

# Wellingborough Station platform canopies

## Historic Building Recording Report

### Level 3

Prepared for Network Rail  
September 2019





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# Executive summary

## Planning context

This report has been commissioned by Network Rail to present the findings of a Level 3 investigation of the canopies at Wellingborough Station undertaken in July 2019. The HER UID is ENN109607.

This follows the approval of listed building consent for alterations to the canopies on platforms 1–3, including cutting back the trackside brackets of the historic Driver canopies on platform 1. The application also included the restoration of historic details, the replacement of roof coverings and alterations to the Goods Shed in order to bring it into public use.

## History and description of the canopies at Wellingborough Station

Wellingborough Station opened in 1857 as part of the Midland Railway's Leicester to Hitchin Line. It was designed by Charles Henry Driver in a simplified Gothic style. The platform canopies at Wellingborough are:

### Platform 1

Attached to the main station building on Platform 1 is the original four-bay canopy of 1857, designed by C. H. Driver. This is the last example of this type of canopy to survive. Attached to the southern end are two bays added in 1883, of similar design but with detailed differences. There is a separate short canopy to the north added in 1936, with a flat roof and decorative timber valences.

### Platforms 2–3

The line through Wellingborough was quadrupled in 1882 transforming platform 2 into an island platform. The island was rebuilt in 1894 with a new building and canopy to serve platform 3. The existing flat canopy on platforms 2–3 has a decorative wooden valence, which dates to when the line was quadrupled and the island platform created in 1892.

# 1.0 Introduction

## 1.1 Site location and description

The site comprises the three station canopies at Wellingborough Railway Station, located on Midland Road in Wellingborough, Northamptonshire (NN8 1NQ), approximately 1.5km from the Town Centre. The National Grid Reference for the site is SP 90351 68133.

Wellingborough Station opened in 1857 but has since undergone multiple alterations and extensions. Today it comprises:

- the main station building to Platform 1, on the west side of the line, built in 1857 to designs by Charles Henry Driver for the Midland Railway, with later extensions
- distinctive 1857 cast iron canopy on Platform 1 attached to the station building, with two similar bays attached to the southern end in 1883 and two timber bays of different design to the north, dating to 1936
- the Victorian Goods Shed to the south of the main building (separately listed at Grade II)
- platforms 2 and 3, which form an island between the east (slow) and west (fast) tracks, with an 1894 building with steel and timber canopies
- the 2011 footbridge with stepways and lifts across the south end of the station (excluded from the listing)
- the remnants of the disused Platform 4, on the east side of the tracks, with an early twentieth century building (excluded from the listing).



Figure 1: Site location

Contains Ordnance Survey data © Crown copyright and database right 2016

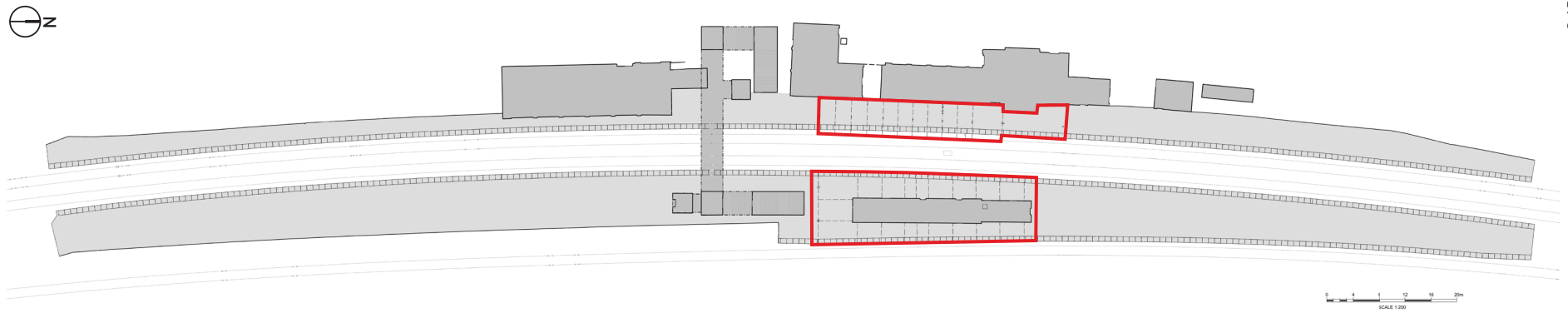


Figure 2: Site boundary

— Site boundary

## 1.2 Planning background

On 11 April 2019 listed building consent (application ref. WP/19/00082/LBC) was granted by Wellingborough District Council for alterations to the canopies on platforms 1–3, including cutting back the trackside brackets of the iron canopy on platform 1 and making structural repairs to it. The application also included the restoration of historic details, and alterations to the Goods Shed in order to bring it into public use. A condition of this consent is that:

*7. No development shall take place within the area indicated until the applicant, or their agents or successors in title, has secured the implementation of a programme of archaeological work in accordance with a written scheme of investigation which has been submitted by the applicant and approved in writing by the local planning authority.*

*Reason: To ensure that features of archaeological interest are properly examined and recorded, in accordance with National Planning Policy Framework Paragraph 199.*

The Goods Shed is the subject of a separate recording exercise.

## 1.3 Listing

Wellingborough Station was listed at Grade II in 1981. The Goods Shed was separately listed at Grade II at this time. The list entry for Wellingborough Station has been revised by Historic England. The revised list entry was published on 20 November 2014 and is reproduced in Appendix B.

The extent of the listing is the main station building and the platforms, platform buildings and canopies. The 2011 footbridge, stepways and lift; telegraph office on Platform 1; permanent way inspector's office on Platform 4; palisade fencing to Platforms 1 and 3; modern shelters, signage, seating, lamp posts and bike racks; and modern internal alterations are all excluded from the listing.

The revised list entry for Wellingborough Station identifies the following reasons for its designation:

- Architectural interest: as a station built in a distinctive Venetian Gothic style, a common design language shared with other buildings along the Midland Railway's Leicester to Hitchin Line;
- Historic interest: as a station built for the Midland Railway company, one of the most important and ambitious companies of the era of railway development in England;
- Architect: as a station designed by Charles Henry Driver, a notable railway architect and expert in the architectural use of ironwork with several listed buildings to his name;
- Engineering interest: the last remaining example of the many canopies designed by Driver for the Leicester to Bedford extension of the MR. These are very finely detailed canopies of a clever, functional design and featuring complex castings. It is unusual to find surviving cast iron canopies on the operational rail network.
- Group value as a station that possesses group value with the adjacent listed railway Goods Shed, built to a similar design by the same architect in 1857.



## 2.0 The survey

### 2.1 Scope

The scope of the archaeological work was set out in detail in the Written Scheme of Investigation (Alan Baxter, 26 June 2019).

All work was carried out in accordance with Historic England specifications in *Understanding historic buildings: a guide to good recording practice* (HE 2016) and other applicable standards and guidance (e.g. *CIFA Standard and guidance for archaeological investigation of standing buildings or structures*, 2014).

The investigation of the site corresponds to a Level 3 standing building survey according to the Historic England specifications (HE 2016). A Level 3 investigation produces an analytical record and is comprised of an introductory description followed by a systematic account of the structure's origins, development and use.

The survey focused on those areas of the canopies that will be altered under the listed building consent, namely:

- the outer brackets and roof structure of the iron canopy on platform 1
- the canopies on platforms 2 and 3 where these will be cut back

A detailed photographic survey of one example bay of each date on platform 1 is also included to record details.

The survey of the canopies took place prior to alteration and dismantling works.

The works to the Goods Shed are the subject of a separate recording exercise and report (because this is a separate listed building).

### 2.2 Methodology of the survey

A site visit, undertaken by Alan Baxter on 24 July 2019, entailed the observation of the canopies. The investigation on site was non-intrusive and aimed at specific details in the fabric of the building relevant to the listed building consent application WP/19/00082/LBC. The information gathered from the site inspection was then combined with the information found in written sources to form a history and description of the building's development.

It is the nature of existing buildings that details of their construction and development may be hidden or may not be apparent from a visual inspection. The conclusions and any advice contained in this report – particularly relating to the dating and nature of the fabric – are based on our research, and on observations and interpretations of what was visible at the time of our site visits. Further research, investigations or opening up works may reveal new information which may require such conclusions and advice to be revised.

### 2.3 Authorship

This report has been prepared by Alan Baxter Ltd, as heritage consultants to Network Rail.

## 3.0 Historical background of the site

### 3.1 Construction of Wellingborough Station

Wellingborough Station dates to the construction of the Midland Railway's Leicester to Hitchin Line, which opened in 1857. The stations on that line were designed by Charles Henry Driver in a distinctive simplified Gothic style. Wellingborough was built to the same design as Kettering, which was pictured in an *Illustrated London News* story on the opening of the route (23 May 1857). Driver worked with the Midland Railway's engineer for the route, Charles Liddell.

The line through Wellingborough was quadrupled in 1882, and Platform 2 became an island between the lines. In 1894 the slow lines came into passenger use: the island was rebuilt with a new building and canopy to also serve Platform 3 on its east side, and Platform 4 was added on the east side of the tracks (but closed in 1959).

### 3.2 Canopies on platform 1 (1857, 1883 and twentieth century)

Attached to the main station building on Platform 1 is the original four-bay canopy of 1857, designed by C. H. Driver. Attached to the southern end of this are two bays added in 1883, of similar design but with numerous detailed differences. There is a separate short canopy to the north constructed in 1936 in front of the parcels office, with a flat roof and decorative timber valences.

The 1857 canopy is significant as the sole surviving example on the network of the elaborate design Driver developed for the Midland Railway. He published superb detailed drawings of the canopy at the time, which are reproduced as Figures 7–9). The canopy is significant as a very rare surviving example of the unusually ornate and high-quality pattern Driver designed for the Midland Railway, and the only example left on the operational network.

### 3.3 Canopies on platforms 2–3 (1892)

Platforms 2 and 3 have a flat canopy with decorative wooden valences, around the 1892 island building: this is a typical late-nineteenth-century structure, less significant than the Driver canopies on Platform 1, with early steel beams and decorative cast-iron spandrels.



Figure 3: 1857 station at Kettering - identical as built to Wellingborough - as depicted in the *Illustrated London News* (23 May 1857). The Kettering building was demolished and replaced in the 1890s.

