

# Wellingborough Goods Shed Historic Building Recording Report Level 2

Prepared for Network Rail  
September 2019



Alan Baxter



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# Executive summary

This report has been commissioned by Network Rail to present the findings of a Level 2 investigation of the Goods Shed at Wellingborough Station undertaken in July 2019. The HER UID is ENN109608.

It consists of a written and photographic record of this typical mid-Victorian railway goods shed, which was built by the Midland Railway.

Unused for several decades, it has been identified by Historic England as one of the best preserved on the operational rail network and is listed at Grade II. In particular, it retains the internal wooden platform and its two goods cranes.

Recording precedes refurbishment and incorporation of the building into the public areas of platform 1.

# 1.0 Introduction

## 1.1 Site location and description

The site comprises the Goods Shed at Wellingborough Railway Station, located on Midland Road in Wellingborough, Northamptonshire (NN8 1NQ), approximately 1.5km from the Town Centre. The National Grid Reference for the site is SP 90351 68133.

Wellingborough Station opened in 1857 but has since undergone multiple alterations and extensions. Today it comprises

- the main station building to Platform 1, on the west side of the line, built in 1857 to designs by Charles Henry Driver for the Midland Railway, with later extensions
- distinctive 1857 cast-iron canopy to Platform 1, later extended south and north
- the Goods Shed to the south of the main building (separately listed at Grade II)
- platforms 2 and 3, which form an island between the east (slow) and west (fast) tracks, with an 1894 building with timber canopies
- 2011 footbridge with stepways and lifts across the south end of the station (excluded from the listing)
- The remnants of the disused Platform 4, on the east side of the tracks, with an early twentieth century building (excluded from the listing).



Figure 1: Site location

Contains Ordnance Survey data © Crown copyright and database right 2016

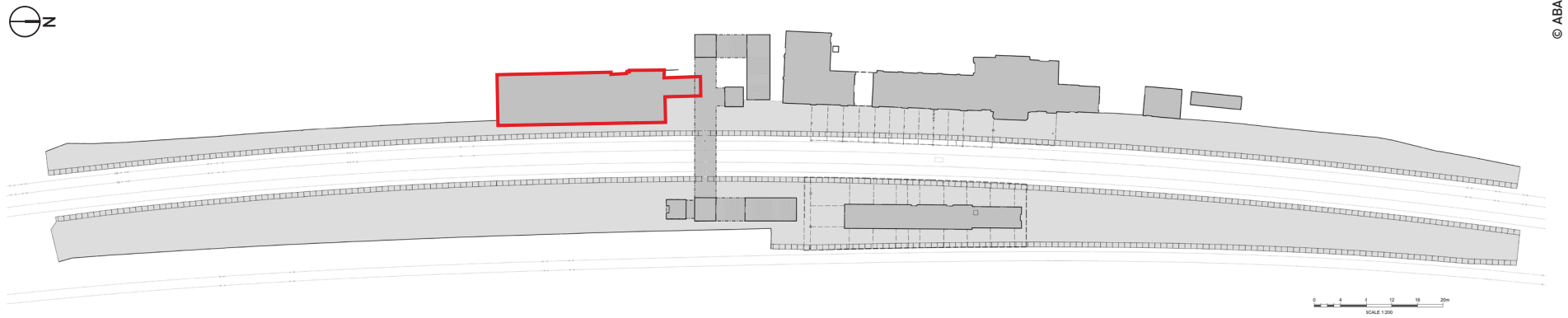


Figure 2: Site boundary

— Site boundary

## 1.2 Planning background

On 11 April 2019 listed building consent (application ref. WP/19/00082/LBC) was granted by Wellingborough District Council for alterations to the canopies of the station to allow for electrification of the Midland Main Line to Corby. Also included are works to the Goods Shed in order to bring it into public use as part of platform 1. These include repairing the envelope and installing a steel platform on the former trackbed connected to the existing platform 1 at each end.

A condition of this consent is that:

*7. No development shall take place within the area indicated until the applicant, or their agents or successors in title, has secured the implementation of a programme of archaeological work in accordance with a written scheme of investigation which has been submitted by the applicant and approved in writing by the local planning authority.*

*Reason: To ensure that features of archaeological interest are properly examined and recorded, in accordance with National Planning Policy Framework Paragraph 199.*

### 1.2.1 Designations

Wellingborough Station was listed at Grade II in 1981. The Goods Shed was separately listed at Grade II at this time (Appendix B).



## 2.0 The survey

### 2.1 Scope

The scope of the archaeological work was set out in detail in the Written Scheme of Investigation (Alan Baxter, 26 June 2019).

All work was carried out in accordance with Historic England specifications in *Understanding historic buildings: a guide to good recording practice* (HE 2016) and other applicable standards and guidance (e.g. CIFA *Standard and guidance for archaeological investigation of standing buildings or structures*, 2014).

The investigation of the site corresponds to a Level 2 standing building survey according to the Historic England specifications (HE 2016). A Level 2 investigation produces a descriptive record and is comprised of an analysis of the building's development and use.

The survey of the Goods Shed took place prior to alteration works. Both the exterior and the interior of the building were recorded.

### 2.2 Methodology of the survey

A site visit, undertaken by Alan Baxter on 24 July 2019, entailed the observation of the Goods Shed.

The investigation on site was non-intrusive and aimed at specific details in the fabric of the building relevant to the listed building consent application WP/19/00082/LBC. The information gathered from the site inspection was then combined with the information found in written sources to form a history and description of the building's development.

It is the nature of existing buildings that details of their construction and development may be hidden or may not be apparent from a visual inspection. The conclusions and any advice contained in this report – particularly relating to the dating and nature of the fabric – are based on our research, and on observations and interpretations of what was visible at the time of our site visits. Further research, investigations or opening up works may reveal new information which may require such conclusions and advice to be revised.

### 2.3 Authorship

This report has been prepared by Alan Baxter Ltd, as heritage consultants to Network Rail.

## 3.0 Historical background of the site

### 3.1 Wellingborough Station

Wellingborough Station dates to the construction of the Midland Railway's Leicester to Hitchin Line, which opened in 1857. The stations on that line were designed by Charles Henry Driver in a distinctive simplified Gothic style. Wellingborough was to the same design as Kettering, which was pictured in an *Illustrated London News* story on the opening of the route (23 May 1857).

The line through Wellingborough was quadrupled in 1882, and Platform 2 became an island between the lines. In 1894 the slow lines came into passenger use: the island was rebuilt with a new building and canopy to also serve Platform 3 on its east side, and Platform 4 was added on the east side of the tracks (but closed in 1959).

### 3.2 Wellingborough Goods Shed

Wellingborough Goods Shed is believed to have been designed and built as part of the Midland Railway station development in 1857, with matching architectural features. C. H. Driver, the architect of Wellingborough Station, may therefore have had a hand in its design.

It is an archetypal, small 'through-type' goods shed, as described in historian John Minnis' survey of the building type (Historic England, 2016). It was built to facilitate the arrival and despatch of all manner of general goods, serving the town of Wellingborough. Figure 3 shows how it operated, with a siding passing through, a platform with cranes for loading and unloading railway wagons on the siding, and cart bays for receiving and despatching goods via road to businesses and homes in the town.

The goods shed was extended to the south and to the west at some point prior to 1941, trebling its size (see Figure 5). That extension has since been demolished.

More information on the use and architectural style of goods sheds can be found in Historic England's *The Railway Goods Shed and Warehouse in England* (2016).

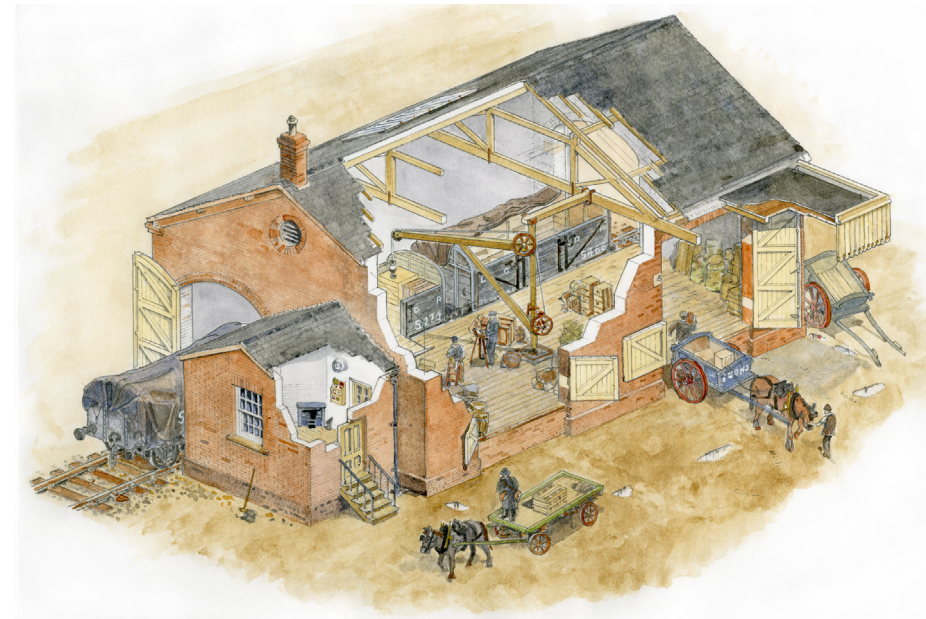


Figure 3: Cut away drawing showing the arrangement of a goods shed almost identical to Wellingborough. Reproduced from *The Railway Goods Shed and Warehouse in England*, John Minnis, Historic England

