

# Scotland Street Bridge, Stoke by Nayland

## **Desk-Based Assessment**

SCCAS Report No. 2012/097

**Client: Suffolk County Council**

Author: J. A. Craven

June 2012

© Suffolk County Council Archaeological Service



# Scotland Street Bridge, Stoke By Nayland

Archaeological Excavation Report

SCCAS Report No. 2012/097

Author: J. A. Craven

Contributions By: A. M. Breen

Illustrator: Gemma Adams

Editor: Richenda Goffin

Report Date: June 2012



## HER Information

<b>Site Code:</b>	<b>N/A</b>
<b>Site Name:</b>	<b>Scotland Street Bridge, Stoke by Nayland</b>
<b>Report Number</b>	<b>2012/097</b>
<b>Planning Application No:</b>	<b>N/A</b>
<b>Date of Fieldwork:</b>	<b>N/A</b>
<b>Grid Reference:</b>	<b>TL 9967 3675</b>
<b>Oasis Reference:</b>	<b>129748</b>
<b>Curatorial Officer:</b>	<b>Dr Abby Antrobus</b>
<b>Project Officer:</b>	<b>John Craven</b>
<b>Client/Funding Body:</b>	<b>Suffolk County Council</b>
<b>Client Reference:</b>	<b>N/A</b>

Digital report submitted to Archaeological Data Service:  
<http://ads.ahds.ac.uk/catalogue/library/greylit>

### **Disclaimer**

Any opinions expressed in this report about the need for further archaeological work are those of the Field Projects Team alone. Ultimately the need for further work will be determined by the Local Planning Authority and its Archaeological Advisors when a planning application is registered. Suffolk County Council's archaeological contracting services cannot accept responsibility for inconvenience caused to the clients should the Planning Authority take a different view to that expressed in the report.



# Contents

## Summary

<b>1. Introduction</b>	<b>1</b>
1.1 Project Background	1
1.2 Site description	2
1.3 Topography and geology	2
1.4 Scope of this report	2
1.5 Aims	5
1.6 Methods	5
1.7 Legislative frameworks	6
<b>2. Results</b>	<b>8</b>
2.1 Suffolk HER search	8
All known archaeological sites within the PDA	8
All known archaeological sites within 1km of the PDA	8
2.2 Listed buildings	11
2.3 Scheduled Monuments, Conservation Areas and SSSI's	14
Scheduled Ancient Monuments	14
Conservation Areas	14
Registered Parks and Gardens	14
Sites of Special Scientific Interest	14
2.4 Documentary and historic map study summary	15
2.5 Site inspection	18
<b>3. Assessment of impacts and effects</b>	<b>20</b>
3.1 The archaeological potential of the PDA	20
3.2 Potential level of preservation of archaeological remains within the PDA	21
3.3 Impact of the development on the archaeological resource	22
<b>4. Mitigation measures</b>	<b>23</b>
<b>5. Conclusions/Recommendations</b>	<b>24</b>
<b>6. List of contributors and acknowledgements</b>	<b>24</b>

## List of Figures

Figure 1. Location map	3
Figure 2. Topography and geology	4
Figure 3. HER sites within 1km of the PDA	10
Figure 4. Listed buildings and Conservation Areas within 1km of the PDA	13
Figure 5. Location of the PDA on Hodskinson's map of 1783 (not to scale).	16
Figure 6. Location of the PDA on 1st Edition Ordnance Survey, 1886.	16
Figure 7. Location of the PDA on 2nd Edition Ordnance Survey, 1904.	17
Figure 8. Location of the PDA on 3rd Edition Ordnance Survey, 1926.	17

## List of Plates

Plate 1. Scotland Street Bridge, looking north at inscribed girder	18
Plate 2. Scotland Street Bridge, looking east.	19
Plate 3. Underside of Scotland Street bridge	19

## List of Appendices

Appendix 1.	Brief and Specification
Appendix 2.	Documentary and Cartographic Study
Appendix 3.	Suffolk HER search results for 'Cast Iron Bridge'
Appendix 4.	Cast Iron bridges known to SCC Economy Skills and Environment

## List of Abbreviations used in the text

DBA	Desk Based Assessment
HER	Historic Environment Record
PAS	Portable Antiquities Scheme
PDA	Proposed Development Area
PPG 16	Planning Policy Guidance 16
SAM	Scheduled Ancient Monument
SCCAS/FT	Suffolk County Council Archaeological Service/Field Team
SCCAS/CT	Suffolk County Council Archaeological Service/Conservation Team
SSSI	Site of Special Scientific Interest



## Summary

A desk based assessment of Scotland Street Bridge, Stoke by Nayland, prior to its proposed renovation, has highlighted the importance of the structure as an example of a mid 19th century cast iron bridge in Suffolk and its position within the Stoke by Nayland Conservation Area. Documentary research has been unable to find any other examples manufactured by the fabricator E R & F Turner of Ipswich.

The DBA has also established the existence of references to a bridge on the site dating to at least the 14th century, implying that the site has high potential for archaeological deposits relating to earlier medieval or post-medieval bridges or crossings.

Recommendations have been made for recording the bridge prior to and during its dismantling, for the reinstatement of as much of the original fabric as possible to the renovated structure, and for archaeological monitoring of groundworks.



# 1. Introduction

---

## 1.1 Project Background

This archaeological DBA has been prepared by SCCAS/FT for Suffolk County Council.

This DBA is the first stage of a programme of archaeological works to access the archaeological potential of the PDA and has been requested by SCCAS/CT to inform the determination of conservation area consent.

The proposed development is for the replacement of the deck of Scotland Street Bridge to avoid potential over-stress and sudden failure. The SCCAS/CT Brief and Specification for the project (Appendix 1) describes the bridge as follows:

*'The current bridge comprises brick abutments supporting a deck of 5 cast iron girders, with transverse concrete slabs cast between them. The parapet is formed from cast iron post and rail balustrading (contemporary to the main structure?). The southern-most girder is inscribed with 'J. H. Rinder Surveyor, 1867' and the name of the fabricator, 'E R & F Turner of Ipswich'*

*E R and F Turner were an important firm of Ipswich-based milling engineers, with offices around the world. Members of the Suffolk Industrial Archaeological Society know of no other existing bridges made by this company (though there might well be others), and have indicated that the bridge, although of relatively late date, is likely to be rare and therefore of historical importance.'*

The current proposed works will retain the existing cast iron edge beams as non-structural fascias to a proposed new reinforced concrete bridge deck on the existing abutments and, if possible, to retain the existing balustrades.

## **1.2 Site description**

The subject of this DBA covers an area of approximately 0.016ha centred at TL 9967 3675, in the parish of Stoke by Nayland (Fig. 1).

The present land use is as a road bridge (with a 3t weight limit) on Scotland Street where it crosses the River Box.

## **1.3 Topography and geology**

The PDA is located at a crossing of the River Box at c.15m above OD (Fig. 2). The localised topography of the PDA is the floodplain of the River Box, a valley running roughly north-west to south-east towards the River Stour.

The PDA is on alluvial river soils (813b) (Ordnance Survey 1983) (Fig. 2).

## **1.4 Scope of this report**

In order to set the PDA in its archaeological context a study area of a 1km radius from its centre was selected for examination (Figs. 2-4).

In accordance with the NPPF, the Government's guidance on archaeology and planning, (<http://www.communities.gov.uk/publications/planningandbuilding/nppf>) and based on a SCCAS/CT specification, this assessment examines the available archaeological sources. These include the Suffolk HER, reports of any archaeological investigations, all readily available cartographic and documentary sources and a site walkover.

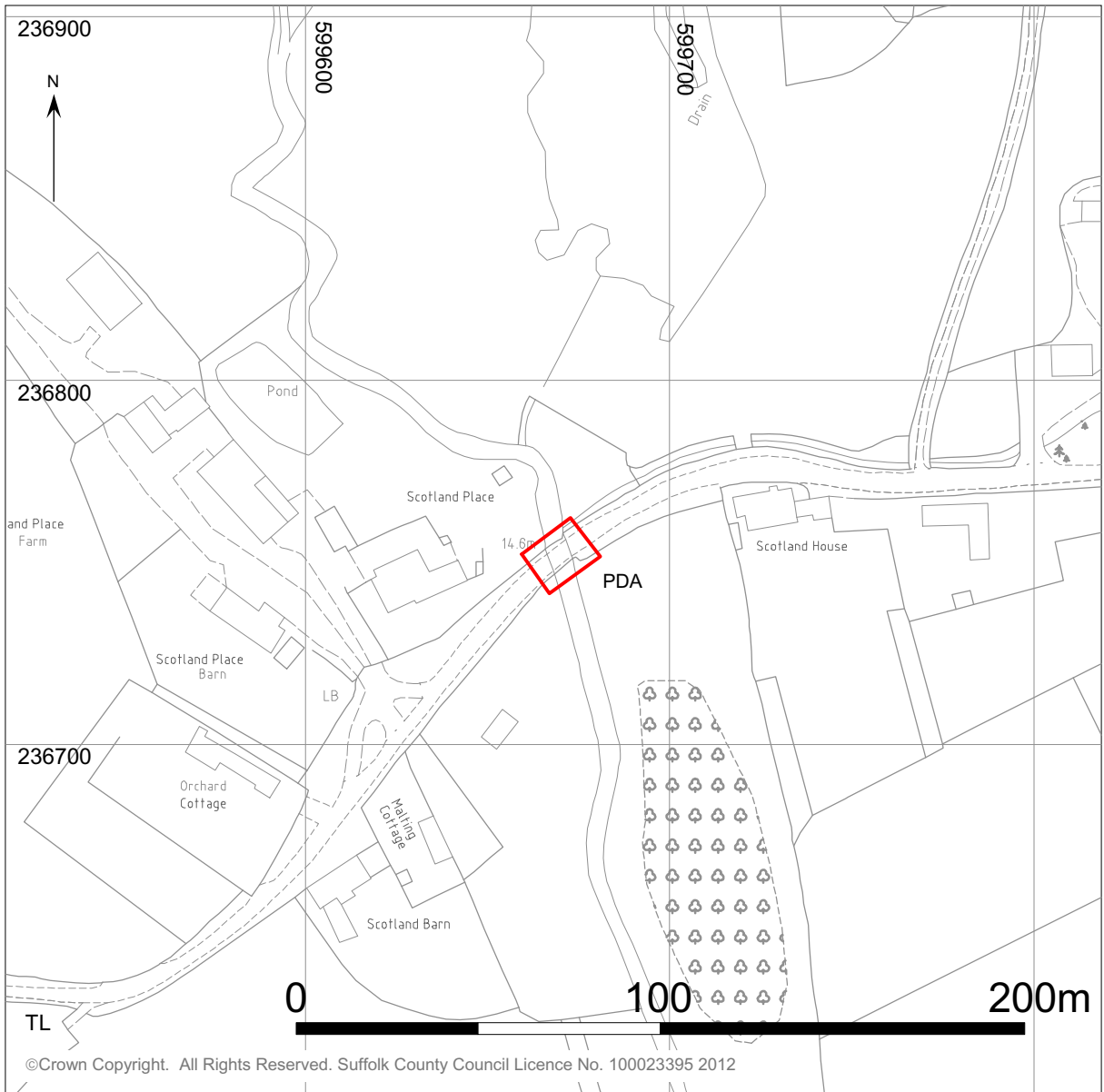
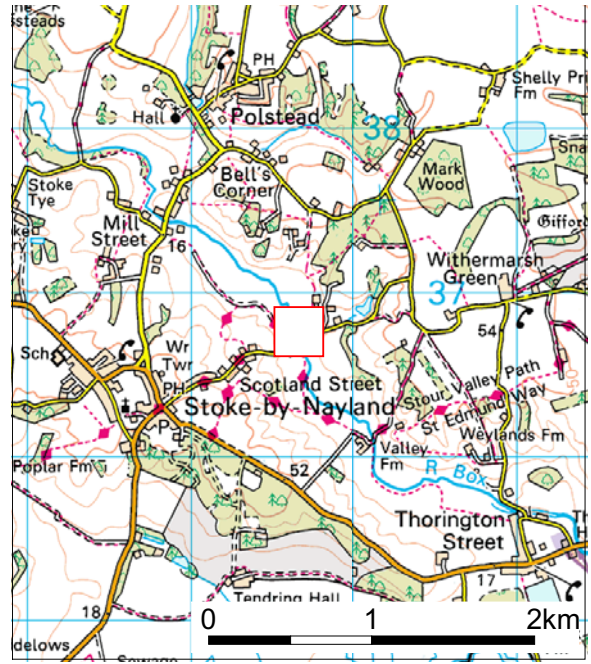
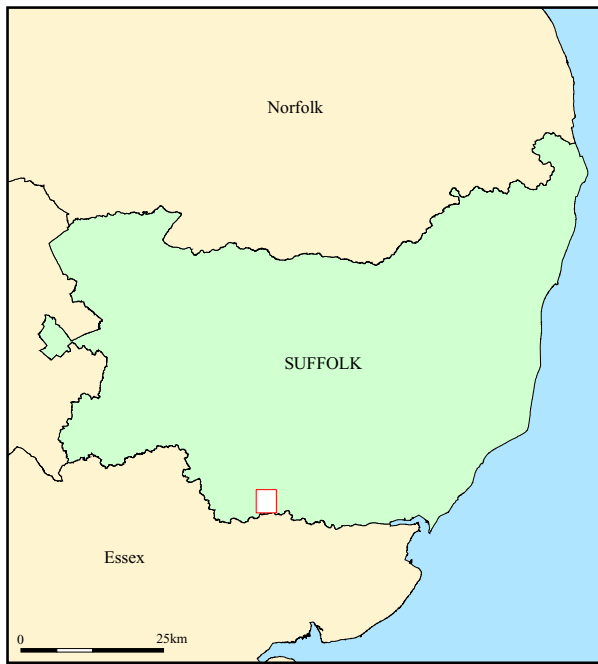
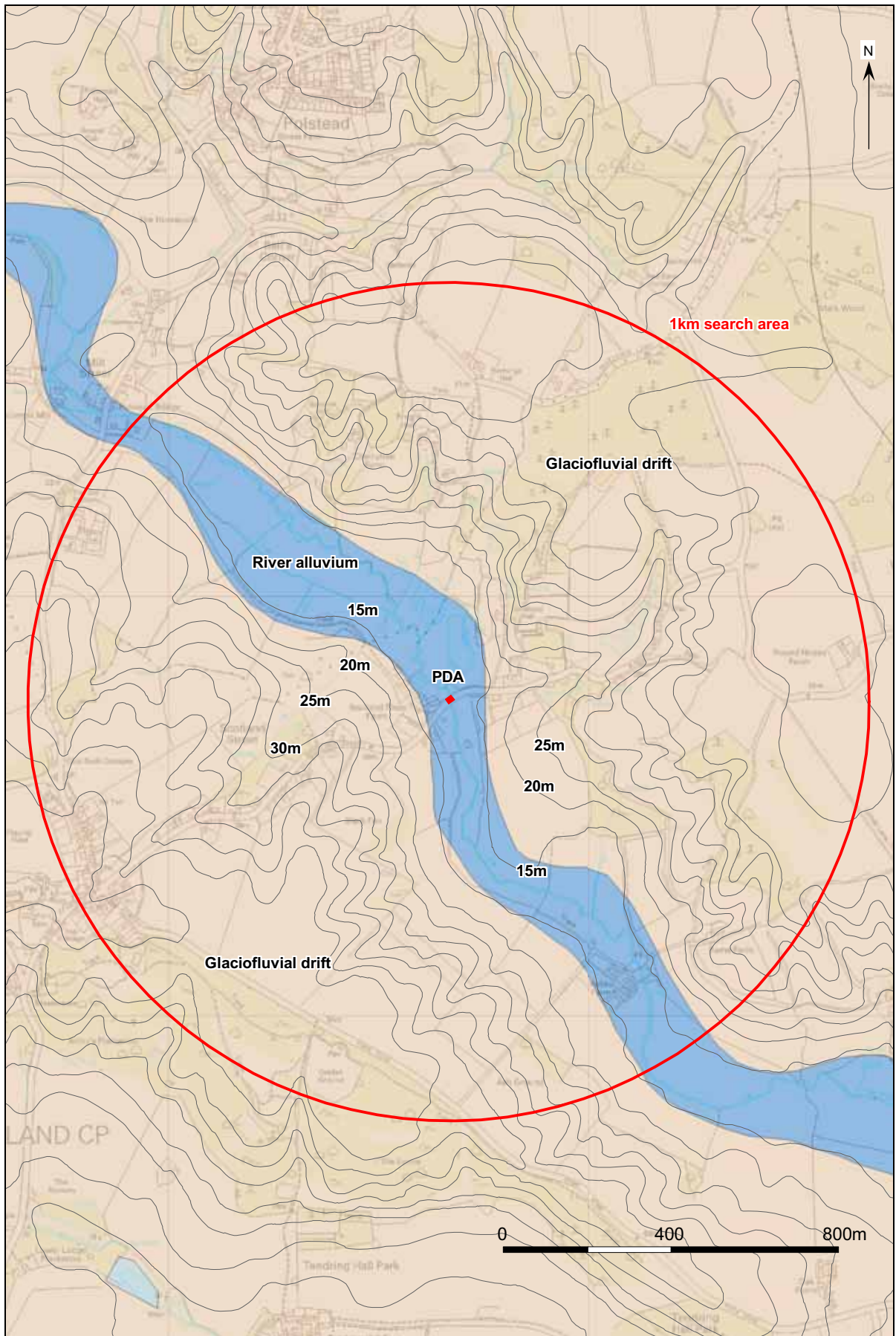