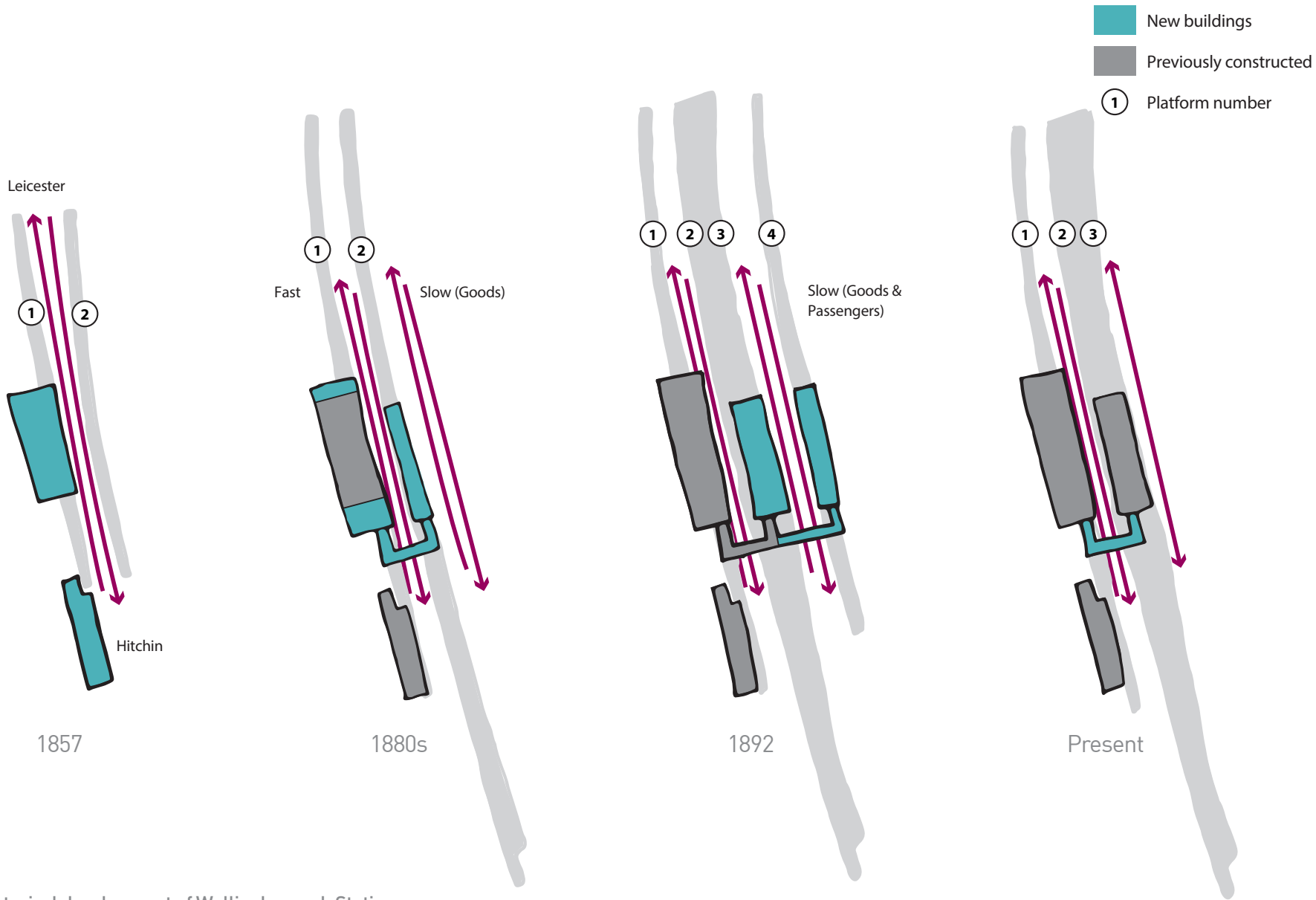


Figure 4: Historical development of Wellingborough Station

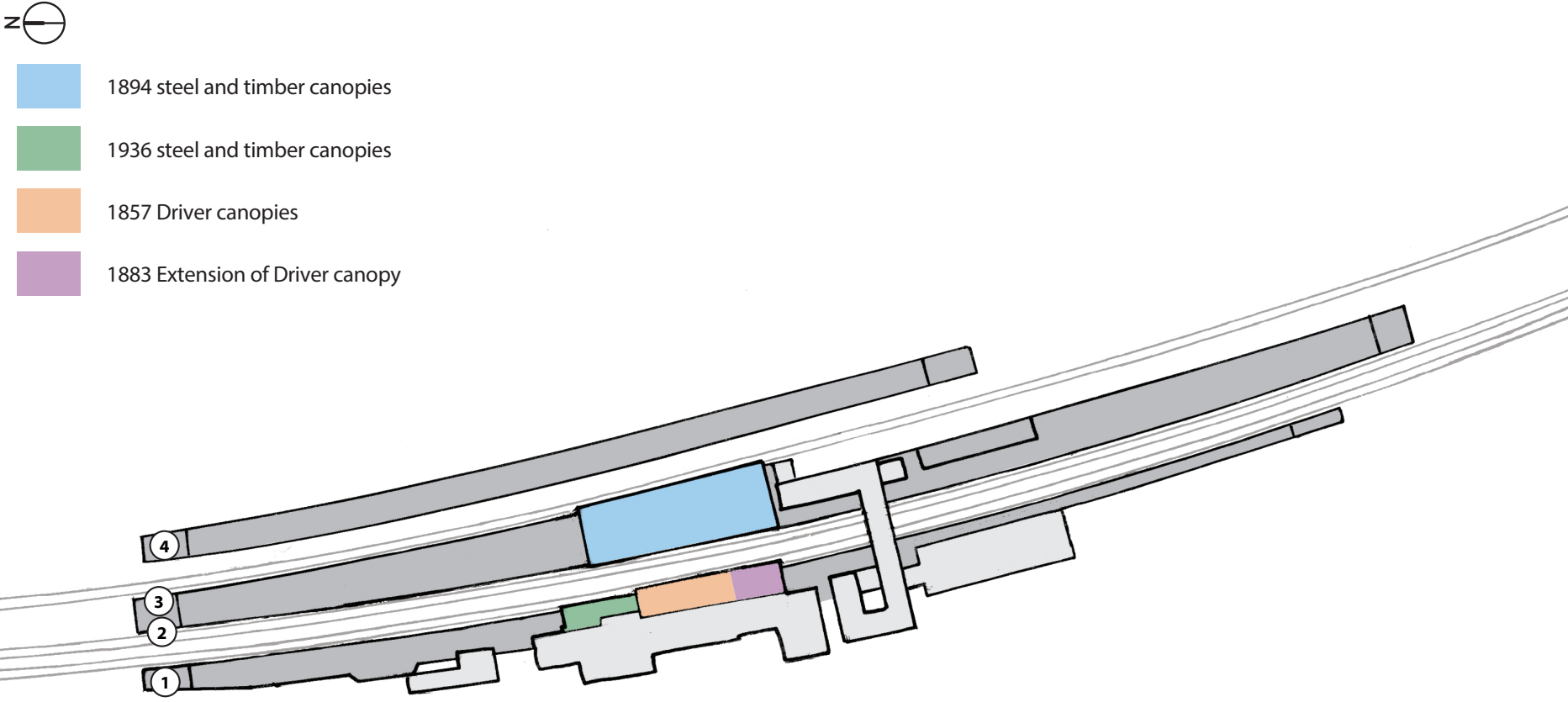


Figure 5: Age of fabric of station canopies at Wellingborough Station

## 4.0 Description of the standing fabric

### 4.1 Introduction

The canopies on platforms 1–3 at Wellingborough Station were photographed by the author on 24 July 2019. This section should be read in conjunction with the site photographs (Section 4.3).

### 4.2 Platform 1

#### **Steel and timber canopy**

The northern-most part of the canopy on platform 1 is of a flat-roofed, timber-valence type similar to that seen on platforms 2–3, on a steel frame. According to historic drawings it dates from 1936.

#### **Cast-iron canopy**

The six bays forming the southern part of the platform 1 canopy can be divided into two types: the original 1857 design (northern four bays) and the later 1883 addition (southern two bays). What follows is a general description of the structure of these two types, which are broadly similar, followed by an identification of the differences.

For reference, the clever way in which the various components are slotted together is illustrated by the exploded axonometric drawing in Figure 6 and the detail of the original 1857 section is shown in three superb drawings published in 1858 (Figures 7–9). Detailed photographs of the components can be found in sections 4.4.6 and 4.4.7.

The canopy consists of cast-iron columns supporting cast-iron brackets, with decorative filigree ironwork in the spandrels. Brackets spring in four directions: the principal brackets are perpendicular to the platform and the secondary are in line with the platform, forming the rafters of the roof. These brackets are attached via cross bolts. The rearward spandrels are restrained by the brick wall of the station building.

A cast-iron gutter is fixed to the top of the principal brackets, discharging down the inside of the columns. Secondary tied rafters joined at the ridge are fixed to the flanges of the gutter. As such, the gutter is an integral part of the roof structure. Purlins span between rafters providing support for the roof finishes.

Rainwater from the roof is drained down the centre of the columns. At some point the northernmost column fractured and has been repaired.

Originally, the canopies were finished with decorative ironwork in the gable ends, similar to those surviving on platform 3 at Kettering, and ornate ironwork weather vane finials at each gable crown, as shown on historic drawings (see Figure 8). These have been removed, it is believed when the canopies were cut back in the c. 1980s. The glazing was almost certainly originally plain lightweight 'greenhouse' glass. It is now Georgian wired glass.

### Differences between 1857 and 1883 canopies

Although superficially the same design, there are numerous design differences:

- all mouldings are subtly different and the brackets of slightly different geometry
- all mouldings are of a larger section, and therefore more robust
- the brackets are attached, both to each other and to the column, in a different way. In the 1857 bay, the brackets are fixed together via cross bolts which run through the centre of the column (now with additional support provided by later iron strapping which is also bolted). In contrast, the brackets on the 1883 bay are bolted together around the circumference of the column. The advantage of this is that the bolts are not corroded by rainwater discharging down the centre of the columns
- the 1883 bays are slightly wider than their 1857 counterparts
- the 1857 bracket is connected to the gutter beam via five bolts rather than three (as seen on 1883 bays)
- the outer gutters of the 1857 section had a vertical rather than flared outer side. This required a workaround where the canopy was extended south in 1883

### 4.3 Platforms 2 and 3

The canopy on platforms 2–3 is cantilevered off a brick-built single storey building. Early steel beams, on cast iron brackets, support a flat roof with timber sarking on the underside and asphalt covering over the top. Decorative timber valences trim the edge.

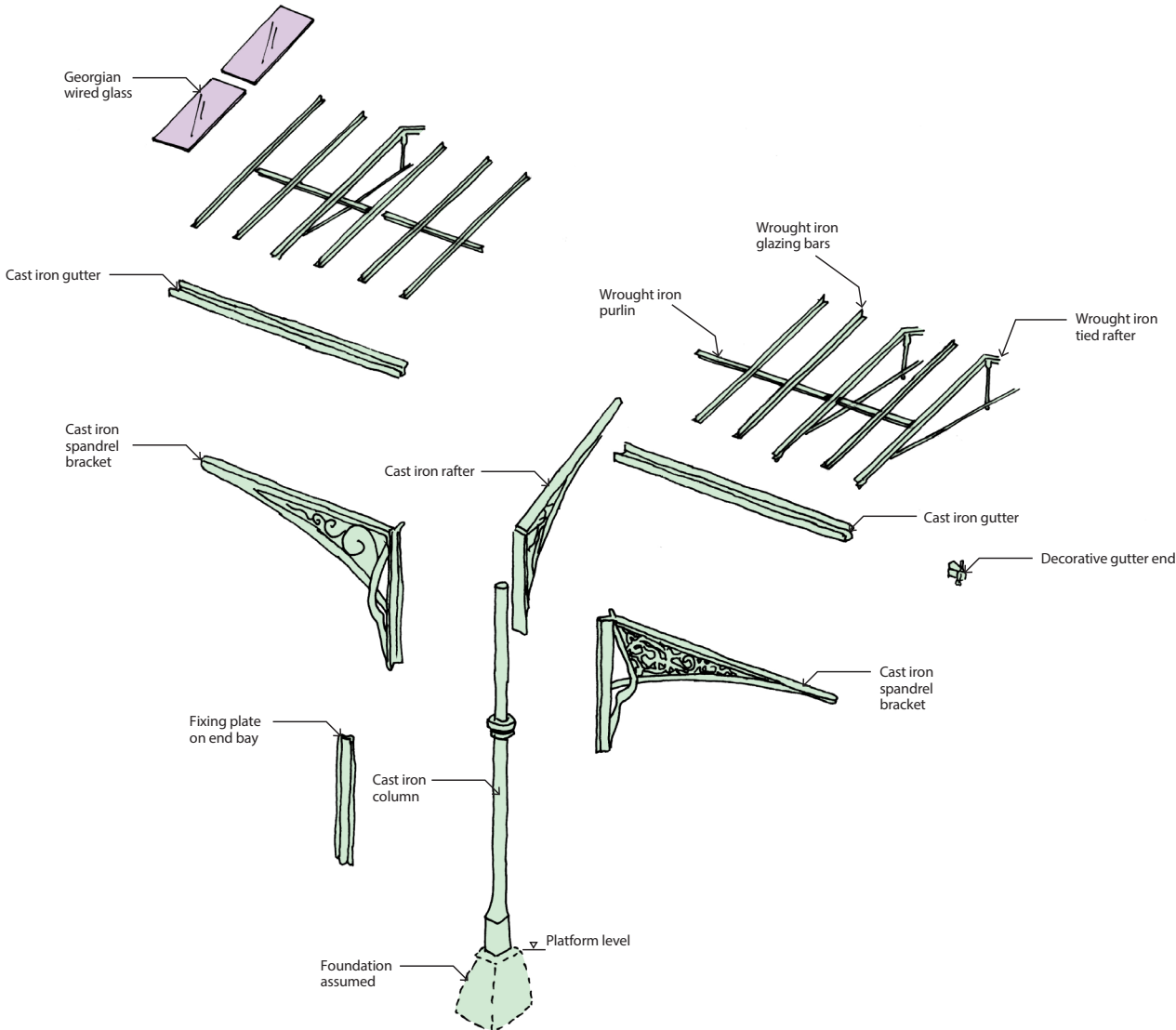


Figure 6: Exploded drawing of a typical bay of the 1857/1883 canopies, showing the components and their age

1857/1883 canopy  
Later addition

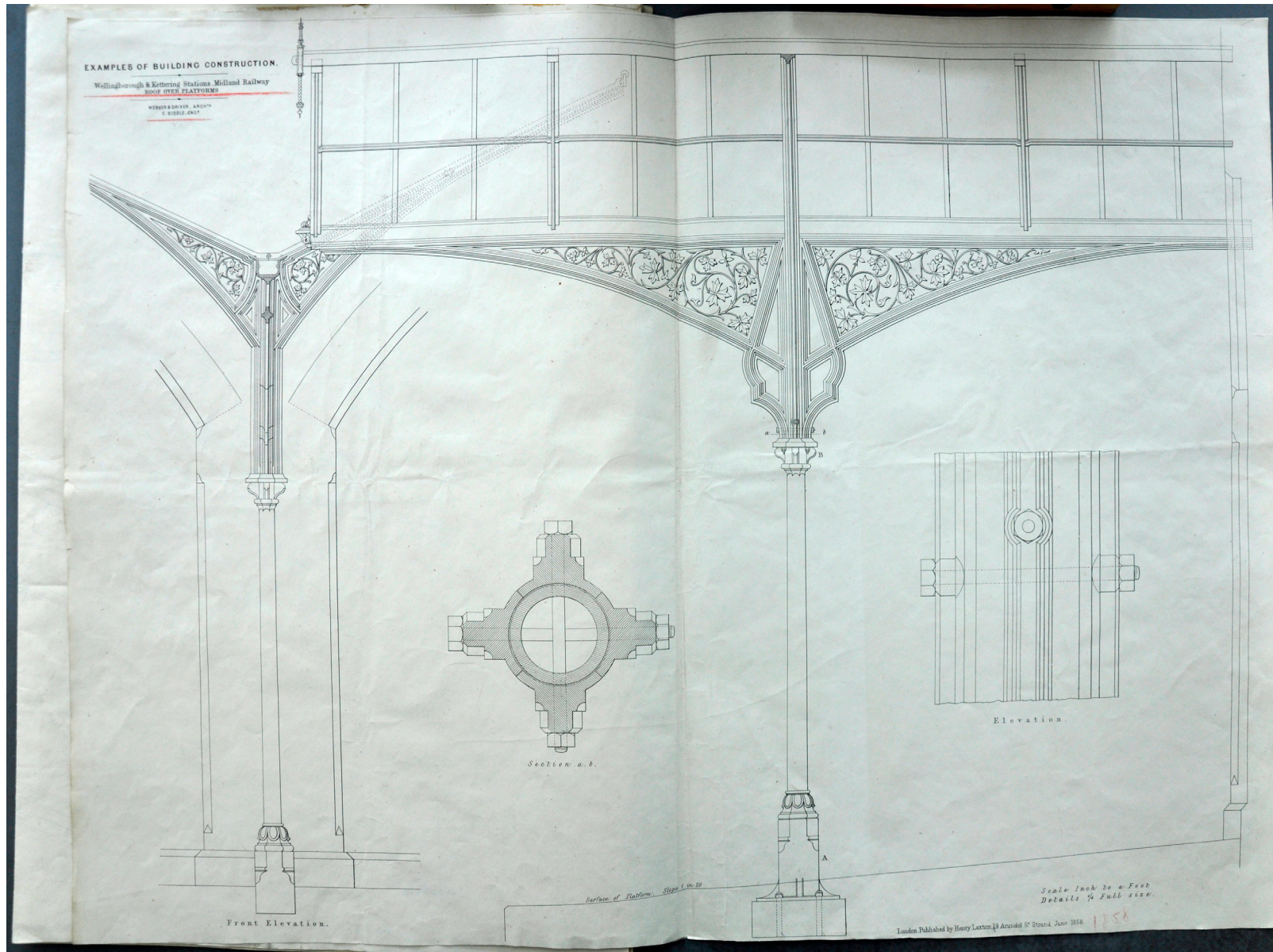


Figure 7: Detailed drawings of the 1857 canopy signed by Driver and reproduced in Henry Laxton's *Examples of Building Construction* in 1858