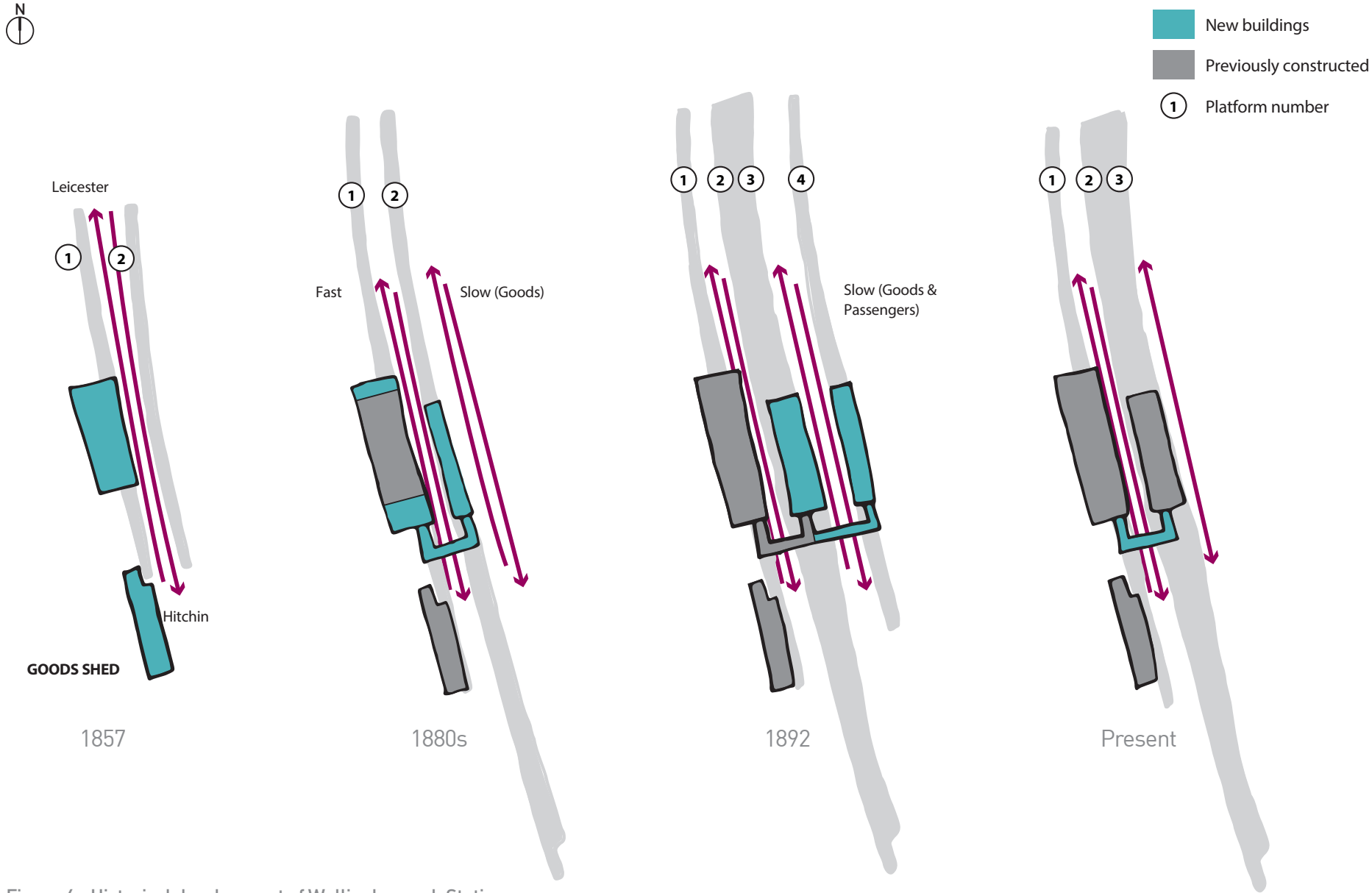


Figure 4: Historical development of Wellingborough Station

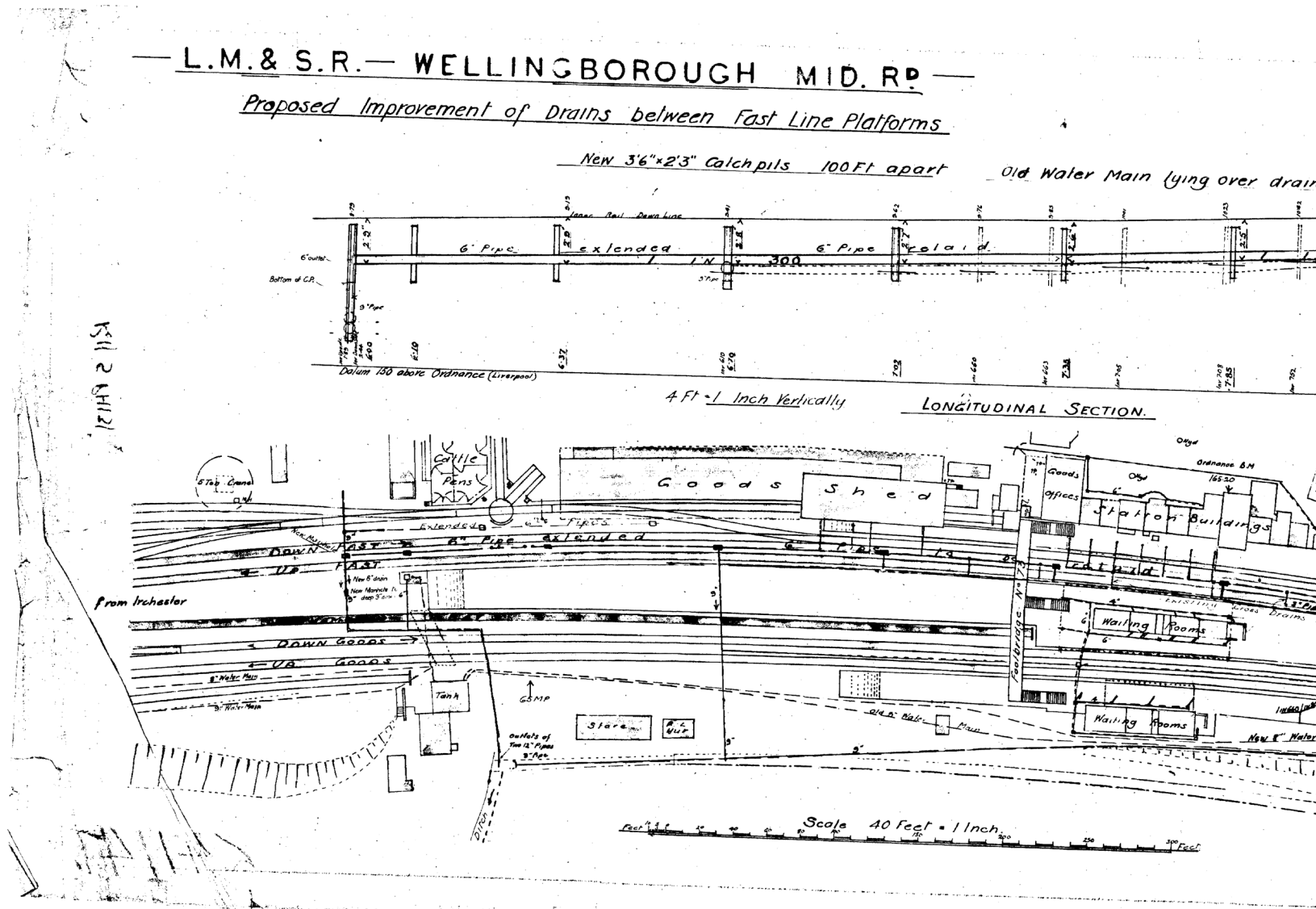


Figure 5: Drawing from the Network Rail Archives, dated 1941, showing a large extension to the goods shed on the south and west sides

## 4.0 Description of the standing fabric

### 4.1 Introduction

The Goods Shed at Wellingborough Station was photographed by the author on 24 July 2019. This section should be read in conjunction with the site photographs (Section 4.4).

### 4.2 Exterior

The Goods Shed is a single-storey, masonry building formed of two parts; the goods shed to the south and the goods office – a small, rectangular extension – to the north. These two parts do not appear to be bonded together. Unless this was an intended design feature, it is likely that the office was added later and the northern elevation made good between the two internal piers and up to the line of the arch. However, no evidence for this has been found in historic maps.

The general arrangement of the main shed has arches and trackbed for a siding running through the building alongside the eastern wall, and a loading platform filling the rest of the shed fitted with two cranes, and two cart loading bays and associated arches in the western elevation.

The building is currently accessed via a short flight of four steps which lead into the former goods office. The foundations are assumed to be corbelled strip footings on concrete.

The Goods Shed is constructed using polychromatic bricks laid in an English bond. The masonry walls appear to be continuous with local thickenings at piers forming six bays of four-centred-arch, blank arcades along the east and west elevations. Within each bay is a narrow, round-headed window with an iron frame and decorative latticed glazing bars. The sills are stone supported on two small, stone corbels.

On the east side there are two cart openings within the arched brickwork recesses, now missing their top hung timber sliding doors (though fixtures for these survive inside).

At some point the railway siding entry on the south side was substantially widened. This may have been associated with the large extension to the shed that is visible on old plans and photographs (see Figure 5; the date of its demolition is not known).