Figure 1. Location map



©Crown Copyright. All Rights Reserved. Suffolk County Council Licence No. 100023395 2012

Figure 2. Topography and geology

## **1.5 Aims**

The aim of the DBA is to determine as far as reasonably practicable from the existing records, the previous landuse, the nature of the archaeological resource and the potential resource within the PDA. In particular the DBA will:

- Collate and assess the existing information regarding a bridge and crossing at this site.
- Assess the potential for unrecorded archaeological sites within the application area.
- Assess the likely impact of past land uses and the potential quality of preservation of deposits/structure, and where possible model these deposits.
- Assess the known documentary sources relating to the bridge in order to provide information relating to the history of the site that is of relevance to the interpretation.
- Assess the known documentary sources relating to bridge/girder manufacturing by E, R & F Turner and to consult the HER.

## **1.6 Methods**

The methodology involved interrogating the following sources of data to meet the aims of this DBA.

A search of the Suffolk HER for any records within 1km from the centre of the PDA. The results are described and mapped in section 2 below.

An examination of the literature with reference to archaeological excavations within the study area.

A search for listed buildings that may have a line of sight to the PDA was carried out. A summary is presented in section 2.2 below.

A historical documentary search, including an assessment of all cartographic sources was commissioned. The results have been summarised in Section 2.4 below, with the full report presented in Appendix 2.

A site walkover was conducted on the 26/06/2012, for which notes and digital photographs were taken.

## **1.7 Legislative frameworks**

National Planning Policy Framework (which replaced PPS5 in March 2012) provides guidance for planning authorities, developers and others on planning and the historic environment (paragraphs 128 & 129 below).

**128.** In determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets' importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. Where a site on which development is proposed includes or has the potential to include heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation.

**129.** Local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this assessment into account when considering the impact of a proposal on a heritage asset, to avoid or minimise conflict between the heritage asset's conservation and any aspect of the proposal.

The Ancient Monuments and Archaeological Areas Act of 1979 statutorily protects Scheduled Ancient Monuments (SAMs) and their settings as nationally important sites.



Listed buildings are protected under the Listed Buildings and Conservation Areas Act of 1990. This ensures that listed buildings are given statutory protection against unauthorised demolition, alteration and extension. Buildings are listed because they are of special architectural importance, due to their architectural design, decoration and craftsmanship; also because they are of historical interest. This includes buildings that illustrate important aspects of the nation's social, economic, cultural or military history or have a close association with nationally important persons or events.

Conservation areas are designated for their special architectural and historic interest, usually by the local planning authority. Any alterations to properties, structures, trees etc in a conservation area may need permission from the local planning authority.

A Site of Special Scientific Interest (SSSI) is an area that has been notified as being of special interest under the Wildlife and Countryside Act of 1981, due to its flora, fauna or geological or geomorphological features.

A Registered Park or Garden is a site included on the 'Register of Historic Parks and Gardens of special historic interest in England' which is maintained by English Heritage. It currently identifies over 1,600 sites assessed to be of national importance. Registration is a 'material consideration' in the planning process, meaning that planning authorities must consider the impact of any proposed development on the special character of a registered park or garden.

## 2. Results

---

### 2.1 Suffolk HER search

The HER only represents the archaeological material that has been reported, this is the 'known' resource. It is not therefore, a complete reflection of the whole archaeological resource of this area because other sites may remain undiscovered, this is considered as the 'potential' resource. Figure 3 shows all sites recorded in the HER within 1km of the PDA.

#### All known archaeological sites within the PDA

There is one HER entry recorded within the PDA, that for Scotland Street Bridge itself (SBN 072), which is noted as being of post-medieval date and present on Hodkinson's 1783 map of Suffolk.

#### All known archaeological sites within 1km of the PDA

There are a further twelve HER entries recorded within 1km of the PDA.

#### **Roman (43 AD – 409 AD)**

**PLS 009.** Findspot of a Late Roman bronze buckle. 830m NW of the PDA.

#### **Anglo-Saxon (410 AD – 1065 AD)**

**PLS 009.** Findspot of an Anglo-Saxon bronze strap end and coin hoard. 830m NW of the PDA.

#### **Medieval (AD 1066 – AD 1539)**

**PLS 034.** Findspot of a papal Bull of Gregorius VIII (1227-1241). 830m NW of PDA.

**SBN 091.** Scotland Place Farm. 16th century farmhouse with 16th century barn and 18th cartlodge and stable (see listed building entries). Adjacent to north-west side of PDA.

#### **Post-medieval (AD 1539 – AD 1900)**

**PLS 041.** Former site of The Red Barn, 18th/19th century, site of a famous Victorian murder. 1km NE of the PDA.

**SBN 061.** Postmill with roundhouse at Withermarsh Green.

**SBN 069.** Tendring Hall Park. Registered Park and Garden, elements dating from the 18th century. 900m SW of the PDA.

**SBN 084.** Dovecote, Scotland Place. Late 18th century dovecote (see listed building entry).

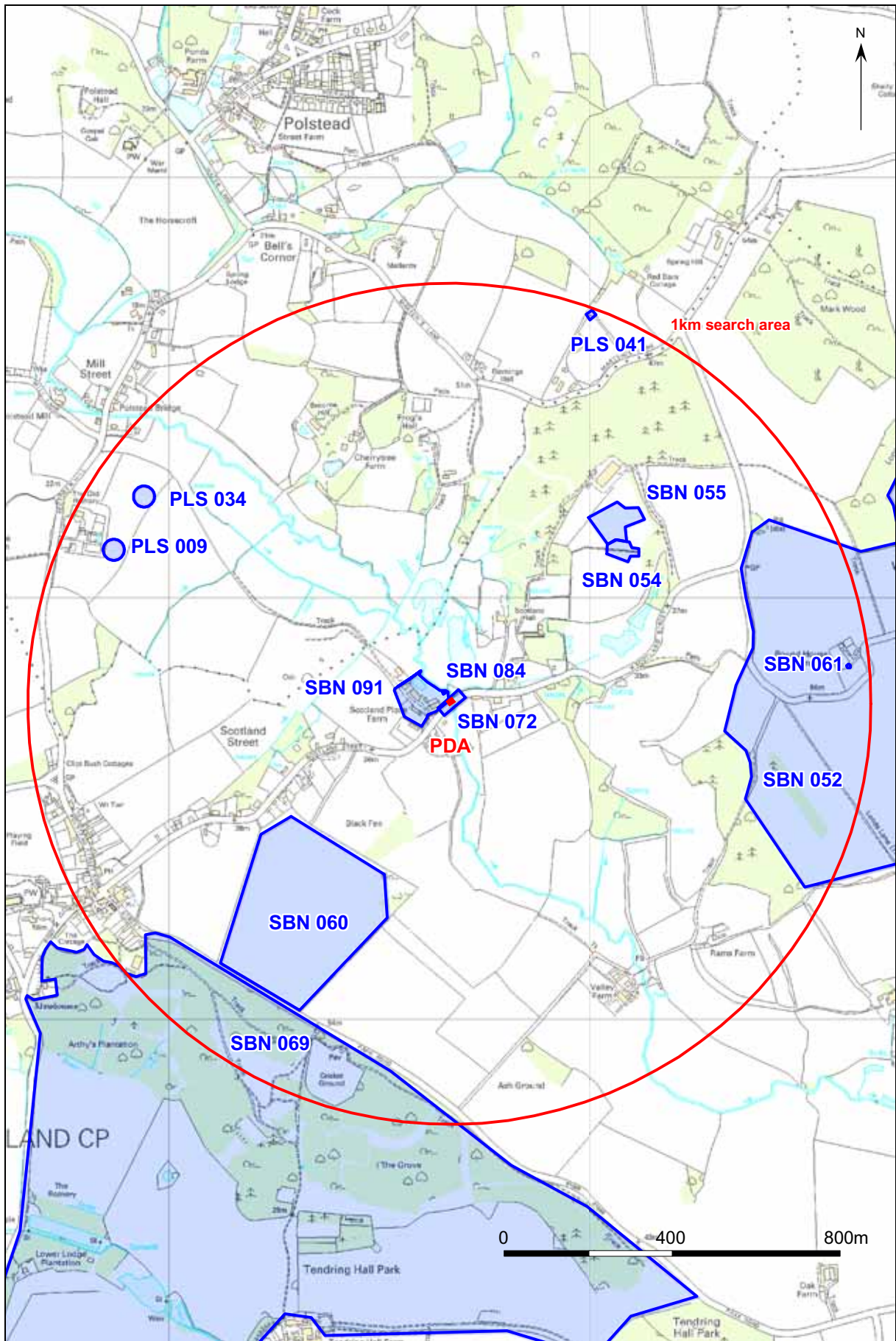
**Undated**

**SBN 052.** Undated possible driveway running SE-NW and a series of field boundaries to W and SW of Green Farm. 650m E of the PDA.

