

# Kettering Station platform canopies Historic Building Recording Report Level 3

Prepared for Network Rail  
September 2019





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# Executive summary

## Planning context

This report has been commissioned by Network Rail to present the findings of a Level 3 investigation of the canopies at Kettering Station undertaken in July 2019. The HER UID is ENN109606.

This follows the approval of listed building consent (application ref. KET/2019/0075) for alterations to the platform canopies, including the replacement of the trackside brackets on platforms 2–4, reinstatement of historic details and the installation of new roof coverings. The gutter of the platform 1 canopy will also be altered.

## History and description of the canopies at Kettering Station

Kettering Station opened in 1857 as part of the Midland Railway's Leicester to Hitchin Line, now the main line to St Pancras. It was designed by Charles Henry Driver in a simplified Gothic style. The four platform canopies at Kettering are of two different types:

### Platform 1

The canopy on platform 1 dates to 1895–98 when the original 1857 station building and attached canopy were demolished and replaced. It comprises an early steel, hipped, ridge-and-furrow canopy of a type then standard on the Midland Railway.

### Platform 2-4

The canopies on platforms 2–4 date from 1879 when the line through Kettering Station was quadrupled. They are constructed of multiple iron casting, with elaborate pierced foliate brackets. Although they are effectively reproductions of Driver's original 1857 canopies (as surviving today at platform 1 at Wellingborough Station) there are numerous detailed differences in their construction, which are apparent by comparing them with the surviving 1857 canopy at Wellingborough Station.

After the 1960s, the canopies on platforms 2 and 4 were trimmed. On all three platforms, the original glass glazing was replaced with corrugated polycarbonate sheets in the 1970s.

# 1.0 Introduction

## 1.1 Site location and description

The site comprises the four platform canopies at Kettering Railway Station, located on Station Road in Kettering, Northamptonshire (NN15 7HJ), approximately 0.5km from the town centre. The National Grid Reference for the site is SP 86378 78027.

Kettering Station initially opened in 1857 but was extended and altered several times during the second half of the nineteenth century. Today it comprises:

- the main station building on the east side of the line built by the Midland Railway in 1895-98, with canopy to platform 1
- platforms 2 and 3, which form an island between the east (slow) and west (fast) tracks, with canopies, two buildings and a modern platform shelter
- platform 4, on the west side of the tracks, with a canopy and a building
- a modern footbridge with associated canopies connecting the platforms across the south end of the platforms.

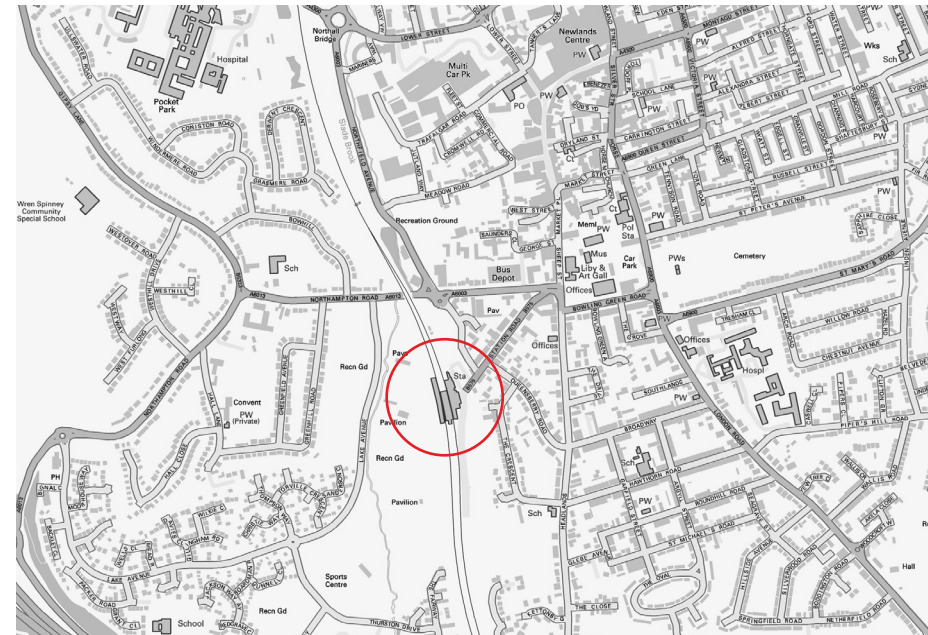


Figure 1: Site location

Contains Ordnance Survey data © Crown copyright and database right 2016

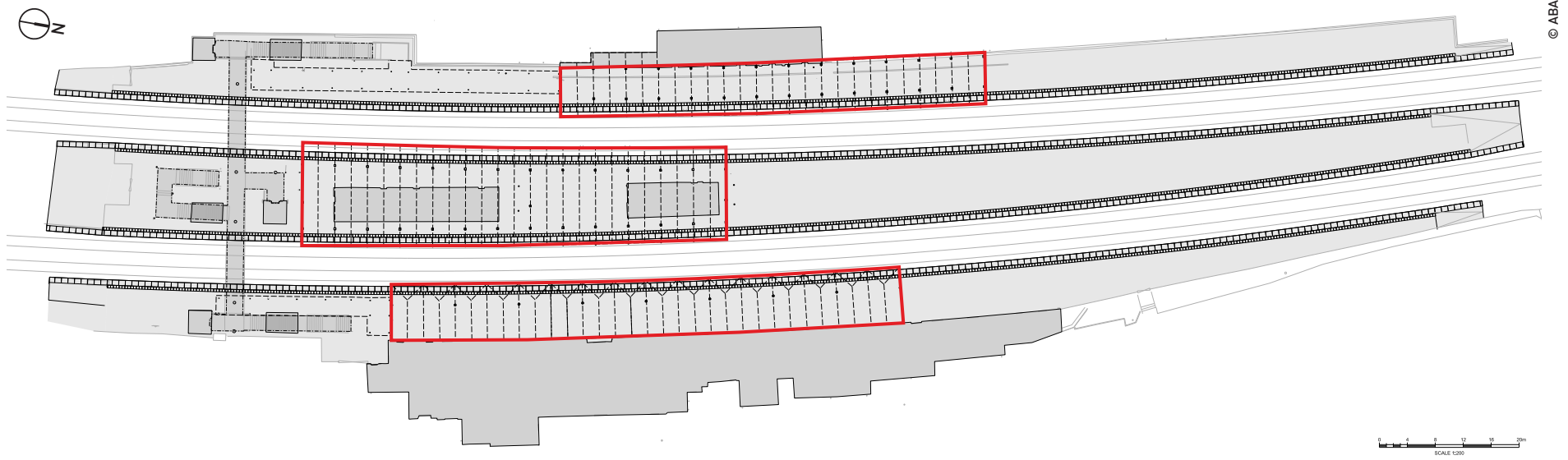


Figure 2: Site plan

— Site boundary

## 1.2 Planning background

On 8 May 2019 listed building consent (application ref. KET/2019/0075) was granted by Kettering Borough Council for alterations to the platform canopies at Kettering station. These works consist of:

- replacement of the trackside brackets on platforms 2–4, reinstatement of historic details and the installation of a Twinfix roof.
- minor alterations to the edge and gutter of the canopy on platform 1 and like for like replacement of broken glazing will also be altered.

A condition of the consent is that:

*No works shall take place until a programme of archaeological work, in accordance with a written scheme of investigation, has been submitted to and approved in writing by the Local Planning Authority. The works shall only be carried out in accordance with the approved details.*

*REASON: These details are required to ensure that features of archaeological interest are properly examined and recorded, in accordance with Policy 16 Paragraph 199 of the NPPF.*

## 1.3 Listing

Kettering Station was listed as Grade II in 1981, with one list entry for the main station building and another for the platforms. These list entries have been revised and rationalised into a single entry as a part of the Midland Main Line Upgrade. The revised list entry was published on 26 November 2014 and is reproduced in Appendix B.

The extent of the listing is the main station building and the platforms, platform buildings and canopies. The steel and glass-covered footbridge, its lifts, stepways and canopies at the south end of the station, constructed 1999–2000, as well as the platform signage, lighting, seating, lamp posts, and bicycle storage racks, which date from the late-twentieth century or afterwards, are excluded from the listing.

The revised list entry for Kettering Station identifies the following reasons for its designation:

- Historic interest: as a station built for the Midland Railway company, one of the most important and ambitious companies of the era of railway development in England;
- Historic interest: as a station associated with Charles Henry Driver, a notable railway architect and expert in the architectural use of ironwork with several listed buildings to his name, and with Charles Trubshaw, a significant railway architect responsible for listed railway buildings such as the Midland Hotel in Manchester;
- Rarity: as a station with a ridge and furrow canopy with decorative cast-iron columns and spandrels, a rare surviving canopy of this type;
- Architectural interest: the station has developed but each stage of development has architectural and historic interest and survives well.



## 2.0 The survey

### 2.1 Scope

The scope of the archaeological work was set out in the Written Scheme of Investigation (WSI) (Alan Baxter, 26 June 2019).

All work was carried out in accordance with Historic England specifications in *Understanding historic buildings: a guide to good recording practice* (HE 2016) and other applicable standards and guidance (e.g. *CIFA Standard and guidance for archaeological investigation of standing buildings or structures, 2014*).

The investigation of the site corresponds to a Level 3 standing building survey according to the Historic England specifications (HE 2016). A Level 3 investigation produces an analytical record and is comprised of an introductory description followed by a systematic account of the structure's origins, development and use.

The survey focused on those areas of the canopies that will be altered under the listed building consent, namely:

- the canopy edge and gutter on platform 1
- the outer brackets, roof structure and roof covering of the canopies on platforms 2-4

A detailed photographic survey of an example bay on platform 2 is also included to record details.

The survey of the canopies took place prior to alteration and dismantling works.

### 2.2 Methodology of the survey

A site visit, undertaken by staff from Alan Baxter on 22 July 2019, entailed the observation of the canopies. The investigation on site was non-intrusive and aimed at specific details in the fabric of the building relevant to the listed building consent application KET/2019/0075. The information gathered from the site inspection was then combined with the information found in written sources to form a history and description of the building's development.

It is the nature of existing buildings that details of their construction and development may be hidden or may not be apparent from a visual inspection. The conclusions and any advice contained in this report – particularly relating to the dating and nature of the fabric – are based on our research, and on observations and interpretations of what was visible at the time of our site visits. Further research, investigations or opening up works may reveal new information which may require such conclusions and advice to be revised.

### 2.3 Authorship

This report has been prepared by Alan Baxter Ltd, as heritage consultants to the Midland Main Line Upgrade Project of Network Rail.

## 3.0 Historical background of the site

### 3.1 Phases of development

Kettering Station has evolved over a number of stages, which are illustrated in Figure 4 on the following page. The resulting age of surviving fabric is shown on the following drawing, Figure 5.

### 3.2 Construction of Kettering Station

The first phase of development dates to the construction of the Midland Railway's Leicester to Hitchin Line, which opened in 1857. The stations on that line were designed by Charles Henry Driver in a distinctive simplified Gothic style and Kettering was featured in an *Illustrated London News* story on the opening of the route (23 May 1857, see fig. 3). It had a short attached canopy, and there may have been a freestanding canopy on the opposite platform too (not shown in the engraving).

Although the original building has been demolished (see section 3.3 below), nearby Wellingborough Station survives to a similar design today. Driver worked with the Midland Railway's engineer for the route, Charles Liddell.

### 3.3 Canopies on platforms 2–4 (1879)

In 1879 the number of lines through Kettering Station was doubled from two to four. The canopies on platforms 2–4 date from then. They are cleverly designed from multiple iron castings, incorporating elaborate pierced foliate brackets and other decorative details.

Although superficially reproductions of the 1857 canopies designed by Driver for the original station, they have numerous detail differences in their construction which are apparent by comparing them with the surviving 1857 canopy at Wellingborough Station.

The canopies on platforms 2 and 4 were cut back prior to 1979 to improve clearances for passing trains. They lost decorative end details at this point. In 1979 the original sheet glass and associated glazing bars were removed from all three platforms, and replaced with the current profiled fibreglass/plastic sheeting (see historic drawing reproduced as Figure 9).

Although the fabric of the canopies on Platforms 2–4 does not date to the 1857 station, these canopies are significant for the outstanding quality of the casting and their extent. Together, these make them unusual survivors on the national rail network.



Figure 3: 1857 station at Kettering as depicted in the *Illustrated London News* (23 May 1857)

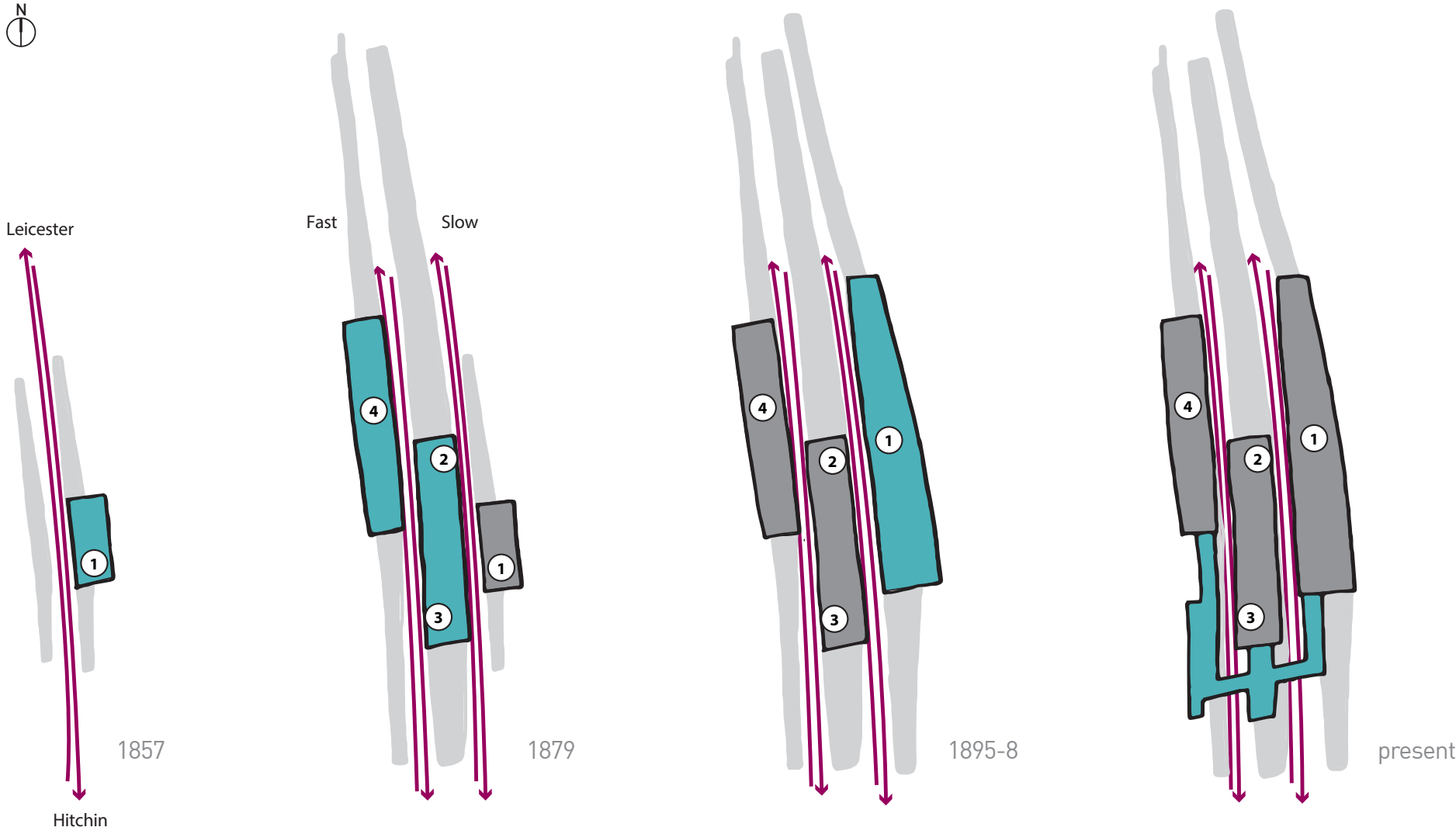


Figure 4: Historical development of Kettering Station

### 3.4 Canopies on platform 1 (1895–98)

As the size of Kettering and importance of the station grew, the original 1857 entrance building on platform 1 proved inadequate, and in 1895-98 it and its canopy were demolished and replaced by a building probably designed by Midland Railway architect Charles Trubshaw. Attached to this is a hip-roofed canopy, very typical of the Midland Railway in this period.