The siding entry on the north side appears to survive largely as built (as pictured at the matching shed at Sharnbrook on a postcard of c. 1908), though the lintel to the opening has been replaced with a steel beam.

The Goods Shed's gable roof is carried by timber wall plates which sit within the masonry walls. The slate roof covering and three skylights on its western side are carried by timber queen post trusses which also brace the internal hand cranes (see Section 4.4.10).

### 4.3 Interior

The interior is the most significant part of Wellingborough Goods Shed: it survives as well preserved example of a 'through-type' shed, demonstrating many characteristic features with its timber loading platform adjacent to the western wall, whitewashed interior, two 30cwt timber jib cranes fixed to the open roof trusses, rooflights, and attached goods office. The cranes are a particularly rare survival. The goods office retains some features, such as a fireplace.

## 4.4 Photographic record

### 4.4.1 Exterior – south elevation

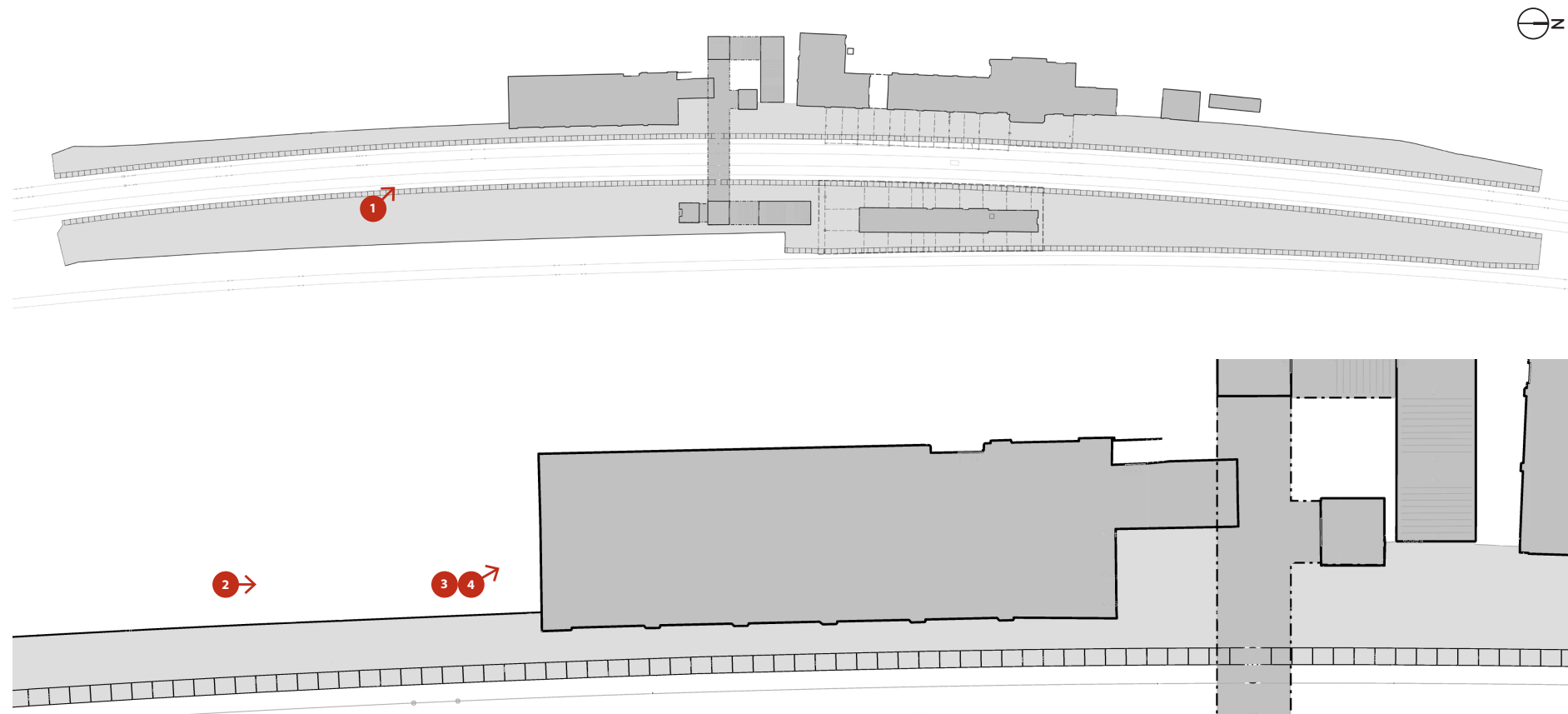


Figure 6: Key plan showing location and direction of photos of the exterior south elevation of the Good Shed

## 4.0 Description of standing fabric



South and east elevations as viewed from southern end of Platform 2



South elevation in relation to passenger bridge as viewed from the car park



South elevation



Detail of south elevation showing polychromatic brickwork and padstone supporting the steel lintel to the enlarged opening

4.4.2 Exterior – east elevation

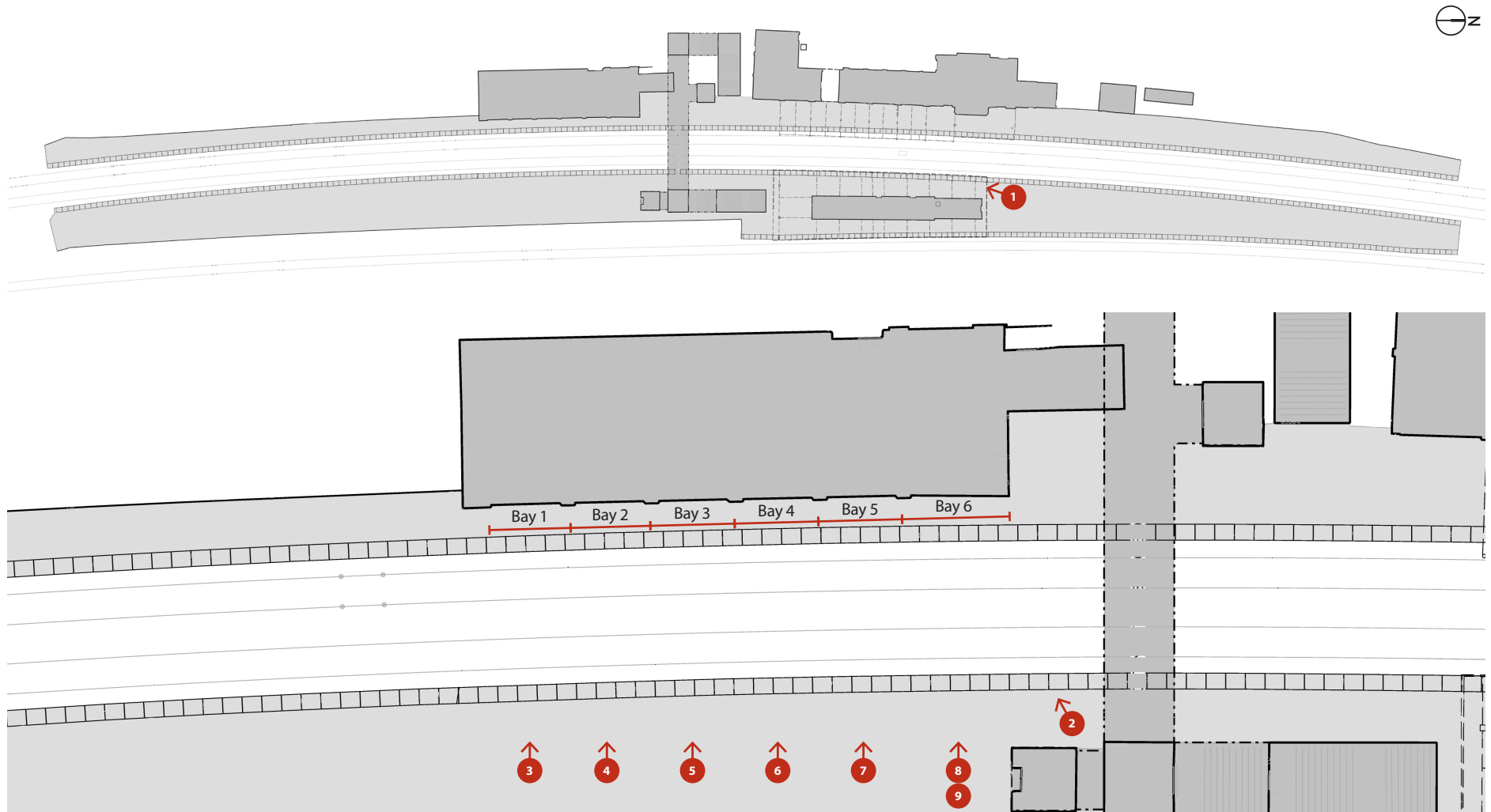


Figure 7: Key plan showing location and direction of photos of the exterior east elevation of Goods Shed



## 4.0 Description of standing fabric



North and east elevations in context of canopies and passenger bridge as viewed from northern end of Platform 2