**SBN 054.** D-shaped enclosure with possible entrance/s to N, NE and S. 500m to NE of PDA.

**SBN 055.** Undated field boundaries and trackway running NW-SE and turning into enclosure SBN 054. 500m to NE of PDA.

**SBN 060.** Undated linear ditches outlining a field system. 400m SW of the PDA.



©Crown Copyright. All Rights Reserved. Suffolk County Council Licence No. 100023395 2012

Figure 3. HER sites within 1km of the PDA

## 2.2 Listed buildings

A search was carried out on the Suffolk HER and on the listed building web site <http://list.english-heritage.org.uk>. There are fifty-four Listed Buildings within 1km of the PDA (Fig. 4), of which thirty lie in the Stoke by Nayland settlement core, c.900 to the west, twelve along Scotland Street between 260m and 860m from the PDA and a further four scattered throughout the search area at a distance of 650m+.

The remaining seven listed buildings lie within 250m of the PDA, effectively forming a small settlement on either side of the river crossing, and potentially have a line of sight to the bridge.

**1033474. Barn to South of Scotland Hall.** Grade II. 17th century timber-framed and weatherboarded barn of 9 bays with 2 gabled entrance bays on the west side. (One of the bays is built over). Roof thatched.

**1033482. Maltings Cottage.** Grade II. 17th-18th century timber-framed and plastered house, renovated in the 20th century. 2 storeys. 3 window range, 20th century casements. A central 6-panel door has a shallow hood on brackets. Roof tiled, with a central rectangular chimney stack

**1033519. Dovecote immediately North East of Scotland Place.** Grade II. 18th century red brick square dovecote with a doorway on the west side. Roof tiled, with a hipped dormer. The inside retains the banks of nesting boxes.

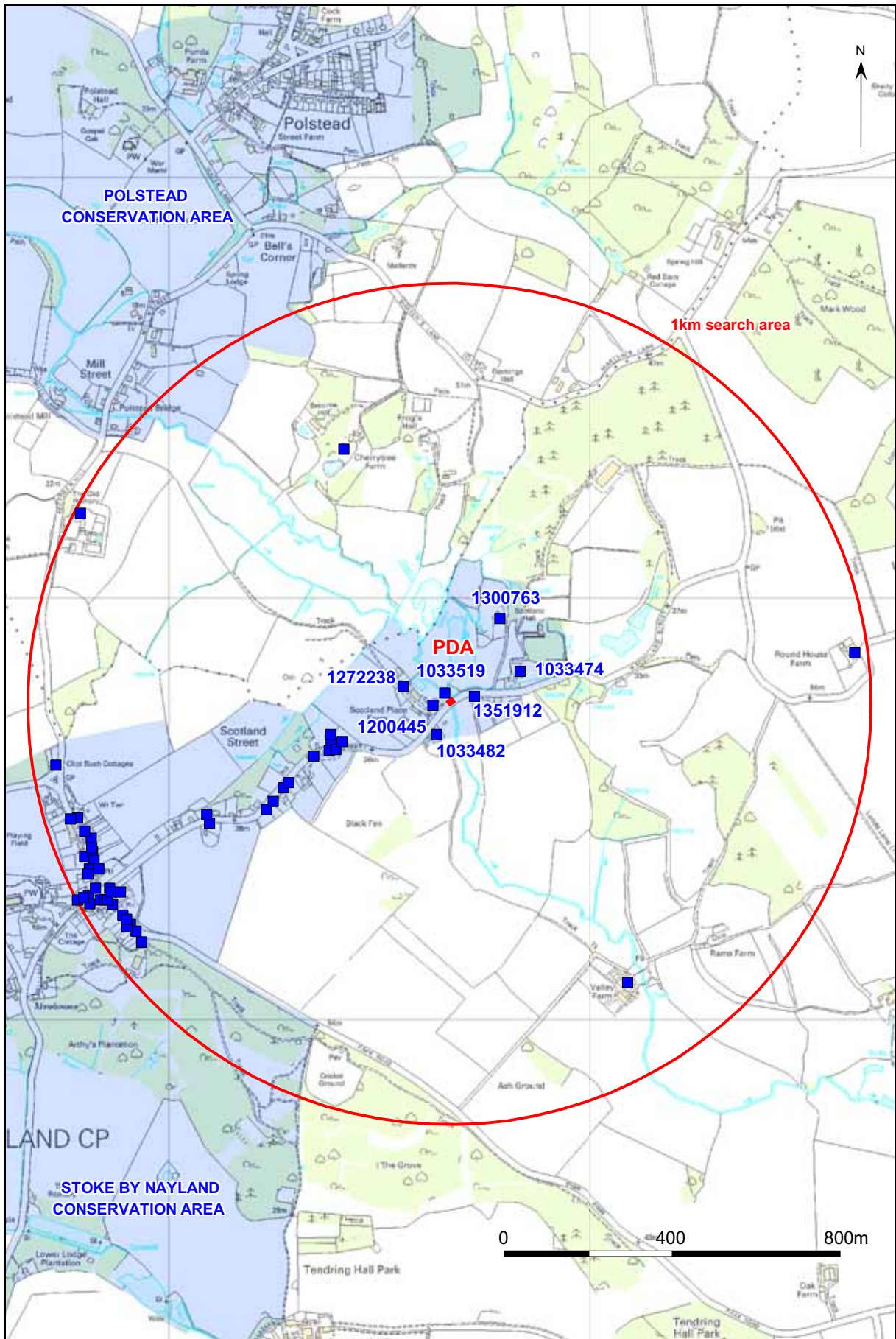
**1200445. Scotland Place.** Grade II. 16th-17th century timber-framed and plastered house, considerably altered in the 18th century when blocks were added at the rear. The earlier block has a cross wing at the west end, with a later addition to the west of the cross wing. 2 storeys. The windows on the south front are mainly leaded casements and at the west end a tall window rises through 2 storeys. There is a doorway with pilasters and cornice. Roofs tiled, with a plaster eaves cornice and a 17th century shafted chimney stack at the west end

**1272238. Cart Lodge to North West of Scotland Place.** Grade II. 18th century timber-framed and weatherboarded with plaintiled roof. A lean-to extension along the rear, but formerly open at front and rear. 4 bay, 2 floored over with an enclosed loft above. The

main posts have kneed braces to the tie-beams and to the wallplates. Clasped purlin roof with a ridge-piece and rafters set on edge. This is a better-built and earlier example than most cart-lodges surviving.

**1300763. Scotland Hall. Grade II.** 18th century red brick fronted south block with older timber-framed and plastered wings at the rear. 2 storeys and attics. 3 window range, double-hung sashes with glazing bars, in flush cased frames, with louvred shutters. A central doorway has a rectangular fanlight and cornice. Roof tiled, with 3 hipped dormers

**1351912. Scotland House. Grade II.** 17th-18th century timber-framed house. Renovated and restored in the 20th century. There are modern additions at the west end and at the rear. Partly 2 storeys and partly 1 storey and attics. The timber framing has been exposed on the upper storey of the main block and the windows are casements, with leaded lights on the main block. A modern gabled porch projects on the front. Roofs tiled, with end chimney stacks to the main block and 1 hipped dormer window to the west addition.



©Crown Copyright. All Rights Reserved. Suffolk County Council Licence No. 100023395 2012

Figure 4. Listed buildings and Conservation Areas within 1km of the PDA

## **2.3 Scheduled Monuments, Conservation Areas and SSSI's**

### Scheduled Ancient Monuments

There are no SAM's within the 1km search area.

### Conservation Areas

The PDA lies within the Stoke by Nayland Conservation Area, which extends from the settlement core along the line of Scotland Street as far east as Scotland Hall (Fig. 4). To the north the southern fringe of the Polstead Conservation Area lies c.900m from the PDA.

### Registered Parks and Gardens

The northern edge of Tendring Hall Park (SBN 069, Fig. 3) just extends into the southern part of the 1km search area. The park was created between the mid and late 18th century and contains remnants of a formal 18th century landscape including a canal, walled kitchen garden, Grade II listed dovecote and temple, and an oak grove.

### Sites of Special Scientific Interest

There are no SSSI's within 1km of the PDA.



## **2.4 Documentary and historic map study summary**

A study of available documentary and cartographic sources indicates that a bridge has probably stood on or near the site since at least the mid 14th century, with references to a 'Skotlodebregge' in a court record of 1387. Scotland Bridge is repeatedly referred to in later medieval and post-medieval documents and so appears to be a long established crossing point on the River Box by the time of Hodskinson's map of 1783 (Fig. 5), which clearly shows Scotland Street and the position of the crossing to the south of Scotland Hall.

The documentary search has identified various documents relating to the construction of the existing bridge by the Lavenham Highway Board under the supervision of their surveyor John Henry Rinder in 1867. Although the records for this board are incomplete, some activities were reported in local newspapers, and it has been possible to establish details concerning the commissioning and funding of the bridge. There is however no information as to the nature of any preceding bridge and, although the names of the local contractor and the supplier of the iron girders are known, there appear to be no other surviving records relating to the construction of the bridge.

The subsequent editions of Ordnance Survey maps, drawn in the late 19th and early 20th centuries (Figs. 6-8), show very little variation, if any to the layout of road, buildings and farmland in the immediate vicinity of the bridge.

The full documentary report is included as Appendix 2.



Figure 5. Location of the PDA on Hodkinson's map of 1783 (not to scale).

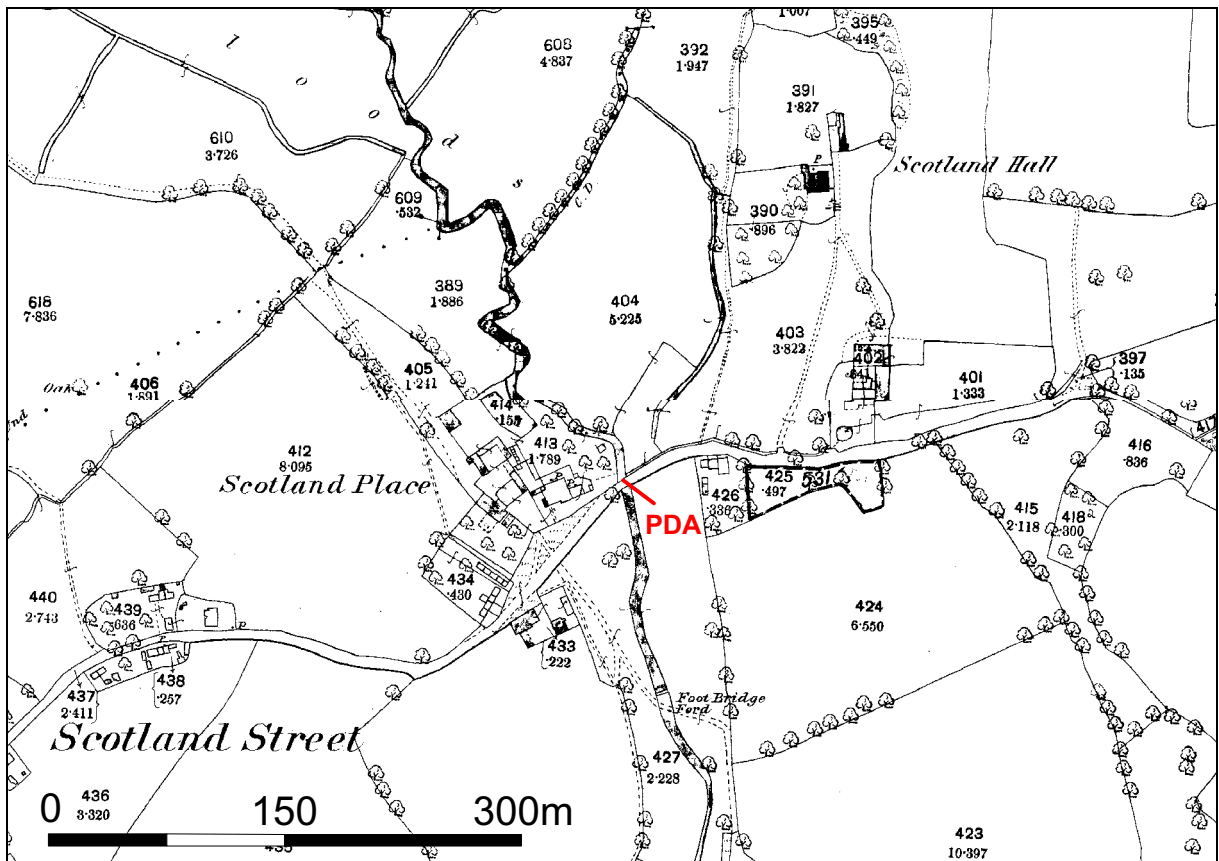


Figure 6. Location of the PDA on 1st Edition Ordnance Survey, 1886.