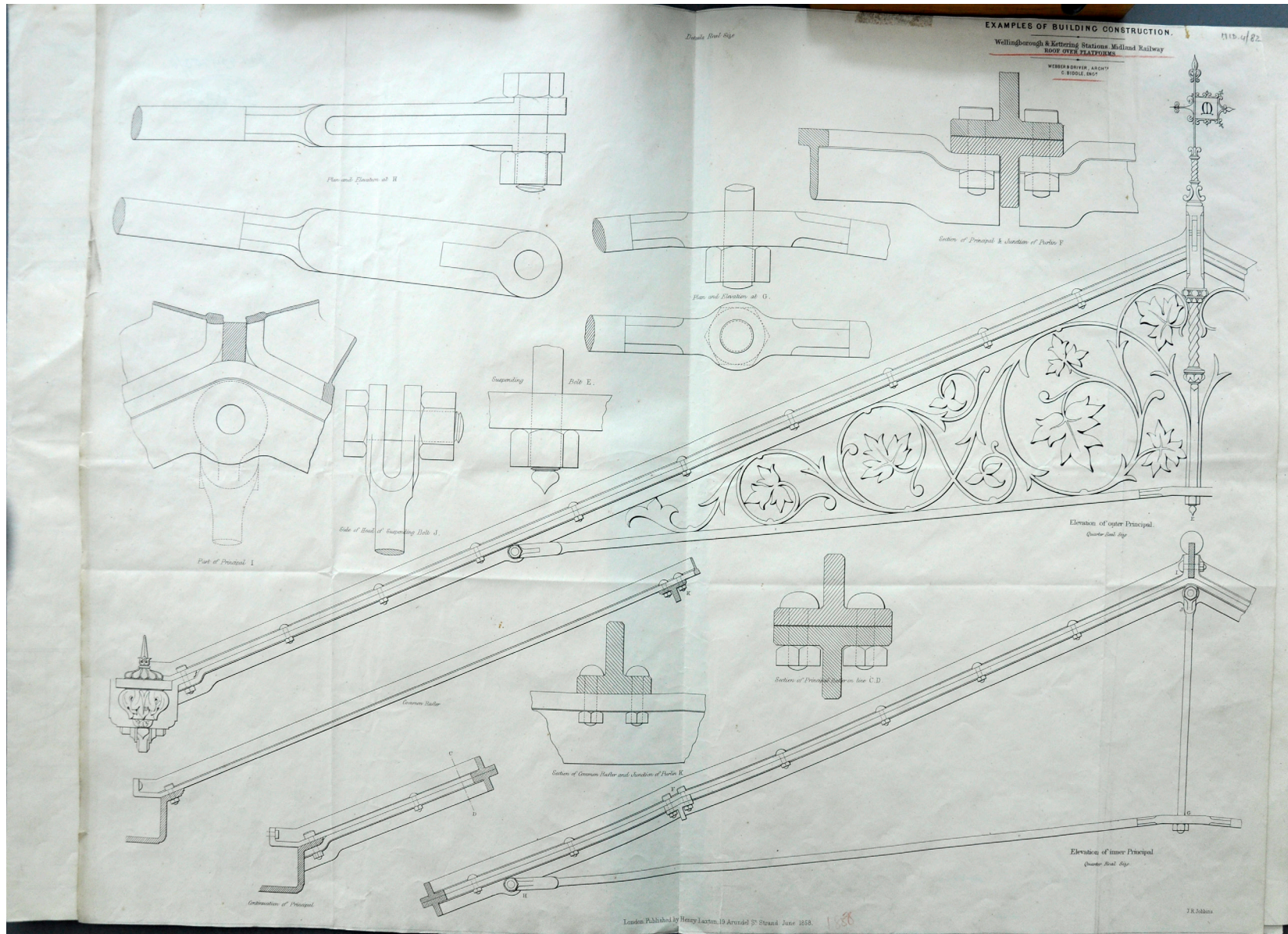


Figure 8: Detailed drawings of the 1857 canopy signed by Driver and reproduced in Henry Laxton's *Examples of Building Construction* in 1858

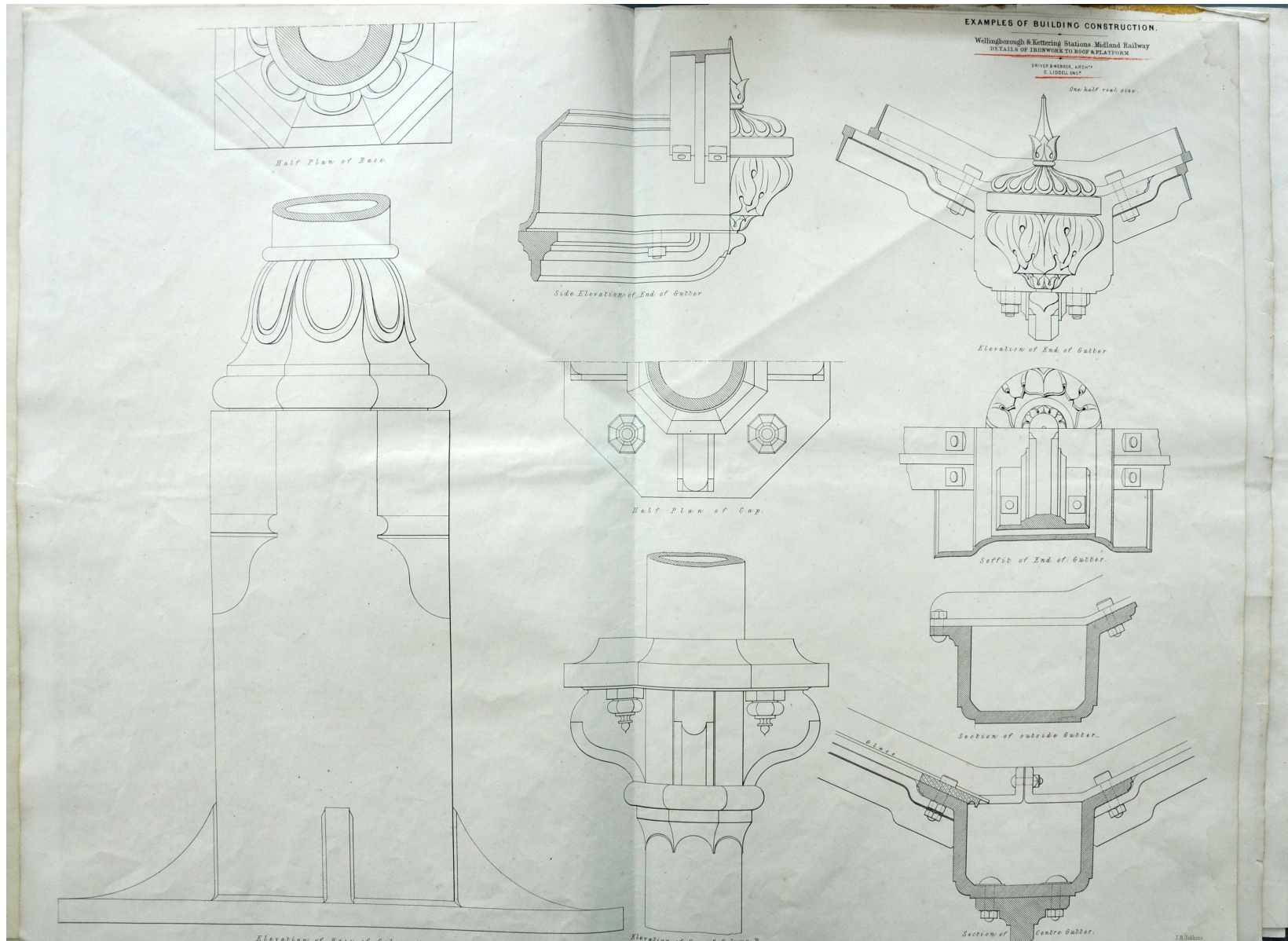


Figure 9: Detailed drawings of the 1857 canopy signed by Driver and reproduced in Henry Laxton's *Examples of Building Construction* in 1858

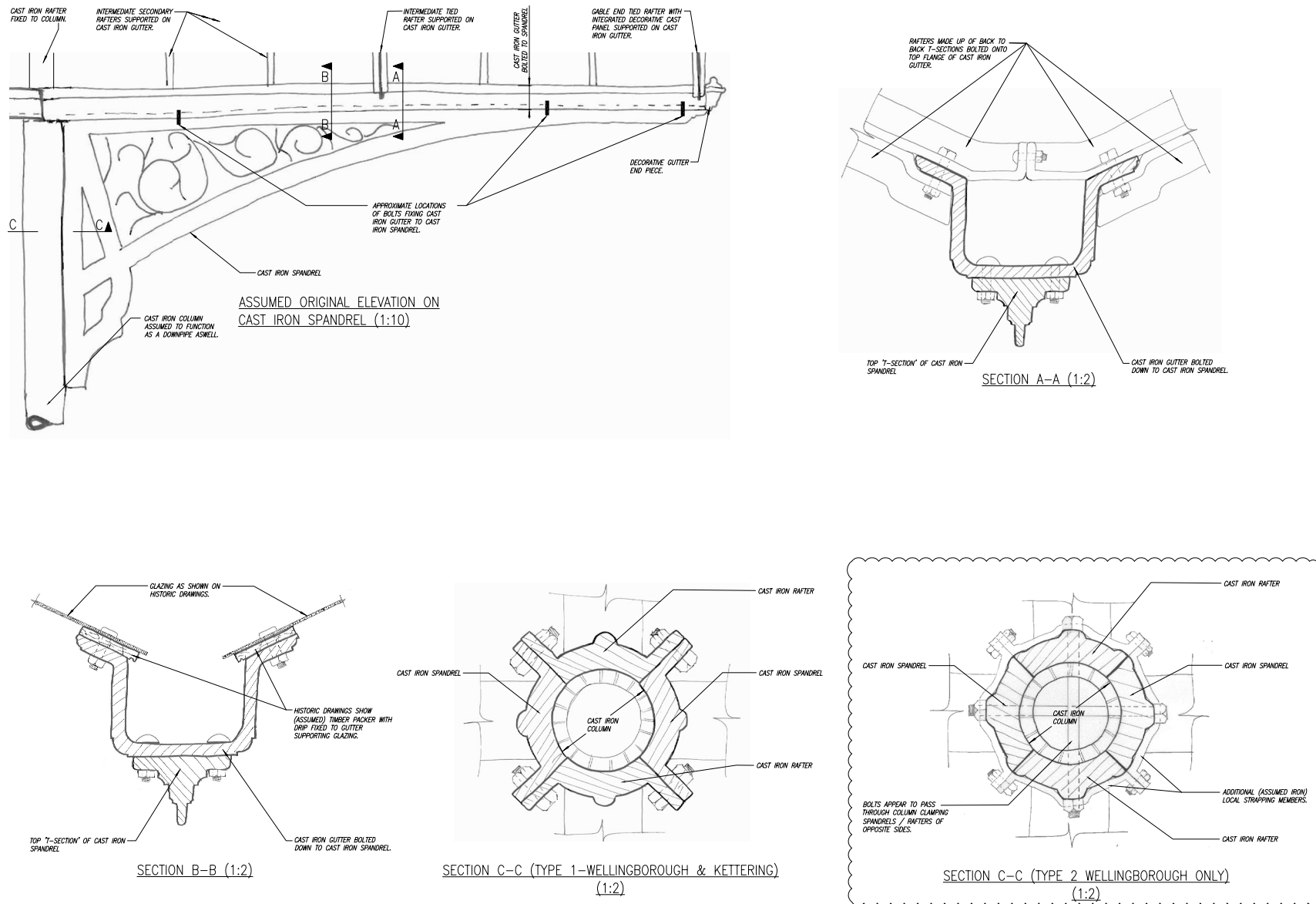


Figure 10: Sketch drawing from observation of existing 1857 and 1883 canopies on platforms 1 showing bracket details

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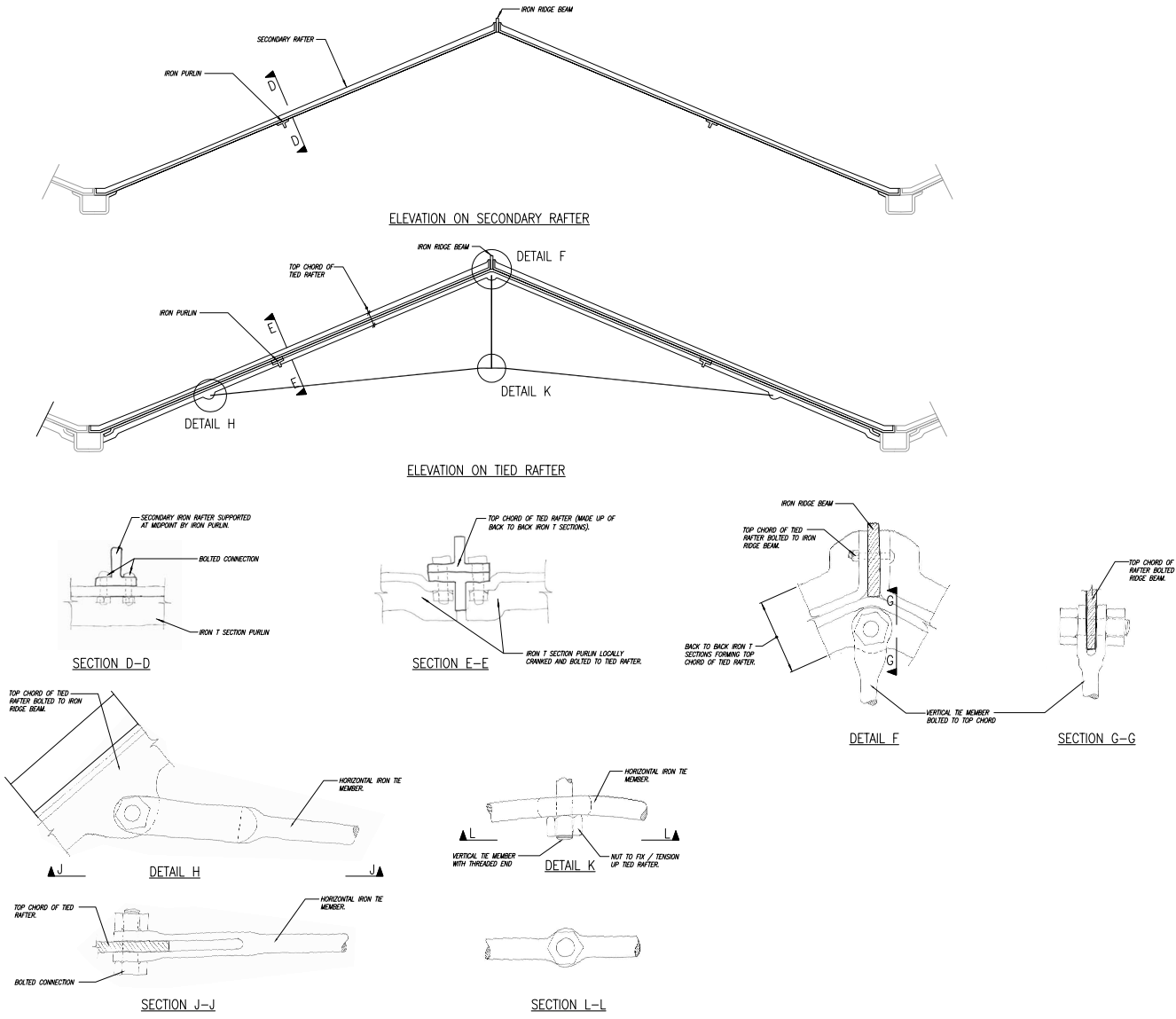