### 3.5 Later changes

Since the war a number of alterations have been made, in addition to the canopy changes described above. The extensive railway yards and sheds that surrounded the station have been swept away. On the station itself, the 1879 subway was replaced by a footbridge at the southern end of the platforms in 1999–2000.

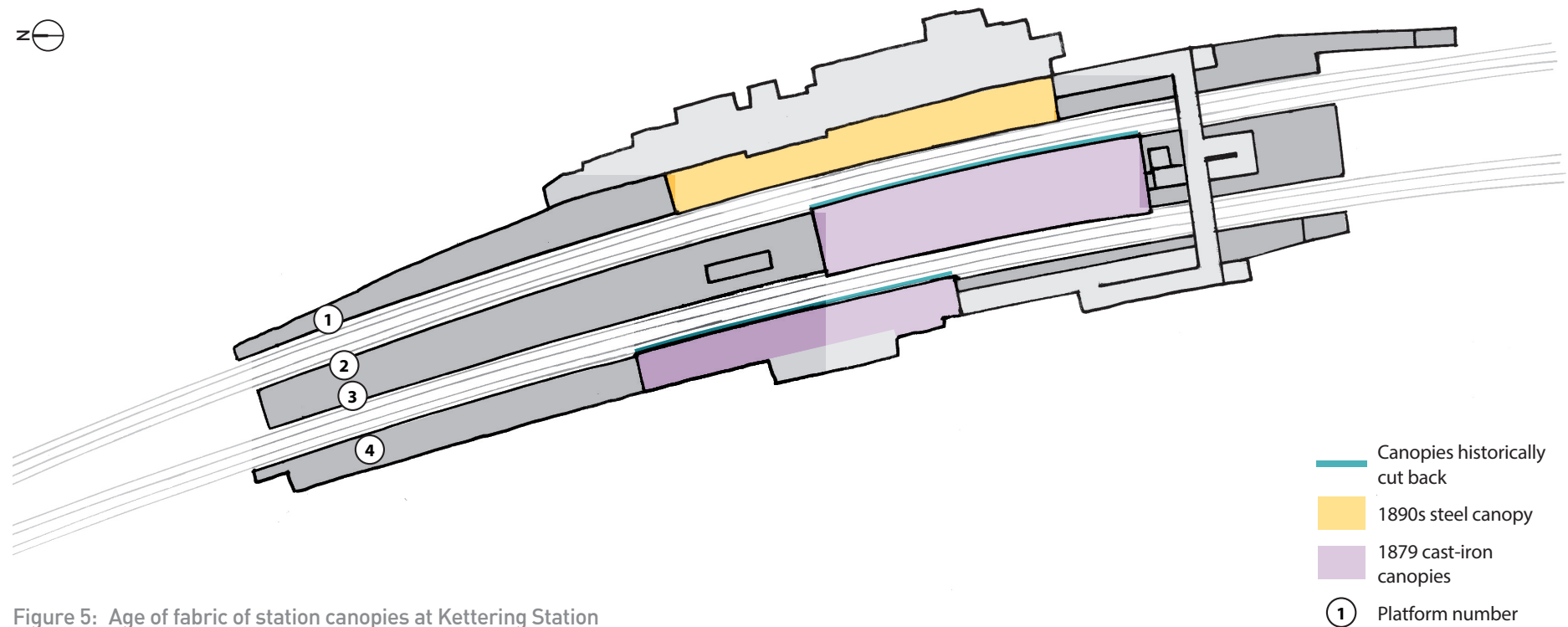


Figure 5: Age of fabric of station canopies at Kettering Station

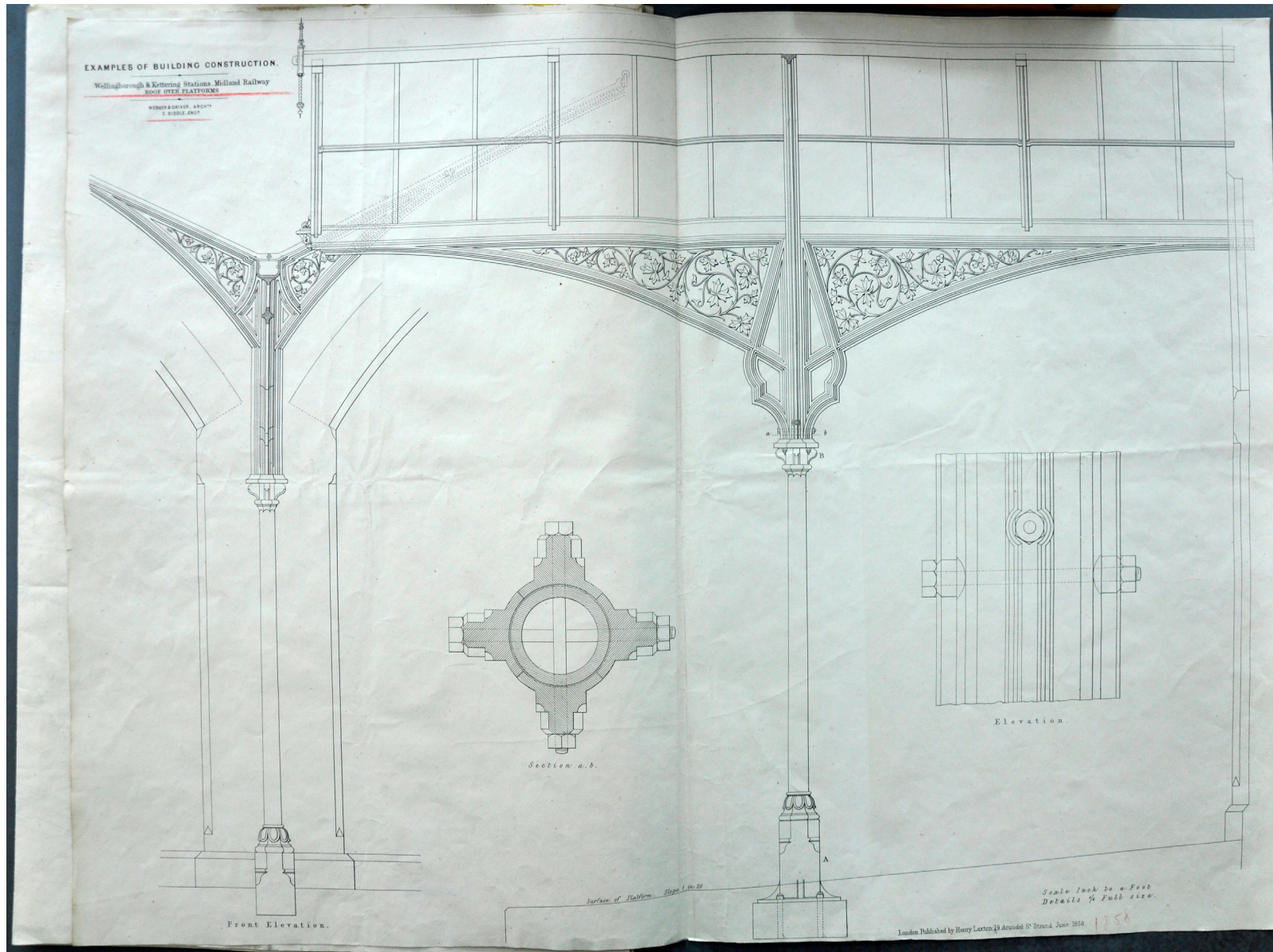


Figure 6: Detailed drawings of the 1857 canopy signed by Driver and reproduced in Henry Laxton's *Examples of Building Construction* in 1858. The surviving 1879 canopies at Kettering were based on this earlier design.

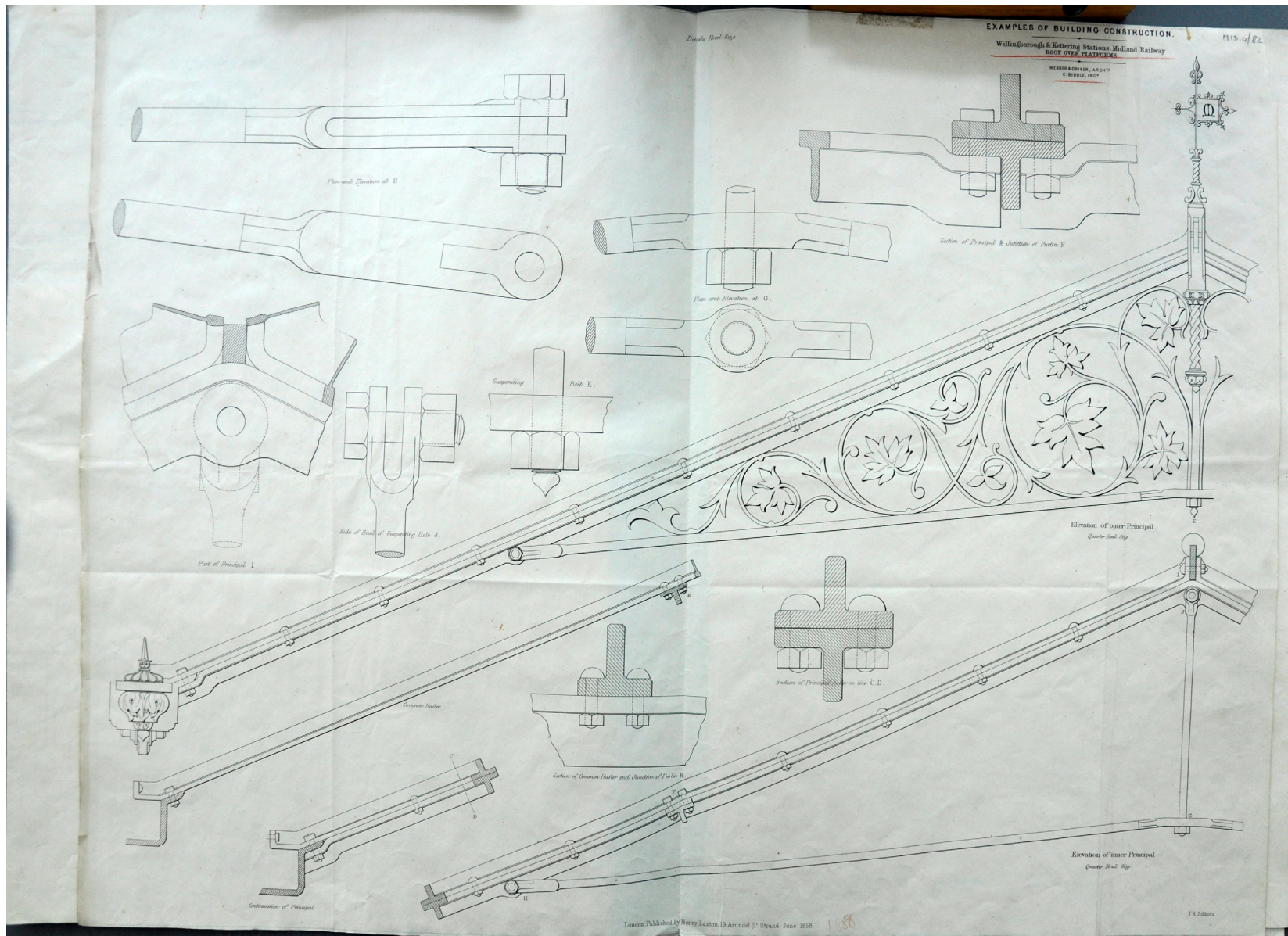


Figure 7: Detailed drawings of the 1857 canopy signed by Driver and reproduced in Henry Laxton's *Examples of Building Construction* in 1858. The surviving 1879 canopies at Kettering were based on this earlier design.

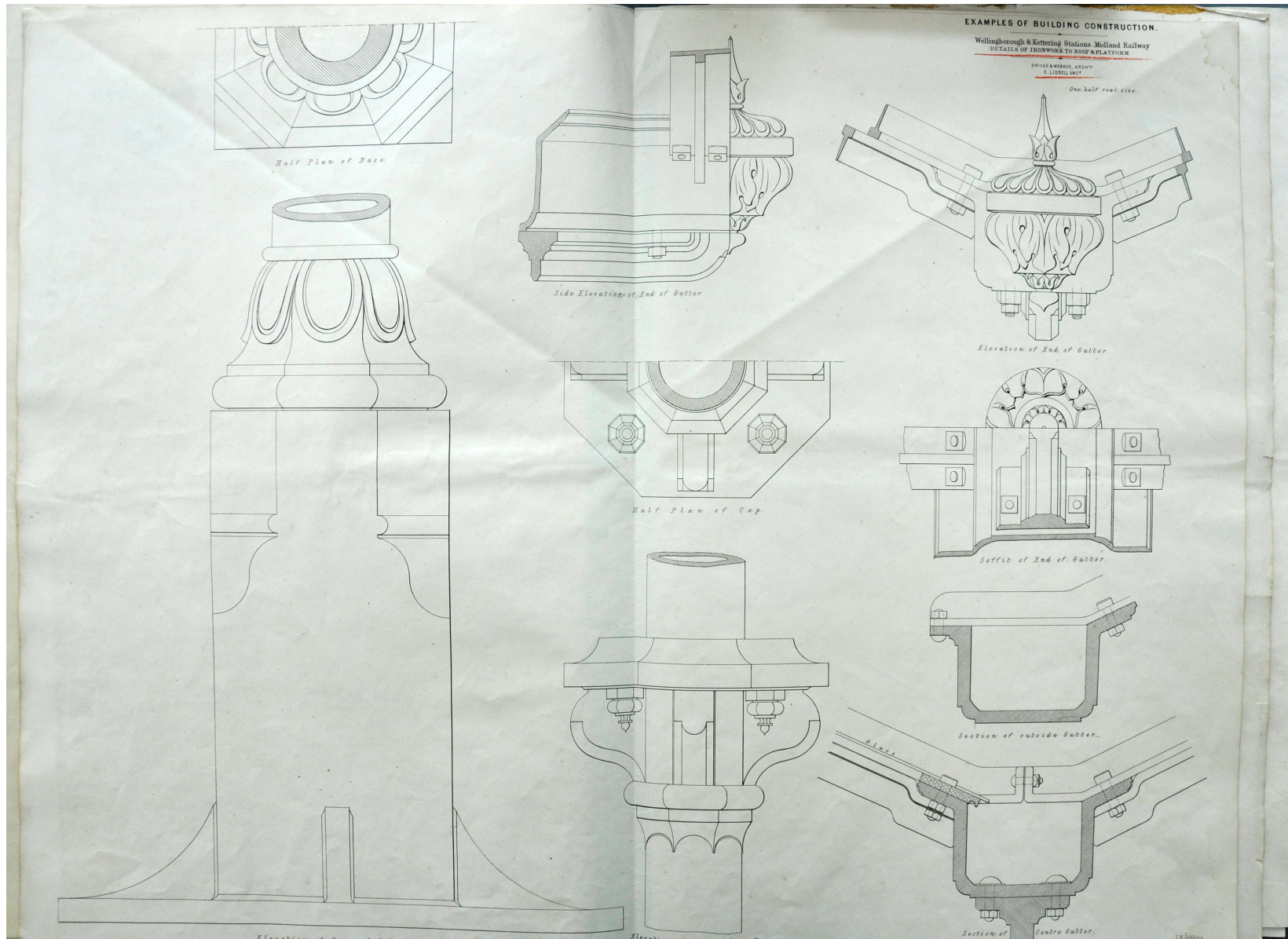


Figure 8: Detailed drawings of the 1857 canopy signed by Driver and reproduced in Henry Laxton's *Examples of Building Construction* in 1858. The surviving 1879 canopies at Kettering were based on this earlier design.