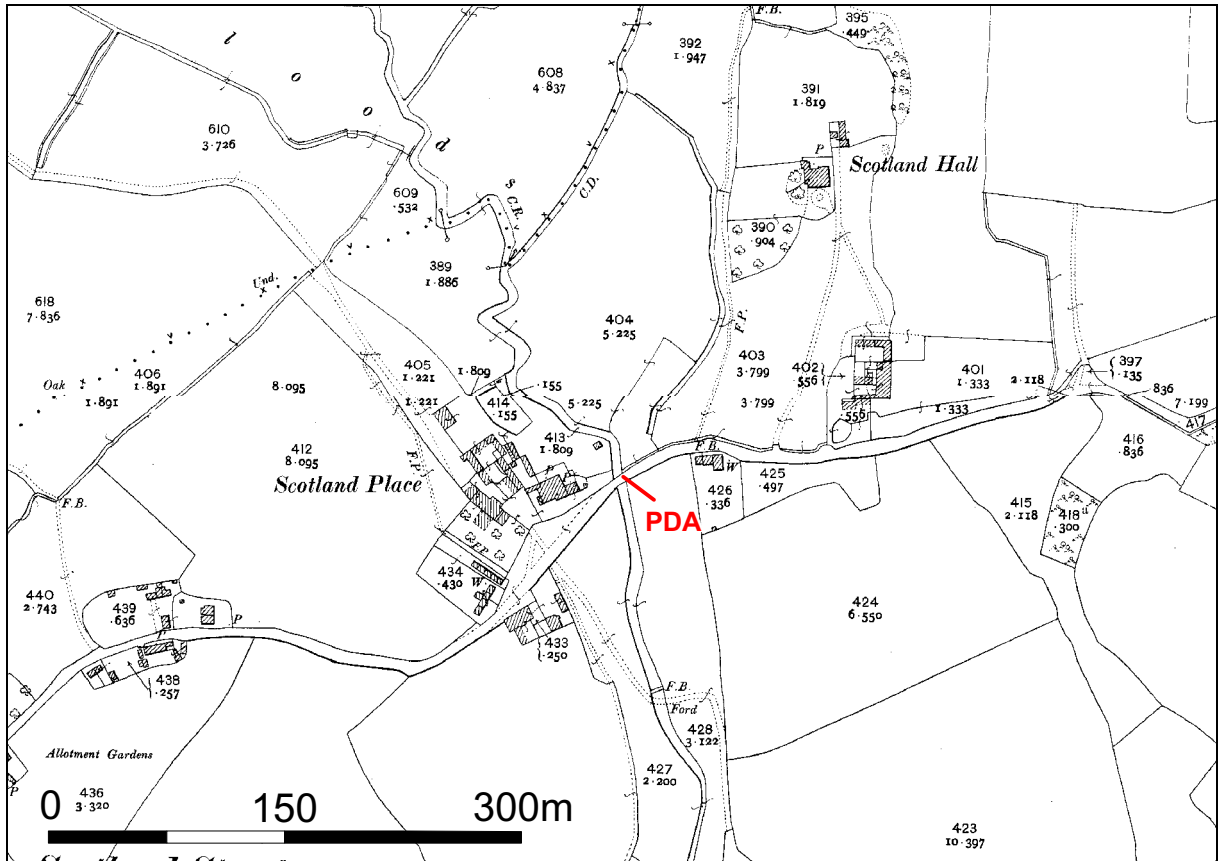


Figure 7. Location of the PDA on 2nd Edition Ordnance Survey, 1904.

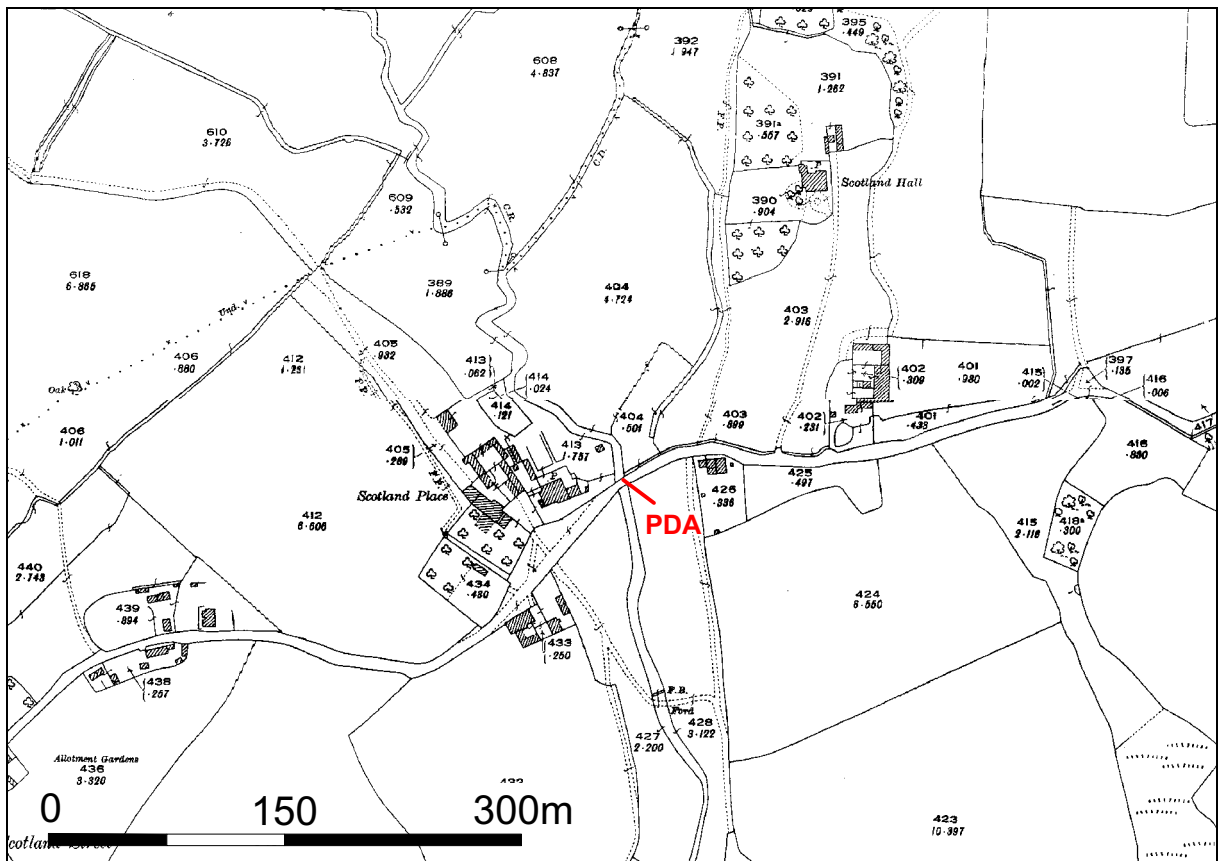


Figure 8. Location of the PDA on 3rd Edition Ordnance Survey, 1926.

## 2.5 Site inspection

A visit was made to the PDA, primarily to examine the position of the bridge, its potential relationship to any earlier crossing, and whether any evidence was visible in bank or river bed for an earlier structure.

The site was examined from on the bridge itself, access to the sides being limited by vegetation etc. With a water level of c.0.3m-0.4m deep the river bed was partially visible amongst vegetation, but there was no visible evidence of any earlier structure. The river banks, c.1m in height, were totally obscured by vegetation in the immediate vicinity of the bridge. The western brick abutment was seen to extend northwards for c.8m however as a revetment wall, until the bank height reduced to 0.3m where the lawns of Scotland Place Farm sloped down to the river edge.

Photographs of the bridge from the road and from the riverbed have previously been supplied by SCC and are included below.



Plate 1. Scotland Street Bridge, looking north at inscribed girder



Plate 2. Scotland Street Bridge, looking east.



Plate 3. Underside of Scotland Street bridge

### **3. Assessment of impacts and effects**

---

#### **3.1 The archaeological potential of the PDA**

The small size of the PDA, its location over a river, and relative lack of known archaeological sites in the near vicinity means that it has little potential for archaeological deposits relating to past activity except in one particular respect relating to its position on a historic crossing of the River Box.

The position of the road and existing bridge, passing to the south of Scotland Place and its dovecote, corresponds to its depiction on both Hodskinson's map of 1783, the farm survey map of 1808 by William Cole and the enclosure map of 1817 (see appendix 2), suggesting that the position of both road and bridge has changed little since the medieval period.

Although the HER entry for SBN 072 states that the crossing appears to be shown as a ford on Bowen's map of 1755 there are references to a bridge dating back to the 14th century and it is highly likely that the bridge of 1867 directly replaced an earlier structure. Any surviving evidence of a former crossing is highly likely to lie within the PDA. Although now only a minor lane the depiction of Scotland Street on Hodskinson's map implies it was a substantial route, presumably with an equally substantial structure where it crossed the river.

The existing bridge is a relatively rare example of a 19th century cast iron bridge in Suffolk, as suggested in the Brief. A search of the Suffolk HER has identified eight other examples (Appendix 3) ranging in date from early to mid 19th century. The type and significance of these bridges varies widely; two lie within parks, the bridge at Culford School being Grade I listed, while the remainder are more typical road bridges. Of these two are Scheduled Monuments while another is Grade II listed. As with Scotland Street four of the six are noted as being shown on Hodskinson's map of 1783 and Bowens map of 1775 and are known to have replaced earlier structures. A further six examples of cast iron bridges, previously unknown to the Suffolk HER, have been suggested to Colin Pendleton of SCCAS by Stuart Heald, Structures Asset Manager at SCC

Economy, Skills and Environment, but details as to the date or form of these are currently lacking.

While Scotland Street is a relatively late and small example of a cast iron bridge, and is not statutorily protected, its importance is increased due to its apparent uniqueness as the only known example of a bridge manufactured by E R & F Turner of Ipswich. The documentary study has been unable to find any evidence for the firm manufacturing bridges as its records appear to be no longer extant.

The cast iron balustrades are probably original, there being a reference to iron fencing in a committee meeting report concerning the commissioning of the bridge.

### **3.2 Potential level of preservation of archaeological remains within the PDA**

Although the site visit did not observe any evidence of a previous structure there is potential for evidence of an earlier bridge or crossing to survive, either on the riverbed or in the banks.

The construction of the 19th century bridge, particularly the brick abutments, will have caused considerable damage to potential remains on the river banks but may have left the river bed relatively intact. Other activities such as natural erosion and general maintenance, e.g dredging, may have also caused considerable disturbance to potential archaeological deposits on river bed or bank. However there is still thought to be moderate potential for structural remains from any former bridge or crossing, such as foundation settings or waterlogged timbers, to survive in both bank or riverbed, particularly if the bridge position has shifted slightly.

### **3.3 Impact of the development on the archaeological resource**

The exact level of ground disturbance to be caused by the renovation of the bridge is unclear but may be relatively minor as the existing brick abutments are to be reused to support the new structure and there may be little requirement for excavation of new foundations etc. The main impact on potential archaeological deposits will probably come from use of heavy plant, particularly if these are situated on the bank or in the river itself.

The main impact of the renovation will be on the bridge itself, with the proposal being to dismantle the structure entirely before rebuilding on the brick abutments.

The renovation of the bridge may also have a visual impact upon the Conservation Area and the setting of listed buildings in the immediate vicinity.



## 4. Mitigation measures

---

The renovation of the existing bridge will have a major impact on its fabric and character but, due to its very low weight limit, safety issues need to be balanced against this impact and complete preservation *in situ* is not considered to be feasible. The impact of renovation on potential archaeological deposits may be relatively minor. A suitable mitigation strategy could greatly reduce the impact of the renovation on both the bridge itself, the Conservation Area, and on potential archaeological deposits.

Prior to works a historic building record survey is recommended (English Heritage level 2) while Robert Malster of the Suffolk Industrial Archaeological Society, who supplied the original information to SCCAS/CT concerning E R & F Turner of Ipswich, has indicated he would like to make a photographic record as the bridge is dismantled. Any survey should utilise any existing structural plans held or created by SCC.