Figure 11: Sketch drawing from observation of existing 1857 and 1883 canopies on platforms 1 showing roof details

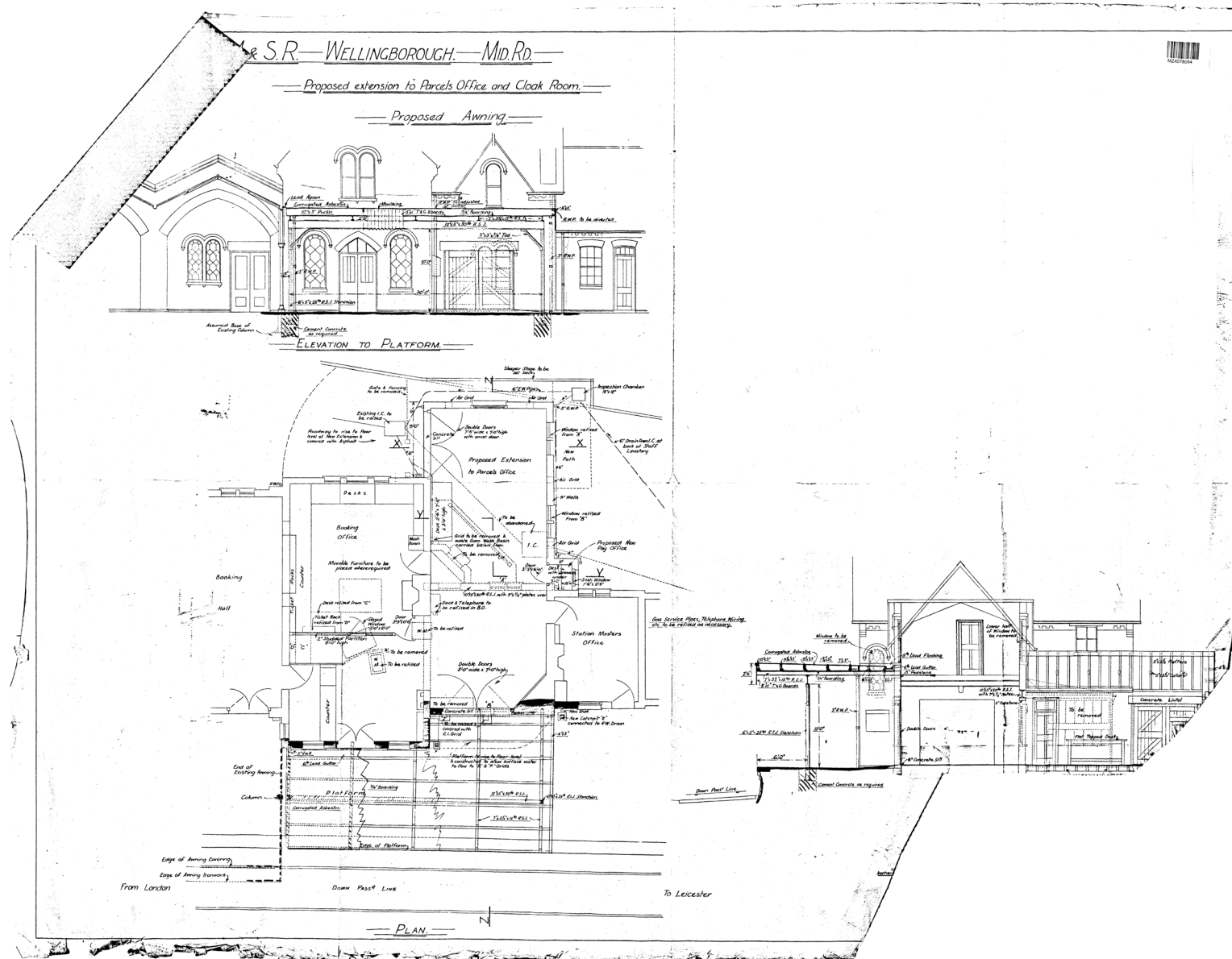
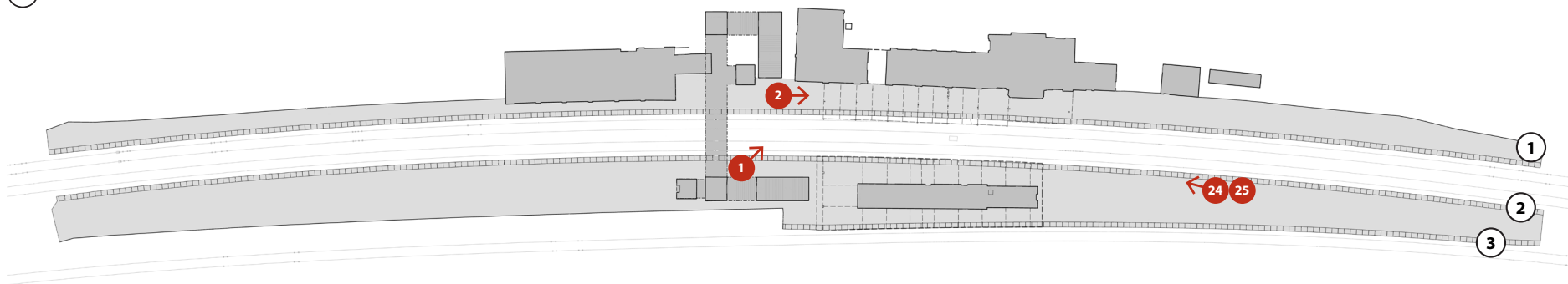


Figure 12: One of 2 drawings of the northern canopy on platform 1 to survive in the Network Rail archives. The other is dated 1936.

## 4.4 Photographic record

The photographic record is divided into sections, starting with platform 1, then moving on to platform 2 and platform 3 and concluding with a detailed look at a bay of the 1857 canopy on platform 1 and for comparison a bay of the 1883 extension.

### 4.4.1 Platform 1 general views





-  Photo location
-  Platform number

Figure 13: Key plan of Wellingborough Station showing location and direction of photos of platform 1



Platform 1 canopy from southern end of Platform 2



View from south of Platform 1 showing southern 1883 canopy



View of Platform 1 timber canopy from Platform 2



View of Platform 1 from northern end of Platform 2

4.4.2 Platform 1 canopies



Figure 14: Key plan (top) and elevation (bottom) of platform 1 showing location and direction of photos. The direction of photographs taken of the roof of each bay is shown in the top plan.



#### 4.0 Description of standing fabric



Southern end of Platform 1 canopy



Southern elevation of bay 1



Roof of bay 1



Northern elevation of bay 1

#### 4.0 Description of standing fabric



Southern elevation of bay 2



Roof of bay 2



Northern elevation of bay 2



Southern elevation of bay 3



Roof of bay 3



Northern elevation of bay 3

#### 4.0 Description of standing fabric



Southern elevation of bay 4



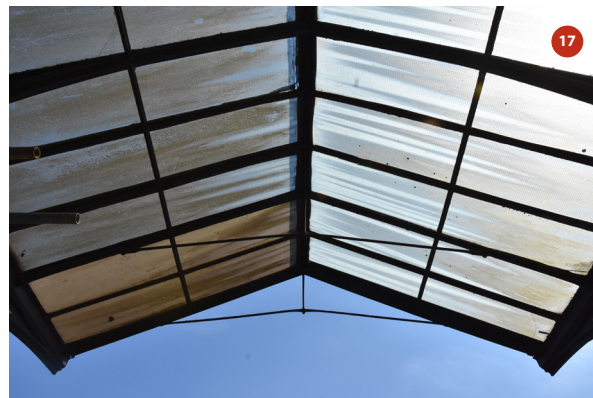
Roof of bay 4



Northern elevation of bay 4



Southern elevation of bay 5



Roof of bay 5



Northern elevation of bay 5

4.0 Description of standing fabric



Southern elevation of bay 6



Location of spandrel removed for trial dismantling



Timber canopy southern corner internal view



Timber canopy northern corner internal view



Timber canopy general view

4.4.3 Platform 2

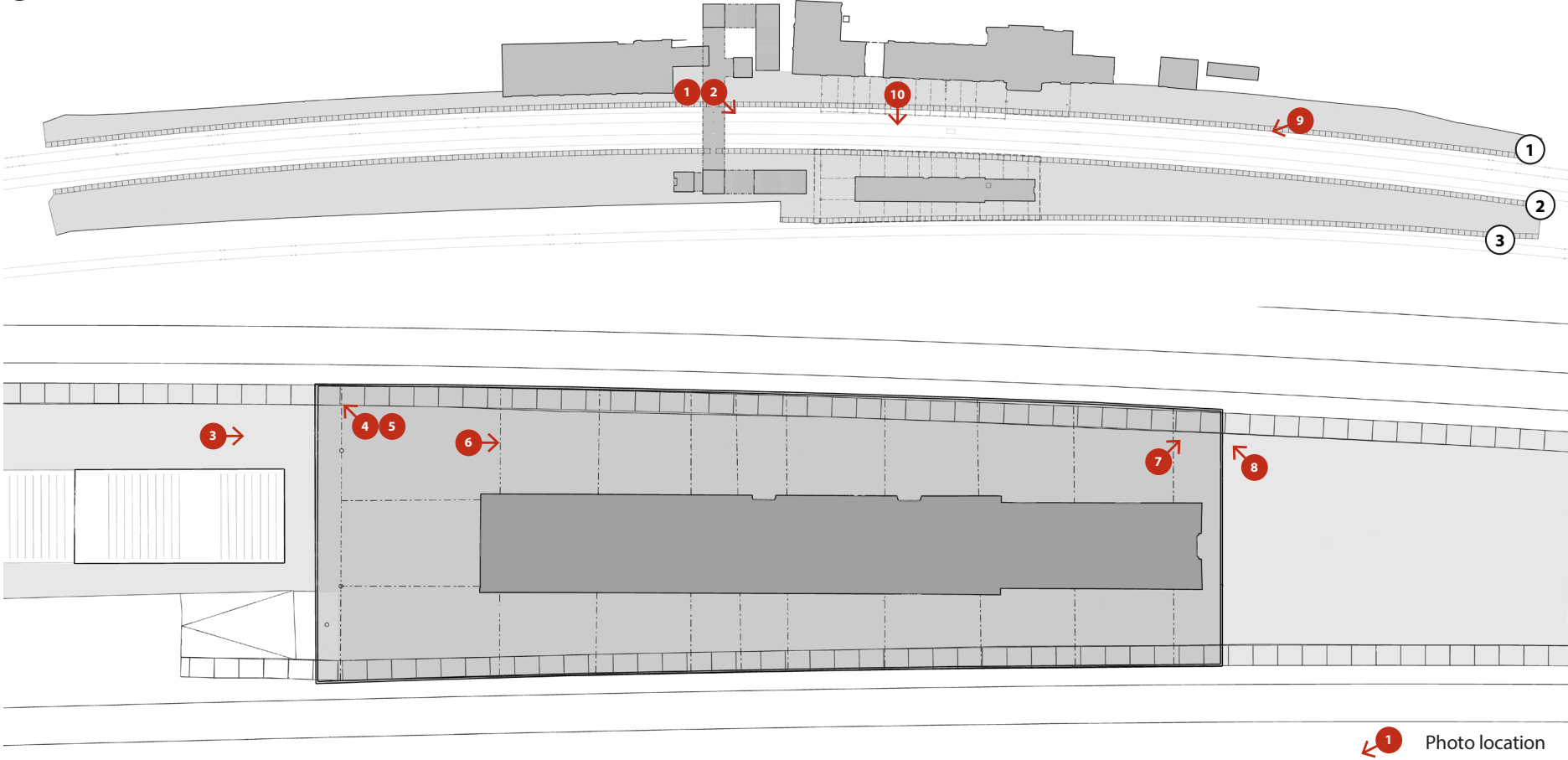


Figure 15: Key plans of platform 2 showing location and direction of photos.