North and east elevations as viewed from Platform 2



Bay 1



Bay 2



Bay 3



Bay 4

#### 4.0 Description of standing fabric



Bay 5



Bay 6



Detail of bay 6 showing the round-headed window

4.4.3 Exterior – north elevation

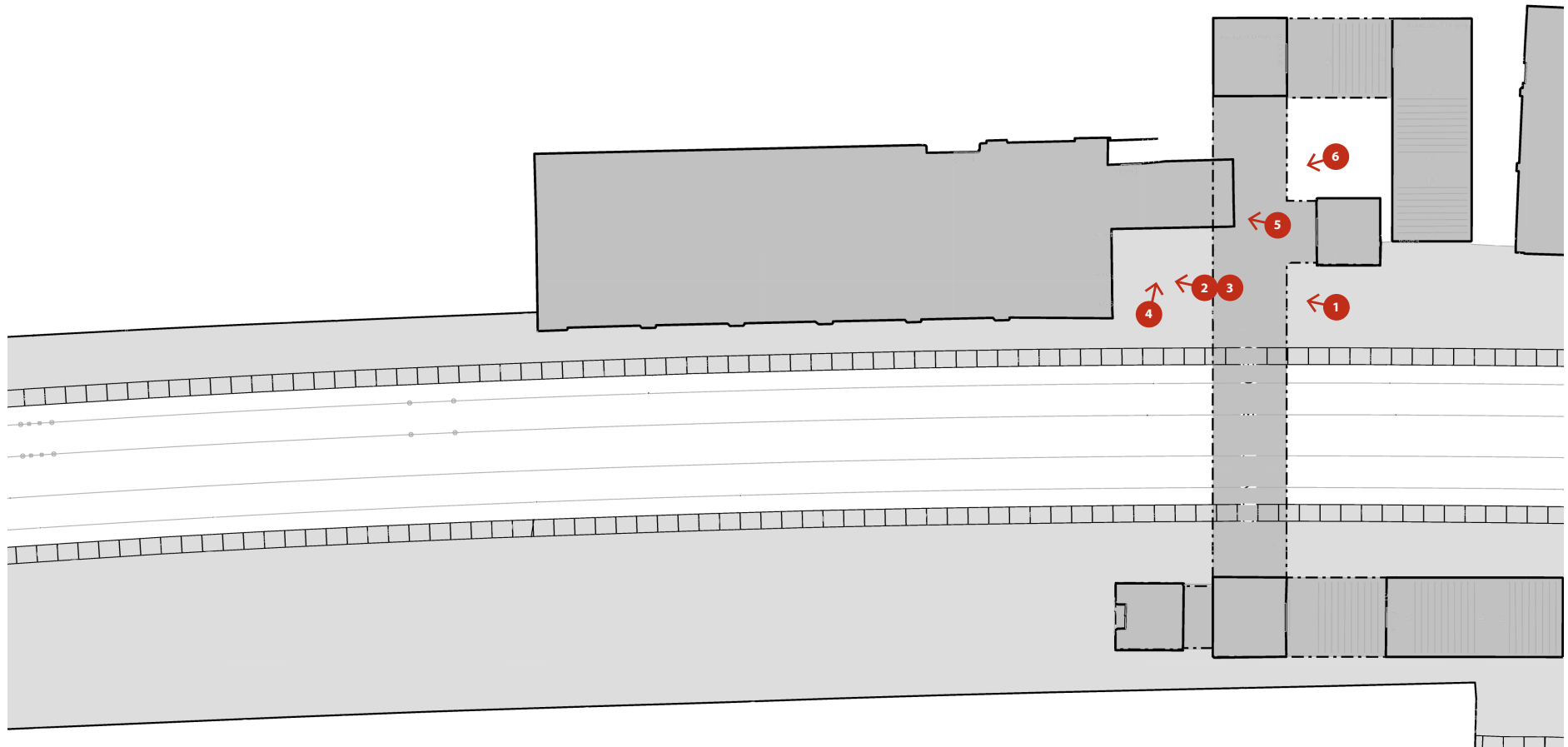


Figure 8: Key plan showing location and direction of photos of the exterior north elevation of Goods Shed

#### 4.0 Description of standing fabric



General view of north elevation as viewed from Platform 1



Detail of north elevation showing existing repair to brickwork



Detail of north elevation showing the west support to the lintel over the wagon entry opening



East elevation



North elevation



North and west elevations of goods office and good shed beyond

4.4.4 Exterior – west elevation

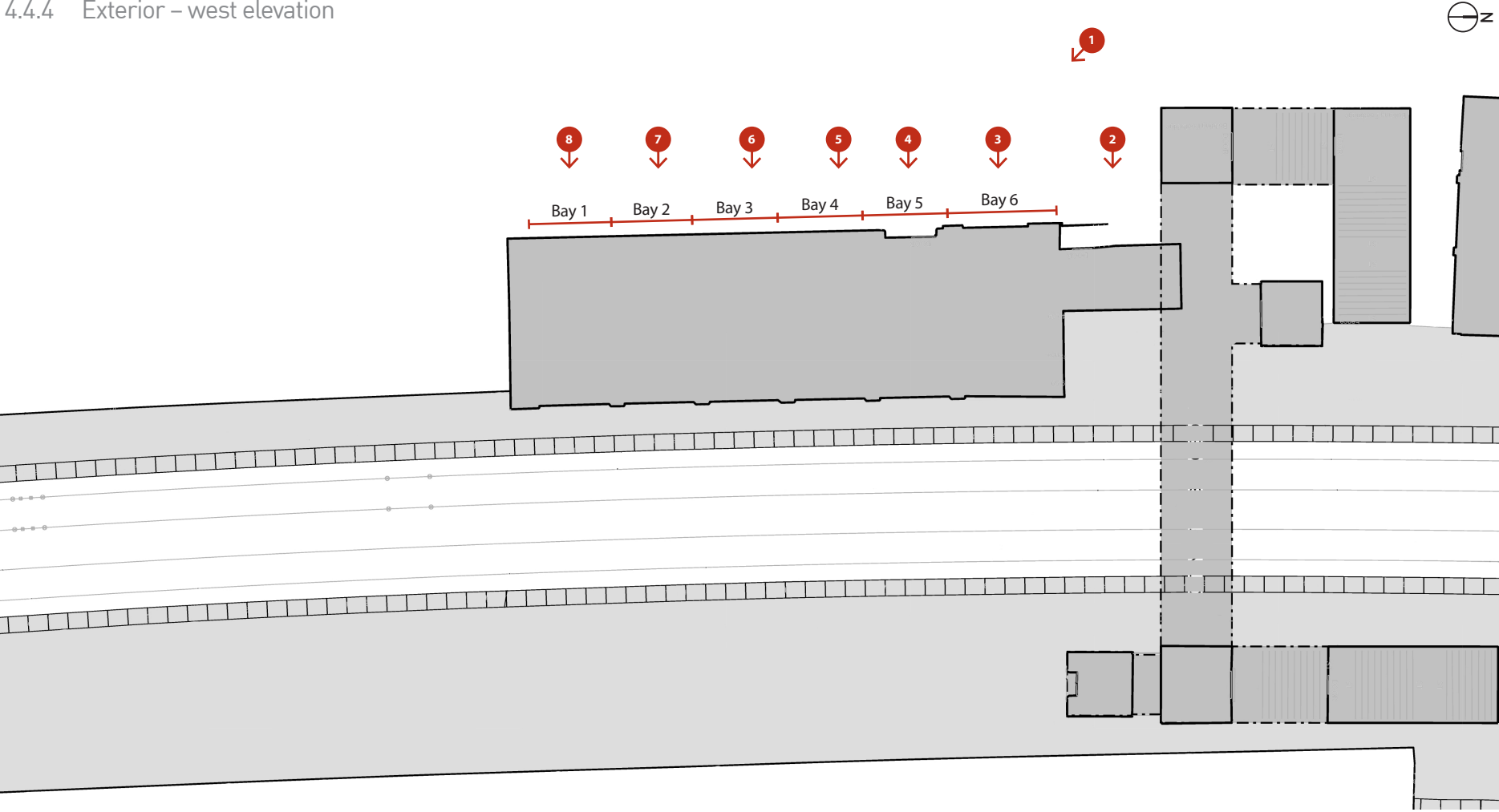


Figure 9: Key plan showing location and direction of photos of the exterior west elevation of Good Shed