Although not statutorily protected the bridge is an important structure and elements of its structure and appearance should be preserved wherever possible. The suggested retaining of the brick abutments, reinstatement of the exterior girders as a fascia to the new bridge and reinstatement of the balustrades appears to be the most effective solution that can be achieved whilst making the bridge fit for purpose and modern traffic. This reinstatement will also minimize the visual impact of the works on the Conservation Area and setting of nearby listed buildings.

Secondly the proposed renovation may impact upon heritage assets relating to earlier bridges or crossings, particularly if any groundworks are required or heavy machinery enters the watercourse. In this case, with the proposed works consisting of improvements to the existing road bridge, there is no scope for prior archaeological evaluation to determine the presence and extent of potential archaeological deposits, as is usually recommended before determination of any planning application. In view of the likelihood for such deposits relating to an earlier bridge or crossing a suitable mitigation strategy of archaeological monitoring during renovation works is recommended to preserve any such archaeological deposits by record.

## **5. Conclusions/Recommendations**

---

Through an examination of the Suffolk HER, a documentary search and site inspection, this DBA has set the PDA within its immediate archaeological landscape.

The PDA has high potential for archaeological evidence of an earlier bridge or crossing over the River Box, potentially dating to at least the 14th century, although the construction of the existing bridge may have had an adverse effect on the potential for the survival of archaeological remains. It is recommended that any groundworks associated with the demolition and reconstruction of the bridge should be archaeologically monitored to preserve archaeological deposits by record.

The existing bridge itself is relatively late and of minor size. Although it appears to be the only one known using cast iron girders by E R & F Turner of Ipswich it is arguably not of sufficient merit to warrant preservation *in situ* once safety factors concerning its strength are taken into account.

Instead of preservation it is recommended that an English Heritage Level 2 Historic Building Record of the bridge is carried out prior to commencement of works and that the Suffolk Industrial Archaeological Society is given the opportunity to observe the dismantling of the bridge. The proposed reuse of the brick abutments and reinstatement of the balustrade and the exterior girders as a façade to the new bridge, is also recommended as a minimum to preserve the character and appearance of the bridge as far as possible.

Consultation with the LPA and its Archaeological Advisors is recommended at the earliest possible opportunity to determine the actual program of mitigation works required as such investigations could have considerable time and cost implications.

## **6. List of contributors and acknowledgements**

---

This project was funded and commissioned by Suffolk County Council. The desk based assessment was carried out by John Craven, of SCCAS/FT, the documentary study by Anthony M. Breen, a freelance local history researcher.

Environment and Transport Service Delivery  
Shire Hall  
Bury St Edmunds  
Suffolk  
IP33 2AR

## **Brief and Specification for Desk-Based Assessment**

### **SCOTLAND STREET BRIDGE, STOKE BY NAYLAND, SUFFOLK**

#### **1. Background**

- 1.1. Scotland Street Bridge, Stoke by Nayland, is a 19<sup>th</sup> century structure that spans the River Box (Historic Environment Record SBN 072, TL 996 367). The bridge is historic, and is within a conservation area and in the vicinity of listed buildings (278632, 278619). A bridge is shown on Hodskinson's 1783 map, and there is likely to have been a bridge or crossing at this point for some considerable time.
- 1.2. The current bridge comprises brick abutments supporting a deck of 5 cast iron girders, with transverse concrete slabs cast between them. The parapet is formed from cast iron post and rail balustrading (contemporary to the main structure?). The southern-most girder is inscribed with 'J. H. Rinder Surveyor, 1867' and the name of the fabricator, 'E R & F Turner of Ipswich'
- 1.3. E R and F Turner were an important firm of Ipswich-based milling engineers, with offices around the world. Members of the Suffolk Industrial Archaeological Society know of no other existing bridges made by this company (though there might well be others), and have indicated that the bridge, although of relatively late date, is likely to be rare and therefore of historical importance.
- 1.4. Work to replace the deck of Scotland Street Bridge, Stoke by Nayland, has been proposed by Suffolk County Council, Environment and Transport Division to avoid over-stress and sudden failure. The bridge has been assessed and found to have a capacity of 3T, which has been applied as a weight restriction. This is, however, difficult to enforce. The working proposal is to retain the existing cast iron edge beams as non-structural fascias to a proposed new reinforced concrete bridge deck on the existing abutments. It is also proposed to retain, as far as is possible, the balustrades.
- 1.5. An assessment of the historic significance of the structure is needed to inform determination of conservation area consent.
- 1.6. The assessment would also inform the scope of any further archaeological work prior to and/or during works that would be necessary to record and understand the fabric. The site may retain important archaeological information concerning the construction, character, date, and context of the bridge, as well as the longer development of the site.

1.7 This brief sets out the requirement for the desk based assessment.

## **2. Objectives**

2.1 To collate and assess the existing information regarding a bridge and crossing at this site.

2.2 To assess the potential for unrecorded archaeological sites within the application area.

2.3 To assess the likely impact of past land uses and the potential quality of preservation of deposits/structure, and where possible to model those deposits.

2.4 To assess the known documentary sources relating to the bridge in order to provide information relating to the history of the site that is of relevance to the interpretation.

2.5 To assess the known documentary sources relating to bridge/girder manufacturing by E, R and F Turner and to consult the HER for other examples of surviving cast iron bridges to set this one into context.

2.5 An outline specification, which defines certain minimum criteria, is set out below. In accordance with the standards and guidance produced by the Institute of Field Archaeologists this brief should not be considered sufficient to enable the total execution of the project. A Written Scheme of Investigation (WSI) based upon this brief and the accompanying outline specification of minimum requirements, is an essential requirement. This must be submitted by the developers, or their agent, to the Conservation Team of the Archaeological Service of Suffolk County Council for approval. The work must not commence until this office has approved both the archaeological contractor as suitable to undertake the work, and the PD/WSI as satisfactory. The PD/WSI will provide the basis for measurable standards.

## **3. Specification**

3.1 The assessment shall be undertaken by a recognised professional. The contractor is expected to follow the Code of Conduct of the Institute of Field Archaeologists.

3.2 Collation and assessment of the County Historic Environment Record to identify known sites and to assess the potential of the application area.

3.3 Collation and assessment of all cartographic sources relevant to the site. Where it is possible, copies should be included in the report. Draft guidelines on appropriate cartographic sources to be used desk-based assessments are available from this office.

3.4 Collation and assessment of historic documentation relevant to the site that would contribute to the archaeological investigation of the site. Draft guidelines on appropriate archive and documentary sources to be used desk-based assessments are available from this office.

3.5 If deemed appropriate, site walk-over survey.

#### **4. Report Requirements**

4.1 The report shall be submitted within a length of time (but not exceeding 1 month) to be agreed between the developer and archaeological contractor, with a copy supplied to the County Historic Environment Record. A full digital copy of the report will be supplied to Suffolk County Council Archaeological Service Conservation Team (SCCAS/CT).

4.2 Assemble, summarise and order the available evidence.

4.2 Synthesise the evidence and place it in its local and/or regional context.

4.3 The Report must include a discussion and an assessment of the archaeological evidence within the regional context. The conclusions must include a clear statement of the archaeological potential of the site, highlighting any research priorities, and the significance of that potential in the context of the Regional Research Framework (*East Anglian Archaeology*, Occasional Papers 3 & 8, 1997 and 2000).

4.4 Comment on the reliability of the evidence and give an opinion on the necessity and scope for further assessment including field evaluation.

4.5 A comprehensive list of all sources consulted (with specific references) should be included.

4.6 A copy of the report should be deposited with the County Historic Environment Record within six months.

4.7 At the start of work (immediately before fieldwork commences) an OASIS online record <http://ads.ahds.ac.uk/project/oasis/> must be initiated and key fields completed on Details, Location and Creators forms.

4.8 All parts of the OASIS online form must be completed for submission to the County Historic Environment Record. This should include an uploaded .pdf version of the entire report (a paper copy should also be included with the archive).

4.9 The IFA *Standard and Guidance for Archaeological Desk-Based Assessments* (1999) should be used for additional guidance in the execution of the project and in drawing up the report.

4.10 Publication of the results, at least to a summary level (i.e. round up of archaeology in the annual 'Archaeology in Suffolk' section of the *Proceedings of the Suffolk Institute for Archaeology*), shall be undertaken in the year following the archaeological field work. An allowance shall be made within the costs for full publication in an appropriate journal.

4.11 The involvement of SCCAS/CT shall be acknowledged in any report or publication generated by this project.

## **5. Monitoring**

- 5.1 SCCAS/CT will be responsible for monitoring progress and standards throughout the project. This will include the fieldwork, post-excavation and publication stages.
- 5.2 Notification of the start of work shall be given to SCCAS/CT one week in advance of its commencement.
- 5.3 Any variations to the written scheme of investigation shall be agreed with SCCAS/CT prior to them being carried out.

Specification by: Abby Antrobus

Suffolk County Council  
Archaeological Service Conservation Team  
Economy, Skills and Environment  
Shire Hall  
Bury St Edmunds  
Suffolk IP33 1RX

Tel: 01284 741231  
Email: [abby.antrobus@suffolk.gov.uk](mailto:abby.antrobus@suffolk.gov.uk)  
Date: 17<sup>th</sup> May 2012  
Reference: 2012/Encheck Scotland

**This brief and specification remains valid for six months from the above date. If work is not carried out in full within that time this document will lapse; the authority should be notified and a revised brief and specification may be issued.**

## **Appendix 2. Documentary and Cartographic Study**

Antony M Breen

### **Introduction**

The Suffolk Archaeological Service has been commissioned to prepare of a report on Scotland Street Bridge, Stoke By Nayland. This bridge was built in 1867 by the Lavenham Highway District Board. This board and others were established under the 1862 Highways Act.

The preservation of the records of the Lavenham Highway District Board and other boards incorporated under the same legislation is poor. All the surviving records of the Lavenham Highway District Board are now held at the Suffolk Record Office in Bury St Edmunds. The boards took over the work of the parish highway surveyors whose own records are equally sparse.

Scotland Street in Stoke by Nayland takes its name from De Scotland family who held the lordship of Scotland Hall Manor in the early fourteenth century. The manor and site of Scotland Hall were part of the estates of the Mannock family whose records are held at the Suffolk Record Office in Ipswich (ref. HA 246). The Rowley family of Tendring Hall, Stoke By Nayland though they held the titles to other manors were also the tenants to their neighbours the Mannocks and held land of the manor of Scotland Hall. The farm Scotland Place depicted on an 1808 estate map (HA 108/10/2/15) was part of the Tendring Hall Estate.

This bridge when it was rebuilt in 1867 was not a county bridge however the Quarter Sessions Minutes for 12 March 1867 record the application from the Lavenham Highway Board for a loan for the rebuilding of this and Thorrington Water Bridge in Stoke by Nayland. The minutes further note a 'request of the said Board that the County Surveyor should be directed to superintend the erection of such bridges with a view to the same becoming hereafter County Bridges This court doth hereby appoint Mr George Smith of Rattlesden in the said county to be the surveyor for the purposes aforesaid' (ref. B105/2/106 p. 192). The Board had previously in October 1866 sought the approval of Quarter Sessions to borrow £180 for a bridge in Lavenham but the earlier minutes do not include a request for the bridge to be adopted by the county.

Amongst the county council records there is a Photostat copy of the Lavenham Highway District's 1864 'Survey of Roads' (ref. SCC 2357/1). It is a duplicate copy of the printed survey held at the Suffolk Record Office in Bury St Edmunds in the Layham Parish Collection (ref. FB 82/H1/1). There are detailed engineer's plans of Thorrington Street Bridge, Stoke by Nayland prepared in 1981. This bridge known as Thorrington Water Bridge was also rebuilt by the Lavenham Highway Board in 1867 (ref. 2051/1110). The committee minutes and correspondence relating the 1981 have not been filed with the plans.

## **Highway District Boards 1864-1894**

Before the establishment of the county councils and district councils in the late nineteenth century the administration of the county was the responsibility of the Quarter Sessions. At a parish level the work of public administration was divided between the elected churchwardens and overseers who at their vestry meetings appointed other parish officers such as the constables and high surveyors. The sessions did not employ permanent staff and much of their work was conducted through ad hoc committees established for specific purposes. Both the sessions and its sub-committees did consult and often employed lawyers to act as their clerks. The growth of public administration during the nineteenth century led to the employment of some staff on a temporary or permanent basis. In addition parliament passed a number of acts that led to the formation of separate administrative bodies incorporated under the seal of the quarter sessions. These incorporated bodies were ultimately empowered through the various acts of parliament and their work was only superseded with the establishment of the later county and district councils.