## 4.0 Description of standing fabric



General view from south end of Platform 1



View of south west corner of timber canopy from Platform 1



South elevation of canopy



Southern corner internal view



Detail of steel beam and wooden valances



General view of underside of canopy



Northern corner internal view



North elevation of canopy



General view from north end of Platform 1



Detail of timber valances taken from Platform 1

4.4.4 Platform 3

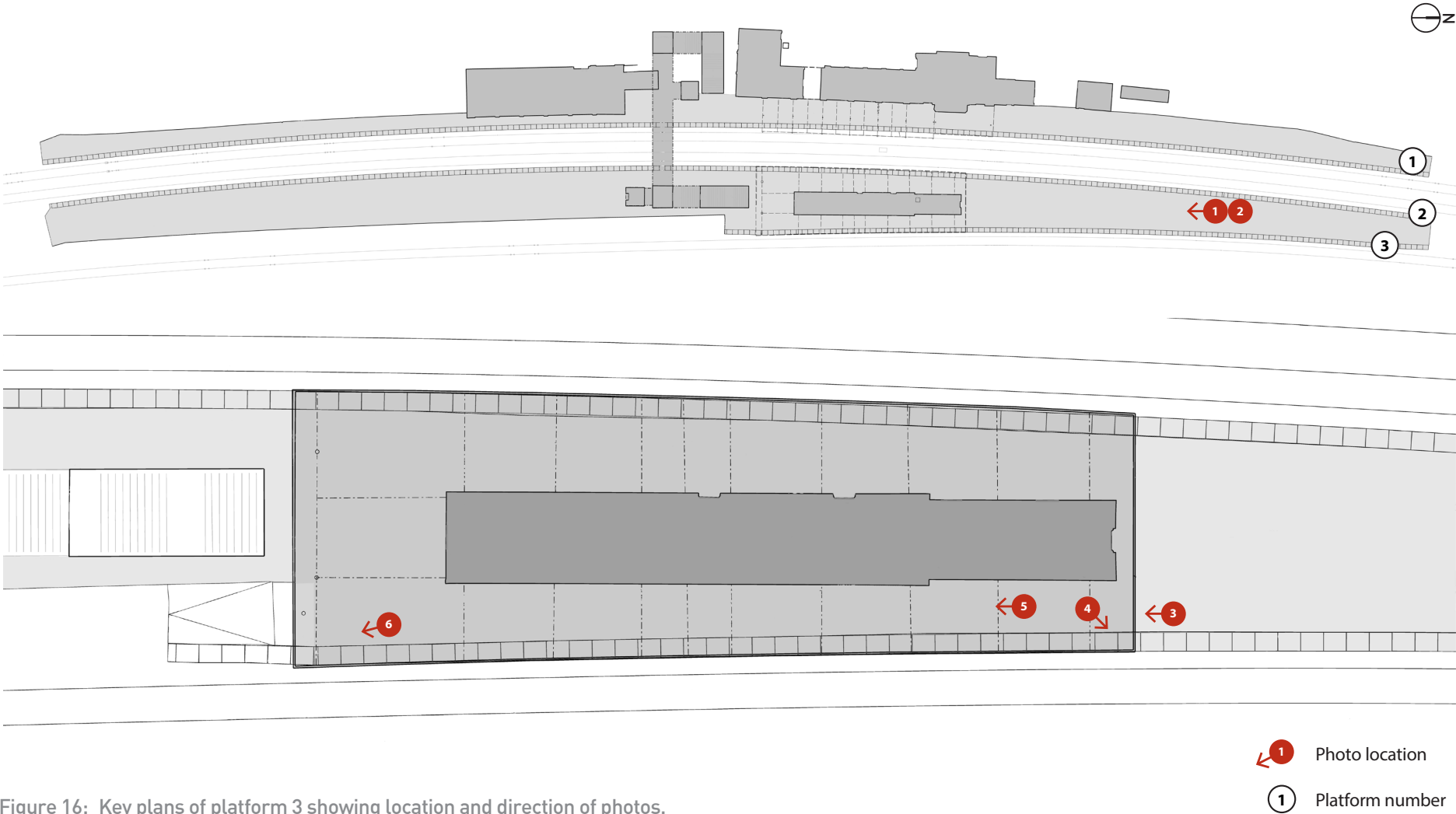


Figure 16: Key plans of platform 3 showing location and direction of photos.



#### 4.0 Description of standing fabric



General view from north end of Platforms 2 & 3



North elevation of canopy over Platforms 2 & 3



North elevation of canopy over Platform 3



Northern corner internal view



General view of underside of canopy



Southern corner internal view

4.4.5 Platform 1 roof bosses

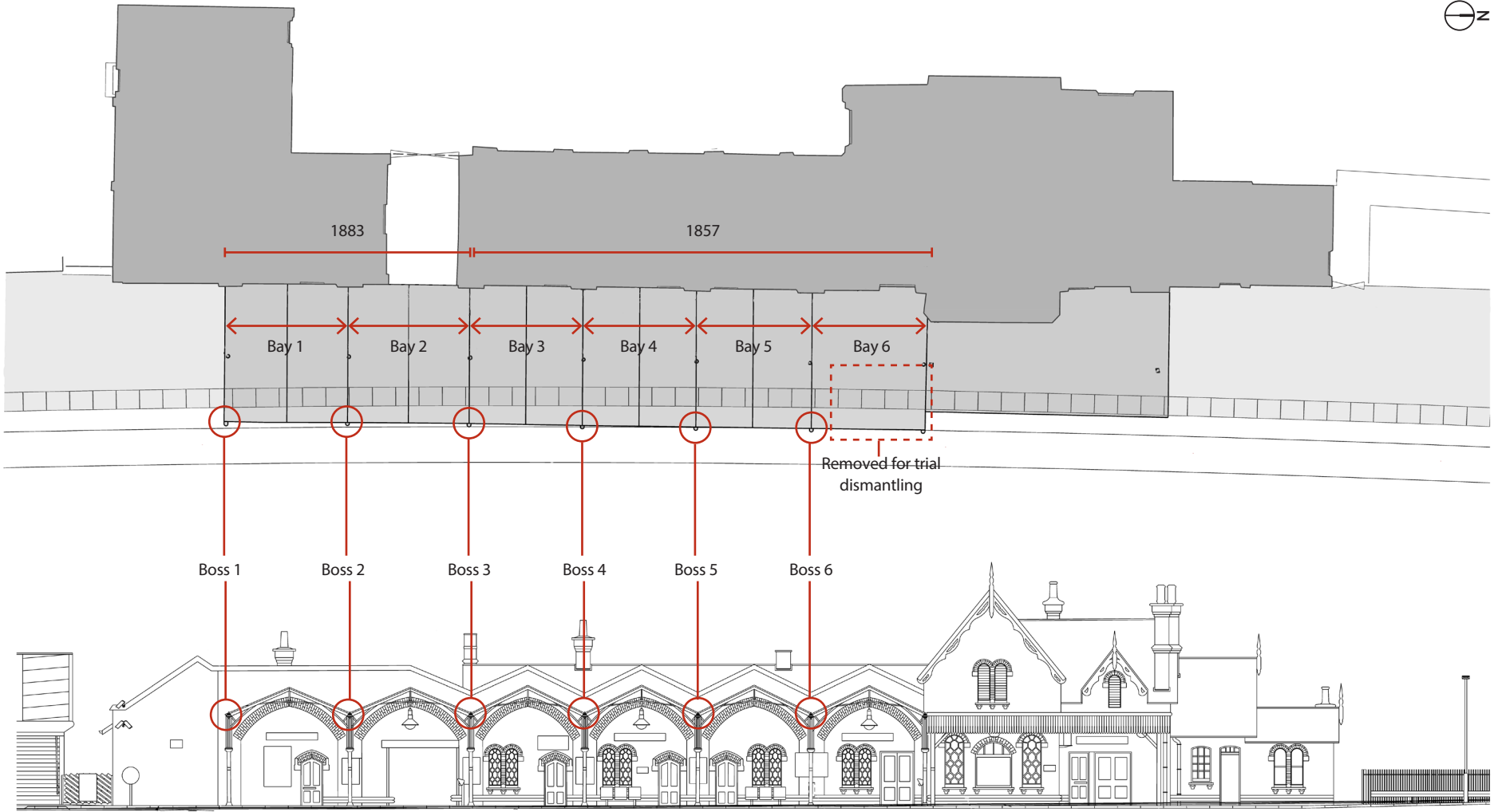
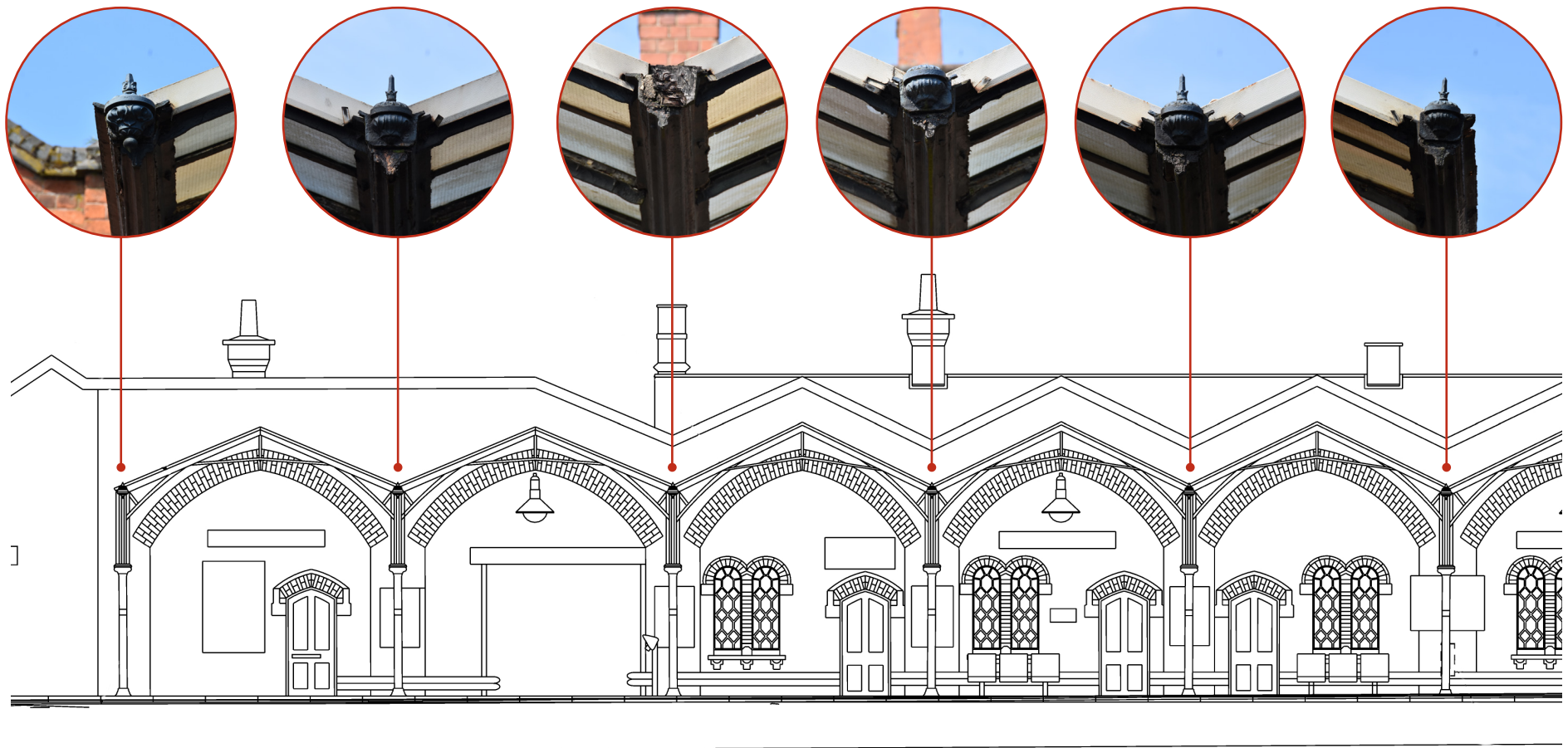


Figure 17: Key plan (top) and elevation (bottom) of platform 1 showing location and direction of photos of the canopy bosses



4.4.6 Platform 1: 1857 extension in detail

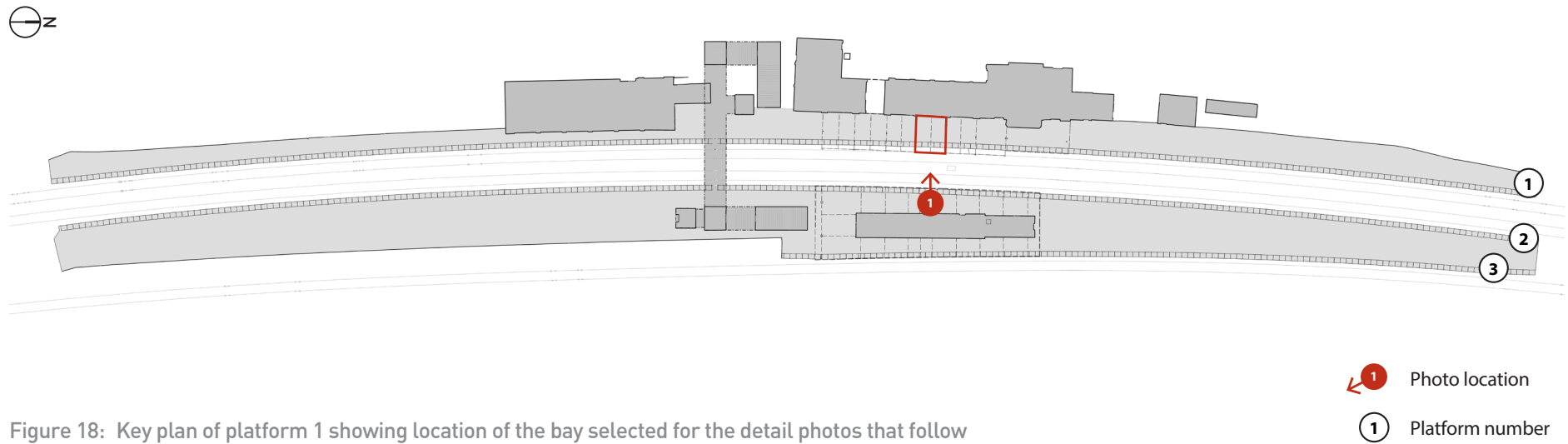


Figure 18: Key plan of platform 1 showing location of the bay selected for the detail photos that follow