## 4.0 Description of standing fabric



West elevation of goods office and goods shed as viewed from the car park



Detail showing the goods office and relationship to passenger bridge



Bay 1



Bay 2



Bay 3



Bay 4



Bay 5



Bay 6

4.4.5 Interior – Goods Office

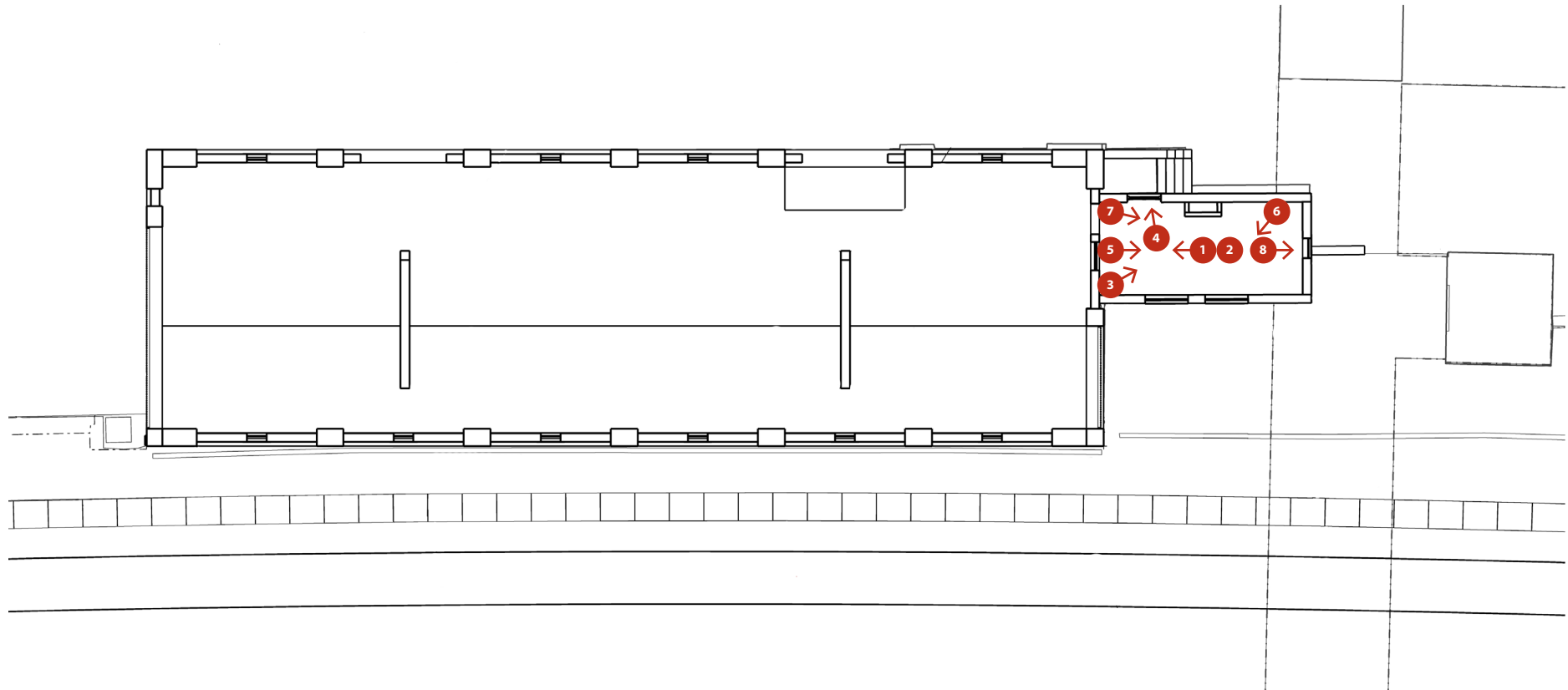


Figure 10: Key plan showing location and direction of photos of the interior of the Goods Office



#### 4.0 Description of standing fabric



South elevation



Detail of south elevation showing internal gazed window



North elevation



Detail of west elevation showing external door



Detail of north elevation showing round headed window



East elevation showing boarded up windows onto Platform 1

#### 4.0 Description of standing fabric



Detail showing the timber flooring



Detail showing the ceiling access hatch in the centre of the room

4.4.6 Interior – east elevation

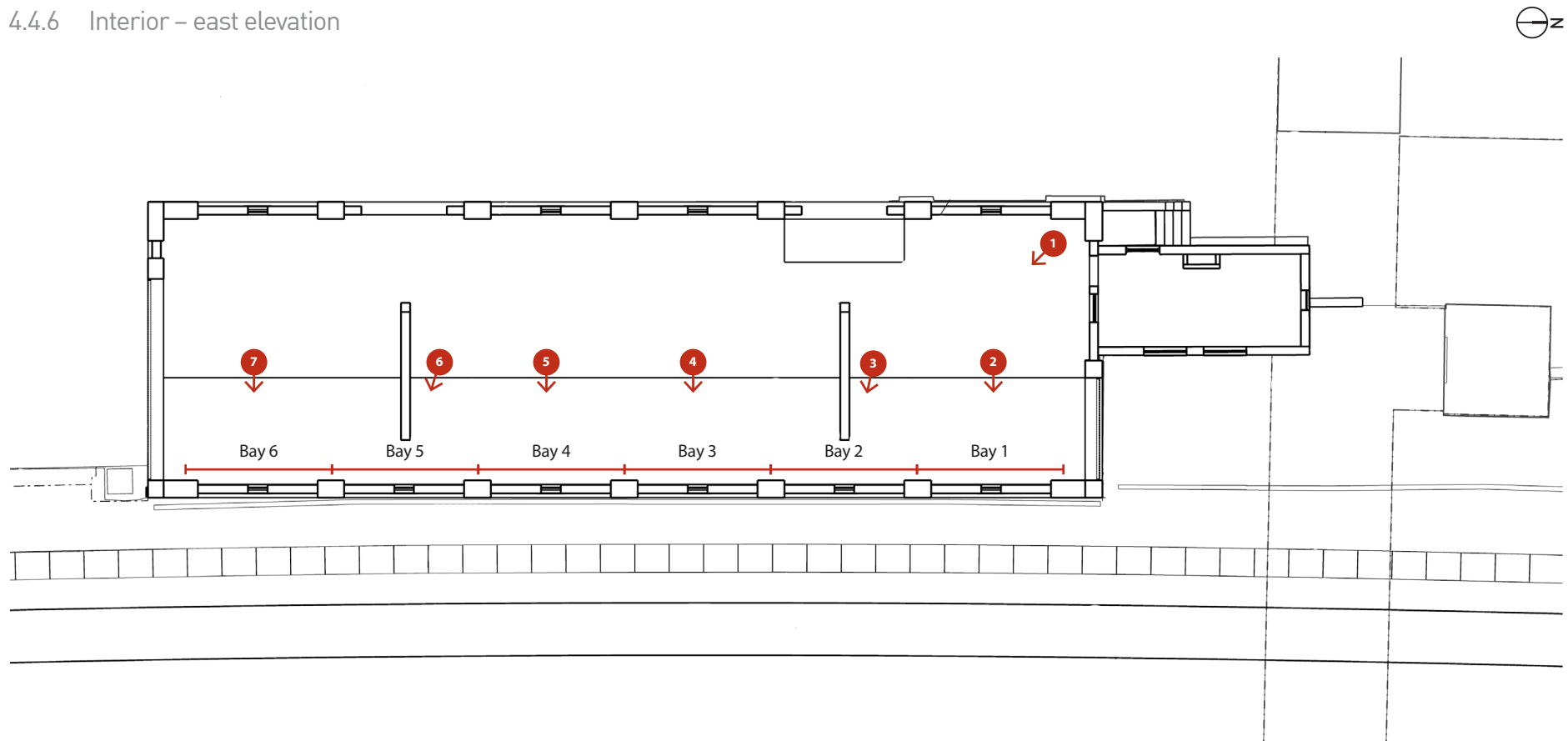


Figure 11: Key plan showing location and direction of photos of the interior east elevation of the Goods Shed

## 4.0 Description of standing fabric



General view of the interior from the north west corner



Bay 1



Bay 2 and northern jib crane



Bay 3



Bay 4



Bay 5 and southern jib crane



Bays 6

4.4.7 Interior – south elevation

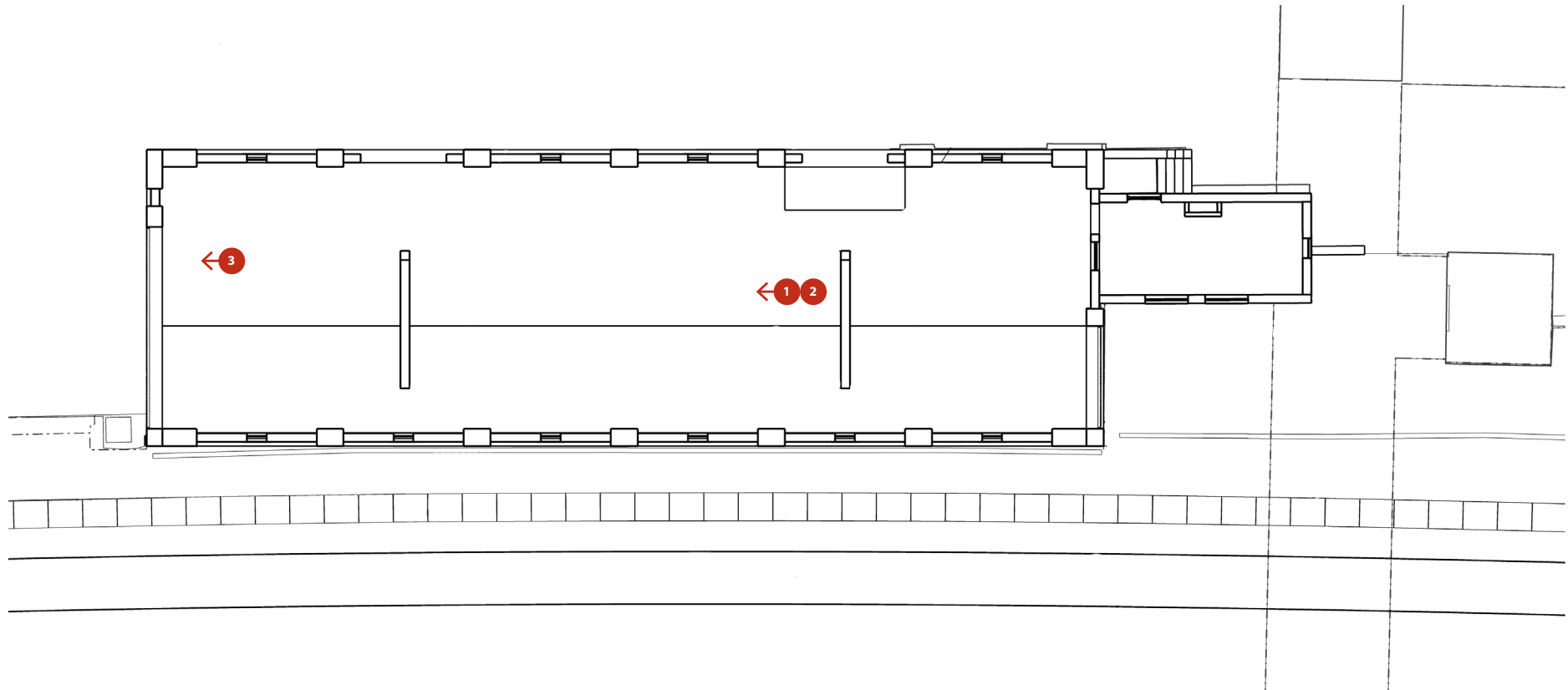


Figure 12: Key plan showing location and direction of photos of the interior south elevation of Goods Shed