The Lavenham Highway District Board was established in 1862 under 'An Act for the better Management of Highways in England' (25 & 26 Vict., c. LXI) known by its short title of Highway Act, 1862. The act is subdivided into 43 sections but its work has been summarised as 'Under that act quarter sessions was empowered to create highway districts each governed by a highway board consisting of waywardens, elected by constituent parishes, and of the justices resident in the district. The new highway boards when constituted became surveyors of highways for the parishes situated in their districts' (Hart 1957). Sections 12-16 of the act dealt with the appointment of the officers 'a Treasurer, Clerk, and District Surveyor' and also empowered the boards 'if they think fit, appoint an Assistant Surveyor'. The duties of the clerk are described under section 15, in that he or his deputy 'shall conduct the correspondence thereof, and enter and keep, in Books provided for the Purpose, Notes, Minutes, or Copies, as the case may require, of the Meetings, Acts, Orders, Resolutions, Proceedings, and Correspondence of such Board, and shall keep all Books, papers, and Documents committed to his Charge'. The clerk of the Lavenham Highway District Board, when first formed, was Richard Newman a solicitor based in Hadleigh.

## **Archive Collections**

The records of the Lavenham Highway Board (ref. 1445/7) are held at the Suffolk Record Office in Bury St Edmunds in a solicitors' collection for the firm H.W.F Grimwade of Hadleigh and its predecessors. In its origins the firm was originally established by Richard Newman who had entered a partnership with Charles Harper by 1855, by 1874 Charles James Grimwade had joined the firm. Apart from the Highway Board, this collection includes further examples of a firm of solicitors acting in the role of public administration. The collection includes the records of the Hadleigh District Court (1847-1935), of the Cosford District Taxation records (1837-1938), of the Cosford Poor Law Union (1712-1923) including papers relating to registration of Births, Deaths and Marriages and census records



(1845-1912) and electoral papers (1868-1880) as well as records relating to the commercial side of the business. The arrangement of these records is not unusual and other solicitors' collections also include records of public administration. At an earlier period the ad hoc committees of the quarter sessions were often formed entirely of local land owners who as freeholders were entitled to vote in elections and as such were also obliged to serve on the jury at the quarter sessions. The county's magistrates were drawn from this same class. It is not uncommon for records relating to public administration to be part of some of the larger estate collections.

The work of this board, consisting of 54 parishes, and others boards was formed in 1862 under the Highways Act continued until 'Highway Districts were abolished by the Local Government Act, 1894'. The solicitors' collection includes the clerk's records up to 1878 only. The records are far from being a complete record of the board's work during this period and the collection consists of loose papers only. There are no minute books in this collection.

In 1894 the administrative area of the Highway Board was divided between the various district councils and the Hadleigh Urban District Council. The record office's collection for the Hadleigh District Council does include records for the highway department listed under various headings in the collection's catalogue (ref. EF 504). None of these records predate the formation of the Urban District Council. Of the rural district councils, there are no highway records in the collection of records for Cosford (ref. EF 503). Stoke by Nayland was within the area covered by the Melford Rural District Council and their records include a single minute book for the Highway Committee for the years 1932-1933 (ref. EF 501/1/19). The record collections of other rural district councils are equally sparse in relation to the work of the highway committees.

There are a few additional records relating to the work of the Lavenham Highway District Board that have survived within individual parish collections, such as the printed 'Survey of Roads 1864' in the Layham Parish Collection (ref. FB82/H1/1). This survey lists all the roads within each parish that had been maintained at public expenses and whose maintenance was by 1864 the responsibility of the newly established Highway Board.

The records of the all boards formed under this act are incomplete and a search of the record office's current computerised index CALM under the search term 'Highway Board' produces only 14 references.

Much of the work of these boards was the subject of articles published in local newspapers with details of the dates of their meetings, annual accounts and details of contracts open for tender. Copies of the 'Bury and Norwich Post' can now be searched online through '19th Century British Library Newspapers'. Access to this website prepared by Gale Cengage Learning is available through the Suffolk County Council Library Service.

## **The Records**

Amongst the County Quarter Session records held at Bury St Edmunds there is one document

relating to the incorporation of the board.

The board was provisionally incorporated at a meeting of the county quarter sessions held 'by adjournment' at Bury St Edmunds on 13 January 1863. After publishing the provisional order in the 'London Gazette' on 20 January 1863 and in 'one or more newspapers circulating in the District affected by the said provisional order' and also by sending copies of the order to the 'Overseers of every parish with the proposed Highway District', the board was established and held its first meeting at the Swan Inn, Lavenham on 31 March 1863 (ref. Q/SH119).

Shortly after the first meeting of the board they would have had to advertise for the post of surveyor. Amongst the surviving records there is a handwritten notice, dated 1864 for the posts of District Surveyor at an annual salary of £250 and Assistant Surveyor with the salary of £100. The district surveyor was expected to keep two horses out of his salary and the assistant surveyor one horse from his salary (ref. 1445/7/4).

The printed 'Survey of Roads 1864' bears the name of 'F. Sexton Surveyor and Estate Agent, Kersey, Near Hadleigh, Suffolk'. Frederick Sexton is listed in Harrod's 1864 'Directory of Suffolk' under Kersey and is described as a 'land surveyor, land agent & collector of poor rates and assessed taxes & postmaster' his work for the Highway Board is not mentioned. The survey is subdivided by parish with the names of roads, description and distances given in miles, furlongs, chains and links. A furlong is an eighth of a mile with 10 chains to a furlong and 100 links to a chain. On pages 59-60 there is a summary for the entire district further subdivided under the headings 'Hard Roads', 'Green Roads' and 'Turnpike Roads' with a total for the district of 433 miles 4 furlongs (ref. FB82/H1/1).

In the Stoke by Nayland Parish Collection, there is an envelope containing two copies of handwritten notices both bearing the seal of the Lavenham Highway Board relating to an application to the quarter sessions '*for their consent an approval*

*1. To borrow and take up at interest the sum of £480 for the purpose of defraying the expences of building two bridges in the Parish of Stoke by Nayland ... over certain streams called respectively Thorrington Water and Scotland Water and making the necessary approaches to the same the cost of which said bridges and approaches are estimated to amount to the said sum of £480.*

*2. That the said some of £480 and the interest thereon be contributed raised and paid by the said parish of Stoke by Nayland*

*3. That the said principal sum of £480 so proposed to be borrowed together with interest be paid off in 12 years and that the sum of £40 be set apart in each year for paying off the same'.*

The notices are dated 18 January 1867 and signed by Richard Newman, clerk. One of the notices is endorsed '*A Notice corresponding with this was sent to Sir C Rowley, to be present at the Quarter Sessions at Bury St Edmunds on the 12<sup>th</sup> of March 1867 Edward J Parson*' (ref. FB80/H1/5).

In 1860 the three eastern divisions of the quarter sessions were amalgamated to form the Eastern Division and the Bury St Edmunds Division became the Western Division. Though some quarter session records for the Western Division are held at Bury St Edmunds including Highway Diversion Orders from 1777, the records relating to this application for additional borrowing is held at the Suffolk Record Office in Ipswich and the relevant minutes have been quoted in the introduction to this report.

The Lavenham Highway Board financial records consist of various files of loose papers. These include published annual general statements sub-divided by the name of each parish, the mileage of their highways, in later accounts subdivided into both parish and districts roads (former turnpikes) and receipts of funds. The expenditure accounts includes sums of manual labour including contract work, team labour, materials got or prepared or supplied under contract, tradesmens' bills, improvements including contract work, charges on the common fund and interest of debt. There is a handwritten statement of income for 1863 and printed statements for 1864-67 1869-1870, a handwritten statement for 1873 and printed statements for 1875-76 and 1878 (ref. 1445/7/1). In the Lavenham Parish Collection there is a printed statement dated 31 December 1869. The total mileage for the district given in these accounts does vary over time no totals were given in the accounts for 1864, in 1865 the total was 431 miles 2 furlongs, by 1866 this had been reduced to 431 miles and by 1869 the miles were further reduced to 406 miles 5 furlongs. In 1870 there were 388 miles and 7 furlongs under the care of the board and this figure is given in all the later accounts to 1878.

The accounts offer evidence of extraordinary items of expenditure. In 1866 there is a sum of £188 18s listed under Lavenham for 'improvements'. In 1867 there were further improvements with £80 10s 11d spent at Cockfield, £205 2s 4d spent at Preston and £701 11s 7d spent at Lavenham. In the statement for 1867 the rateable value of 'Stoke' is given as £9294 and the length of the highways was 23 miles 4 furlongs. The cash balance at the start of the year was £18 11s 3d and the rate assessment was £233, added to these sums an amount of £480 had been borrowed and there were further receipts of £12 5s 1d. During that year £31 4s 2d was spent on manual labour including contract work, £5 15s 6d was spent on team labour, only £7 0s 3d was spent on materials and £35 9s 10d on trades men's bills. The sum of £701 11s 7d was spent on 'improvements' and £19 7s 3d was spent on salaries and common charges. Some £11 19s 4d was paid in interest and £20 of the money borrowed had been repaid.

A hand written estimate of the costs of roads for 1868 is in a separate file (ref. 1445/7/2) together with separate papers for Acton dated 1866 and a table of receipts from 17 parishes dated 1869-1870. The current account pass book covers the years 1874-1879 only (ref. 1445/7/3).

The administrative records included a list of elected 'waywardens' which includes the names of E L Parson and William Mudd under Stoke by Nayland. Sir C R Rowley and Captain J T Rowley both of Stoke by Nayland are listed as officers of the board (ref. 1445/7/4).

Various cases papers are in other files and these include a report of the committee appointed to ‘confer with the surveyor respecting the erection of the proposed bridges at Thorrington Street and Scotland Street’.

*‘Your committee met at Thorrington Street on Friday the 2nd instant, present Captain J T Rowley and Messrs Stannard, Munford, Lungley, Goldsmith & Crooks when the surveyor submitted, plans, specifications and estimates of cost for our approval.*

*Your committee having minutely examined the sites of the two proposed bridges, also the plans specifications and estimate of cost and taking into consideration the locality and amount of traffic decided by a majority to recommend for your sanction the erection of two bridges off the following dimensions,*

*Viz. Twenty feet span, five feet in height and with a clear carriageway of sixteen feet, the same to be constructed in accordance with the Plans &c submitted by the surveyor with the exception of the width of carriageway and having Iron fencing in place of the brick parapets.*

*And your committee also recommend that tenders be advertised for, from persons willing to undertake the entire completion of the two bridges and that the surveyor be empowered to employ manual & team labour in completing the approaches*

*Signed on behalf of the committee J T Rowley’*

This document is not dated (ref. 1445/7/7).

The only reference to any other bridge that may have been built by the board is in a letter dated 1863 from the ‘waywarden of Brent Eleigh to the Highway Board on the construction of a bridge over a ford in Cock Lane at Brent Eleigh’ (ref. 1445/7/5).

## **Bury and Norwich Post**

In the period 1 January 1864 to 1 January 1870 some 54 reports of the work of the Lavenham Highway Board were published in the ‘Bury and Norwich Post’. An account of the board’s meeting held on 1 June 1866 was published in the newspaper on 5 June 1866. Part of the meeting was devoted to an appointment of a new surveyor and amongst the 13 applicants who attended the meeting there was:

‘13. Mr John Henry Rinder, Rochester’. He ‘had been Surveyor of the Rochester district and was 39 years of age’. His terms were £250 per annum and he had enclosed a number of testimonials in support of his application, ‘but none of very recent date- on being called in he explained that he had not produced a testimonial from Rochester Board, as they only met once in three months, and their meeting was not till the second Tuesday in June. His terms were £280 a-year; if he wrote £250 it was a mistake, which he wished to correct. His present district contained 206 miles of road, and he had no doubt of his ability to undertake the surveyorship of a district as large as Lavenham’. He ‘had been at Rochester two years’. He ‘had not been accustomed to drawing plans, but could measure brickwork, and would

undertake to procure plans whenever they might be required'. He 'did not profess to be an engineer, but a real practical road surveyor'. He 'had previously been engaged as Surveyor for the parish of Battersea, and before that was similarly engaged at Staleybridge, near Manchester'. He was elected on the 4th ballot.

The inscriptions 'J.H.Rinder, surveyor, 1867' and 'ER & F Turner of Ipswich' appear on one of the bridge's girders. The firm ER and F Turner was later taken over the Bull Electric Ltd, Engineers of Ipswich and though the firm's records have been deposited at the Suffolk Record Office in Ipswich the only records to have survived from the earlier firm of ER and F Turner are the minute books from 1901-1925 and 1933-1946 (ref. HC 465). A printed catalogue of the firm's products dated 1875 held at the Museum of English Rural Life at Reading does not contain any references to bridges.

A further reference to the bridge in the newspaper names the local contractor as a Mr Kingsbury. No advertisement relating to the construction of this bridge has been found in this newspaper.

### **Enclosure Map and Award**

Lands in the parishes of Stoke by Nayland and Nayland with Wissington, Assington and Polstead were enclosed under a single act of parliament passed in 1815. The enclosure map and award deposited with the county quarter sessions are dated 1817 (ref. Q/IR 34). The award describes only the nine roads that were then newly set out under the terms of the act. Though a river crossing is shown on the map it is not depicted in any detail. Two small plots of land to the west of the bridge numbered 82 and 83 were awarded to Sir William Rowley. The plot numbered 82 to the north of the road measured just 28 perches and the plot to the south 83 measured just 1 rood. There were 40 perches to a rood and four roods to an acre.

