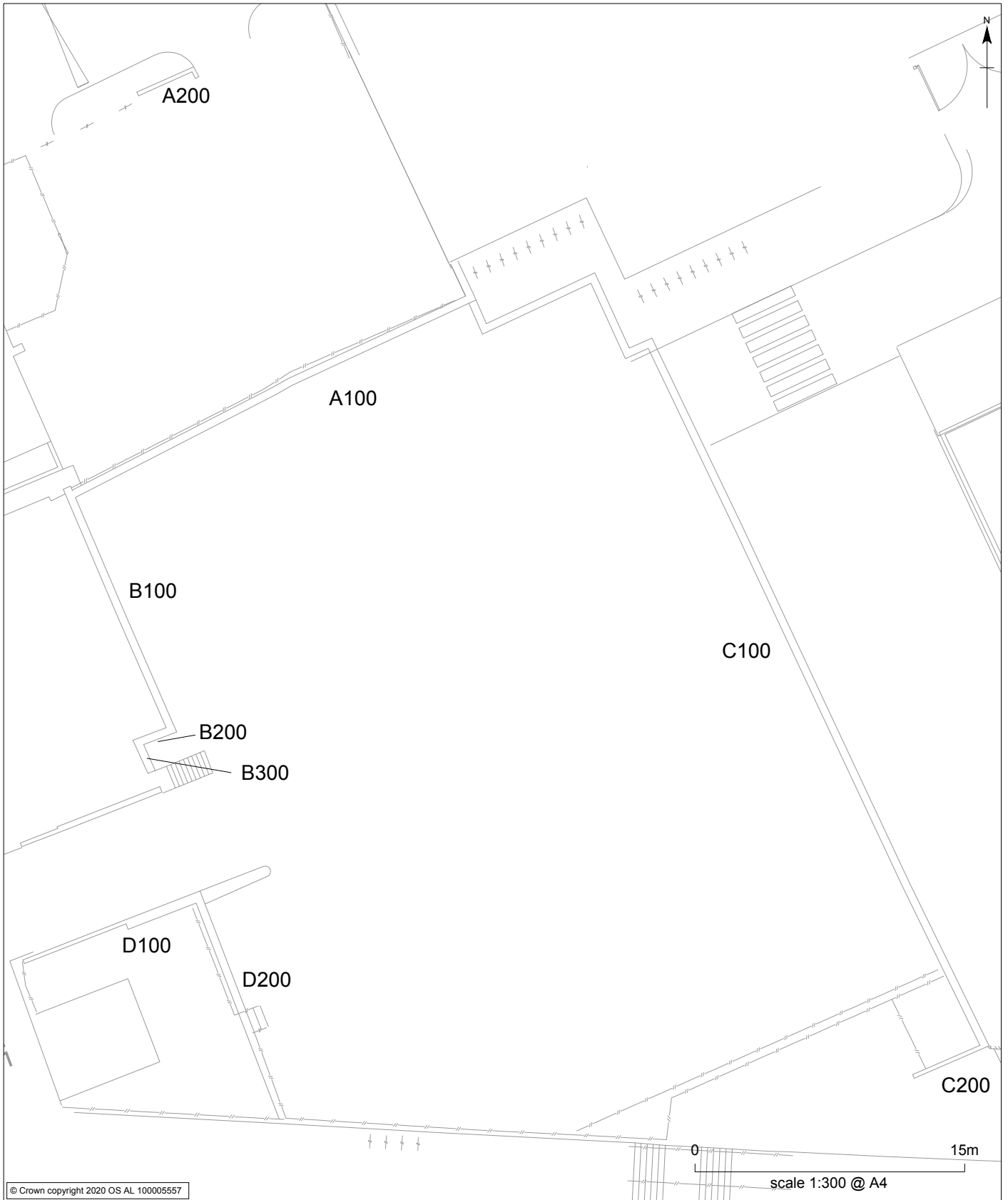
Figure 9



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