## 4.0 Description of the standing fabric

### 4.1 Introduction

The canopies on platforms 1–4 at Kettering Station were photographed by the author on 22 July 2019. This section should be read in conjunction with the photographic record (Section 4.3).

### 4.2 Platform 1

The canopy on platform 1 is comprised of an early steel, hipped, ridge-and-furrow canopy of a type favoured by the Midland Railway. It consists of 16 hipped bays. The rear of the canopy abuts the station building, and the beams are embedded into its brickwork. A column supports each beam mid span, beneath a longitudinal beam running the full length of the canopy. The columns are likely to be cast-iron because of the material's compressive strength. The columns have an octagonal section with classical base and capital details, and simple brackets to support the longitudinal beam. The canopy is glazed with Georgian Wired Glass with four panes between each principal rafter.

### 4.3 Platforms 2-4

The gabled 1879 canopies on platforms 2–4 are impressively integrated and beautifully cast engineering structures. The clever way in which the various components are slotted together is illustrated by the exploded axonometric drawing Figure 9. Detailed photographs of the components can be found in section 4.4.5.

The single free-standing island structure on platforms 2–4 has 13 bays. The canopy on platform 4, which is set out further north, is also of 13 bays, and is backed by a timber wall along the rear of the platform that acts as a wind and rain screen.

On platforms 2 and 3 a single structure supports the canopy over the island platform. Here the structure is generally made up of three rows of cast iron columns between which cast-iron beams span. Cast-iron brackets cantilever

from the columns to support the canopy in four directions: the principal brackets are perpendicular to the platform and the secondary are in line with the platform, forming the rafters of the roof. Figure 10 shows how these are bolted around a spindle at the top of the column. The brackets have elaborate foliate decoration in the spandrels, created by highly complex and beautifully executed casting.

Cast iron rafters are attached to each column completing the primary frame of the canopy. A cast iron gutter is fixed to the cast iron brackets and beams, discharging down the inside of the columns. Secondary tied rafters joined at the ridge are fixed to the flanges of the cast iron gutter. As such, the gutter is an integral part of the roof structure. Purlins span between rafters providing support for the roof finishes (see Figures 11 and 12).

The canopy on platform 4 is similar, but owing to it being on a single platform, it only has cantilever brackets projecting from one column.

Comparing the brackets both to each other and the historic drawings demonstrates that the brackets have been previously cut back (c.1960-1979). This was done so that they track here could be 'canted' (angled over) so that trains could run at faster speeds around the bend. Platform 4 was cut back the most, platform 3 only very slightly. It is likely that at this time the decorative gable end panels were removed. These survive on platform 3. However, the 'bosses' that decorate the end of the gutters were reinstated.

In 1979 the original glazing and secondary iron glazing bars were removed and replaced by corrugated polycarbonate sheets attached to wooden battens (see 1979 drawing, Figure 13). End boards and wooden finials were installed at this time. It is not known when the original iron finials were removed (see Figure 7), but it may have been when the canopies were cut back.



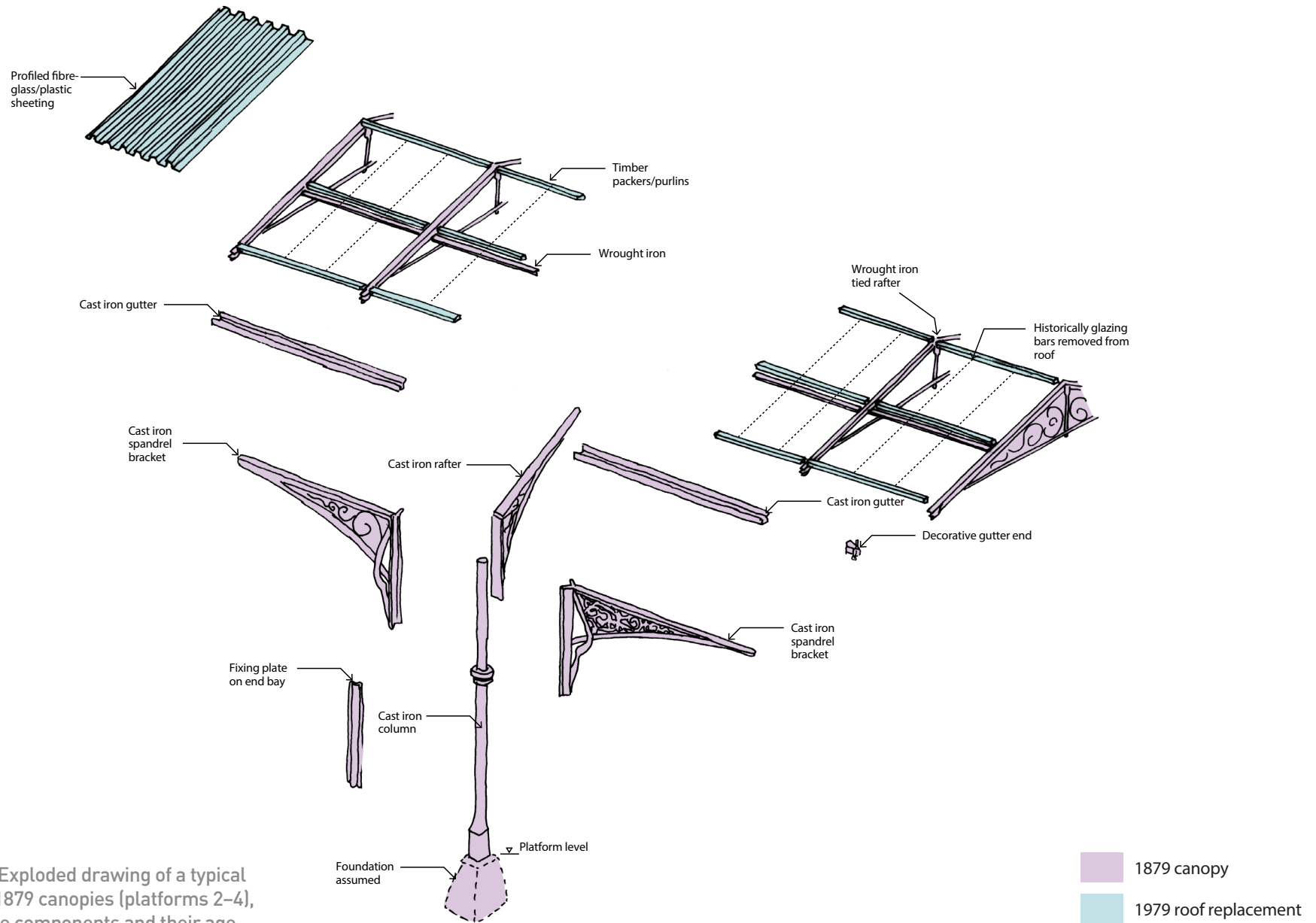


Figure 9: Exploded drawing of a typical bay of the 1879 canopies (platforms 2-4), showing the components and their age

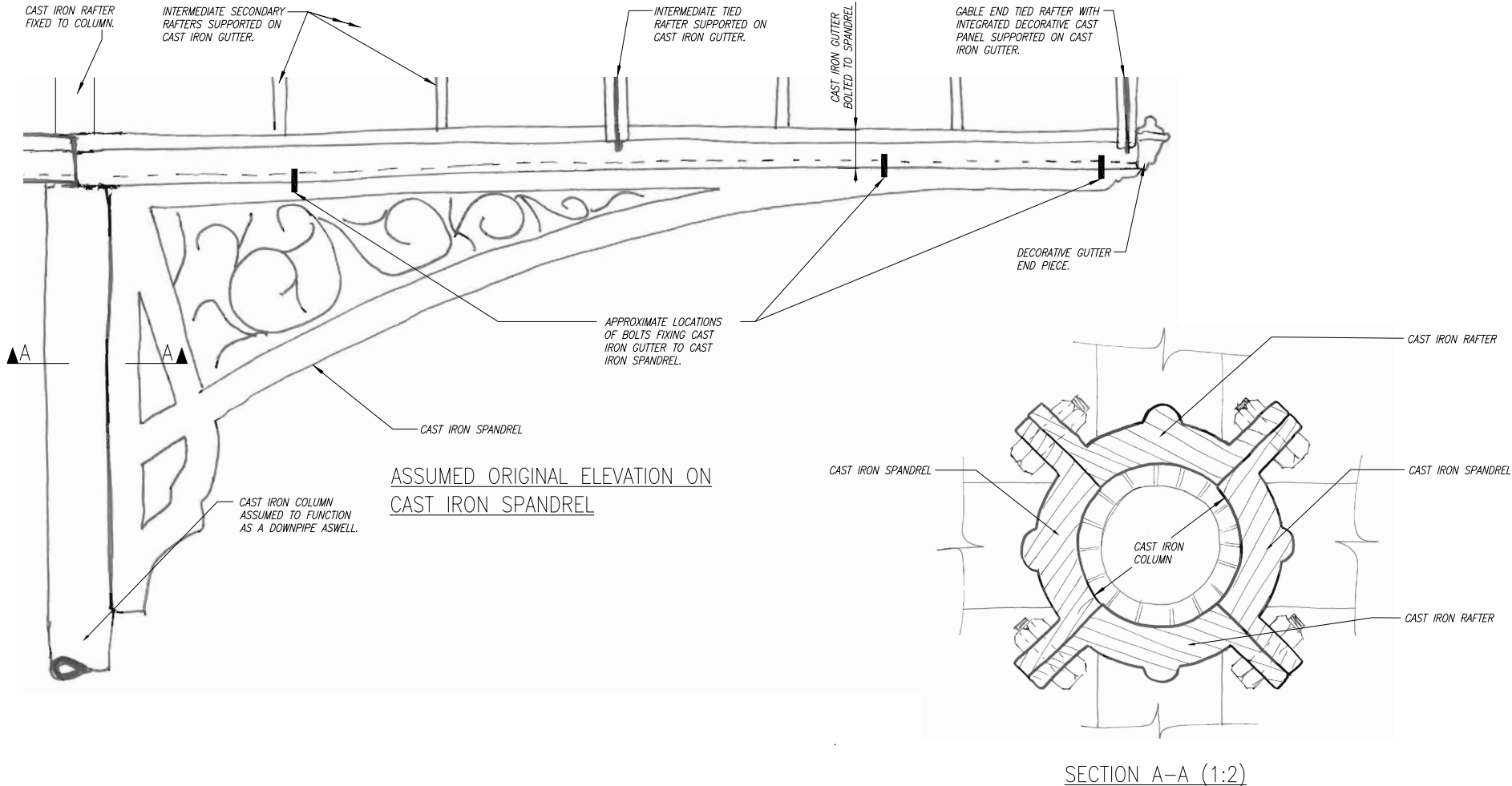


Figure 10: Sketch drawing from observation of existing 1879 canopies on platforms 2-4 showing bracket details

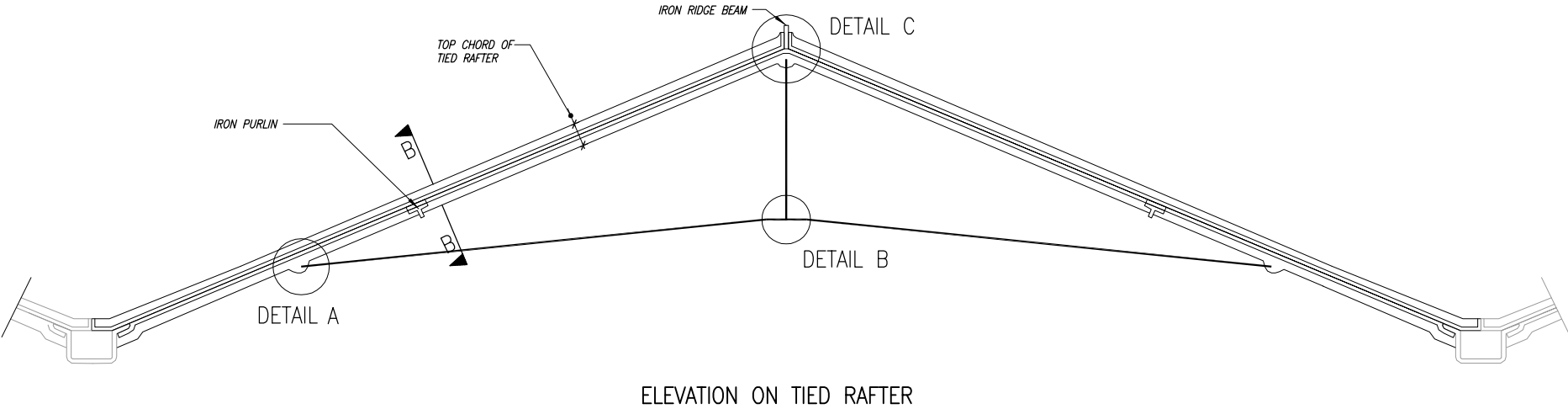


Figure 11: Sketch drawing from observation of existing 1879 canopies on platforms 2-4 showing roof details