There is a second copy of this map in the Rowley, Tendring Hall Collection in Ipswich (ref. HA108/10/9).

### **Deeds Bury St Edmunds**

There are two early references to this bridge in deeds in the form of indentures dated 1662. The indentures are between Judith Rosier of Stoke by Nayland widow and William Gibbs and are dated 25 April 1662. Part of the property conveyed is described with the spellings modernised as 'one orchard or toft whereupon a tenement sometimes stood thereunto also adjoining and now belonging ... as all the same premises are situate lying and being between Scotland Bridge and Weeping Hill in Stoke next Nayland' (ref. E3/10/88.2,3).

In another deed dated 23 October 1695 between John Archer of Stoke by Nayland and his son Francis Archer, the property is described as 'all that messuage or tenement ... the same are situate lying and being between Scotland Bridge and Weeping Hill' (ref. E3/10/88.5).

There are further references to the property adjoining the bridge and the road leading to the bridge 'Scotland Street' in the Rowley of Tendring Hall Collection and the Mannock Family Collection both held at the Suffolk Record Office in Ipswich.

### **Estate Maps and Surveys Ipswich**

There is a large collection of estate maps in the Rowley, Tendring Hall Collection. The earliest maps are in the form a survey completed in 1723-24 the work of the surveyor Joseph Kendall. These do not show the area of the bridge (ref. HA 108/10/1). A second volume covering the years 1784-1862 does include a '*Survey of a Farm in the Occupation of Mr Thomas Wilsmore in the parishes of Stoke and Polstead ... belong to Sir William Rowley Bart., surveyed 1808 by William Cole*' (ref. HA 108/10/2/15). A later pencil annotation written on the map identifies this farm as '*Scotland Place*'. The location of the farm house is given as '*adjoining to the brook, at a place called Scotland Street*'. The name 'Scotland' also appears in the field names of the meadows adjoining the brook and in '*Scotland Barn*', some way to the south of the bridge and on the eastern side of the brook. There are no further surveys of this farm another volume of maps and plans covering the years 1802-1862 (ref. HA 108/10/5).

Amongst the various manors in Stoke by Nayland the manors of Netherhall and Withermarsh both became part of the Rowley estates. In a survey of '*the maners of Stokehall, Withermersh Netherhall & Sprotts with other purchased lands in Stoke, Polsted & Neylande ... sett downe the eleventh of John Anno 1580 by Radulph Agas*' (ref. HA 108:10515 Box19) there are references to Robert Ruckwoode esquire who held freely '*the manor called Scotland Haule*' at a yearly rent of just 5 shillings and the survey further notes '*The same Robert holdeth as before one medoe called Bridge Medoe payeth yearly*' 6d. The same survey also notes that another freeholder of the manor Richard Webb '*holdeth freely one tenement in Scotland Street beyond the Bridge lying by the way leading from the same Bridge to Hadleygh*' for which he paid 2d annual rent.

There are very few surveys or maps in the Mannock Estate Collection and none appear to relate to lands in this area.

### **Manor of Scotland Hall**

According to Copinger's account of the manor, Margaret, the widow of John de Scotland, who had been granted the manor by her son, married Robert de Rokewode and they held the manor jointly by 1357 (Copinger 1905). The manor was held by various members of the Rokewode family including the Robert 'Ruckewode' mentioned in the 1580 survey. This family like the Mannocks were Roman Catholics and suffered various penalties for their non adherence to the established church and for their involvement in various treasonable plots.

The manorial records for this manor provide additional references to the bridge. Amongst a series of fines imposed on the various tenants for failing to clear the brook listed at a court held on 18 April 1587, Robert Coe was fined 3d for his stretch of the brook from 'Scotland Bridge towards the meadow of Bartholomew Griffen' (ref. HA 246/A8/5). The bridge existed from a much earlier date. At a court held on Wednesday next after the feast of St Margaret the virgin (feast day 20 July) 1372 two of the manor's tenants Roger Kyng and Thomas Aldwyne were fined for allowing the livestock to graze 'Breggemedwe'. Roger Kyng paid a fine of 3d for allowing his pigs to graze the 'Breggemedwe' at another court held on the Thursday next after the same feast in 1362 (ref. HA 246/A8/1).

The meadow 'Breggemedwe' is mentioned the compotus or account rolls of Robert Arnold the reeve for this manor covering the year from Michaelmas 1386 to Michaelmas 1387. The meadow was rented to William de Trendyngge for a quarterly rent of 14d. Of greater importance, this roll mentions under the new rents the easement into several of the lord's pastures at 'Skotlodebregge' granted to John Enelde (ref. HA 246/A8/13). The earliest surviving record for this manor is a rental for 1304-1305 (ref. HA 246/A8/16) and it does not contain additional references to the bridge but confirms that the manor was known as Scotland Hall at that date.

Nearly all the early records of the manor are written in Latin.

## **Conclusion**

Scotland Street Bridge was rebuilt by the Lavenham Highway Board under the supervision of their surveyor John Henry Rinder in 1867. The records for this board are incomplete but the board's activities were reported at some length in the local newspapers. This board in common with other highway boards was established under the terms of the 1862 Highway Act. In order to gain funds for the rebuilding of the bridge the board borrowed money from the county Quarter Sessions and also requested that the bridge be adopted as a county bridge. Though the names of the local contractor and the supplier of the iron girders are known there appear to be no other surviving records relating to the construction of this bridge.

A bridge, known as Scotland Bridge near this location is mentioned in earlier records including the two deeds held at the Suffolk Record Office in Bury St Edmunds dated 1662 and 1695. It is again mentioned in a manorial survey dated 1580 in the Rowley Tendring Hall Collection held at Ipswich. The bridge is also mentioned in a manorial court roll dated 1587 part of the Mannock family collection also held at Ipswich and in earlier court records there are references to 'Skotlodebregge' from as early as 1387 and 'Breggemedwe' as early as 1362.

**Anthony M Breen June 2012**

## **References**

### **Suffolk Record Office Bury St Edmunds**

#### **Lavenham District Highway Board**

1445/7/1 Accounts 1864-1878

1445/7/2 Weekly Accounts for Account and estimates for 1868-1869, receipts from sale of road sand 1869-1870

1445/7/3 Lavenham Highway Board pass book' 1874-1879

1445/7/4 Copy of Advertisement for a District Surveyor, list of members of the Highway Board including ex-officio members and other papers 1864-1877

1445/7/5 Letter from the Waywarden of Brent Eliegh to Highway Board on the construction of a bridge over a ford in Cock Lane

1445/7/7 Reports of committees appointed to investigate various matters includes a report on 'The erection of two proposed bridges at Thorrington Street and Scotland Street at Stoke by Nayland'.

#### **Quarter Session Records**

Q/RI 34 Enclosure map and award Stoke by Nayland and other parishes 1817.

Q/SH 119 Lavenham Highway District 1863

#### **Parish Collection Lavenham**

FL 508/8/1 Printed Statement of accounts of highways in each parish in the Lavenham Highway Board District year ending 31 December 1869

Parish Collection Layham

FB 82/H1/1 F. Sexton, Surveyor and Estate Agent, 'Lavenham Highway District Survey of Roads' pub Knight & Co, London, 1864

#### **Parish Collection Stoke by Nayland**

FB 80/HI/5 Notice concerning the borrowing of £480 and interest by the Lavenham Highway Board 18 January 1867



## **Stoke By Nayland Deeds**

E3/10/88.2,3 Indenture 25 April 1662

E3/10/88.5 Release 23 October 1695

## **Suffolk Record Office Ipswich**

### **County Council Records**

B105/2/106 Quarter Sessions Order Book 11 July 1866 to 5 July 1867

2357/1 Photostat Copy F. Sexton, Surveyor and Estate Agent, 'Lavenham Highway District Survey of Roads' pub Knight & Co, London, 1864

### **Mannock Family Collection**

Manor of Scotland Hall

HA 246/A8/1 Court Rolls 1342-1377 (4 membranes)

HA 246/A8/5 Court Roll 18 April 1587

HA 246/A8/13 Compotus of Robert Arnold, reeve 29 September 1386-1387

HA 246/A8/16 Rental 1304

### **Rowley Tendring Hall Collection**

HA 108/10/1 Volume of maps of the Tendring Hall Estate surveyed by Joseph Kendall 1723-24

HA 108/10/2/15 Farm Occupied by Thomas Wilsmore in Stoke by Nayland and Polstead surveyed by Cole 1808

HA 108/10/5 Book of Plans and surveys of the Rowley Estates 1802-1862

HA 108/10/9 Enclosure Map Stoke by Nayland 1817

HA 108:10515 Box 19 'Survey of Stoke by Nayland estate in Stoke, Nayland and Polstead belonging to Sir Thomas Revett taken by Ralph Agas 11 June 1580'

## **Published Sources**

### **Statutes**

'A Collection of the Public and General Statutes passed in the Twenty-Fifth and Twenty-sixth Years of the reign of Her Majesty Queen Victoria', London 1862

### **Directories**

J.G. Harrod and Co's 'Postal and Commercial Directory of Suffolk' London 1864

### **Secondary Works**

William Copinger 'Manors of Suffolk Notes on their History and Devolution ... Hundreds of Babergh & Blackbourn' Volume I, Manchester 1905.

William O. Hart, CMG, BCL, MA 'Hart's Introduction to the Law of Local Government and Administration' sixth edition, London 1957

### **Online Sources**

British Library 19th Century Newspapers online 'Bury and Norwich Post' 6 June 1866

## Appendix 3. Suffolk HER search results for 'Cast Iron Bridge'

---

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
BCC 022 - MSF15331	Beccles Bridge	Monument
Iron built Beccles Bridge constructed in 'early C19' (or 1884?), replacing 3 arch masonry and earlier timber bridges.		<b>Period</b> PMed

### **Location**

TM 4208 9106

### **Monument Types and Dates**

BRIDGE (18th century to 20th century - 1701 AD to 1900 AD)  
Evidence DOCUMENTARY EVIDENCE

### **Description**

Late C18: Bridge shown on Hodskinson's 1783 map, precise location unclear (S1). Possibly same as Med bridge slightly to N - see Med.

Early C19: Iron built Beccles Bridge constructed next to the Hermitage Inn (see BCC 009 for Med bridge chapel). S of earlier bridge/s?

According to Suffolk CC (Environment & Transport) iron bridge was built in 1884, replacing a three arch masonry bridge

### **Sources**

- (M1) Unpublished document: Suffolk Archaeological Service. Parish file. Parish file: (S1)
- (S1) : Hodskinson J, Map, The County of Suffolk surveyed, 1783
- (S2) : Saxton C, SVFFOLKE, 1575
- (S3) : Speede J, Suffolke Described, 1610
- (S4) : Bowen E, An Accurate Map of the County of Suffolk, 1755
- (S5) Bibliographic reference: Email. Carter N (SCC E&T), to Colin Pendleton, 13 July 2006

**SMR Number** BTE 001 - **Site Name** Cast Iron Road Bridge, SE of Bridge Farm.

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
BTE 001 - MSF5824	Cast Iron Road Bridge, SE of Bridge Farm.	Monument
Cast iron road bridge, SE of Bridge Farm. - Scheduled Monument.		<b>Period</b> PMed

**Location**

TL 933 482

**Associated Legal Designations**

Scheduled Ancient Monument - SF145 CAST IRON ROAD BRIDGE SE OF BRIDGE FARM

**Monument Types and Dates**

BRIDGE (Post Medieval - 1540 AD to 1900 AD)  
Evidence STRUCTURE

**Description**

Cast iron road bridge, SE of Bridge Farm. Disused road bridge; single span cast iron, dated 1813. Brick abutments, the whole tied together with further cast iron linking the bases of the buttresses on the river bed. Five segmented ribs support the roadway, which is carried on bricks apparently laid on oak boards supported by the ribs. Modern rails on bridge (S1).

1991: Said to be falling, unmaintained and at risk. "County Surveyor is trying to off-load it" (S2).  
A bridge is shown at this location on Hodskinson's map of 1783 (S3) & Bowen's map of 1755 (S4). Construction date unknown.

Listed as one of 16 most important industrial archaeology sites in Suffolk (S5)(R2).