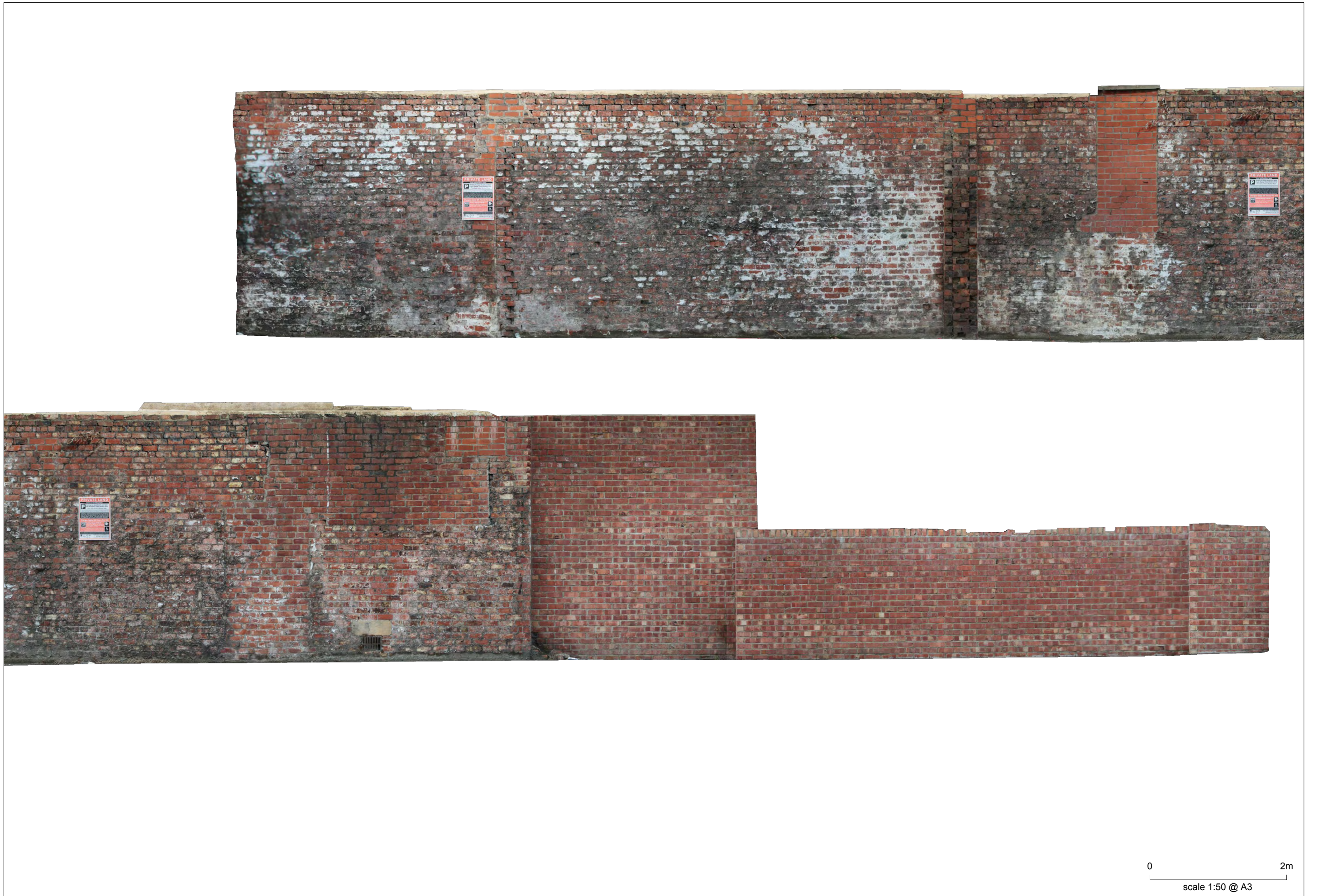
One Strawberry Place, Newcastle: Ordnance Survey map of 1940