General view of interior looking south



South elevation and jib crane



Detail showing the enlarged wagon entry opening

4.4.8 Interior – west elevation

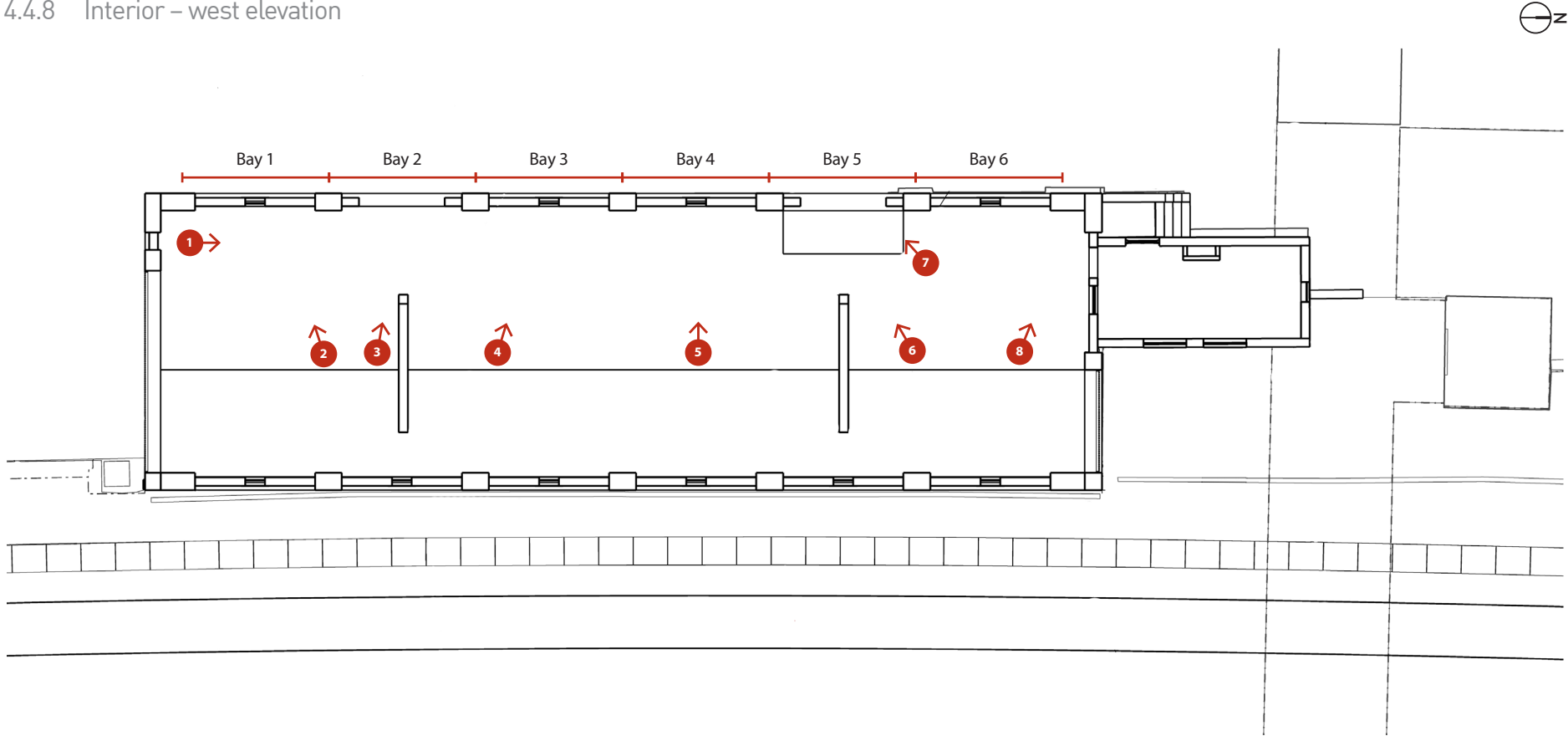


Figure 13: Key plan showing location and direction of photos of the interior west elevation of Goods Shed



4.0 Description of standing fabric



General view of interior looking north



Bay 1



Bay 2 - south opening



Bay 3



Bay 4



Bay 5 - north opening



Detail of Bay 5 showing lowered floor around opening



Bay 6

4.4.9 Interior – north elevation

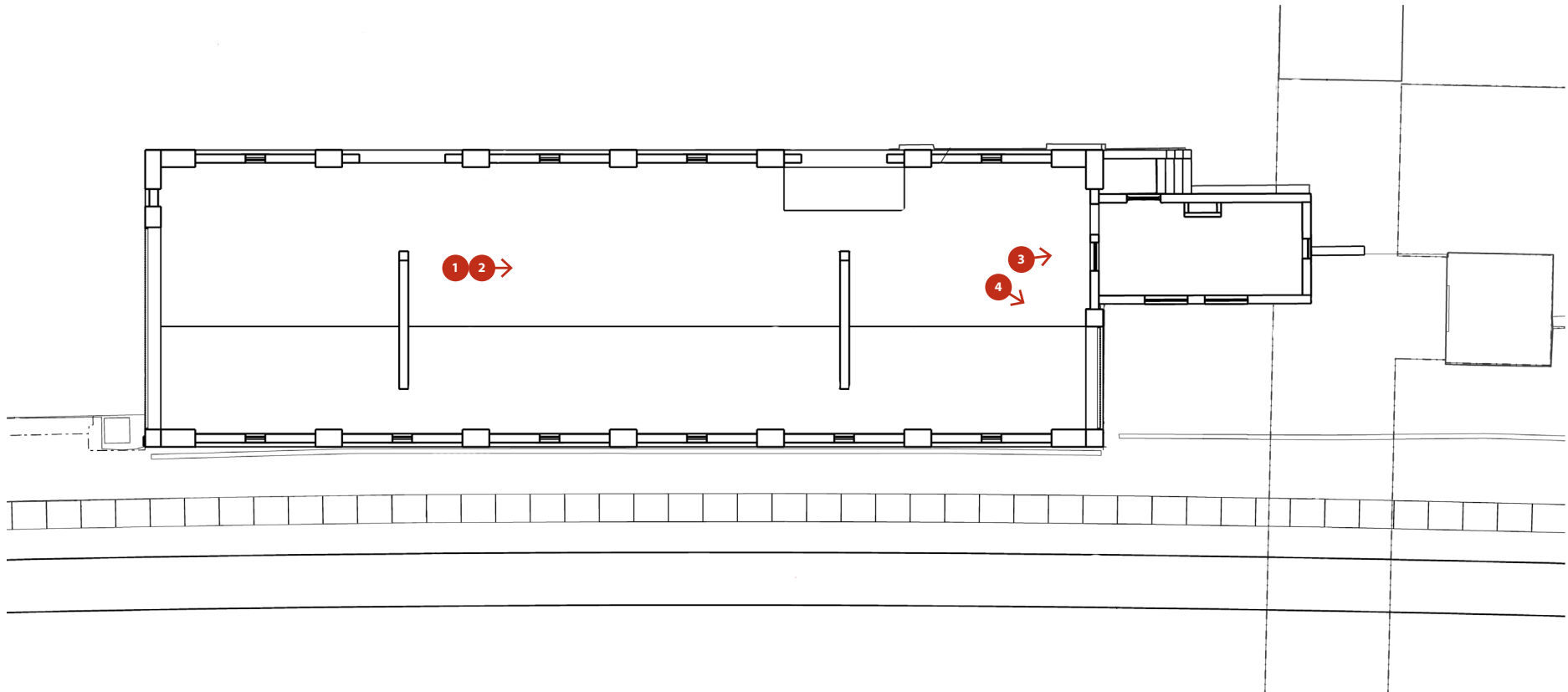


Figure 14: Key plan showing location and direction of photos of interior north elevation of Goods Shed