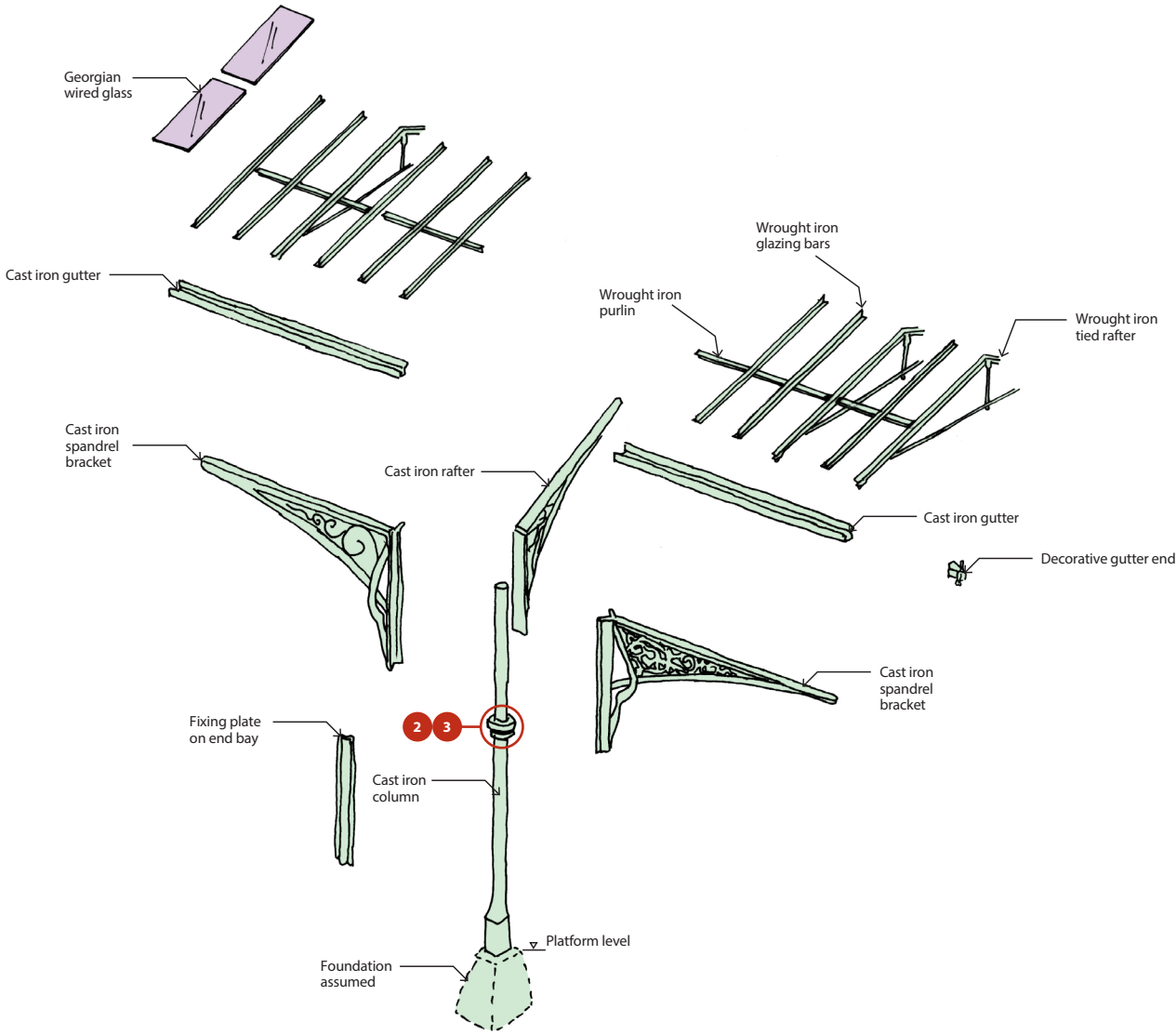


Figure 19: Key plan showing subject of detail photos of 1857 canopy bay

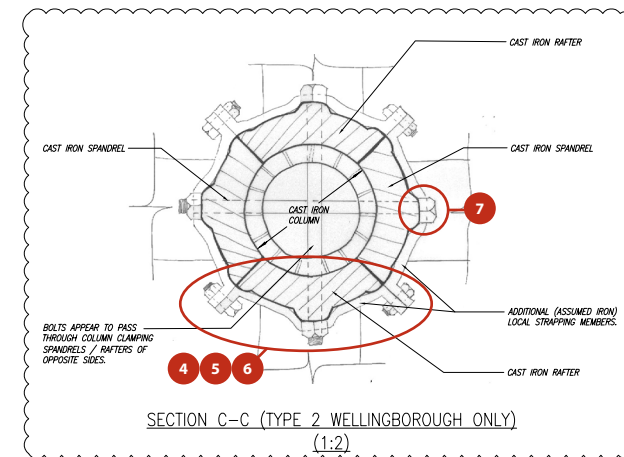
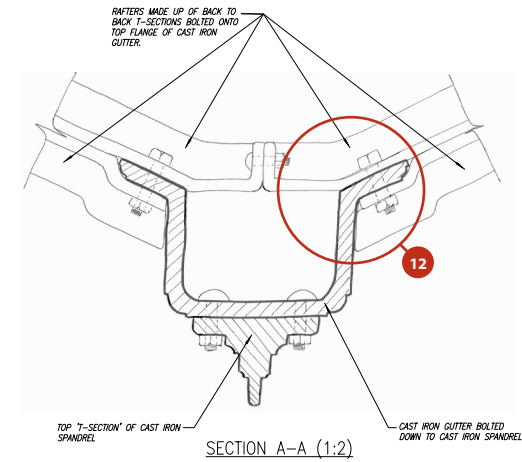
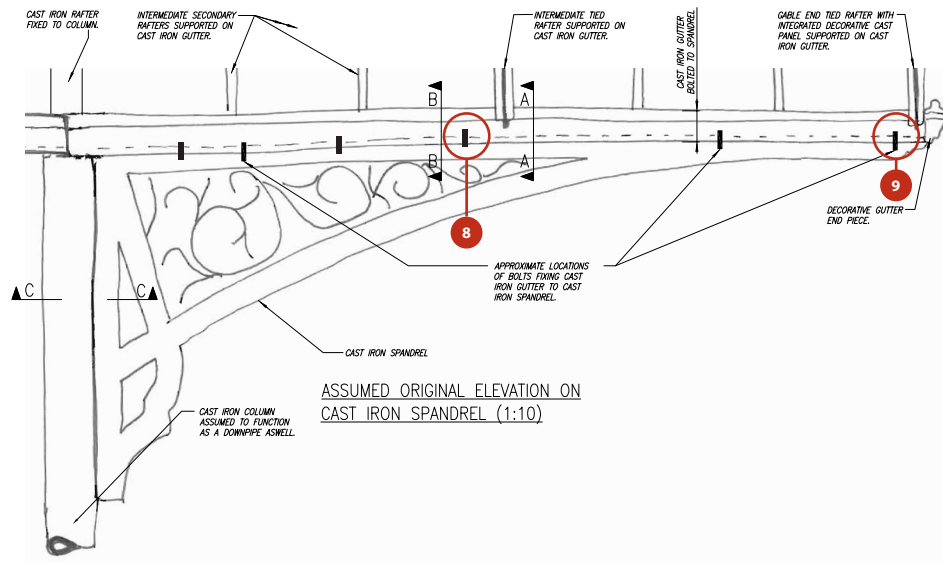


Figure 20: Key showing subject of detail photos of 1857 canopy bay

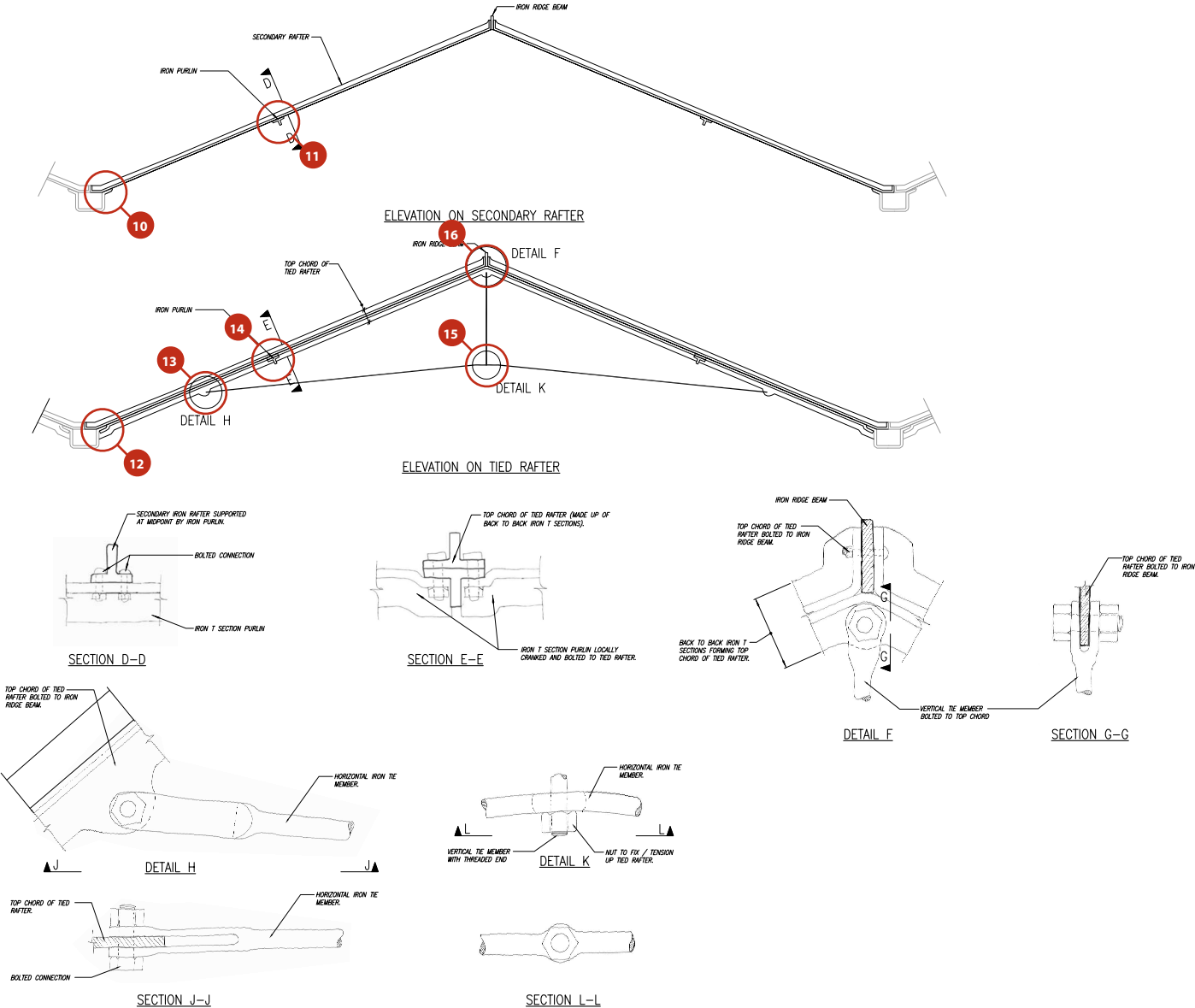


Figure 21: Key showing subject of detail photos of 1857 canopy bay

## 4.0 Description of standing fabric



Elevation of bay 4 taken from Platform 2



Detail of where brackets meet column



Detail of where brackets meet column (close up of photo 2)



Detail of bolts and strapping joining brackets together around central column



Detail of bolts and strapping joining brackets together around central column (close up of photo 4)



Detail of local iron strapping joining brackets together (close up of photo 5)



#### 4.0 Description of standing fabric



Detail of bolt joining brackets together around central column



Detail of bracket connection with gutter beam



Detail of bracket and gutter beam end



Detail of connection between secondary rafter and gutter



Detail of purlin connection to secondary rafter



Detail of connection between tied rafter and gutter

#### 4.0 Description of standing fabric



Detail of connection between top chord of tied rafter and horizontal tie member



Detail of purlin connection to tied rafter



Detail of connection between vertical and horizontal tie member



Detail of top chord of tied rafter and ridge beam

4.4.7 Detail of the 1883 extension to the platform 1 canopy

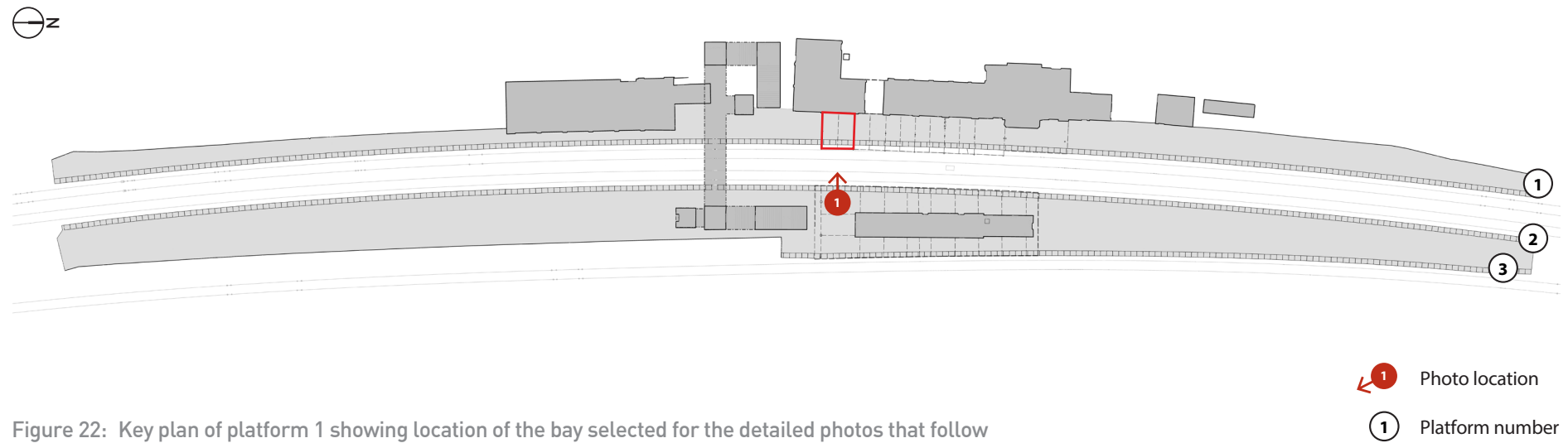


Figure 22: Key plan of platform 1 showing location of the bay selected for the detailed photos that follow