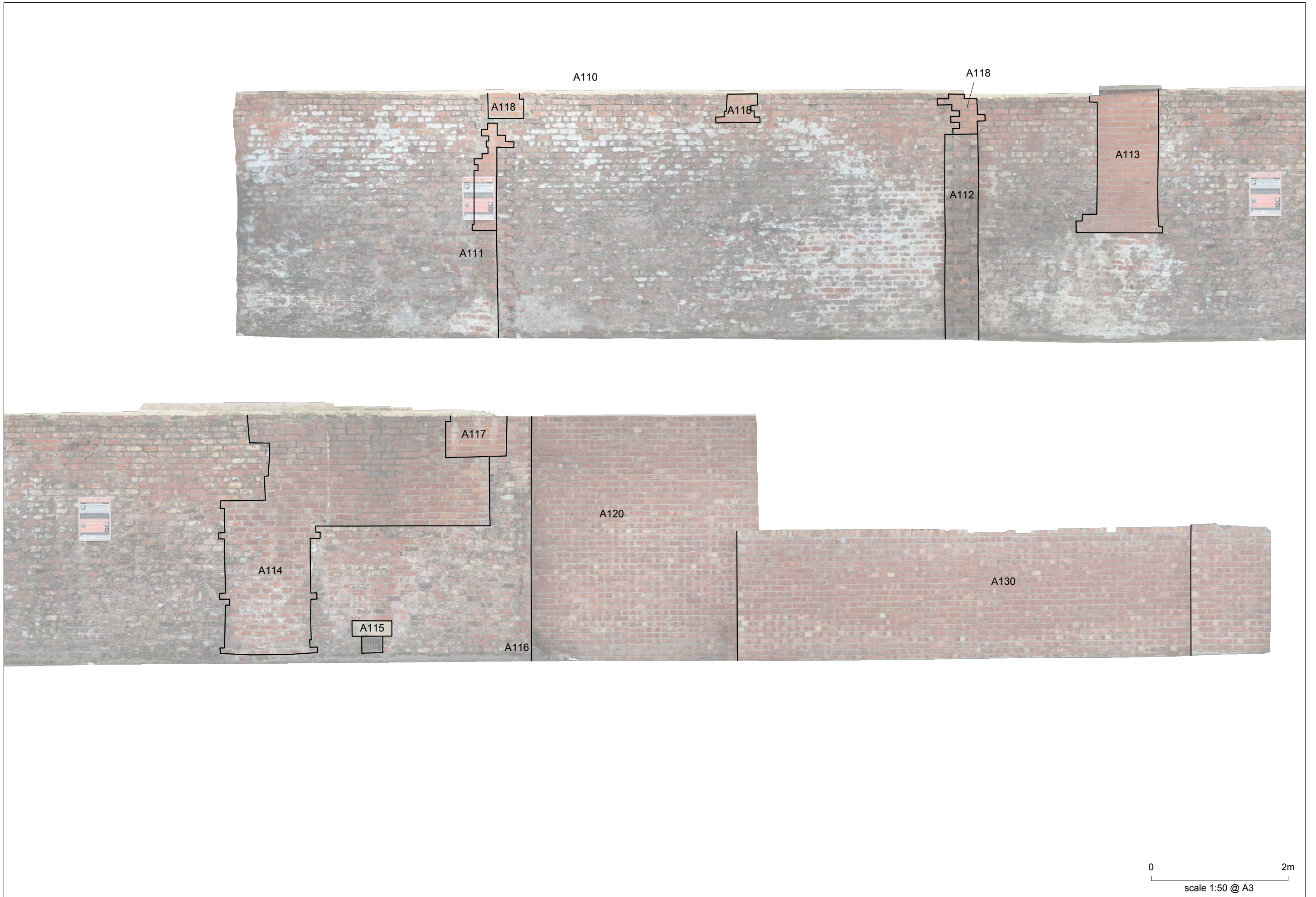
Figure 10



One Strawberry Place, Newcastle: wall numbers

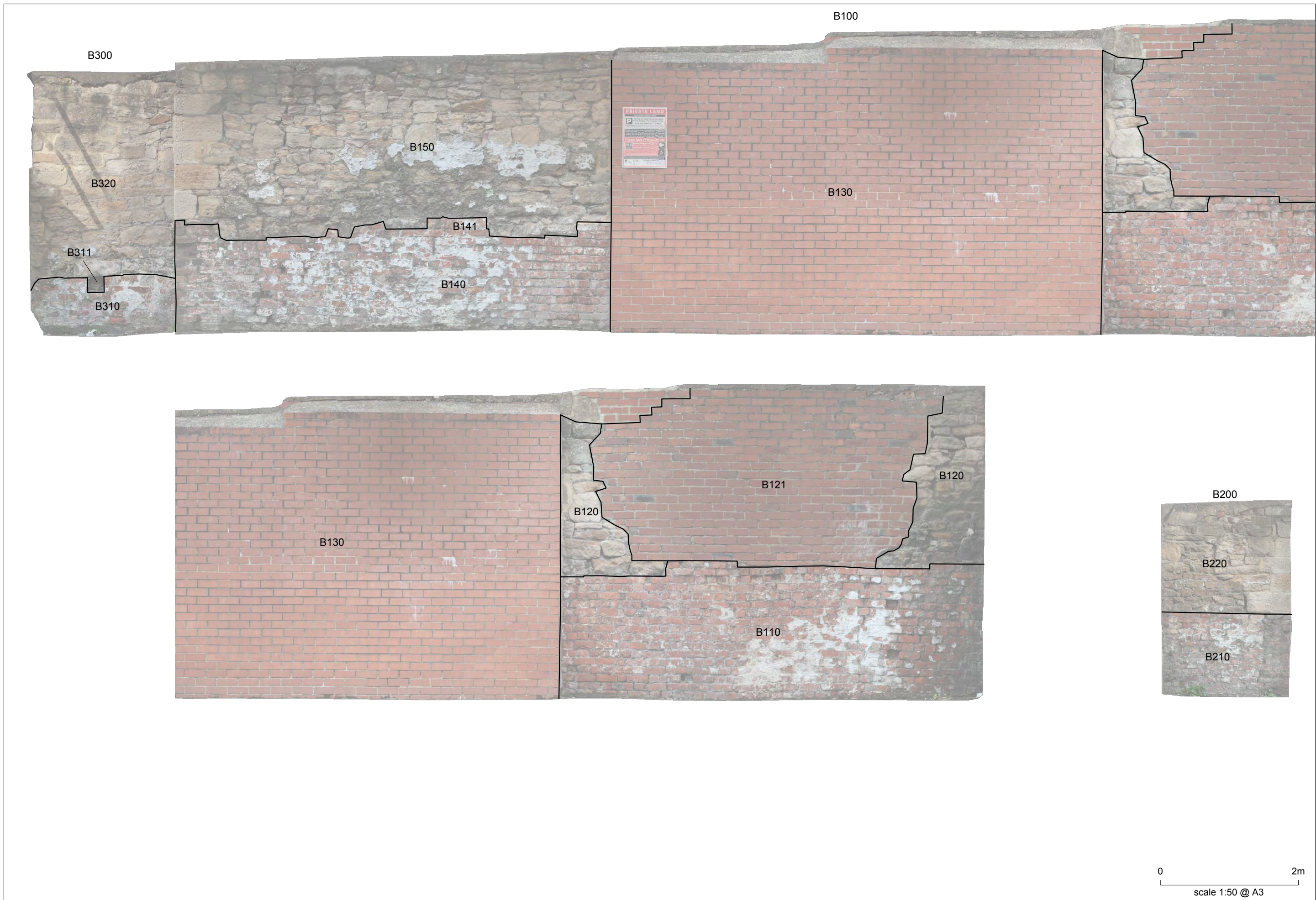
Figure 11