## 4.0 Description of standing fabric



General view of interior looking north



North elevation and jib crane



Detail showing door and window openings into goods office



Detail showing wagon entry opening

4.4.10 Interior – cranes

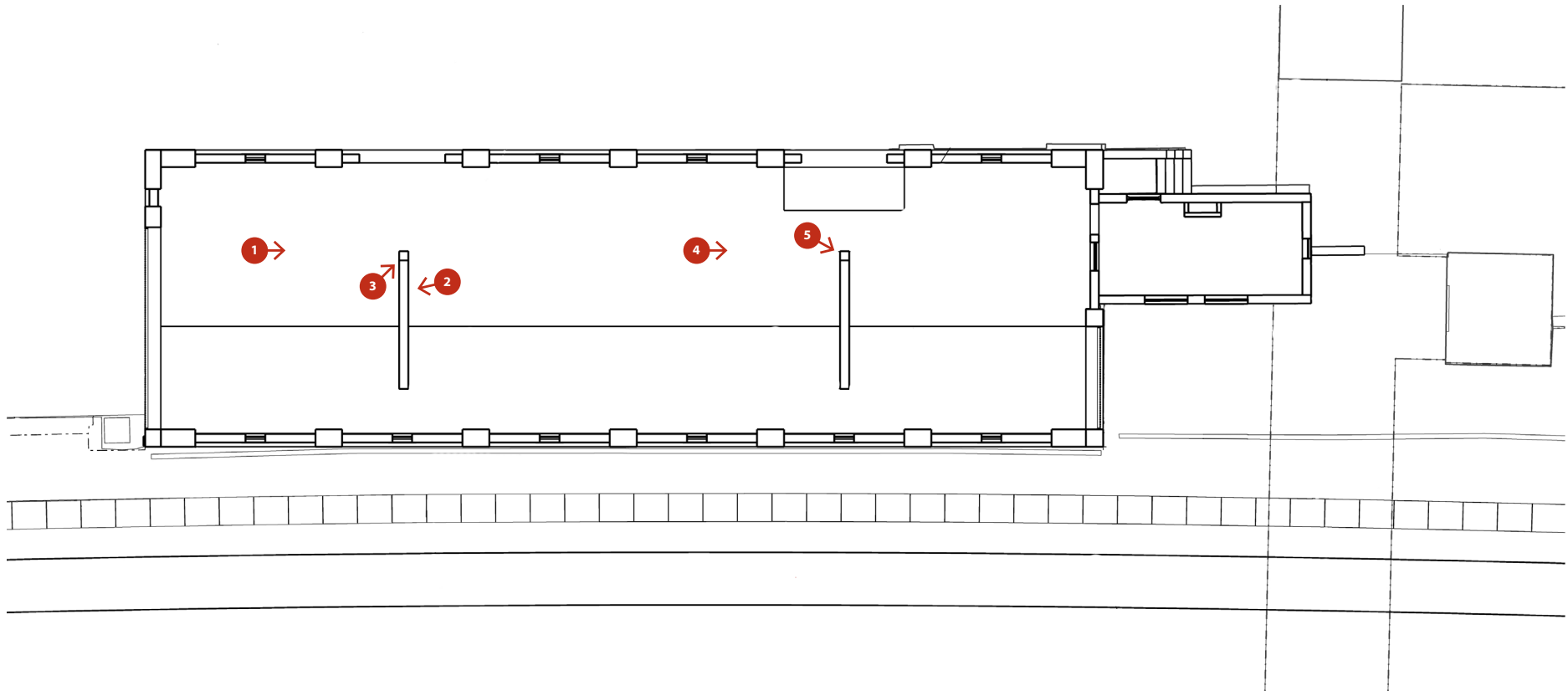


Figure 15: Key plan showing location and direction of photos of the interior cranes in the Goods Shed

#### 4.0 Description of standing fabric



South jib crane

1



Detail showing part of winding mechanism

2



Detail showing base

3



North jib crane

4



Detail showing connection of jib crane to timber roof structure

5

4.4.11 Interior – floor

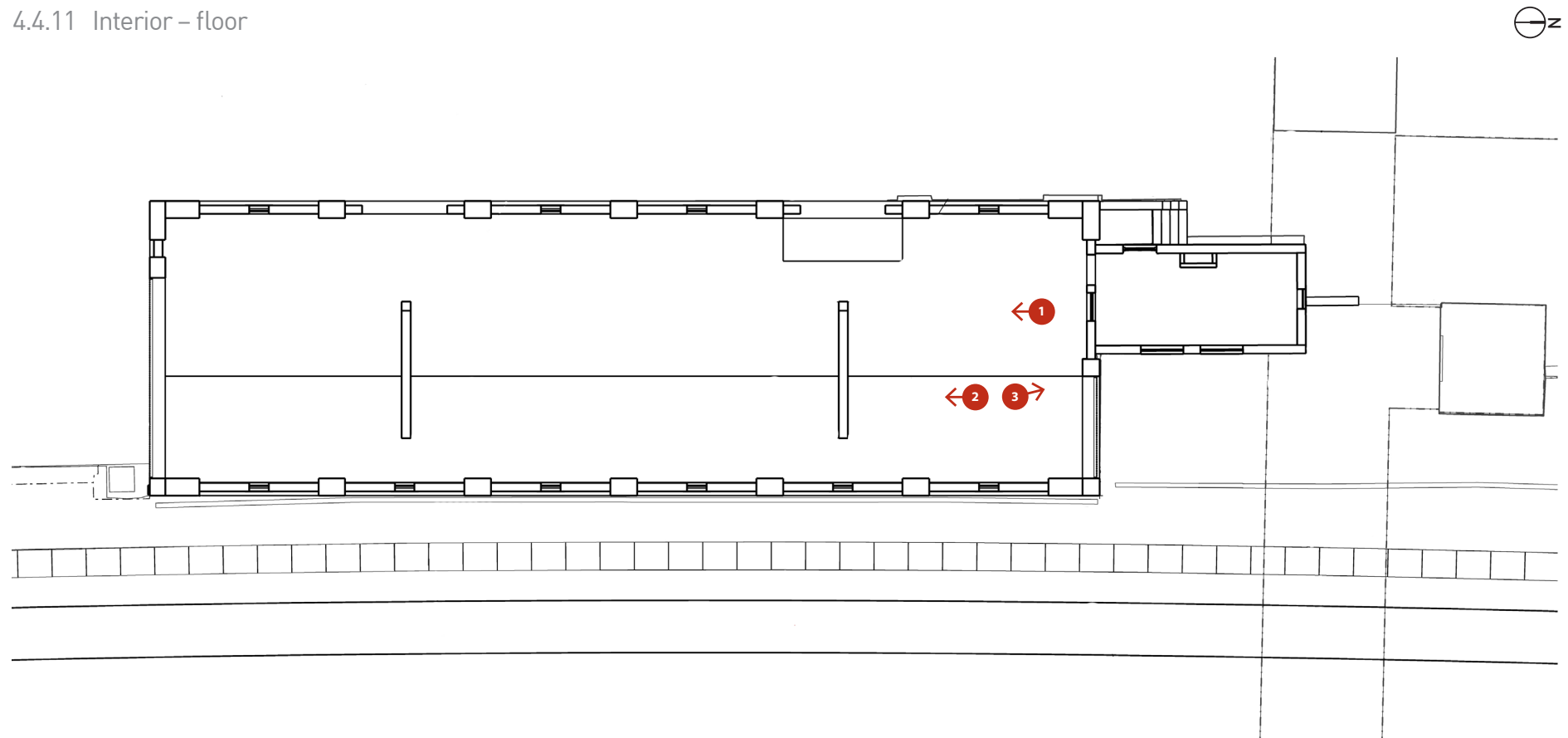


Figure 16: Key plan showing location and direction of photos of the interior floor of the Goods Shed