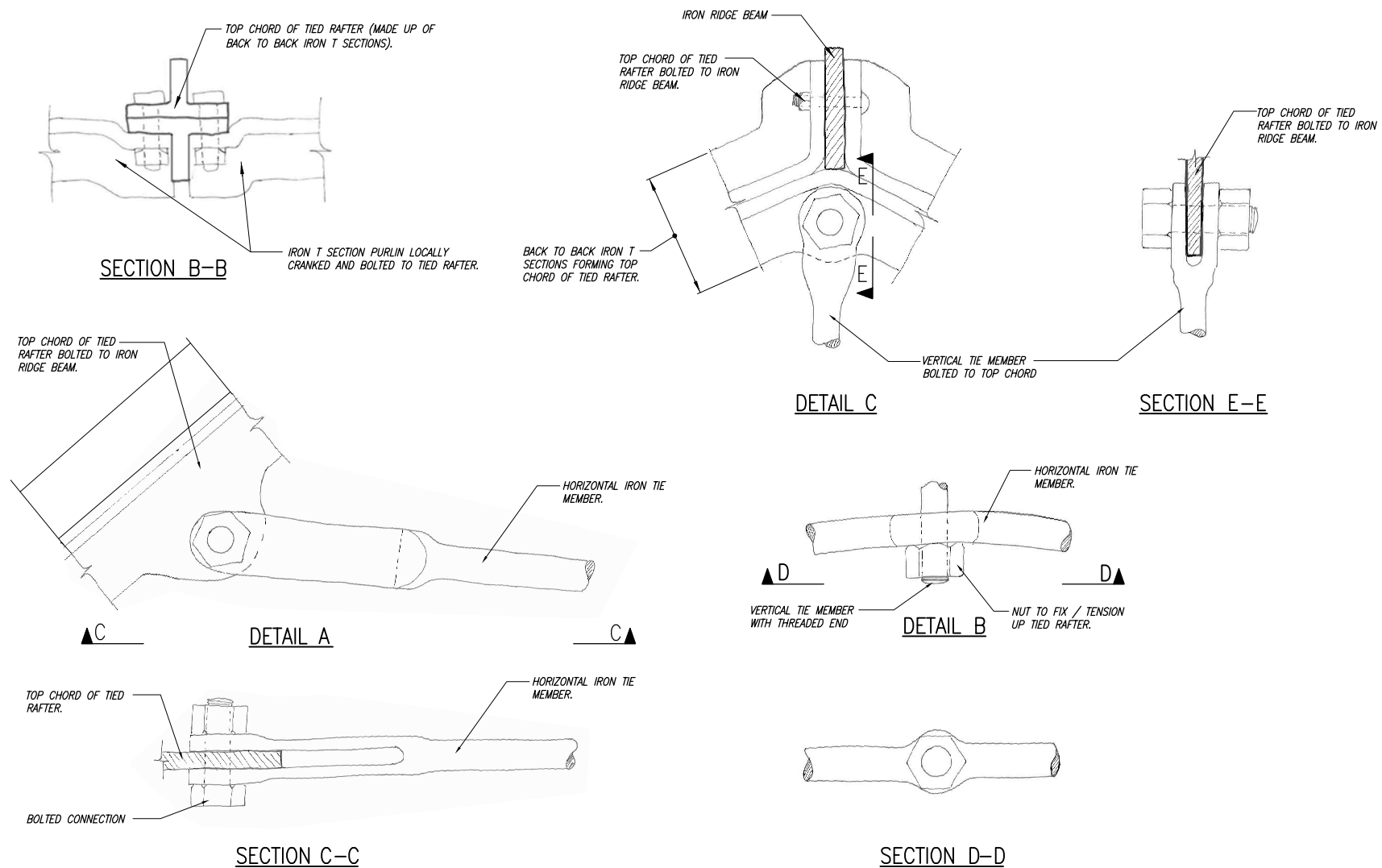


Figure 12: Sketch drawing from observation of existing 1879 canopies on platforms 2-4 showing roof details

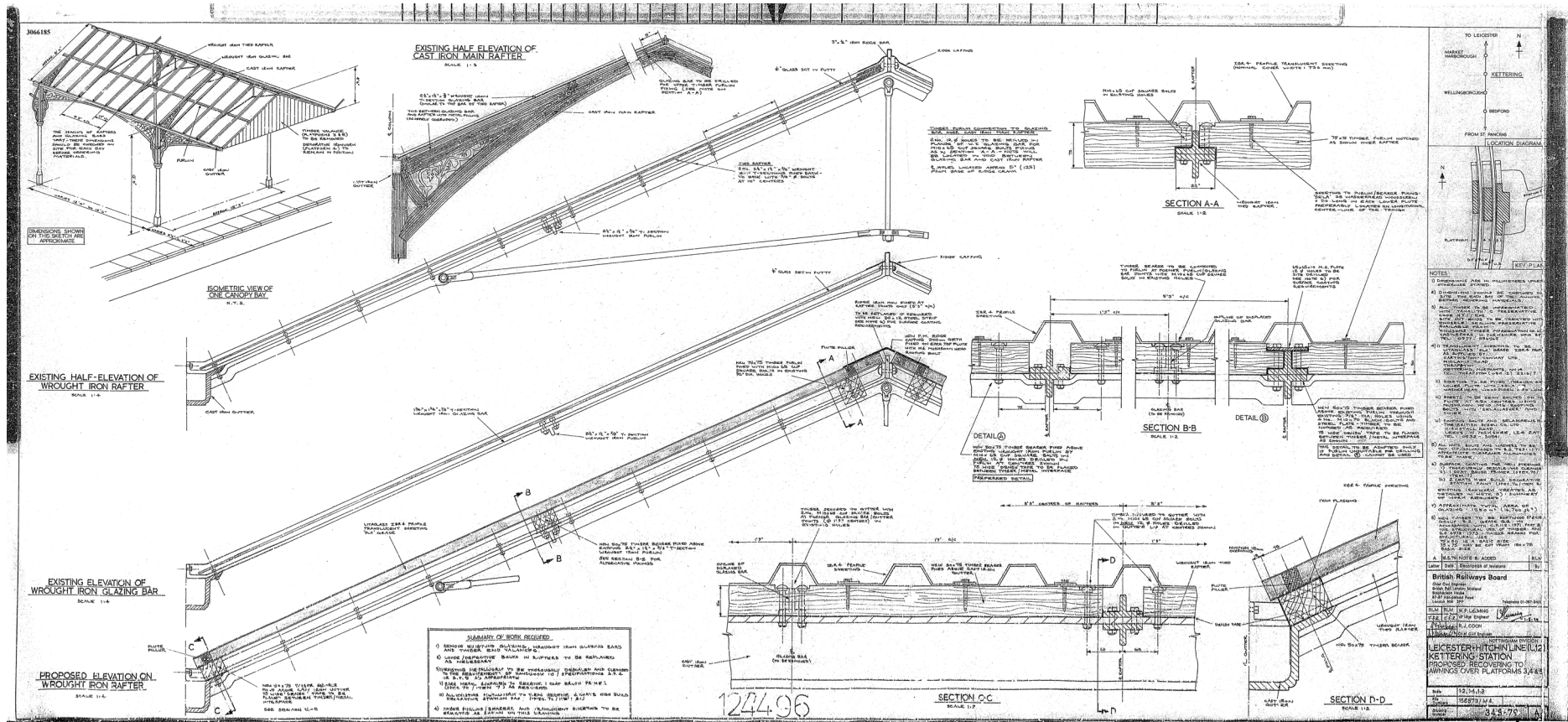


Figure 13: 1979 drawing showing replacement of roof covering and removal of historic glazing bars

### 4.4 Photographic record

#### 4.4.1 Platform 1

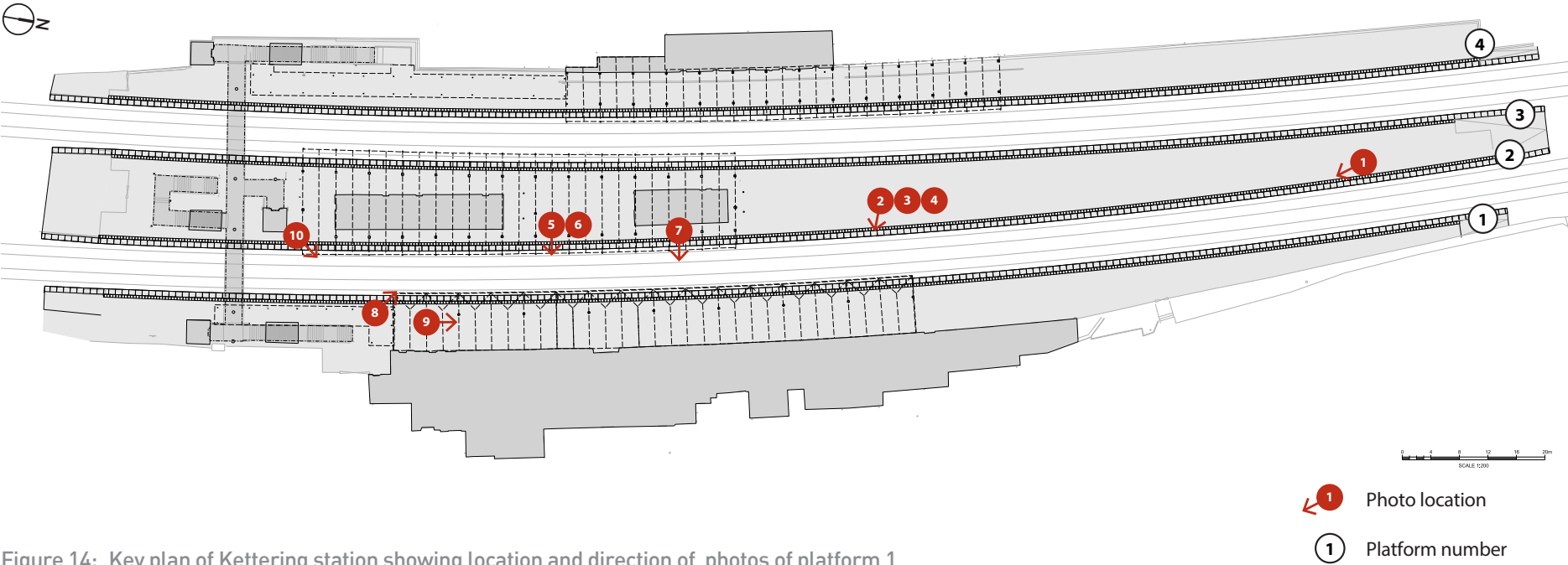


Figure 14: Key plan of Kettering station showing location and direction of photos of platform 1

## 4.0 Description of standing fabric



Platform 1 canopy looking south-east from the northern end of platform 2



Northern end of platform 1 canopy looking east from platform 2



Detail of end bay of northern end of platform 1 canopy (close up of photo 2)



Detail of end bay of northern end of platform 1 canopy (close up of photo 4)



Elevation of platform 1 canopy, seen from middle of platform 2



Detail of elevation of platform 1 canopy, seen from middle of platform 2 (close up of photo 5)

#### 4.0 Description of standing fabric



Detail of missing end detail taken from platform 2



Long view looking north along platform 1 canopy



Detail of southern end of platform 1 canopy



Platform 1 canopy, looking north-east from the southern end of platform 2



4.4.2 Platform 2

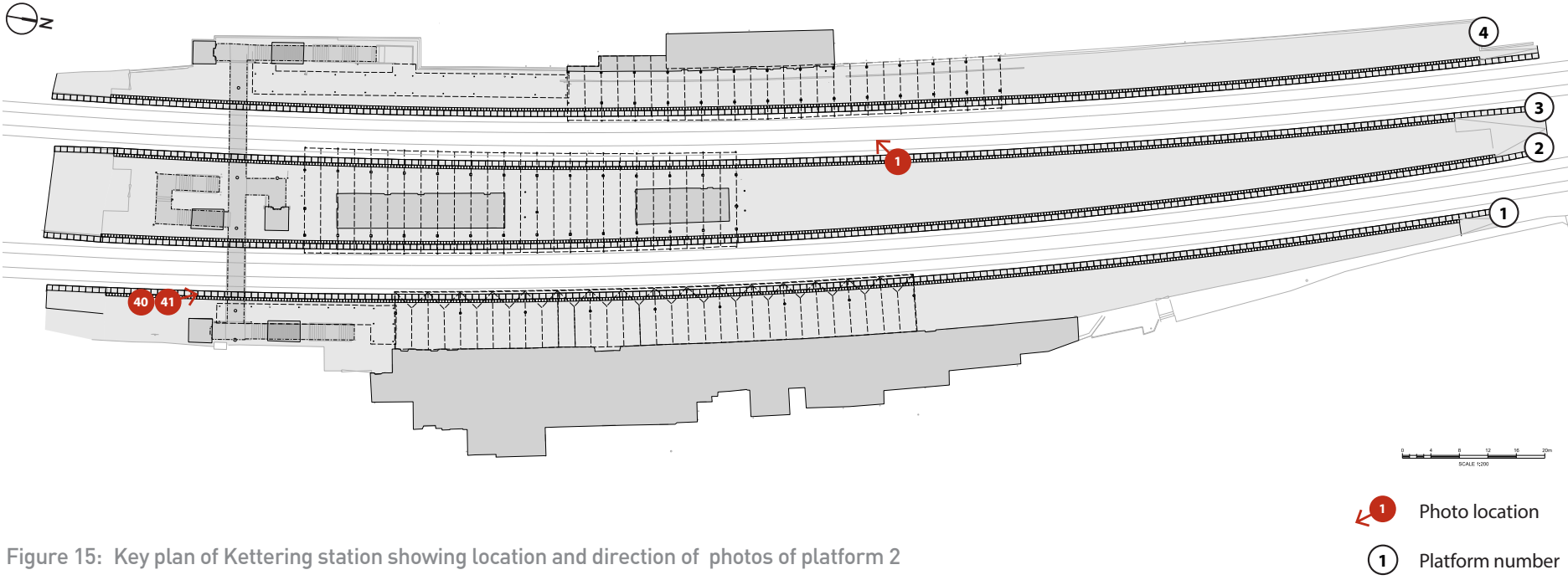


Figure 15: Key plan of Kettering station showing location and direction of photos of platform 2

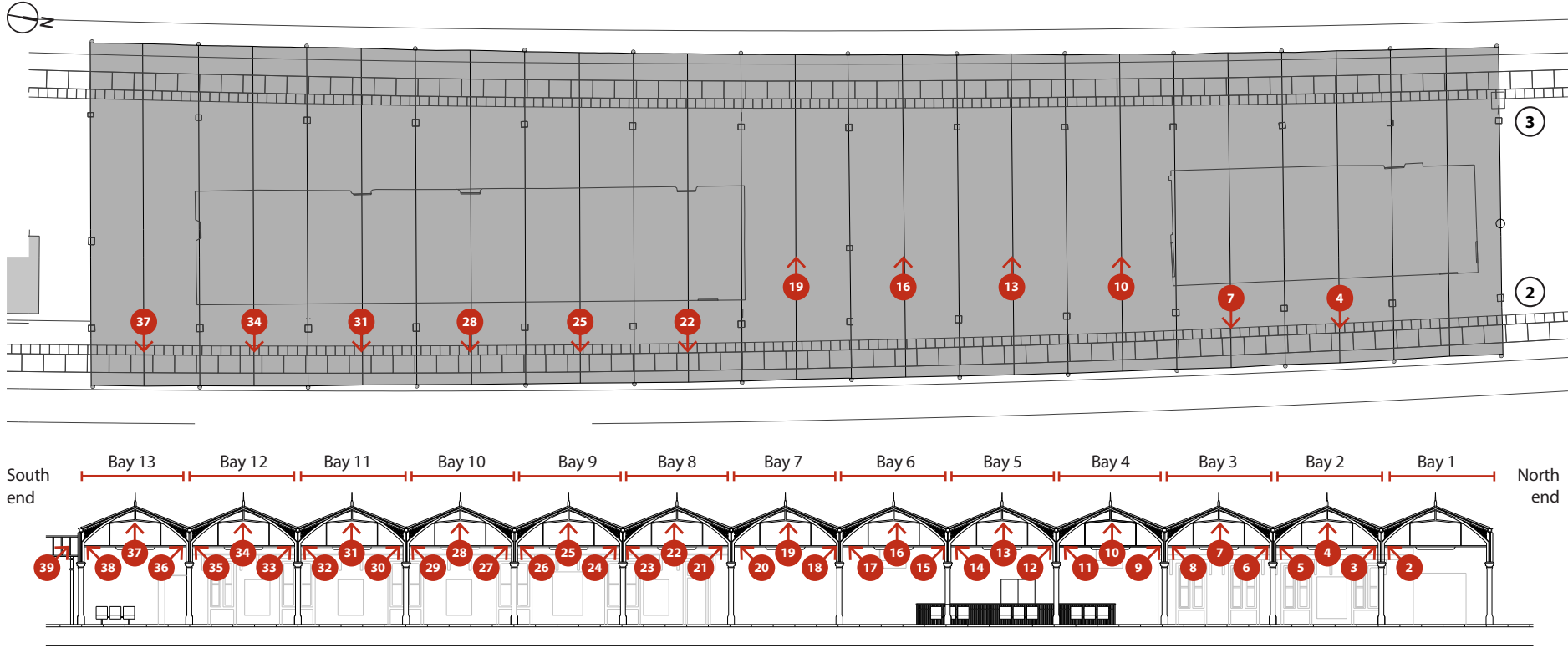




Figure 16: Key plan (top) and elevation (bottom) of platform 2 showing location and direction of photos. The direction of photographs taken of the roof of each bay is shown in the top plan.