**Sources**

- (M1) : SAM file:
- (S1) : DOE scheduling information, 1970
- (R2) Bibliographic reference: Miscellaneous Bibliographic reference. Cossons N, The BP Book of Industrial Archaeology, 1975
- (S2) : Suff Pres Soc (Paul Edwards) survey, table of results, 1991
- (S3) : Hodskinson J, Map, The County of Suffolk surveyed, 1783
- (S4) : Bowen E, An Accurate Map of the County of Suffolk, 1755
- (S5) Serial: Suffolk Industrial Archaeological Society Newsletter. 35 September 1991

**SMR Number** BXF 022 - MSF24866 **Site Name** Church Bridge, Boxford

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
BXF 022 - MSF24866	Church Bridge, Boxford	Monument
Monitoring and building recording identified and recorded the stone-built support piers of a medieval bridge, largely superseded in the 17th and 19th century by a brick and ironwork bridge.		<b>Period</b> Med

**Location**

TL 9623 4052

**Monument Types and Dates**

BRIDGE (Medieval - 1066 AD to 1539 AD)

BRIDGE (17th century to 19th century - 1600 AD to 1899 AD)

Main Building BRICK

Material

Main Building IRON

Material

**Associated Events/Activities**

ESF20586 Church Bridge, Boxford, BXF 022 (Event - Survey. Ref: OASIS-suffolkc1-65134)

**Description**

An archaeological monitoring and building recording was carried out at Church Bridge, Boxford and identified and recorded the stone-built support piers of a medieval bridge, largely superseded in the 17th and 19th century by a brick and ironwork bridge (S1). See also BXF 012.

**Sources**

(S1) Unpublished document: Muldowney, M.. 2010. Archaeological Monitoring Report, Church Bridge, Boxford, BXF 022. Suffolk County Council Archaeological Service Report 2009/228

**SMR Number** CUL 022 - **Site Name** Culford Park; Culford School

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
CUL 022 - MSF11047	Culford Park; Culford School	Monument
Landscape Park Listed by English Heritage as of special interest. Terraced gardens, park, walled gardens etc. Earliest tubular iron bridge in the world (Listed Grade I). Area includes Scheduled barrow in Dixon's Covert.		<b>Period</b> PMed

### **Location**

TL 830 704

### **Associated Legal Designations**

Registered Park or Garden (II) - 3173	CULFORD PARK	Active	DSF210
Scheduled Ancient Monument - 31115	BOWL BARROW IN DIXON'S COVERT	Active	DSF15354

### **Monument Types and Dates**

PARK (18th century to 19th century - 1701 AD to 1800 AD)
Evidence DOCUMENTARY EVIDENCE
BRIDGE (19th century to 20th century - 1801 AD to 1900 AD)
Evidence DOCUMENTARY EVIDENCE

### **Description**

Landscape Park listed by English Heritage as of special interest:- Terraced gardens c. 2ha, pleasure grounds c. 2ha, park c. 200ha, walled gardens c. 4ha. Plan for Culford by Thomas Wright 1742 for Lord Cornwallis shows Avenue to W of house, moated garden to S (CUL 034) and lake (CUL 035, dug 1795) and canal to S (S2). Park from 1792 by Humphry Repton, terrace garden early and later C19. C19 ornamented & balustraded walls and lawns. Specimen trees, walk, shrubbery, fernery (outdoor), well wooded park. Serpentine Lake altered by Repton from existing river (note Wright's map above). Arched dam carries drive. Balustraded bridge, flanked by urns, was designed by Samuel Wyatt in 1800, finished in 1803, and is earliest tubular iron bridge in the world (Listed Grade I)(S3)(S4)(R6). Flanking tree belts. Remains of avenue/s. N wood and two 12 acre coverts form belt on N side. Early C19 park gates. Originally 3 walled gardens, two now cleared and converted to tennis courts & hockey pitches. Proposals by Repton for village. Red book 1792; listed in Repton's sketches 1794, further details in (S1)(R1)-(R7).  
Shown on Hodkinson's 1783 map, OS 1st edition and Tithe map (1839). For Hall see CUL 021.  
Boundary amended (still wrong - EM) after English Heritage map (S1) in October 1995.  
For ice-house see CUL 020.  
For photographs in Suffolk Industrial Archaeological Newsletter see (S6).  
For map of Park in 1778, see (S7).

### **Sources**

- (M1) Unpublished document: Suffolk Archaeological Service. Parish file. (S7)
- (R1) : Binney M & Hills A, Elysian Gardens, 1979
- (S1) : English Heritage, Register of Parks and Gardens of Special Historic Interest in England: Part 39 Suffolk
- (R2) : Carter G, Goode P & Laurie K, Humphry Repton, 1982
- (S2) : SRO (Bury), Wright T, Plan for Culford Park, 1742, E8/1/10
- (R3) : The Garden, 31 May 1873, 412
- (S3) : Paine C, 1997
- (R4) : Journ Horticult & Cottage Gardener, 25 Feb 1875, 164
- (S4) : Perrett D, 'Culford's "Lost" Bridges', SIAS Newsletter, 58, June 1997
- (R5) : Williamson Dr T, Survey of Historic Parks & Gardens in Suffolk
- (S6) : Industrial Archaeol News, 100, Spring 1997 & No 63, August 1998
- (S7) Map: Paterson Lt, Plan of Hengrave & Fornham Camps, 1778, PRO 15533
- (S8) Photograph: Air Photographs. NAU, TL 832 703, JDD 3 & 6, 1 July 1996

**SMR Number** EUN 013 - **Site Name** Rushford Bridge

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
EUN 013 - MSF10836	Rushford Bridge	Monument
Rushford (cast iron) Bridge, crossing Little Ouse river between Brettenham (Norfolk) and Euston parishes.		<b>Period</b> PMed

**Location**

TL 9250 8121

**Associated Legal Designations**

Scheduled Ancient Monument - NF85	RUSHFORD BRIDGE	Active	DSF15906
Scheduled Ancient Monument - SF242	RUSHFORD BRIDGE	Active	DSF16025

**Monument Types and Dates**

BRIDGE (19th century to 20th century - 1801 AD to 1900 AD)

**Description**

Rushford Bridge, crossing Little Ouse river between Brettenham (Norfolk) and Euston parishes. Mid C19, cast iron. Scheduled Ancient Monument, Norfolk 85/Suffolk 242. No further information (S1)(S3).

**Sources**

- (S1) : NAU (Rose E), November 1989
- (S2) : Jervoise E, The Ancient Bridges of Mid and Eastern England, 1932, 111- 112
- (S3) Unpublished document: English Heritage. Scheduling information, January 2003

**SMR Number** FNG 013 - **Site Name** Finborough Hall; Great Finborough Hall Park

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
FNG 013 - MSF14822	Finborough Hall; Great Finborough Hall Park	Monument
	Great Finborough Hall and associated park (R1).	<b>Period</b> PMed

### **Location**

Centroid TM 0175 5834

### **Monument Types and Dates**

GREAT HOUSE (Post Medieval - 1540 AD to 1900 AD)  
LANDSCAPE FEATURE (Post Medieval - 1540 AD to 1900 AD)  
PARK (Post Medieval - 1540 AD to 1900 AD)  
BRIDGE (18th century to 19th century - 1701 AD to 1800 AD)

### **Associated Finds**

FSF32800 MANUSCRIPT (Post Medieval - 1540 AD to 1900 AD)

### **Description**

Great Finborough Hall and associated park (R1). Park defined on HER map approximately as shown on Hodskinson's 1783 map (S1), including lake (? not shown on modern maps and bridge over lake. Park includes Folly on mound known as Devil's Hill (FNG 003).  
Finborough Hall: built (rebuilt) 1795, by F Sandys for Roger Pettiward. Front with a big bow window and a colonnade of six Tuscan columns running round it. Side with four giant Tuscan columns, the middle interstice being wider than the others. Far-projecting eaves (which is also 'Tuscan', i.e. Etruscan motif). Large staircase hall, the stairway with a wrought-iron balustrade (S2)(S3). Owned by Wm Wollaston Esq in 1783 (S1).

### **Sources**

- (R1) : Williamson Dr T, Survey of History Parks and Gardens in Suffolk, 1993  
(S1) : Hodskinson J, Map, The County of Suffolk surveyed, 1783  
(S2) Bibliographic reference: Pevsner N & Radcliffe E. 1974. The Buildings of England: Suffolk. Pevsner N & Radcliffe E, The Buildings of England: Suffolk, 1974 ed, 236  
(S3) : Dickinson P, The Little Guide: Suffolk, 6th ed, 1957, 154

---

**SMR Number** HAD 049 - **Site Name** Hadleigh Bridge

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
HAD 049 - MSF15444	Hadleigh Bridge	Monument
	Hadleigh Bridge, over River Brett.	<b>Period</b> PMed

### **Location**

TM 0234 4287

### **Monument Types and Dates**

BRIDGE (18th century to 19th century - 1701 AD to 1800 AD)

### **Description**

Hadleigh Bridge, over River Brett. Shown on Hodskinson's map 1783 (S1) and on Bowen's 1755 map (S2). Listed Grade II. Construction date unknown.  
Built in replacement of a cast iron bridge from 1843. A span of the 1843 bridge has been re-erected on dry land adjacent to the new structure (S3).

### **Sources**

- (S1) : Hodskinson J, The County of Suffolk surveyed, 1783  
(S2) : Bowen E, Map of the County of Suffolk, 1755  
(S3) Serial: Suffolk Industrial Archaeological Society Newsletter. No 63, August 1998



**SMR Number** HGM 015 - **Site Name** Higham Bridge

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
HGM 015 - MSF15845	Higham Bridge	Monument
Bridge over River Brett shown on Bowen's 1755 and Hodskinson's 1783 maps (S1)(S2).		<b>Period</b> PMed

**Location**

TM 0314 3545

**Monument Types and Dates**

BRIDGE (18th century to 19th century - 1701 AD to 1800 AD)

**Description**

Bridge over River Brett shown on Bowen's 1755 and Hodskinson's 1783 maps (S1)(S2).

1697: Sums of 60 and 150 pounds paid 'for building Higham Bridge - being a County Bridge and rebuilt by order of this court' (S3). The exact site of the pre 1697 bridge is unknown.

1846: The Justices had considered 'pulling down & rebuilding a certain bridge called Higham' in January 1846. Plans submitted at Bury in March by Mr John Whiting. The tender granted to Wm Parkes Ribbons of Ipswich to rebuild the bridge for 290 pounds, using 'materials arising from the old bridge'. It continues, 'The present bridge to be taken down, all the foundations to be taken up and the materials properly cleaned and packed .... the old materials of the present bridge may be used'. The document contains a host of other detail (S4).

In 1880 described as 'Arched iron girder bridge, of 29 feet 3 inches waterway, with parapets of iron standards and rails, & brick piers at ends, and brick wings. Roadway 19 feet .... Rebuilt 1872' (S5). 1996: No trace of pre 1846 bridge survive

**Sources**

- (M1) Unpublished document: Suffolk Archaeological Service. Parish file. Parish file: (S6)a
- (S1) : Hodskinson J, Map, The County of Suffolk surveyed, 1783
- (S2) : Bowen E, Map of the County of Suffolk, 1755
- (S3) : Ipswich Quarter Sessions meeting at Ipswich, 14 January 1697
- (S4) : SCC, Contract for works at Higham Bridge, 9 June 1846, SRO (Ipswich)
- (S5) : County of Suffolk: List of County Bridges in the E Division, 1880
- (S6) : SAU, Newman J, site report, April 1996, 2 slides

**SMR Number** SBN 072 - **Site Name** Scotland Street Bridge

<b>SMR Number</b>	<b>Site Name</b>	<b>Record Type</b>
SBN 072 - MSF15822	Scotland Street Bridge	Monument
Bridge shown on Hodskinson's 1783 map (S1).		<b>Period</b> PMed

**Location**

TL 9962 3670

**Monument Types and Dates**

BRIDGE (18th century to 19th century - 1701 AD to 1800 AD)

**Description**

Narrow bridge shown on Hodskinson's 1783 map (S1). Appears to be shown as a ford on Bowen's 1755 map. Not shown on OS 1st ed map of 1838.

April 2012: existing deck formed by cast iron girder inscribed 'J H Rinden Surveyor' plus the name of the fabricator - 'E R & F Turner (of Ipswich). The balustrading is of refined iron section and may be contemporary (S2). ER&F Turner's foundry was established in 1837 so the cast iron element of the bridge probably dates to the mid 19th century roughly (further documentation could be checked for exact dating) suggesting the 18th century (& earlier?) structure was replaced (extent of survival unknown).

**Sources**

- (S1) : Hodskinson J, Map, The County of Suffolk surveyed, 1783
- (S2) Bibliographic reference: Email. Barnard M (SCC) to Pendleton C (SCCAS), 13 April 2012





# Archaeological services Field Projects Team

**Delivering a full range of archaeological services**

- Desk-based assessments and advice
- Site investigation
- Outreach and educational resources
- Historic Building Recording
- Environmental processing
- Finds analysis and photography
- Graphics design and illustration

**Contact:**

**Rhodri Gardner**

**Tel: 01473 581743 Fax: 01473 288221**

**[rhodri.gardner@suffolk.gov.uk](mailto:rhodri.gardner@suffolk.gov.uk)**

**[www.suffolk.gov.uk/Environment/Archaeology/](http://www.suffolk.gov.uk/Environment/Archaeology/)**