#### 4.0 Description of standing fabric



General view of timber boarding of loading platform



Detail of edge of loading platform looking south



Detail of edge of loading platform looking north



4.4.12 Interior – roof

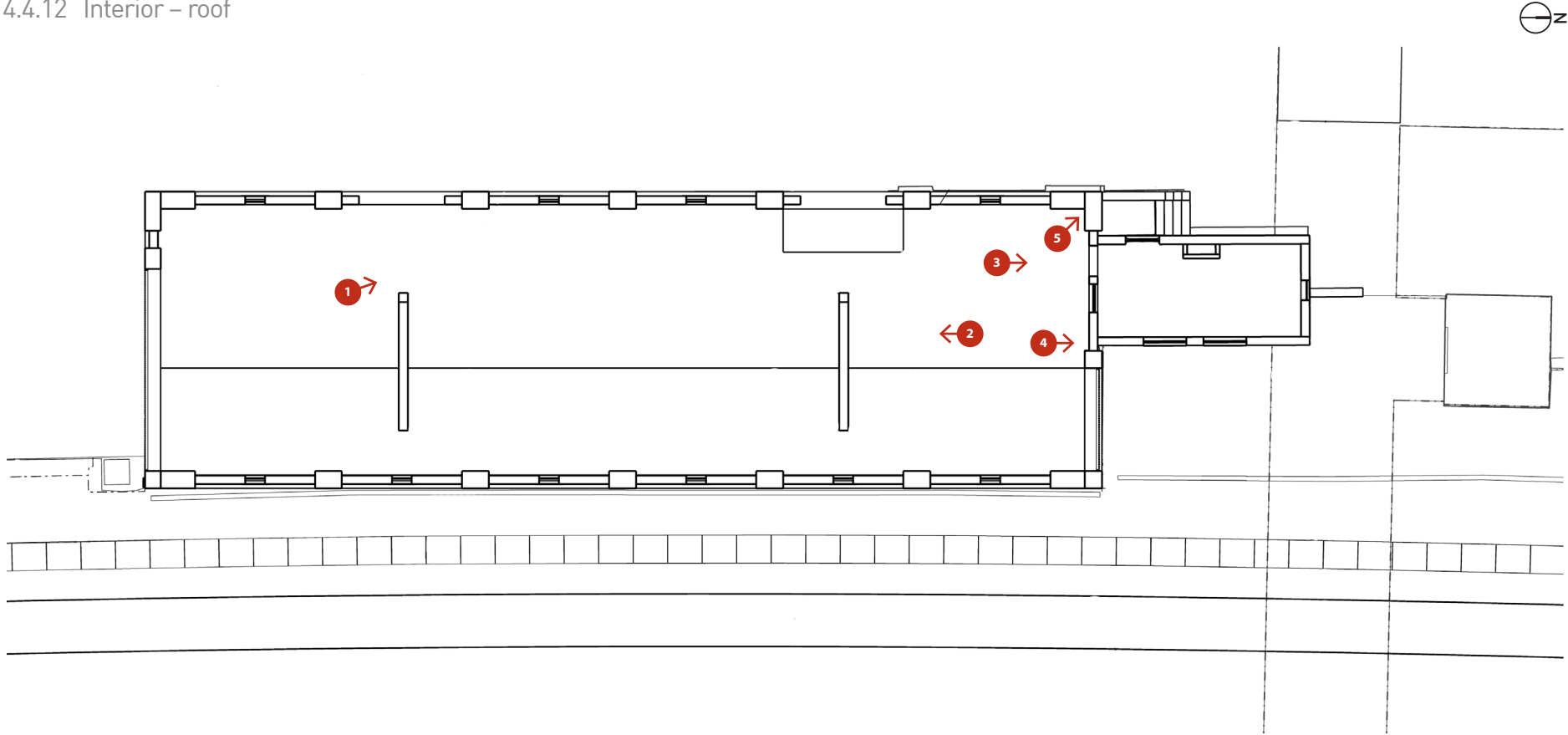


Figure 17: Key plan showing location and direction of photos of interior roof structure of Goods Shed

## 4.0 Description of standing fabric



General view of roof structure from southern end



View of timber truss by central skylight



View of timber truss by north elevation



Detail of ridge



Detail of truss end

## 5.0 Publication and archiving

Information on the results of the survey will be made publicly available by means of a database in digital form, to permit inclusion of the site data in any future academic research.

In view of the potential of the material it is suggested that a summary on the results of the Standing Building Survey project, possibly including figures or photographs, should appear in an appropriate academic journal.

The site archive of original records will be deposited digitally to ADS-easy. They will be stored under the following UID event number:

**ENN109608**

# 6.0 Bibliography and references

## 6.1 Books and journals

Biddle, G, *Britain's Historic Railway Buildings* (2011)

Hendry, R., *An Historical Survey of Selected London, Midland and Scottish Railway Stations* (1986)

Laxton, H., *Examples of Building Construction*, (1858)

Minnis, John, *The Railway Goods Shed and Warehouse in England* (Historic England, 2016)

*Illustrated London News*, (23 May 1857)

## 6.2 Other

Alan Baxter & Associates, *Midland Main Line Statement of History and Significance* (2013)

CIFA (2014) *Standard and guidance for archaeological investigation of standing buildings or structures*

Historical Ordnance Survey mapping

Historic England (2016) *Understanding historic buildings: a guide to good recording practice*

Network Rail, National Records Centre, drawings of various dates

## Appendix A: Historic England guidance on recording

In 2016, Historic England produced *Understanding Historic Buildings – A guide to good recording practice*. This best practice guide is the industry standard for the recording of historic buildings. It sets out four levels plus one of building recording, with escalating requirements for a written account, photography and drawing, depending on the circumstances, purpose and need. Level 1 is the simplest of the four levels and Level 4 is the most detailed; an additional option is a photographic survey. The document also provides guidance for the preservation and dissemination of records.

As the document is very clear, its purpose is to provide guidance, not to precisely prescribe requirements, because every site is unique and every project has different needs. As an overarching principle, however, the recording should be proportionate to the importance of the heritage asset and the degree of change to it that is proposed.

Importance is determined by designation: nationally listed buildings, followed by locally listed and then the rest. A building of high significance with many original features undergoing some minor alterations may require a higher level of recording than a building of low significance with few interior features that is being demolished.

Section 5 of Historic England's guidance describes the 5 levels of recording, this information is summarised below:

- Level 1 is a **basic visual record** supplemented by the minimum of information needed to identify the building's location, age and type. Exterior photography is sufficient.
- Level 2 is a **descriptive record**. Both interior and exterior photos are required along with written information including a short summary of the building's development and use. Some measured drawings may accompany this.
- Level 3 is an **analytical record** that will comprise a systematic account of a building's origins, development and use. The evidence that has been gathered will also be appraised. Drawings as well as photographs, plus information about construction materials, will be required to illustrate the building's structure and support the historical analysis. Level 3 may be appropriate when the fabric of the building is under threat of where the scope for detailed documentary research is limited. This level is suitable for the dismantling of a structure prior to re-erection. The drawn and photographic record will be comprehensive and will be made prior to, and during, dismantling.
- Level 4 is a **comprehensive analytical record** and is appropriate for buildings of special importance. This level builds upon level 3 by including further comment on the architectural, social and historical significance of the building in a wider context, and preparing further detailed drawings.
- Photographic survey is a different type of record primarily providing a **full visual record** of a site but without an equal level of written or drawn level. This option is appropriate when drawings and analysis of the historic sources have been taken care of in the past.

# Appendix B: National Heritage List Entry

## GOODS SHED APPROXIMATELY 20 METRES SOUTH OF WELLINGBOROUGH STATION

### Overview

Heritage Category: Listed

Building Grade: II

List Entry Number: 1040597

Date first listed: 05-May-1981

Date of most recent amendment: 12-Mar-1986

Statutory Address: GOODS SHED APPROXIMATELY 20 METRES SOUTH OF  
WELLINGBOROUGH STATION, MIDLAND ROAD

### Location

Statutory Address: GOODS SHED APPROXIMATELY 20 METRES SOUTH OF  
WELLINGBOROUGH STATION, MIDLAND ROAD

The building or site itself may lie within the boundary of more than one  
authority.

County: Northamptonshire

District: Wellingborough (District Authority)

National Grid Reference: S P 90344 68081

### Details

WELLINGBOROUGH MIDLAND ROAD SP96NW (South side) 5/130 Goods shed  
approx. 20m. S of 05/05/81 Wellingborough station (Formerly listed as goods  
shed at Wellingborough Station)

### GV II

Goods shed. c.1857, by C.H. Driver. Red brick with yellow and blue brick  
dressings and slate roof. Single unit plan. Single storey. Elevation to former  
goods yard of 6 bays of blank arcade of pointed arches with loading bay  
openings between second and fourth arches and single-light arch-head  
windows to other arches. All with Midland Railway style, cast iron lozenge  
glazing. Gable end to right has C20 modified entrance. Elevation to platform  
is similar with blank arcades and single-light windows. Interior retains loading  
bays and original wooden roof structure with cast-iron columns which support  
2 hand cranes which are attached to the building.

Listing NGR: SP9034468081

### Legacy

The contents of this record have been generated from a legacy data system.

Legacy System number: 233788

Legacy System: LBS

# Appendix C: OASIS form

OASIS ID: alanbaxt2-363020

## Project details

Project name	Wellingborough Goods Shed Recording
Short description of the project	Recording of the Goods Shed at Wellingborough Station prior to work repair and repainting works as well as works associated with the electrification and upgrading of the station.
Project dates	Start: 24-07-2019 End: 30-08-2019
Previous/future work	Yes / Yes
Any associated project reference codes	WP/19/00082/LBC - Planning Application No.
Any associated project reference codes	1040597 - NHLE No.
Type of project	Building Recording
Site status	Listed Building
Current Land use	Transport and Utilities 2 - Other transport infrastructure
Monument type	GOODS SHED Post Medieval
Significant Finds	GOODS SHED Post Medieval

Methods & techniques	"Photographic Survey","Survey/Recording Of Fabric/Structure"
Prompt	Planning condition
Prompt	Listed Building Consent
Project location	
Country	England
Site location	NORTHAMPTONSHIRE WELLINGBOROUGH WELLINGBOROUGH Goods Shed approximately 20 metres south of Wellingborough Railway Station
Postcode	NN8 1NQ
Study area	0 Kilometres
Site coordinates	SP 490345 268080 51.937182473808 -1.286662605801 51 56 13 N 001 17 11 W Point
Project creators	
Name of Organisation	Alan Baxter Limited
Project brief originator	Local Planning Authority (with/without advice from County/District Archaeologist)

Project design originator Patrizia Pierazzo  
 Project director/manager Victoria Bellamy  
 Project supervisor Victoria Bellamy  
 Type of sponsor/funding body Network Rail

#### Project archives

Physical Archive Exists? No  
 Digital Archive recipient ABA  
 Digital Archive ID 1769/10  
 Digital Contents "none"  
 Digital Media available "Images raster / digital photography"  
 Paper Archive recipient ABA  
 Paper Archive ID 1769/10  
 Paper Contents "none"  
 Paper Media available "Plan", "Report"

#### Project bibliography 1

Publication type Grey literature (unpublished document/  
 manuscript)

Title Wellingborough Goods Shed:  
 Recording Report - Level 2  
 Author(s)/Editor(s) Bellamy, V.  
 Other bibliographic details 1769/10  
 Date 2019  
 Issuer or publisher Alan Baxter Limited  
 Place of issue or publication London  
 Description A4 Landscape  
 Entered by Victoria Bellamy (vbellamy@alanbaxter.co.uk)  
 Entered on 9 August 2019





# Alan Baxter

**Prepared by** Victoria Bellamy

**Reviewed by** Vicky Simon

**Issued** September 2019

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