-  Photo location
-  Platform number

#### 4.0 Description of standing fabric



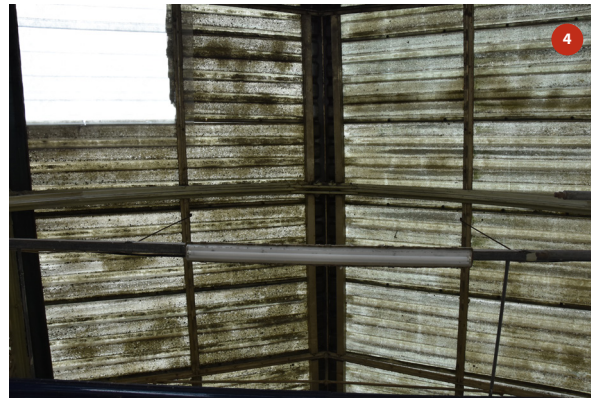
Platform 2 canopy looking north-west from the southern end of the canopy on platform 1



Southern elevation of bay 1



Northern elevation of bay 2



Roof of bay 2



Southern elevation of bay 2

4.0 Description of standing fabric



Northern elevation of bay 3



Roof of bay 3



Southern elevation of bay 3



Northern elevation of bay 4



Roof of bay 4



Southern elevation of bay 4

4.0 Description of standing fabric



Northern elevation of bay 5



Roof of bay 5



Southern elevation of bay 5



Northern elevation of bay 6



Roof of bay 6



Southern elevation of bay 6

#### 4.0 Description of standing fabric



Northern elevation of bay 7



Roof of bay 7



Southern elevation of bay 7



Northern elevation of bay 8



Roof of bay 8



Southern elevation of bay 8

#### 4.0 Description of standing fabric



Northern elevation of bay 9



Roof of bay 9



Southern elevation of bay 9



Northern elevation of bay 10



Roof of bay 10



Southern elevation of bay 10

#### 4.0 Description of standing fabric



Northern elevation of bay 11



Roof of bay 11



Southern elevation of bay 11



Northern elevation of bay 12



Roof of bay 12



Southern elevation of bay 12



#### 4.0 Description of standing fabric



Northern elevation of bay 13



Roof of bay 13



Southern elevation of bay 13



Southern end of platform 2 canopy



Platform 2 canopy seen from the southern end of platform 1



Platform 2 canopy seen from the southern end of platform 1 (close up of photo 40)

4.4.3 Platform 3

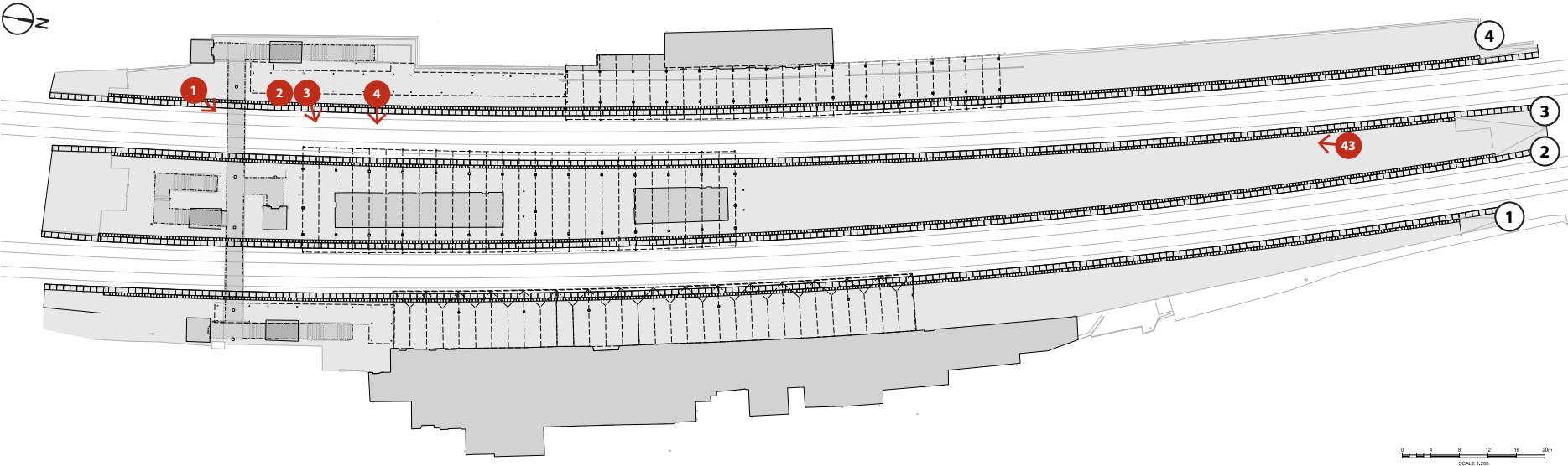




Figure 17: Key plan of Kettering station showing location and direction of photos of platform 3

-  Photo location
-  Platform number

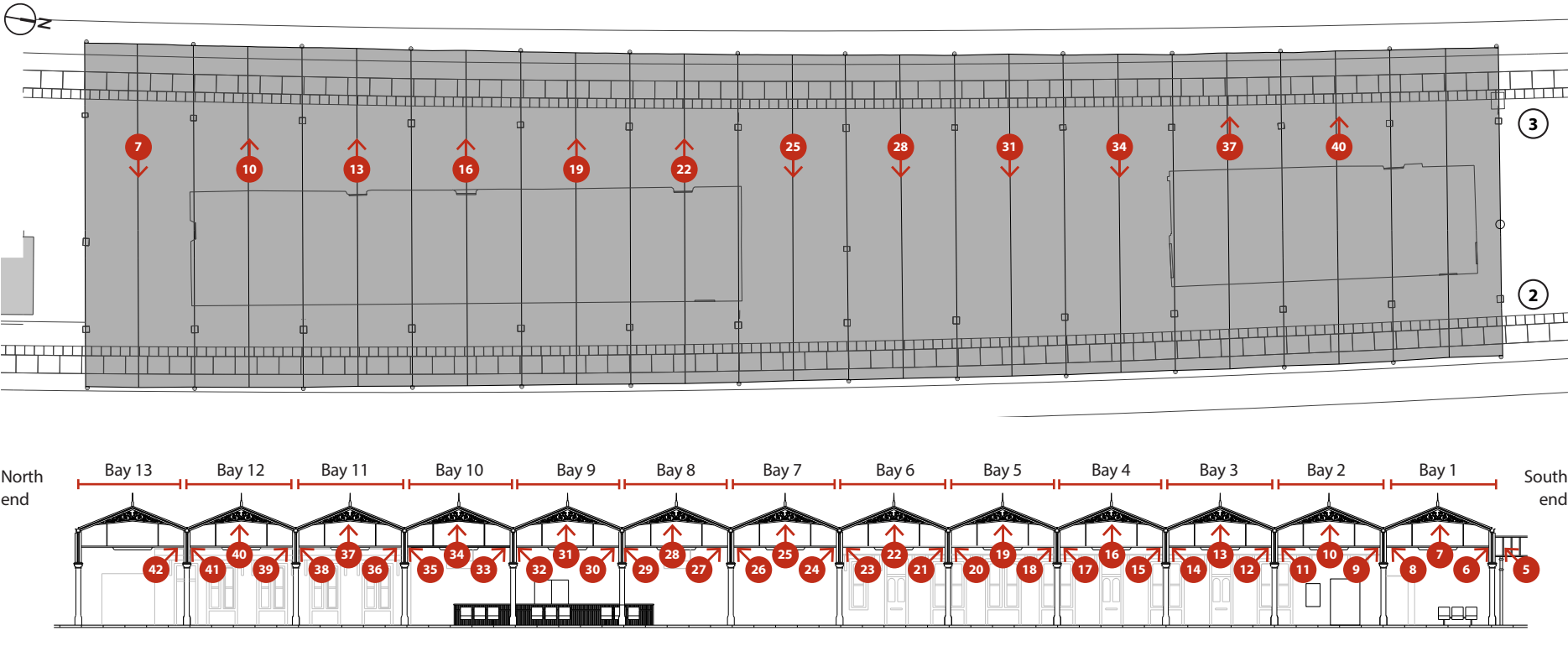




Figure 18: Key plan (top) and elevation (bottom) of platform 3 showing location and direction of photos. The direction of photographs taken of the roof of each bay is shown in the top plan.

-  Photo location
-  Platform number

#### 4.0 Description of standing fabric



Platform 3 canopy seen from the southern end of platform 4



Platform 3 canopy seen from the southern end of platform 4



Southern bay of platform 3 canopy seen from the southern end of platform 4



Detail of southern end bay of platform 3 canopy seen from the southern end of platform 4



Southern end of platform 3 canopy

#### 4.0 Description of standing fabric



Southern elevation of bay 1



Roof of bay 1



Northern elevation of bay 1



Southern elevation of bay 2



Roof of bay 2



Northern elevation of bay 2

#### 4.0 Description of standing fabric



Southern elevation of bay 3



Roof of bay 3



Northern elevation of bay 3



Southern elevation of bay 4



Roof of bay 4



Northern elevation of bay 4

#### 4.0 Description of standing fabric



Southern elevation of bay 5



Roof of bay 5



Northern elevation of bay 5



Southern elevation of bay 6



Roof of bay 6



Northern elevation of bay 6

#### 4.0 Description of standing fabric



Southern elevation of bay 7



Roof of bay 7



Northern elevation of bay 7



Southern elevation of bay 8



Roof of bay 8



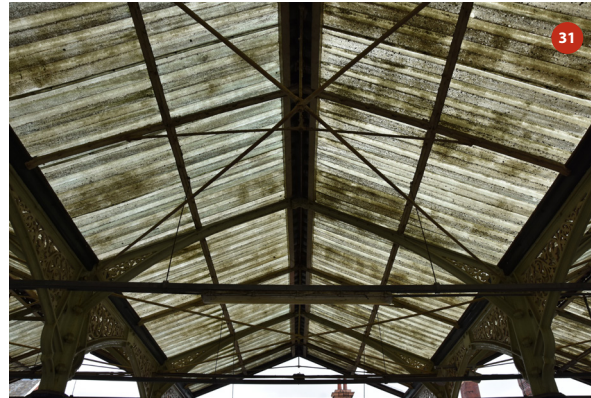
Northern elevation of bay 8



#### 4.0 Description of standing fabric



Southern elevation of bay 9



Roof of bay 9



Northern elevation of bay 9



Southern elevation of bay 10



Roof of bay 10



Northern elevation of bay 10

4.0 Description of standing fabric



Southern elevation of bay 11



Roof of bay 11



Northern elevation of bay 11



Southern elevation of bay 12



Roof of bay 12



Northern elevation of bay 12



Southern elevation of bay 13



Platform 3 canopy seen from the extreme northern end of platform 2/3

4.4.4 Platform 4

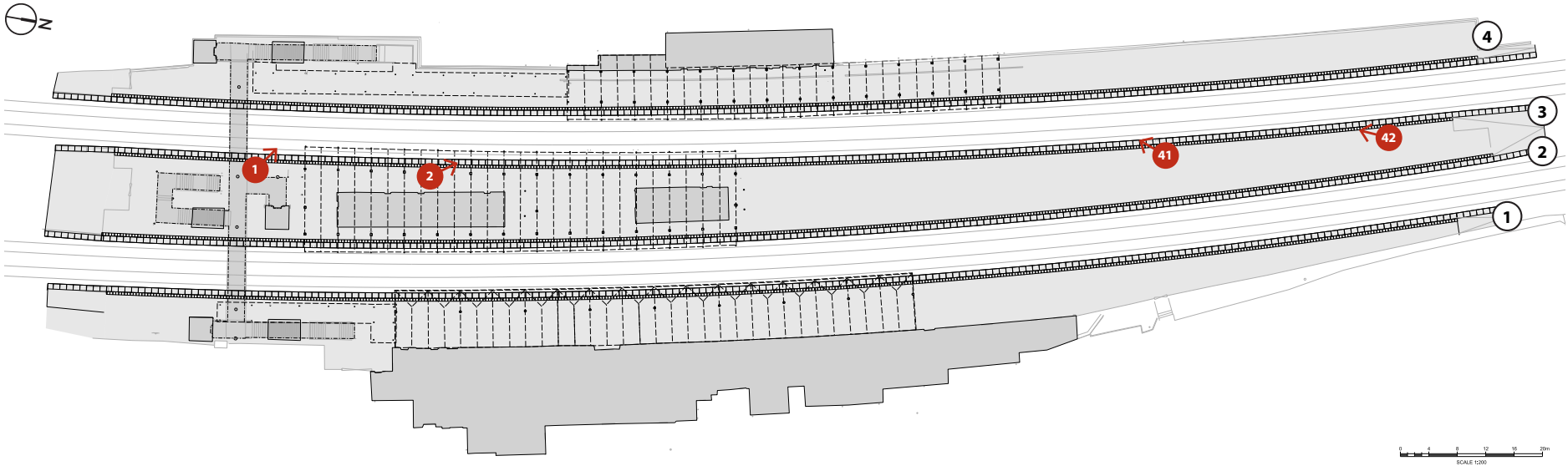


Figure 19: Key plan of Kettering station showing location and direction of photos of platform 4

- ➔ 1 Photo location
- ① Platform number

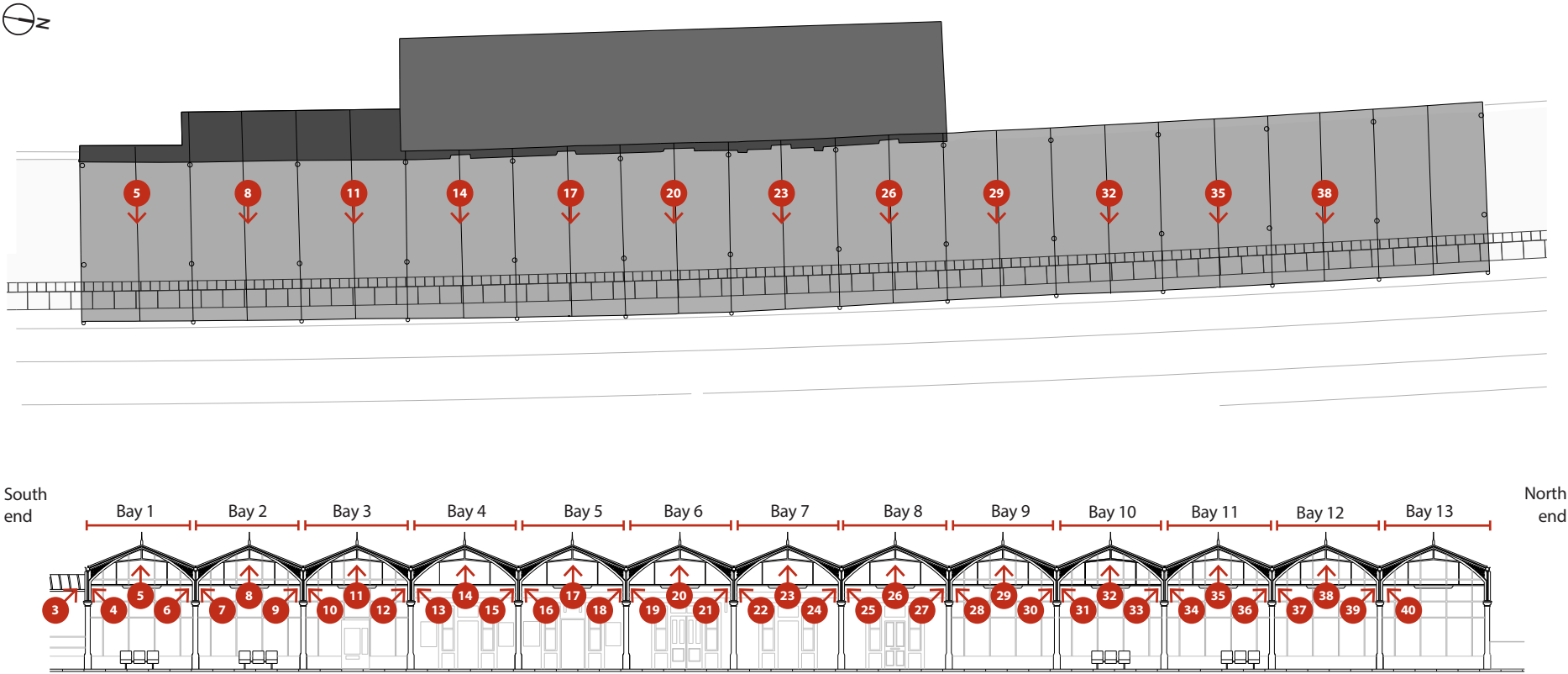




Figure 20: Key plan (top) and elevation (bottom) of platform 4 showing location and direction of photos. The direction of photographs taken of the roof of each bay is shown in the top plan.