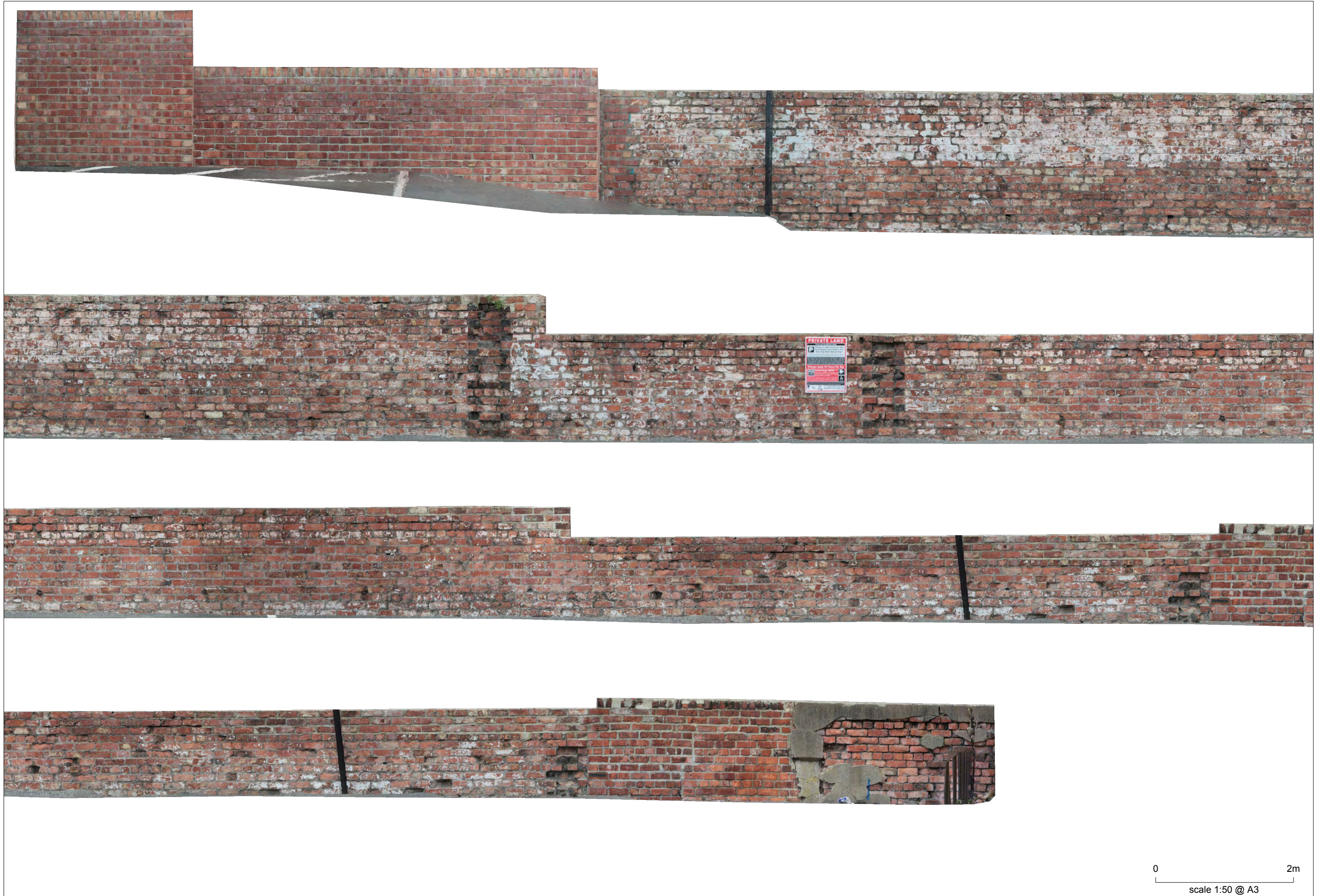


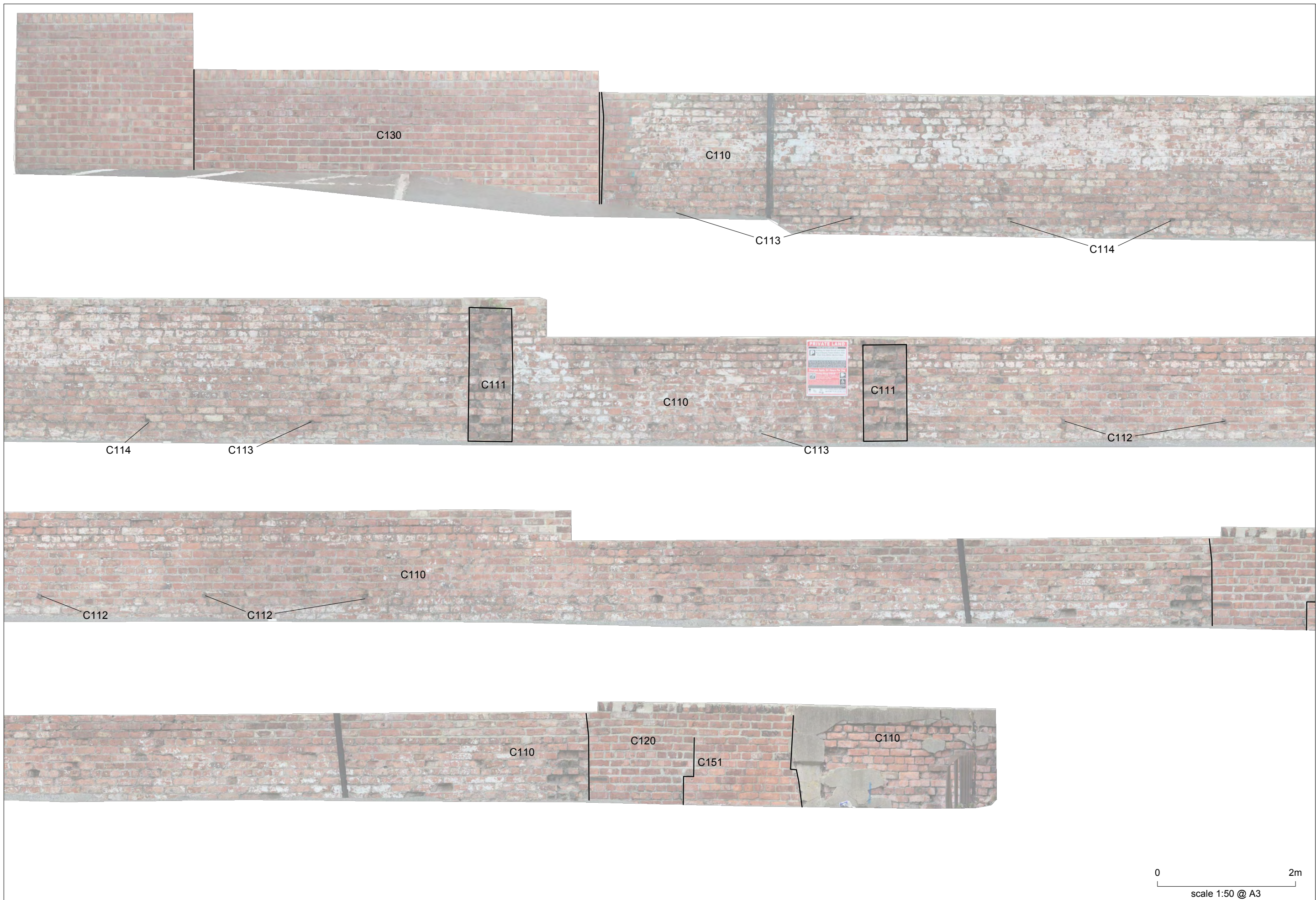


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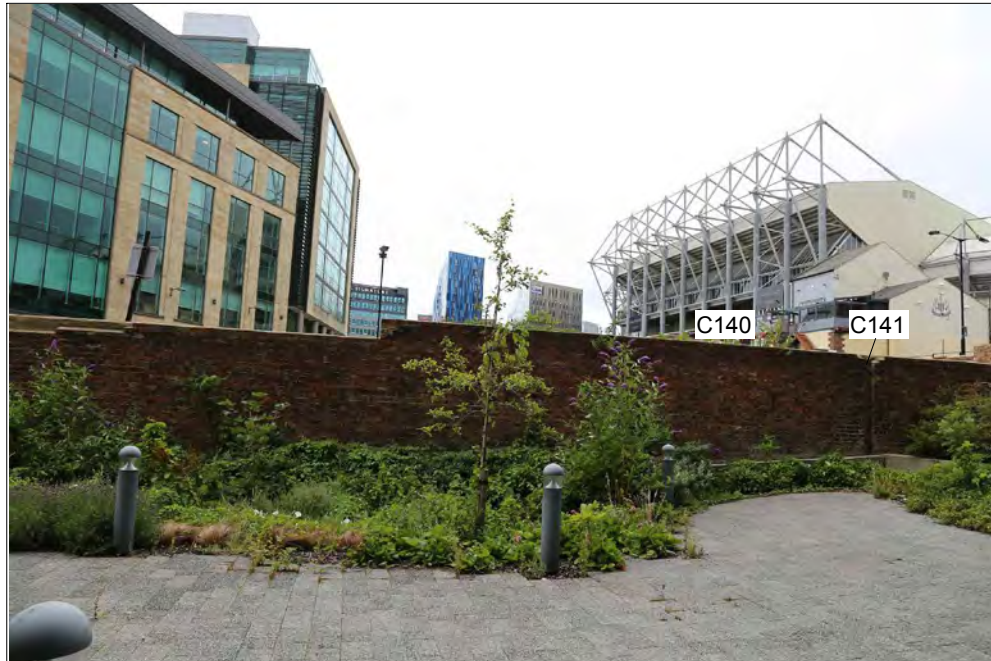




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One Strawberry Place, Newcastle: photo of east-facing elevation of wall C100

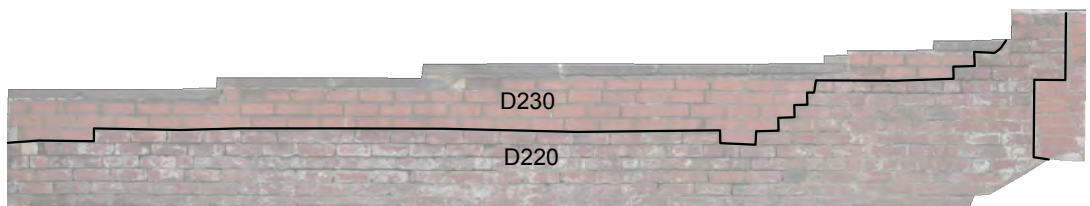
Figure 19



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One Strawberry Place, Newcastle: photo of east-facing elevation of wall C100

Figure 20



0 2m
scale 1:50 @ A3