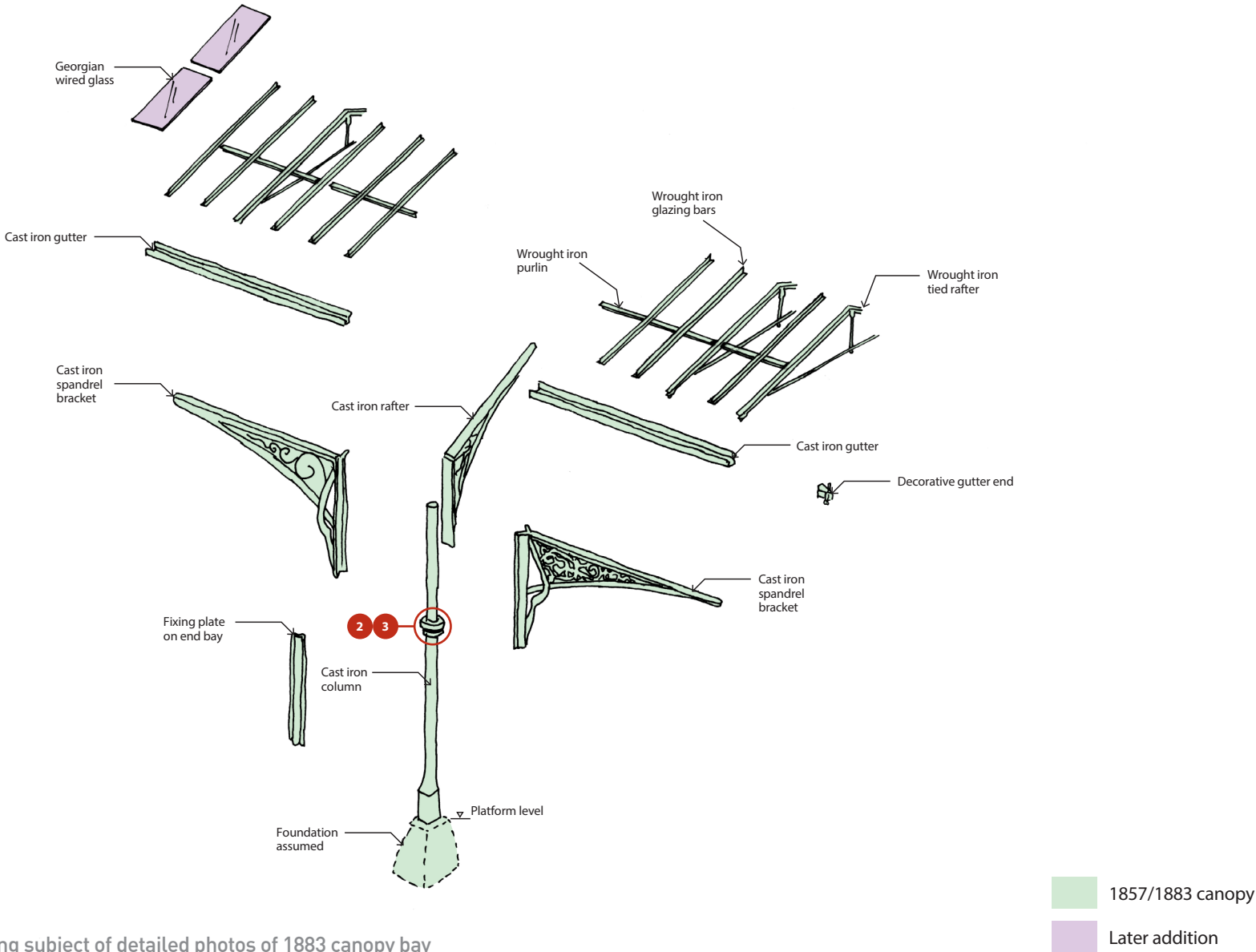
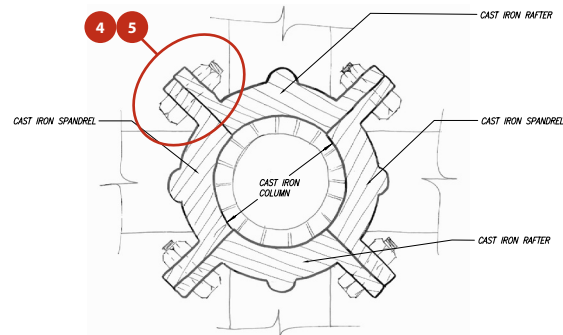
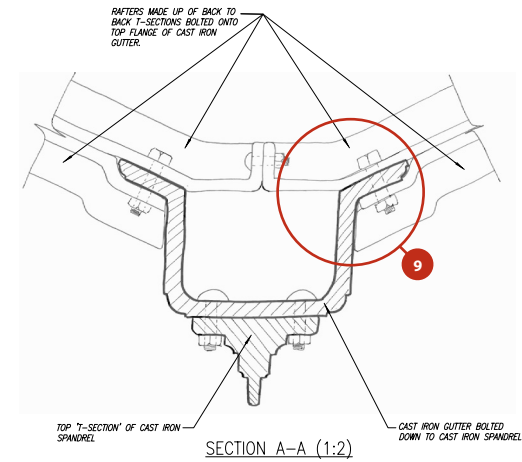
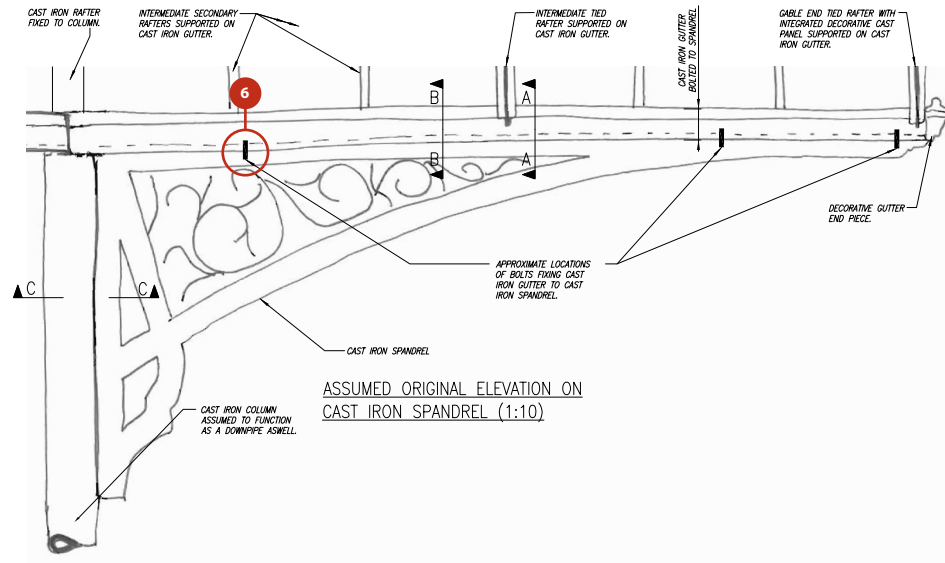


Figure 23: Key showing subject of detailed photos of 1883 canopy bay



SECTION C-C (TYPE 1-WELLINGBOROUGH & KETTERING)  
(1:2)

Figure 24: Key showing subject of detailed photos of 1883 canopy bay

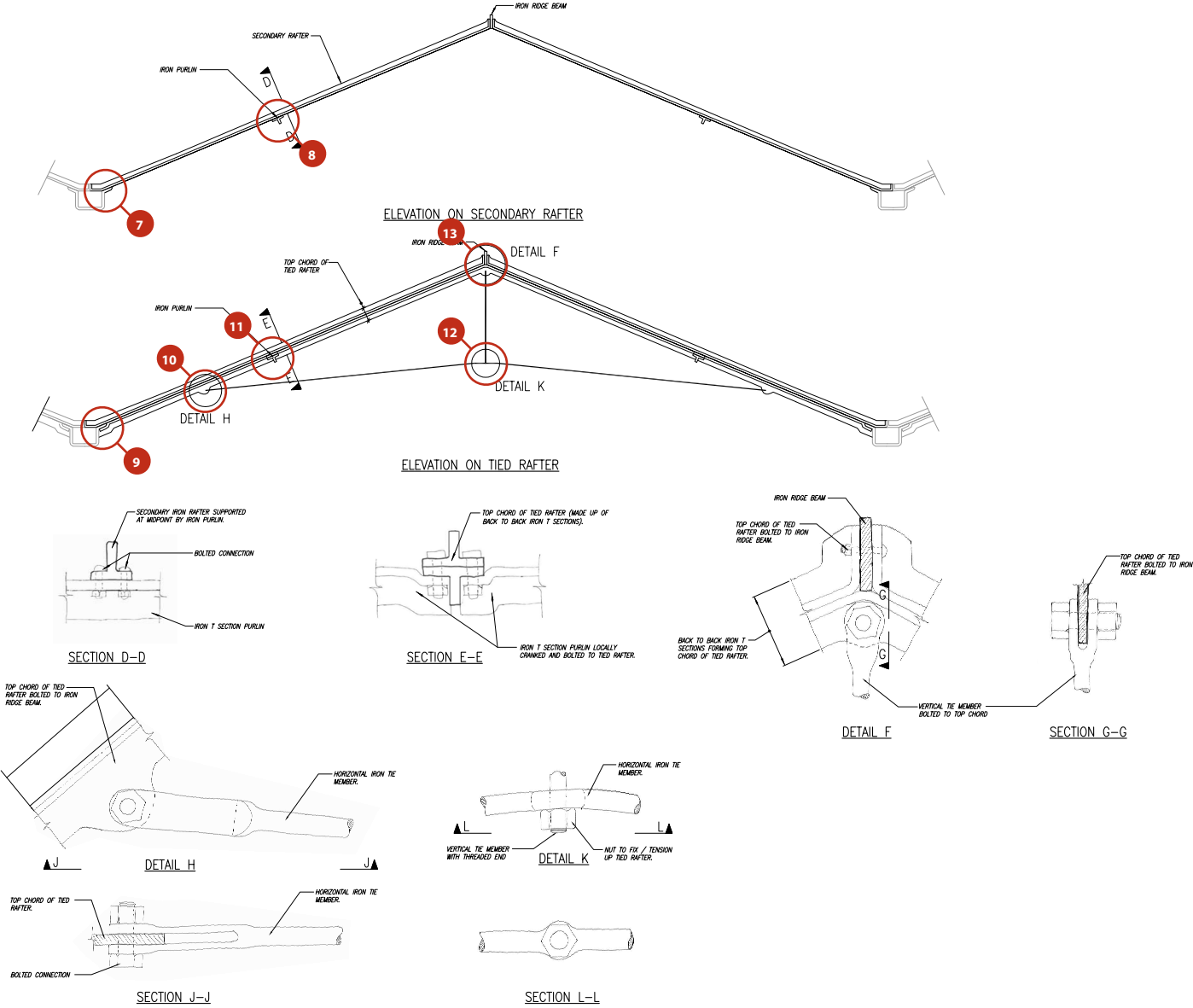


Figure 25: Key showing subject of detailed photos of 1883 canopy bay

## 4.0 Description of standing fabric



Elevation of bay 1 taken from Platform 2



Detail of where brackets meet column



Detail of where brackets meet column (close up of photo 2)



Detail of bolts joining brackets together around central column



Detail of bolts joining brackets together around central column (close up of photo 4)



Detail of bracket connection with gutter beam

#### 4.0 Description of standing fabric



Detail of connection between secondary rafter and gutter



Detail of purlin connection to secondary rafter



Detail of connection between tied rafter and gutter



Detail of connection between top chord of tied rafter and horizontal tie member



Detail of purlin connection to tied rafter



Detail of connection between vertical and horizontal tie member





Detail of top chord of tied rafter and ridge beam

## 5.0 Publication and archiving

Information on the results of the survey will be made publicly available by means of a database in digital form, to permit inclusion of the site data in any future academic research.

In view of the potential of the material it is suggested that a summary on the results of the Standing Building Survey project, possibly including figures or photographs, should appear in an appropriate academic journal.

The site archive of original records will be deposited digitally to ADS-easy. They will be stored under the following UID event number:

**ENN109607**

# 6.0 Bibliography and references

## 6.1 Books and journals

Biddle, G, *Britain's Historic Railway Buildings* (2011)

Hendry, R., *An Historical Survey of Selected London, Midland and Scottish Railway Stations* (1986)

Laxton, H., *Examples of Building Construction*, (1858)

*Illustrated London News*, (23 May 1857)

## 6.2 Other

Alan Baxter & Associates, *Midland Main Line Statement of History and Significance* (2013)

CIFA (2014) *Standard and guidance for archaeological investigation of standing buildings or structures*

Historical Ordnance Survey mapping

Historic England (2016) *Understanding historic buildings: a guide to good recording practice*

Network Rail, National Records Centre, drawings of various dates

## Appendix A: Historic England guidance on recording

In 2016, Historic England produced *Understanding Historic Buildings – A guide to good recording practice*. This best practice guide is the industry standard for the recording of historic buildings. It sets out four levels plus one of building recording, with escalating requirements for a written account, photography and drawing, depending on the circumstances, purpose and need. Level 1 is the simplest of the four levels and Level 4 is the most detailed; an additional option is a photographic survey. The document also provides guidance for the preservation and dissemination of records.

As the document is very clear, its purpose is to provide guidance, not to precisely prescribe requirements, because every site is unique and every project has different needs. As an overarching principle, however, the recording should be proportionate to the importance of the heritage asset and the degree of change to it that is proposed.

Importance is determined by designation: nationally listed buildings, followed by locally listed and then the rest. A building of high significance with many original features undergoing some minor alterations may require a higher level of recording than a building of low significance with few interior features that is being demolished.

Section 5 of Historic England's guidance describes the 5 levels of recording, this information is summarised below:

- Level 1 is a **basic visual record** supplemented by the minimum of information needed to identify the building's location, age and type. Exterior photography is sufficient.
- Level 2 is a **descriptive record**. Both interior and exterior photos are required along with written information including a short summary of the building's development and use. Some measured drawings may accompany this.
- Level 3 is an **analytical record** that will comprise a systematic account of a building's origins, development and use. The evidence that has been gathered will also be appraised. Drawings as well as photographs, plus information about construction materials, will be required to illustrate the building's structure and support the historical analysis. Level 3 may be appropriate when the fabric of the building is under threat of where the scope for detailed documentary research is limited. This level is suitable for the dismantling of a structure prior to re-erection. The drawn and photographic record will be comprehensive and will be made prior to, and during, dismantling.
- Level 4 is a **comprehensive analytical record** and is appropriate for buildings of special importance. This level builds upon level 3 by including further comment on the architectural, social and historical significance of the building in a wider context, and preparing further detailed drawings.
- Photographic survey is a different type of record primarily providing a **full visual record** of a site but without an equal level of written or drawn level. This option is appropriate when drawings and analysis of the historic sources have been taken care of in the past.

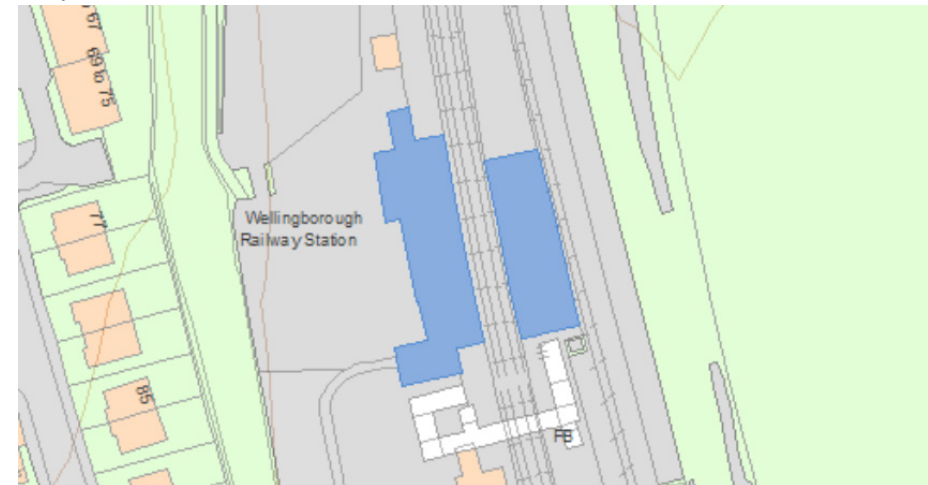
# Appendix B: National Heritage List Entry

## Wellingborough Railway Station

### Overview

Heritage Category:	Listed Building
Grade:	II
List Entry Number:	1191880
Date first listed:	05-May-1981
Date of most recent amendment:	20-Nov-2014
Statutory Address:	Wellingborough Railway Station, Midland Road, Wellingborough, NN8 1NQ

### Map



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The above map is for quick reference purposes only and may not be to scale. For a copy of the full scale map, please see the attached PDF - [1191880 .pdf](#)

The PDF will be generated from our live systems and may take a few minutes to download depending on how busy our servers are. We apologise for this delay.