-  Photo location
-  Platform number

4.0 Description of standing fabric



Platform 4 canopy seen from southern end of platform 3



Platform 4 canopy seen from the middle of platform 3



Southern end of platform 4 canopy



Southern elevation of bay 1



Roof of bay 1



Northern elevation of bay 1

#### 4.0 Description of standing fabric



Southern elevation of bay 2



Roof of bay 2



Northern elevation of bay 2



Southern elevation of bay 3



Roof of bay 3

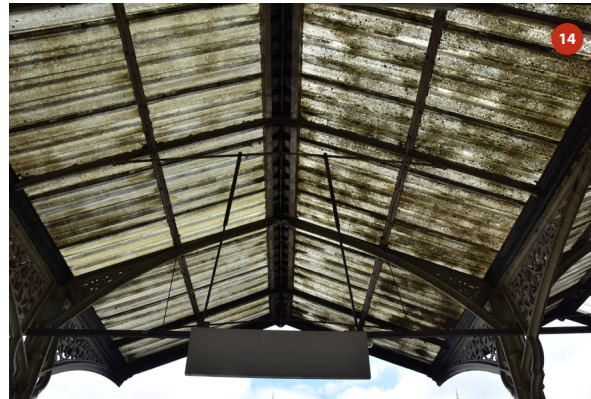


Northern elevation of bay 3

#### 4.0 Description of standing fabric



Southern elevation of bay 4



Roof of bay 4



Northern elevation of bay 4



Southern elevation of bay 5



Roof of bay 5



Northern elevation of bay 5



#### 4.0 Description of standing fabric



Southern elevation of bay 6



Roof of bay 6



Northern elevation of bay 6



Southern elevation of bay 7



Roof of bay 7



Northern elevation of bay 7

#### 4.0 Description of standing fabric



Southern elevation of bay 8



Roof of bay 8



Northern elevation of bay 8



Southern elevation of bay 9



Roof of bay 9

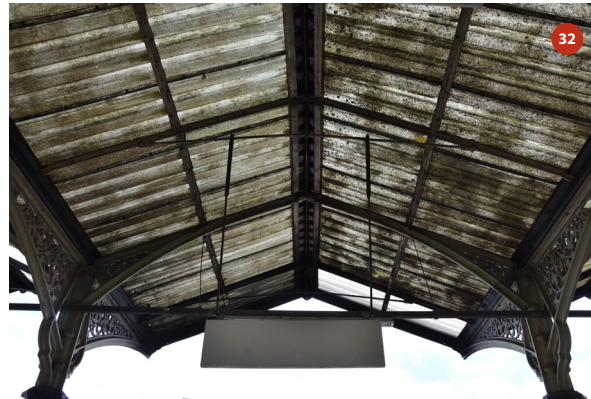


Northern elevation of bay 9

4.0 Description of standing fabric



Southern elevation of bay 10



Roof of bay 10



Northern elevation of bay 10



Southern elevation of bay 11



Roof of bay 11



Northern elevation of bay 11

## 4.0 Description of standing fabric



Southern elevation of bay 12



Roof of bay 12



Northern elevation of bay 12



Southern elevation of bay 13



Platform 4 canopy seen from northern end of platforms 2/3



Platform 4 canopy seen from extreme northern end of platforms 2/3

4.4.5 Platform 2 (bay 4 in detail)

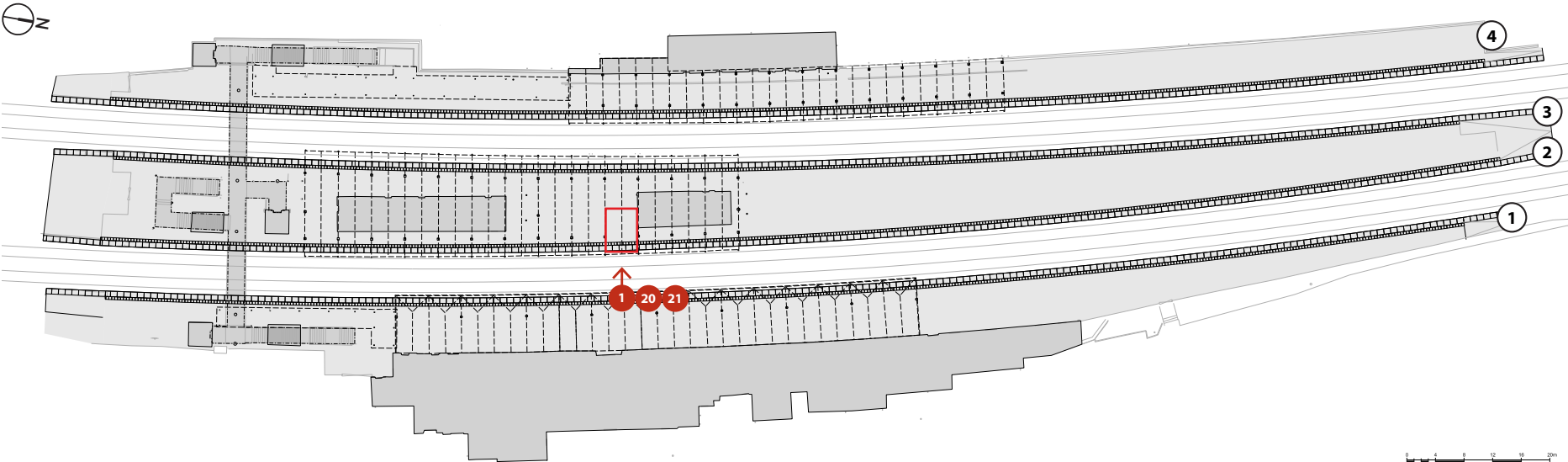


Figure 21: Photoplan of Kettering station showing the location and direction of detail photos taken of bay 4 on platform 2

- Photo location
- Platform number

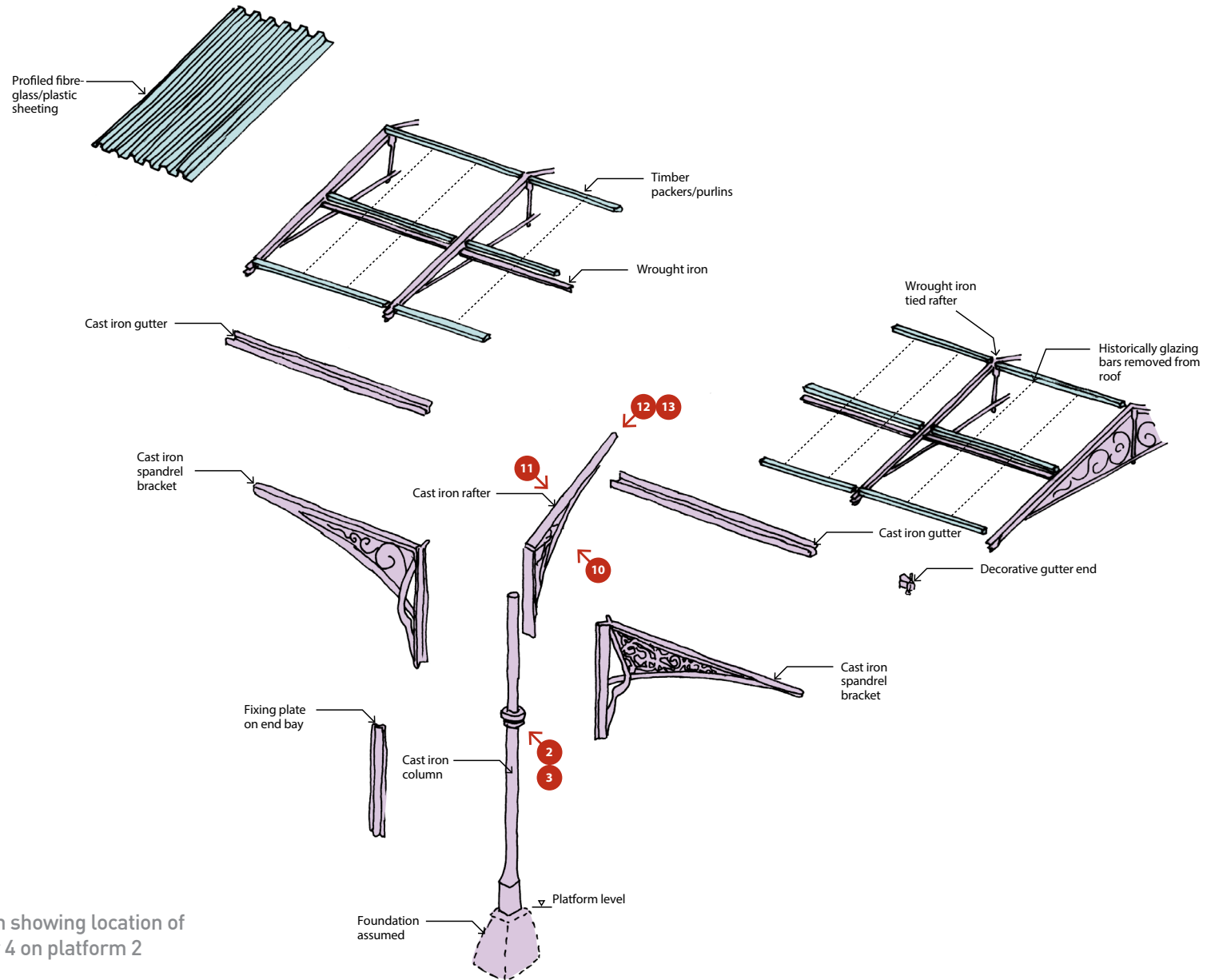


Figure 22: Key plan showing location of detail photos of bay 4 on platform 2

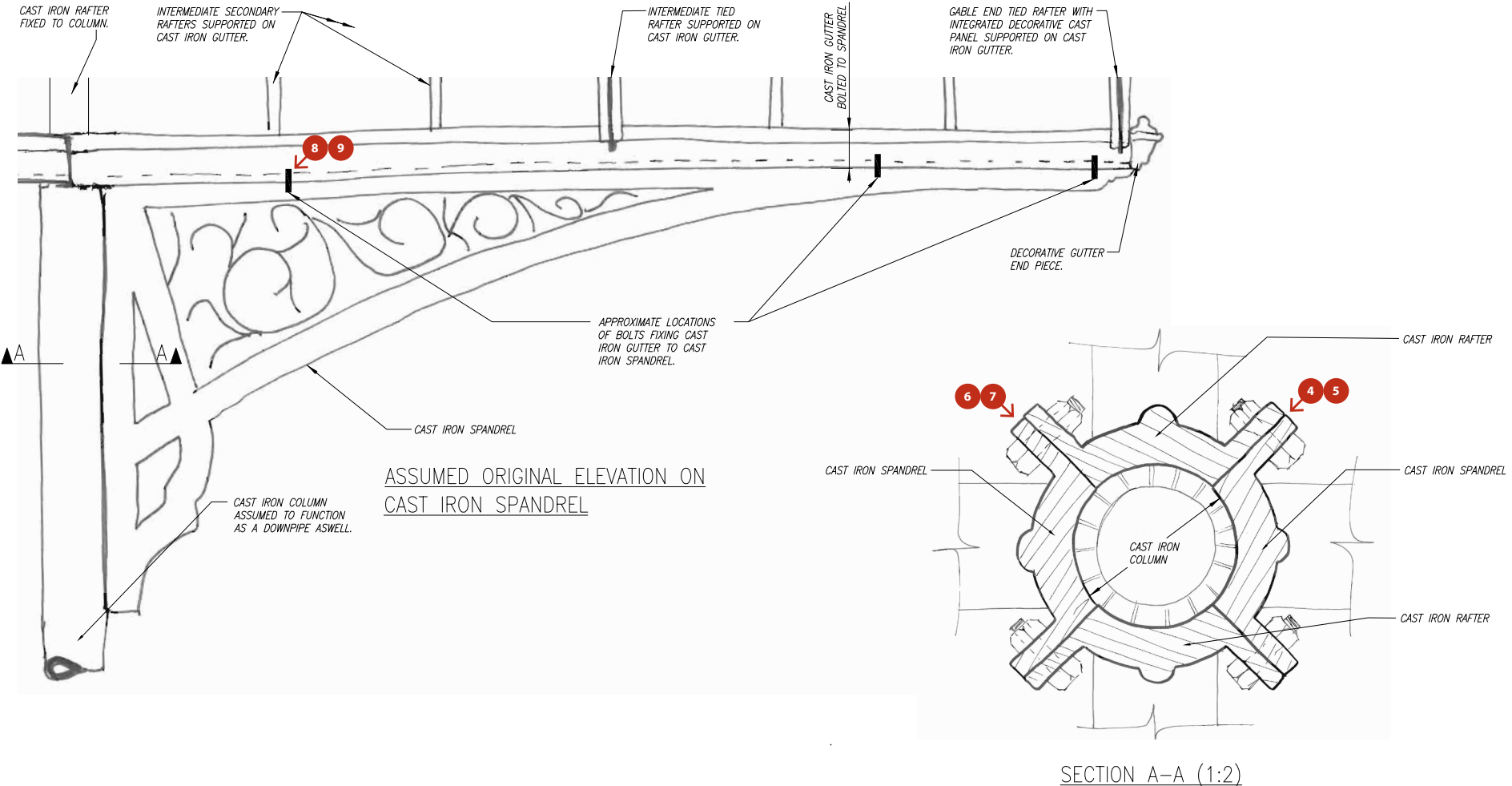


Figure 23: Key plan showing location of detail photos of canopy bracket located in bay 4 on platform 2

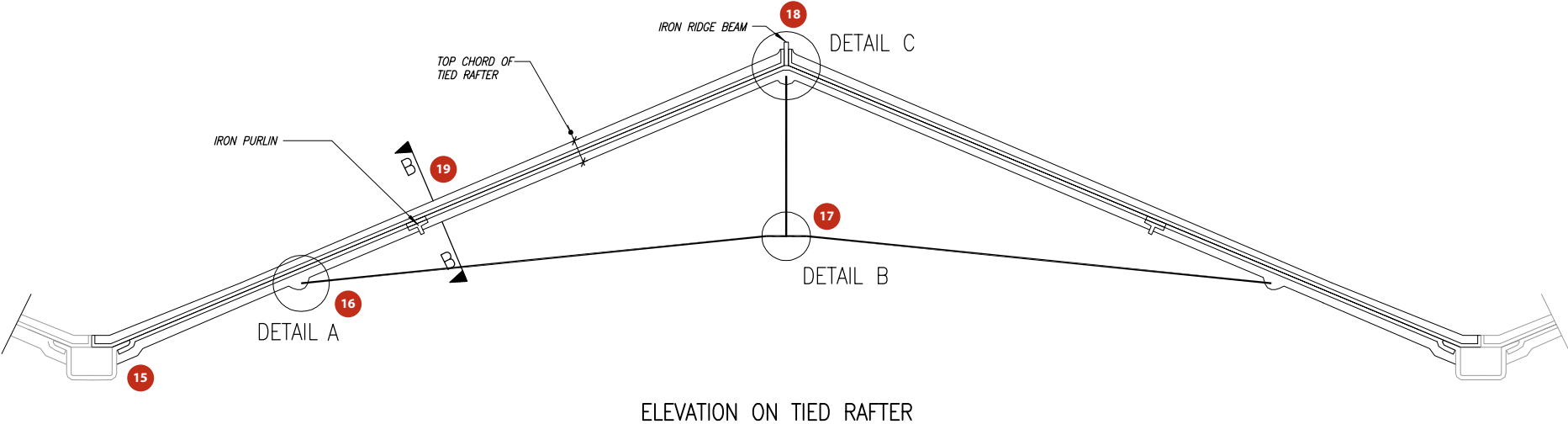


Figure 24: Key plan showing subject of detail photos of canopy bracket located in bay 4 on platform 2



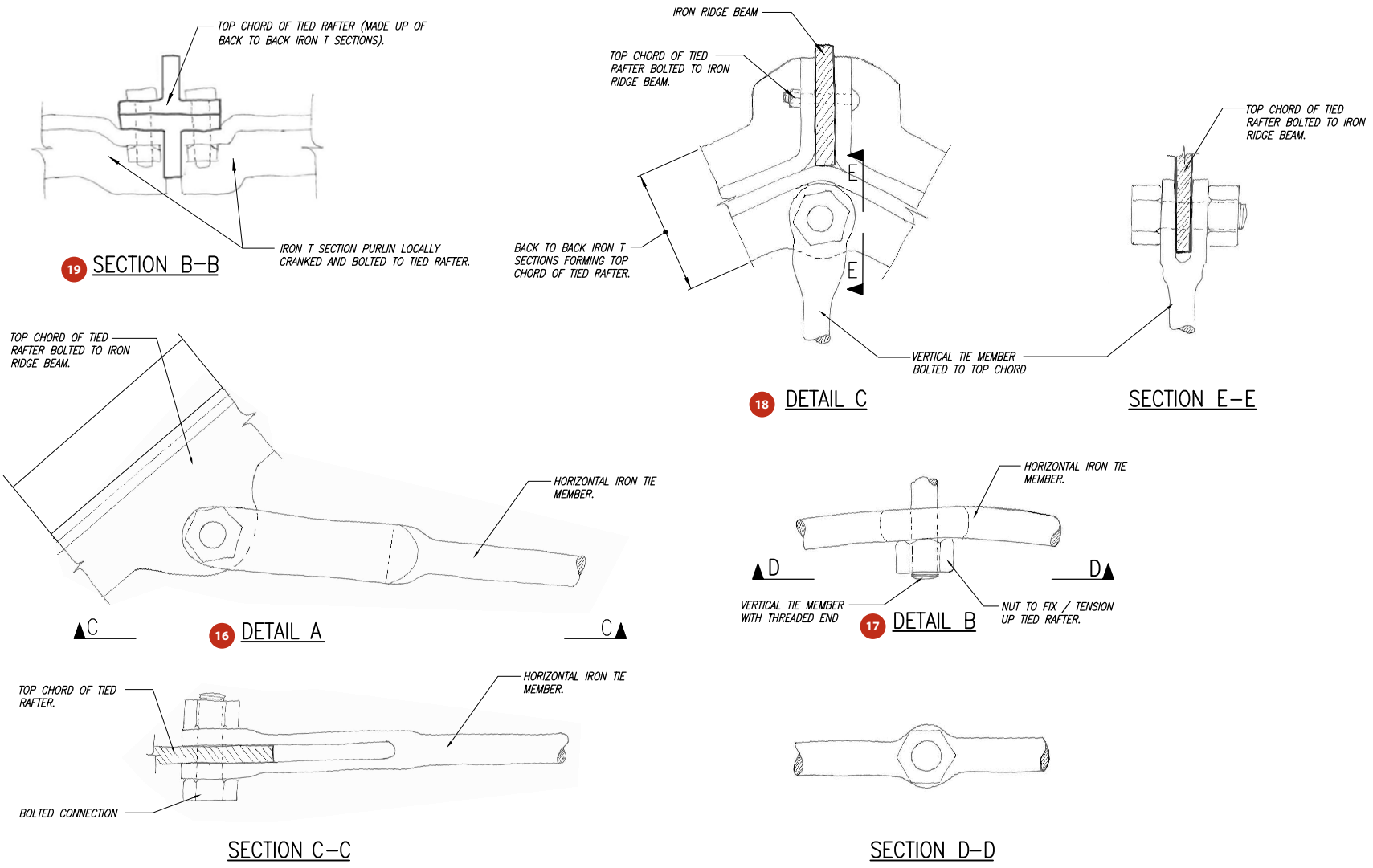


Figure 25: Key plan showing subject of detail photos of canopy bracket located in bay 4 on platform 2



Elevation of bay 4, platform 2 as seen from the middle of platform 1



Detail of where brackets meet column



Detail of where brackets meet column (close up of photo 2)



Detail of bolts joining brackets together around central column



Detail of bolts joining brackets together around central column (close up of photo 4)



Detail of cross-bracing connection to bracket bolt

#### 4.0 Description of standing fabric



Detail of cross-bracing connection to bracket bolt (close up of photo 6)



Bracket connection with gutter beam



Detail of bracket connection with gutter beam (close up of photo 8)



Rafter



Detail of rafter connection with purlin

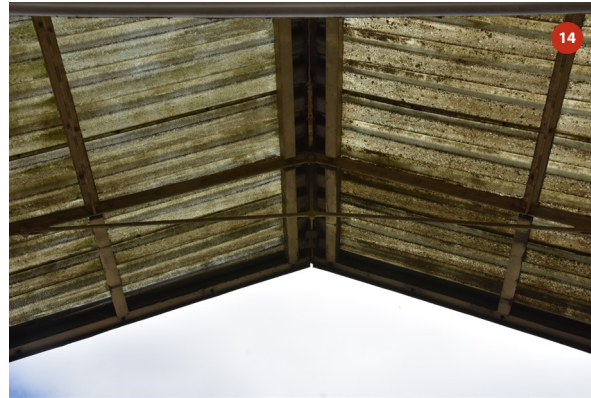


Connection of two rafters at apex of bay roof

#### 4.0 Description of standing fabric



Detail of connection between two rafters (close up of photo 12)



Tied rafter



Detail of connection between tied rafter and gutter beam



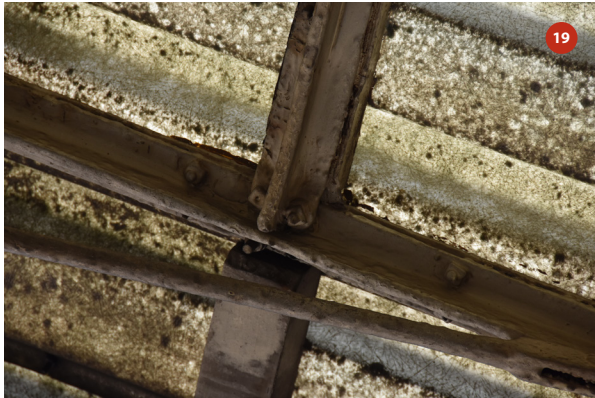
Detail of connection between top chord of tied rafter and horizontal tie member



Detail of connection between vertical and horizontal tie member



Detail of top chord of tied rafter and ridge beam



Detail of tied rafter connection with purlin



Detail of decorative end boss and gutter beam



Detail of decorative end boss

## 5.0 Publication and archiving

Information on the results of the survey will be made publicly available by means of a database in digital form, to permit inclusion of the site data in any future academic research.

In view of the potential of the material it is suggested that a summary of the results of the Standing Building Survey project, possibly including figures or photographs, should appear in an appropriate academic journal.

The site archive of original records will be deposited digitally to ADS-easy. They will be stored under the following UID event number:

**ENN109606**

# 6.0 Bibliography and references

## 6.1 Books and journals

Biddle, G, *Britain's Historic Railway Buildings* (2011)

Hendry, R., *An Historical Survey of Selected London, Midland and Scottish Railway Stations* (1986)

Laxton, H., *Examples of Building Construction*, (1858)

*Illustrated London News*, (23 May 1857)

## 6.2 Other

Alan Baxter & Associates, *Midland Main Line Statement of History and Significance* (2013)

CIFA (2014) *Standard and guidance for archaeological investigation of standing buildings or structures*

Historical Ordnance Survey mapping

Historic England (2016) *Understanding historic buildings: a guide to good recording practice*

Network Rail, National Records Centre, drawings of various dates

## Appendix A: Historic England guidance on recording

In 2016, Historic England produced *Understanding Historic Buildings – A guide to good recording practice*. This best practice guide is the industry standard for the recording of historic buildings. It sets out four levels plus one of building recording, with escalating requirements for a written account, photography and drawing, depending on the circumstances, purpose and need. Level 1 is the simplest of the four levels and Level 4 is the most detailed; an additional option is a photographic survey. The document also provides guidance for the preservation and dissemination of records.

As the document is very clear, its purpose is to provide guidance, not to precisely prescribe requirements, because every site is unique and every project has different needs. As an overarching principle, however, the recording should be proportionate to the importance of the heritage asset and the degree of change to it that is proposed.

Importance is determined by designation: nationally listed buildings, followed by locally listed and then the rest. A building of high significance with many original features undergoing some minor alterations may require a higher level of recording than a building of low significance with few interior features that is being demolished.

Section 5 of Historic England's guidance describes the 5 levels of recording, this information is summarised below:

- Level 1 is a **basic visual record** supplemented by the minimum of information needed to identify the building's location, age and type. Exterior photography is sufficient.
- Level 2 is a **descriptive record**. Both interior and exterior photos are required along with written information including a short summary of the building's development and use. Some measured drawings may accompany this.
- Level 3 is an **analytical record** that will comprise a systematic account of a building's origins, development and use. The evidence that has been gathered will also be appraised. Drawings as well as photographs, plus information about construction materials, will be required to illustrate the building's structure and support the historical analysis. Level 3 may be appropriate when the fabric of the building is under threat of where the scope for detailed documentary research is limited. This level is suitable for the dismantling of a structure prior to re-erection. The drawn and photographic record will be comprehensive and will be made prior to, and during, dismantling.
- Level 4 is a **comprehensive analytical record** and is appropriate for buildings of special importance. This level builds upon level 3 by including further comment on the architectural, social and historical significance of the building in a wider context, and preparing further detailed drawings.
- Photographic survey is a different type of record primarily providing a **full visual record** of a site but without an equal level of written or drawn level. This option is appropriate when drawings and analysis of the historic sources have been taken care of in the past.



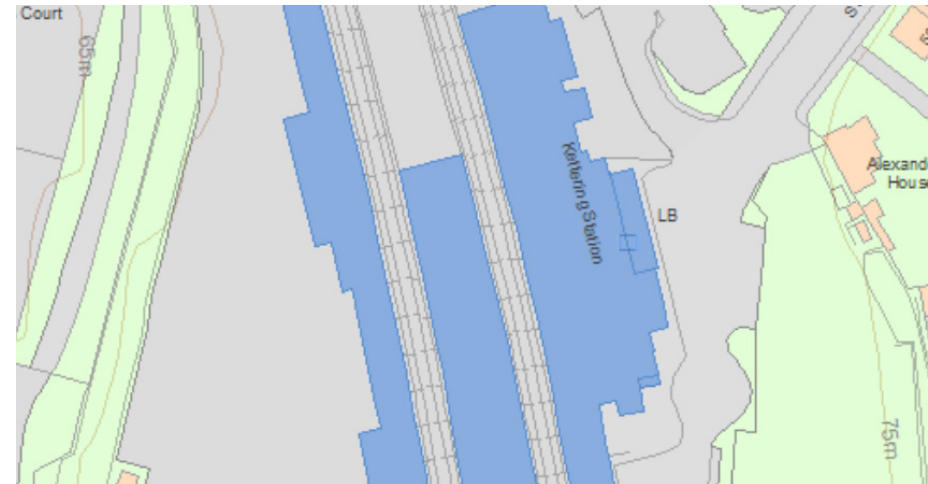
## Appendix B: National Heritage List Entry

Kettering Railway Station, including the main building and platforms 1,2,3 and 4 and their associated buildings and canopies

### Overview

Heritage Category:	Listed Building
Grade:	II
List Entry Number:	1372596
Date first listed:	05-May-1981
Date of most recent amendment:	26-Nov-2014
Statutory Address:	Kettering Railway Station, including the main building and platforms 1,2,3 and 4 and their associated buildings and canopies, Station Road, Kettering, Northamptonshire, NN15 7HJ

### Map



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The above map is for quick reference purposes only and may not be to scale. For a copy of the full scale map, please see the attached PDF - [1372596 .pdf](#)

The PDF will be generated from our live systems and may take a few minutes to download depending on how busy our servers are. We apologise for this delay.

This copy shows the entry on 25-Jul-2019 at 16:26:43.

## Location

Statutory Address: Kettering Railway Station, including the main building and platforms 1,2,3 and 4 and their associated buildings and canopies, Station Road, Kettering, Northamptonshire, NN15 7HJ

The building or site itself may lie within the boundary of more than one authority.

County: Northamptonshire

District: Kettering (District Authority)

Parish: Non Civil Parish

National Grid Reference: SP8637878027

## Summary

Kettering station, including the main building and platforms 1,2,3 and 4 and their associated buildings and canopies, constructed in 1857 to the designs of C H Driver, expanded in 1879, and with a main station building of 1895-8 by C Trubshaw. A footbridge of 1999-2000 at the south end of the station and modern platform fittings lighting and signage are excluded from protection.

## Reasons for Designation

The List entry for Kettering station, including the main building and platforms 1,2,3 and 4 and their associated buildings and canopies, constructed in 1857 to the designs of C H Driver, expanded in 1879, and with a main station building of 1895-8 likely to be by C Trubshaw, has been amended for the following principal reasons:

\* Historic Interest: as a station built for the Midland Railway company, one of the most important and ambitious companies of the era of railway development in England;

\* Historic Association: as a station associated with Charles Henry Driver, a notable railway architect and expert in the architectural use of ironwork with several listed buildings to his name, and with Charles Trubshaw, a significant railway architect responsible for listed railway buildings such as the Midland Hotel in Manchester;

\* Rarity: as a station with a ridge and furrow canopy with decorative cast-iron columns and spandrels, a rare surviving canopy of this type;

\* Architectural interest: the station has developed but each stage of development has architectural and historic interest and survives well.

## History

The first Kettering Railway Station was built as part of the Midland Railway's Leicester to Hitchin Line, which opened in 1857. The line was engineered by Charles Liddell (c.1813–1894) and the stations were designed by the architect Charles Henry Driver (1832–1900). The Midland Railway had been formed in 1844 by the merging of three railway companies which met at Derby: the North Midland Railway, the Birmingham & Derby Junction Railway and the Midland Counties Railway.