This copy shows the entry on 30-Jul-2019 at 15:50:02.

## Location

Statutory Address: Wellingborough Railway Station, Midland Road,  
Wellingborough, NN8 1NQ

The building or site itself may lie within the boundary of more than one authority.

County: Northamptonshire

District: Wellingborough (District Authority)

Parish: Non Civil Parish

National Grid Reference: SP9035168133

## Summary

Main station building and canopy to platform 1 of 1857 by Charles Henry Driver for the Midland Railway, with later additions. Platforms 2 and 3 share a building and canopy of 1894.

## Reasons for Designation

Wellingborough Railway Station, built in 1857 by Charles Henry Driver for the Midland Railway, is listed at Grade II for the following principal reasons:

\*Architectural interest: as a station built in a distinctive Venetian Gothic style to a common design language as other buildings along the Midland Railway's Leicester to Hitchin line;

\* Historic interest: as a station built for the Midland Railway company, one of the most important and ambitious companies of the era of railway development in England;

\* Architect: as a station designed by Charles Henry Driver, a notable railway architect and expert in the architectural use of ironwork with several listed buildings to his name;

\* Rarity: as a station with a ridge and furrow canopy with decorative cast iron columns and spandrels, a rare surviving canopy of this type;

\* Group Value: as a station that possesses group value with the adjacent listed railway goods shed, built to a similar design by the same architect in 1857.

## History

Wellingborough Railway Station was built as part of the Midland Railway's Leicester to Hitchin Line, which opened in 1857. The line was engineered by Charles Liddell (c.1813–1894) and the stations were designed by the architect Charles Henry Driver (1832–1900). The Midland Railway had been formed in 1844 by the merging of three railway companies which met at Derby: the North Midland Railway, the Birmingham & Derby Junction Railway and the Midland Counties Railway.

Driver adopted a distinctive simplified Gothic style for the stations on the Leicester to Hitchin Line, the details of which were later illustrated in Henry Laxton's *Examples of Building Construction* (1858). The station buildings were built to a T-plan comprising a two-storey stationmaster's house with end gables and a single storey ticket office with ridge-and-furrow canopy flanking the platform. Wellingborough, like Kettering, was built as a larger version of this design, extended to a cruciform plan. The appearance of the original station at Kettering is recorded in an illustration in the *Illustrated London News* (23 May 1857); it is likely that Wellingborough was identical.

In 1882 the line through Wellingborough was quadrupled to increase capacity. The new (slow) lines, for goods, were built to the east of the old (fast) lines. Platform 1 was on the west side of the tracks, adjacent to the main station building, while platform 2 became an island between the lines. In 1883 a covered lattice footbridge was built to connect the two platforms across their south ends. At the same time, as shown in contract drawings, the canopy to platform 1 was extended by an additional two bays to meet the footbridge, while a new building with a more modest canopy was built on platform 2. This arrangement is shown in the first edition Ordnance Survey map of 1886, by which time further works had taken place: both ends of the main station building had been extended; two goods sheds had been added to the south of the main station range (the one adjacent to the tracks survives and is Grade II listed); and a signal box had been erected to the north (this was replaced in 1893 by a signal box located between the fast and slow lines, now demolished).

In 1894 the slow lines came into passenger use for the new Higham Ferrers branch. The island for platform 2 was rebuilt with a new building and canopy to accommodate platform 3 on its east side. Platform 4, also with a building and canopy, was added on the east side of the tracks and the footbridge was extended accordingly. The flat canopy with wooden valance at the north end of platform 1, to a similar design as that on the island, may also have been added at this time. The Higham Ferrers branch ceased regular services in 1959, after which the building and canopy to platform 4 were demolished.

The south end of the main station building was extended in the late-C19 for goods offices and there were further alterations and extensions in the C20. Several ancillary buildings were built in the early C20 including a telegraph office on platform 1 and a Permanent Way Inspector's office on platform 4. In 1986 the Wellingborough Civic Society reinstated the station's original decorative scheme, reusing architectural details, and added the porch canopy to the forecourt. The footbridge was replaced by British Rail in 1983 and again by Network Rail in 2011.

### Details

Main station building and canopy to platform 1 of 1857 by Charles Henry Driver for the Midland Railway, with later additions. Platforms 2 and 3 share a building and canopy of 1894.

**MATERIALS:** The main station building is constructed of red brick with white and blue brick and sandstone dressings, under a slate roof with timber barge-boards. Platform and entrance canopies of cast-iron or steel covered in fibre-glass sheeting. Platforms 2 and 3 have a red brick building with a timber canopy on cast-iron brackets.

**PLAN:** The main station range of 1857 is located on the west side of the line. It has a two-storey gabled cross-wing and a long single-storey gabled range (originally forming a T-plan) with late C19 additions to the north; a series of stepped lower gables, and late C19 and early C20 additions to the south; a passageway and goods office. A canopy oversails platform 1 on the east side. On platform 2 and 3 is a single-storey range of 1894 with a flat canopy.

**EXTERIOR:** The main station range of 1857, with later additions, has gable-ends with decorative pierced timber barge-boards and finials; the eaves have dentils of white brick on a band of blue brick. There are several brick chimney stacks with oversailing courses. The fenestration largely comprises distinctive round-headed windows fixed in timber frames with lozenge glazing or sashes (those to the first floor have had their glazing replaced with metal louvres). They have hoodmoulds, with banded blue-and-white-brick reveals, which finish in decorative corbelled label-stops. Most of the windows are arranged in pairs, so that the hoodmoulds merge to a moulded sandstone mullion between. The cills are sandstone and rest on corbels. The doorways have pointed-arch lintels with the same hoodmoulds and similar banded reveals. Above the windows of the single-storey range is a blind arcade formed of arches detailed in patterned blue and white brick.

The main station building has a west-facing façade of ten bays. The first (northernmost) bay is blank but has been altered to incorporate a cash machine; the second has paired-windows without the banded brickwork and with plain one-over-one sashes; the third steps forward to a gable-ended extension; beneath the gable there is a window, a bracketed lamp and double doors with a transom light. Next there is the gable end bay of the original cross-wing. It has paired first-floor windows and tripartite ground-floor windows in a Venetian pattern. The attached single-storey block originally had six bays of paired-windows in round-headed arches. The first bay has a gable-ended extension, without bargeboard but with re-used arch and windows; the second, which originally projected to a porch, has a casement window beneath a glazed ridge-and-furrow canopy of 1986; the third and sixth are unaltered; the fourth and fifth now have a single window. Attached to the south of the main block is the former goods office, now a toilet block, which returns to the forecourt. This has round-headed six-over-six sash windows with white brick arches and blue brick hoodmoulds that carry between as a band-course. The south elevation, facing the pier of the footbridge, has simple six-over-one sashes with flat-arch lintels.

The platform (east) elevation of the main station building has, from north to south: a bay with altered door openings beneath a dormer; the gable-end of the cross-wing with tripartite-windows (the central one is an altered doorway, which has a transom light and pointed-arch lintel); a blind arcade of four bays, each with a pointed arch, a white-and-blue-brick diaper, door and paired-windows. The pier between the first two of these bays carries a Victorian post-box. In the late C19 the arcade was extended by a further two bays. The first bay was subsequently altered to incorporate a passageway to the forecourt. Beyond the arcade is a final blank bay.

Extending from the northern two bays of the main station building is a C20 flat wooden canopy with a pointed valance on steel joists. The southern four bays have a ridge-and-furrow canopy of 1857, which was extended across the two adjacent bays in 1883. It has cast-iron columns with plinths and capitals beneath four-way brackets to decorative spandrels, also of cast iron, with delicate pierced foliate work.

On the island forming platforms 2 and 3 is a red-brick building of 1894 with a blue-brick course below the lintels and a weather-boarded north elevation. The north and south elevations each have a timber door; the east and west have doors and six-over-one casement windows. The building carries a flat wooden canopy with a pointed valance on decorative cast-iron spandrels with sandstone corbels. The south end of the canopy projects on two cast-iron columns.

INTERIOR: The north end of the main station range, including the 1857 portion, houses (from north to south): plant for the cash machine; the former parcels office; the booking office; the entrance lobby and booking hall; and a café. These rooms have been altered but retain some fixtures and fittings, including four-panelled doors, door and window surrounds, cornices and skirting boards. The former parcels office has a partition with glazed joinery and a copper counter, probably of the early C20; the booking office (built as the Stationmaster's office) has been opened-up to the adjacent booking hall with two round-headed openings and partitioned on the forecourt side for a staff kitchen and toilets. The booking hall has an open truss roof of iron beams and chamfered timber rafters. The cash machine room and the first floor were not available for inspection.

The former goods office beyond the passageway at the south end of the main station range was converted to a toilet block in 2011 and has no visible surviving fixtures and fittings.

Of the island platform building, only the room at the southern end was available for inspection. It retains a fireplace with simple surround and dado with dado rail, wainscot and skirting board; the rail carries around as the top rail of the integrated wooden benches, which have ramped arms and baluster legs.



**SUBSIDIARY FEATURES:** To the north of the main station range is an early C20 telegraph office and across the tracks to the east is an early C20 former Permanent Way Inspector's office. At the south end of the station is a covered footbridge with a stepway and lift to each platform built in 2011. These buildings and structures are all excluded from the listing.

Pursuant to s.1 (5A) of the Planning (Listed Buildings and Conservation Areas) Act 1990 ('the Act') it is declared that: the modern palisade fencing to platforms 1 and 3; the C21 single-storey covered metal-and-glass shelters to platform 3 and outside the station; and the C20 and C21 platform signage, metal seating, lamp posts and bicycle storage racks are not of special architectural or historic interest. Internally the plant for the cash machine, the C20 or C21 suspended ceilings, partition walls, toilets and modern services within the main station range are also declared not to be of special architectural or historic interest.

### Legacy

The contents of this record have been generated from a legacy data system.

Legacy System number: 233787

Legacy System: LBS

### Sources

#### **Books and journals**

Biddle, G, Britain's Historic Railway Buildings, (2003)

Hendry, R., An Historical Survey of Selected London, Midland and Scottish Railway Stations, (1986)

Laxton, H., Examples of Building Construction, (1858)" in Illustrated London News, (23 May 1857)

#### **Other**

Alan Baxter & Associates, Midland Main Line Statement of History and Significance, 2013,

Network Rail, National Records Centre, drawings of various dates,

Title: Ordnance Survey 1886 Source Date: 1886 Author: Publisher: Surveyor:

Title: Ordnance Survey 1900 Source Date: Author: Publisher: Surveyor:

Title: Ordnance Survey 1925 Source Date: Author: Publisher: Surveyor:

#### Legal

This building is listed under the Planning (Listed Buildings and Conservation Areas) Act 1990 as amended for its special architectural or historic interest.

The listed buildings are shown coloured blue on the attached map. Pursuant to s.1 (5A) of the Planning (Listed Buildings and Conservation Areas) Act 1990 ('the Act'), structures attached to or within the curtilage of the listed building (save those coloured blue on the map) are not to be treated as part of the listed building for the purposes of the Act.

End of official listing

# Appendix C: OASIS form

OASIS ID: alanbaxt2-363013

## Project details

Project name	Wellingborough Canopy Recording
Short description of the project	Recording of platforms 1-3 at Wellingborough Station in advance of electrification works which will necessitate cutting back of the canopies
Project dates	Start: 24-07-2019 End: 30-08-2019
Previous/future work	Yes / Yes
Any associated project reference codes	WP/19/00082/LBC - Planning Application No.
Any associated project reference codes	1191880 - NHLE No.
Type of project	Building Recording
Site status	Listed Building
Current Land use	Transport and Utilities 2 - Other transport infrastructure
Monument type	STATION CANOPY Post Medieval
Significant Finds	STATION CANOPY Post Medieval

Methods & techniques	"Photographic Survey","Survey/Recording Of Fabric/Structure"
Prompt	Planning condition
Prompt	Listed Building Consent
Project location	
Country	England
Site location	NORTHAMPTONSHIRE WELLINGBOROUGH WELLINGBOROUGH Wellingborough Railway Station
Postcode	NN8 1NQ
Study area	0 Square metres
Site coordinates	SP 490338 268132 51.937229289444 -1.286672046817 51 56 14 N 001 17 12 W Point
Project creators	
Name of Organisation	Alan Baxter Limited
Project brief originator	Local Planning Authority (with/without advice from County/District Archaeologist)
Project design originator	Patrizia Pierazzo

Project director/manager	Victoria Bellamy	Author(s)/Editor(s)	Bellamy, V.
Project supervisor	Victoria Bellamy	Other bibliographic details	1769/10
Type of sponsor/funding body	Network Rail	Date	2019
		Issuer or publisher	Alan Baxter Limited
Project archives		Place of issue or publication	London
Physical Archive Exists?	No	Description	A4 Landscape
Digital Archive recipient	ABA	Entered by	Victoria Bellamy (vbellamy@alanbaxter.co.uk)
Digital Archive ID	1769/10	Entered on	9 August 2019
Digital Contents	"none"		
Digital Media available	"Images raster / digital photography"		
Paper Archive recipient	ABA		
Paper Archive ID	1769/10		
Paper Contents	"none"		
Paper Media available	"Plan","Report"		
Project bibliography 1			
Publication type	Grey literature (unpublished document/ manuscript)		
Title	Wellingborough Station Canopy: Recording Report - Level 3		

# Alan Baxter

**Prepared by** Victoria Bellamy

**Reviewed by** Vicky Simon

**Issued** September 2019

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