Driver adopted a distinctive simplified Gothic style for the stations on the Leicester to Hitchin Line, the details of which were later illustrated in Henry Laxton's *Examples of Building Construction* (1858). The station buildings were built to a T-plan comprising a two-storey stationmaster's house with end gables and a single-storey ticket office with ridge-and-furrow canopy flanking the platform. Kettering, like Wellingborough, was built as a larger version of this design, extended to a cruciform plan. The appearance of the original station at Kettering is recorded in an illustration in the *Illustrated London News* (23 May 1857).

Kettering became an important centre of the Midland Railway's expanding network in the late-C19, and in 1879 the line through the town was quadrupled to increase capacity. The new (fast) lines were built to the west of the old (slow) lines. At Kettering three new platforms were built: 2 and 3, on an island between the fast and slow lines, and 4, to the west of the lines. The island platforms and platform 4 were both given a single-storey weather-boarded waiting room and canopies with cast-iron columns and spandrels to match those designed by Driver in 1857 (their replication is shown in a surviving 1879 contract drawing). The main station building of 1857, on platform 1, was demolished and replaced by the Midland Railway 1895–98. The first part to be erected was the elegant entrance block of 1895. This contained the booking hall, booking office, parcels office and refreshment room; Gordon Biddle (Biddle 2003 2nd ed. 2011) ascribes the design to Midland Railway architect Charles Trubshaw (1841–1917). In the same redevelopment phase plain red-brick additions were built at both ends to house other functions including staff offices and waiting rooms. These additions included an integrated one-and-a-half storey accommodation block of c.1897 with staff bedrooms on the first floor. Platform 1 was given hipped ridge-and-furrow canopies on steel joists, and a weather-boarded building was added to the north of the existing one on the island platforms.

The importance of Kettering as a late-C19 railway centre necessitated an extensive cluster of ancillary buildings in the vicinity of the station. The most significant were: a two-bay engine shed, erected at the north end of the forecourt by the contractors C. Deacon & Co. in c.1875; and a goods shed with offices, built at the south end in c.1894. These were both connected to the lines by sidings. An early, London Midland & Scottish Railway site plan (of 1923 or later) shows further buildings: various sheds and stores in the forecourt; a signal box (of 1913) to the south of the island platform, between the fast and slow lines; and to the south-west of that, beyond the fast lines, a wagon repair shop and associated sidings. The Ordnance Survey map of 1968 shows that a second, larger goods shed with sidings had been constructed to the north of the existing one, and that the engine shed of c.1894 had been demolished by that date. The signal box closed in 1987 and was moved to the heritage railway in Butterley, Derbyshire, where it remains. None of the other buildings described in this paragraph survive today; further investigation may reveal surviving associated elements around the wider site.

Drawings show that internal alterations were made to the main station building in c.1974, and in 1999–2000 the station underwent a programme of refurbishment, including the erection of a new footbridge at the south end (to replace the subway) and the rebuilding of the screen wall to platform 4. In the late-C20, the canopies on platforms 2 and 4 were cut back to the spandrels to improve clearance for passing trains.

### Details

Main station building of 1895–98, including an entrance block likely to be by Charles Trubshaw, with a canopy to platform 1. A shared canopy stands on Platforms 2 and 3 as well as a building of 1879 and one of c.1897. Platform 4 has a canopy and building of 1879, and screen walls of 1999–2000. There is a disused subway of 1879 and a footbridge with associated canopies of 1999–2000.

**MATERIALS:** The main station building of 1895–98 is of red brick with terracotta and sandstone dressings; the two pitched sections have slate roofs. Its canopies to the forecourt and to platform 1 are of steel and glass. The 1879 canopies to platforms 2, 3 and 4 are of cast-iron, covered in places with glass but predominantly with replacement fibre-glass sheeting. The platform buildings of 1879 and c.1897 are of weather-boarded timber. The footbridge of 1999–2000 is steel and glass.

**PLAN:** The 1895–98 main station building is located on the east side of the line. It has a long, shallow plan which is flat against platform 1 to the west but stepped and irregular to the forecourt to the east. It comprises two pitched-roofed buildings, hereafter called the entrance block and the accommodation block, linked and abutted by lower flat-roofed sections. The southern three-quarters of the platform 1 elevation have a glazed hipped ridge-and-furrow canopy. The entrance block is located to the south of the accommodation block and houses the booking hall, booking office and café. The accommodation block houses the café kitchen, and offices and waiting rooms on the ground floor, with an apartment, now vacant, on the first floor.

Platforms 2 and 3 form an island between the east (slow) and west (fast) tracks. On both sides are ridge-and-furrow canopies, which span two flat-roofed, single-storey weather-boarded buildings (of 1879 and c.1897). There is a C21 platform shelter to the north of these. Platform 4, on the west side of the tracks, has a similar ridge-and-furrow canopy to that of the island platforms and a matching weather-boarded building, all of 1879. There is a footbridge of 1999–2000 across the south end of the station, with canopies leading onto those on the platforms. The disused subway of 1879 survives beneath the tracks opposite the station entrance.

**EXTERIOR:** The main station range of 1895–98 is of red brick with painted timber door and window frames, sandstone window cills and metal rainwater goods. The lintels to the platform elevation are of sandstone; while those to the forecourt are mostly shallow, segmental brick arches.

The entrance block is of one storey plus an attic, and is distinguished by its terracotta dressings and five decorated gables: one at the north and south ends, each with a ball finial, and three to the station forecourt, described below. The accommodation block is one-and-a-half storeys, and is much plainer, with two chimneystacks; the flat-roofed sections are in the same plain style.

The forecourt elevation of the main station range is described first, from south to north. The range starts with the men's toilet block, which has altered window openings. This steps forward to the former carriage entrance, which has a semi-circular archway and a pitched metal-and-glass roof. Next is the entrance block, which is of five bays and has three continuous terracotta bands (below the cills, at the transoms, and, larger, across the lintels). The first bay breaks forward to a hatchway and then again to a doorway, both for the former parcels office. The second bay projects and has a large window flanked on each side by a smaller window; above is a pitched gable with decorative 'MR' insignia and a ball finial. The three northerly bays carry a hipped ridge-and-furrow steel-and-glass canopy, carried on brackets between the bays. A shaped-gable chimneystack is decorated with a sunflower motif and a semi-elliptical entrance archway (with C21 metal-framed automatic doors) that has a terracotta arch ring, dropped keystone and

quoins, beneath a low, ramped parapet. A short single-storey link connects the entrance block to the accommodation block, which has three bays at first-floor level, each with a central pitched dormer and casement window that breaks through the dentilled eaves. At ground-floor level there are six bays; the first, fourth and fifth are obscured by single-storey projections (erected for circulation and as a partitioned toilet block for the ladies' waiting rooms behind).

The main station range ends with a long, single-storey wing which steps back to a canted corner at the north end (when built this was parallel with the wall of the engine shed).

The elevation to platform 1 has irregular fenestration, with continuous sandstone strings across the lintels and cills. There is a small, affixed metal milepost (platform 3 has the same) and a large Y-shaped metal gradient-post. The southern three-quarters of the elevation are articulated beneath sixteen bays of hipped ridge-and-furrow steel-and-glass canopies on steel columns. The bay to the station entrance (opposite the archway to the forecourt elevation) has C21 metal-framed automatic doors; adjacent on its south side there are C20 tubular steel railings, which originally surrounded the entrance to the subway.

The island platforms have two, painted weather-boarded buildings with four-panelled doors and paired-one-over-one-sash windows, both with eared architraves. There are paired brackets between and decorative cornices, and skirting boards. The building to the south, of 1879, has a low extension at its south end which is now used for plant. The one to the north, of c.1897, is smaller and has a low extension at its north end, and has a metal plaque commemorating its use as a YMCA forces canteen during the Second World War. Between the weather-boarded buildings there is a low enclosure of decorative cast-iron railings with a gate at its south end, originally surrounding a stairwell to the subway that is now filled in.

A ridge-and-furrow canopy of 1879, supported by cast-iron columns, spans the island platforms. It is thirteen bays long and three columns deep. The columns have simple plinths and capitals beneath four-way brackets to decorative spandrels, also of cast-iron, with delicate pierced foliate work cast to the same pattern as Driver used at the 1857 station. The west elevation, to platform 3, has matching gable screens. These have been lost on the east elevation, to platform 2, where they have been cut back, but the bosses to the gutter ends have been reinstated. The canopy finials to both platforms are replacements and much simpler than those in the Driver drawings (the same is true of the canopy on platform 4). The canopy spans the contemporary southern building with lattice trusses, eliminating the central column and all but the end spandrels. The northern weather-boarded building was constructed around the columns in c.1897. To the north there is a C21 single-storey, metal-and-glass platform shelter.

Platform 4 has thirteen bays of the same 1879 canopies, two columns deep. The spandrel ends above the platform edge have been cut back to the same effect as those on platform 2. From the southern end there are: three bays over a screen wall of 1999–2000, with a door to the subway of 1879; five bays over a weather-boarded building of 1879, which matches those on the island platforms; then five further bays over a 1999–2000 screen wall. A low, engineering brick wall running the remaining length of the platform.

The steel-and-glass covered footbridge at the south end of the station also dates to 1999–2000. It has a stepway, lift and canopy to each platform.\*

The platform signage, lighting, seating, lamp-posts, bicycle storage racks and other subsidiary features are all modern.\*

INTERIOR: The south end of the main station building, including the entrance block, houses (from south to north): the men's toilets; a refuse area; a storeroom; staff offices, toilets and kitchen; the booking office; the booking hall and entrance lobby, with adjacent waiting room; and a café with offices and kitchens. These rooms have been altered, with suspended ceilings, replacement floors, partition walls and modern services, but retain some fixtures and fittings, including four-panelled doors, door and window surrounds, cornices and wide skirting boards. The booking hall and entrance lobby are divided by a segmental archway with engaged octagonal piers. They, and the adjacent waiting room, have plasterwork platbands. The café offices and kitchens were not available for inspection.

Beneath the café and kitchens there is a cellar of exposed, vaulted brick, most recently in use as a public house and now vacant.

The café kitchens continue beyond the entrance block into the accommodation block. Beyond them is the former ladies' waiting room, now disused, with cornice, dado rail, wainscot and skirting board, and four-panelled doors remaining. It has a toilet block which projects into the forecourt behind; the two cubicles have moulded joinery. The first floor of the accommodation block is an apartment, originally with two bedrooms, a bathroom, W.C. and store. The plan-form of the bedrooms survive intact; the bathroom has been converted into a kitchen and the landing and stairs have been rebuilt. Both bedrooms and the kitchen have fireplaces with tiled reveals; in one bedroom and the kitchen these have the same decorative cast-iron grates and chimneypieces.

To the north of the accommodation block there is (from south to north): a modern telecommunications room; the former telegraph office, fireplace and dismantled switchboard equipment; the former gentlemen's waiting room and toilets, with tiling and moulded joinery, and now part-partitioned for a boiler room; the present stationmaster's office and adjacent meeting room, refurbished in the late C20 and with no surviving fixtures or fittings; and a disused storeroom.

The timber interiors of the weather-boarded buildings incorporate skirting boards, panelling, dado rails, wainscots, door and window surrounds and cornices. Some rooms retain their fireplaces, with simple surrounds. Only the waiting room and first-class waiting room at either end of the southern island building and the central waiting room of the platform 4 building are in use today: these have fewer or no visible fixtures and fittings as they have been lined with plasterboard and given suspended ceilings. The northern island building is pierced by the central columns of the canopy above.

The 1879 subway is brick-lined with a vaulted roof and is now used only for services.

\* Pursuant to s.1 (5A) of the Planning (Listed Buildings and Conservation Areas) Act 1990, it is declared that the steel and glass-covered footbridge, its lift, stepways and canopies, at the south end of the station, constructed in 1999-2000, as well as platform signage, lighting, seating, lamp posts, and bicycle storage racks, which date from the late-C20 or afterwards, are not of special architectural or historic interest.

### Legacy

The contents of this record have been generated from a legacy data system.

Legacy System number: 230120

Legacy System: LBS

### Sources

#### Books and journals

Biddle, G, Britain's Historic Railway Buildings, (2003)

Hendry, R., An Historical Survey of Selected London, Midland and Scottish Railway Stations, (1986)

Laxton, H., Examples of Building Construction, (1858)'Illustrated London News' in 23 May, (1857)

#### Other

Alan Baxter & Associates, Midland Main Line Statement of History and Significance, 2013,

Network Rail, National Records Centre, drawings of various dates,

Title: Kettering Town Plan Source Date: 1868 Author: Publisher: Surveyor:

#### Legal

This building is listed under the Planning (Listed Buildings and Conservation Areas) Act 1990 as amended for its special architectural or historic interest.

The listed building(s) is/are shown coloured blue on the attached map. Pursuant to s.1 (5A) of the Planning (Listed Buildings and Conservation Areas) Act 1990 ('the Act'), structures attached to or within the curtilage of the listed building (save those coloured blue on the map) are not to be treated as part of the listed building for the purposes of the Act.

End of official listing

# Appendix C: OASIS form

OASIS ID: alanbaxt2-362969

## Project details

Project name	Kettering Station Canopy Recording
Short description of the project	Recording of the canopies on platforms 1-4 at Kettering Station in advance of electrification works which will require them to be cut back
Project dates	Start: 22-07-2019 End: 30-08-2019
Previous/future work	Yes / Yes
Any associated project reference codes	KET/2019/0075 - Planning Application No.
Any associated project reference codes	1372596 - NHLE No.
Type of project	Building Recording
Site status	Listed Building
Current Land use	Transport and Utilities 2 - Other transport infrastructure
Monument type	STATION CANOPY Post Medieval
Significant Finds	STATION CANOPY Post Medieval

Methods & techniques	"Photographic Survey"; "Survey/Recording Of Fabric/Structure"
Prompt	Planning condition
Prompt	Listed Building Consent
Project location	
Country	England
Site location	NORTHAMPTONSHIRE KETTERING KETTERING Kettering Railway Station
Postcode	NN15 7HJ
Study area	0 Square metres
Site coordinates	SP 486370 278019 51.946153674132 -1.292304208567 51 56 46 N 001 17 32 W Point
Project creators	
Name of Organisation	Alan Baxter Limited
Project brief originator	Local Planning Authority (with/without advice from County/District Archaeologist)
Project design originator	Patrizia Pierazzo
Project director/manager	Victoria Bellamy

Project supervisor	Victoria Bellamy	Title	Kettering Station Canopy: Recording Report - Level 3
Type of sponsor/ funding body	Network Rail	Author(s)/Editor(s)	Bellamy, V.
Project archives		Other bibliographic details	1769/10
Physical Archive Exists?	No	Date	2019
Digital Archive recipient	ABA	Issuer or publisher	Alan Baxter
Digital Archive ID	1769/10	Place of issue or publication	London
Digital Contents	"Industrial"	Description	A4 Landscape
Digital Media available	"Images raster / digital photography"	Entered by	Victoria Bellamy (vbellamy@alanbaxter.co.uk)
Digital Archive notes	n/a	Entered on	9 August 2019
Paper Archive recipient	ABA		
Paper Archive ID	1769/10		
Paper Contents	"Industrial"		
Paper Media available	"Correspondence","Plan","Report"		
Paper Archive notes	n/a		
Project bibliography 1			
Publication type	Grey literature (unpublished document/ manuscript)		





# Alan Baxter

**Prepared by** Victoria Bellamy

**Reviewed by** Vicky Simon

**Draft issued** September 2019

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