

CWRR19



CLYDE WATERFRONT AND RENFREWSHIRE RIVERSIDE

HISTORIC BUILDING RECORDING

PLANNING REF. 17/0486/PP

commissioned by Renfrewshire Council

May 2019

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PROJECT INFO:

HA Project Code **CWRR19** / NGR **NS 5044 6856** / Parish **Blythswood** / Local Authority **Renfrewshire Council** / OASIS Ref. **headland1-349863** / Archive Repository **Historic Environment Scotland**

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PROJECT SUMMARY

Headland Archaeology (UK) Ltd was commissioned to undertake a Level 3 historic building survey prior to the redevelopment of the area at Clyde Waterfront, Renfrewshire. The property, a fabrication shed, formerly formed part of the Shipbuilding facility operated by Simons-Lobnitz Ltd. The original building was constructed c 1854 although this seems to have been demolished prior to remodelling of the buildings prior to 1895. The extant building comprises one large building separated into two very large roofed sheds. The two sheds incorporated fabric from four phases of construction relating to the 19th and 20th century expansion.

The interior of the eastern shed comprised of three long bays separated by brick walls and rows of steel beams. The east end of the central bay provided evidence of the initial expansion phase of the fabrication shed. The bays to the north and south sides of the building had been added later but were constructed to a very similar design with tall round headed doors and windows. The interior still included several overhead cranes supported on large horizontal steel beams. Several later modifications had also been made to the interior of the east shed. The west shed was still in use as part of Christies scrap metal yard although much of the layout and steel roof support beams were of 19th century design.

CONTENTS

1	INTRODUCTION	1
2	SITE LOCATION AND DESCRIPTION	1
3	ARCHAEOLOGICAL BACKGROUND	1
4	HISTORICAL OVERVIEW	1
5	AIMS AND OBJECTIVES	3
6	METHOD	3
	6.1 REPORTING AND ARCHIVES	4
7	RESULTS	4
	7.1 HBR SURVEY INTRODUCTION	4
	7.2 EXTERNAL EAST END (ILLUS 8)	4
	7.3 EXTERNAL NORTH ELEVATION (ILLUS 13)	10
	7.4 EXTERNAL WEST ELEVATION)	10
	7.5 EXTERNAL SOUTH ELEVATION)	10
	7.6 INTERNAL SHED 1 (ILLUS 21)	10
	7.7 INTERNAL SHED 2	13
8	DISCUSSION	13
9	CONCLUSION	16
10	REFERENCES	16
11	APPENDICES	26
	APPENDIX 1 PHOTOGRAPHIC REGISTER	26
	APPENDIX 2 DISCOVERY AND EXCAVATION IN SCOTLAND REPORT	35

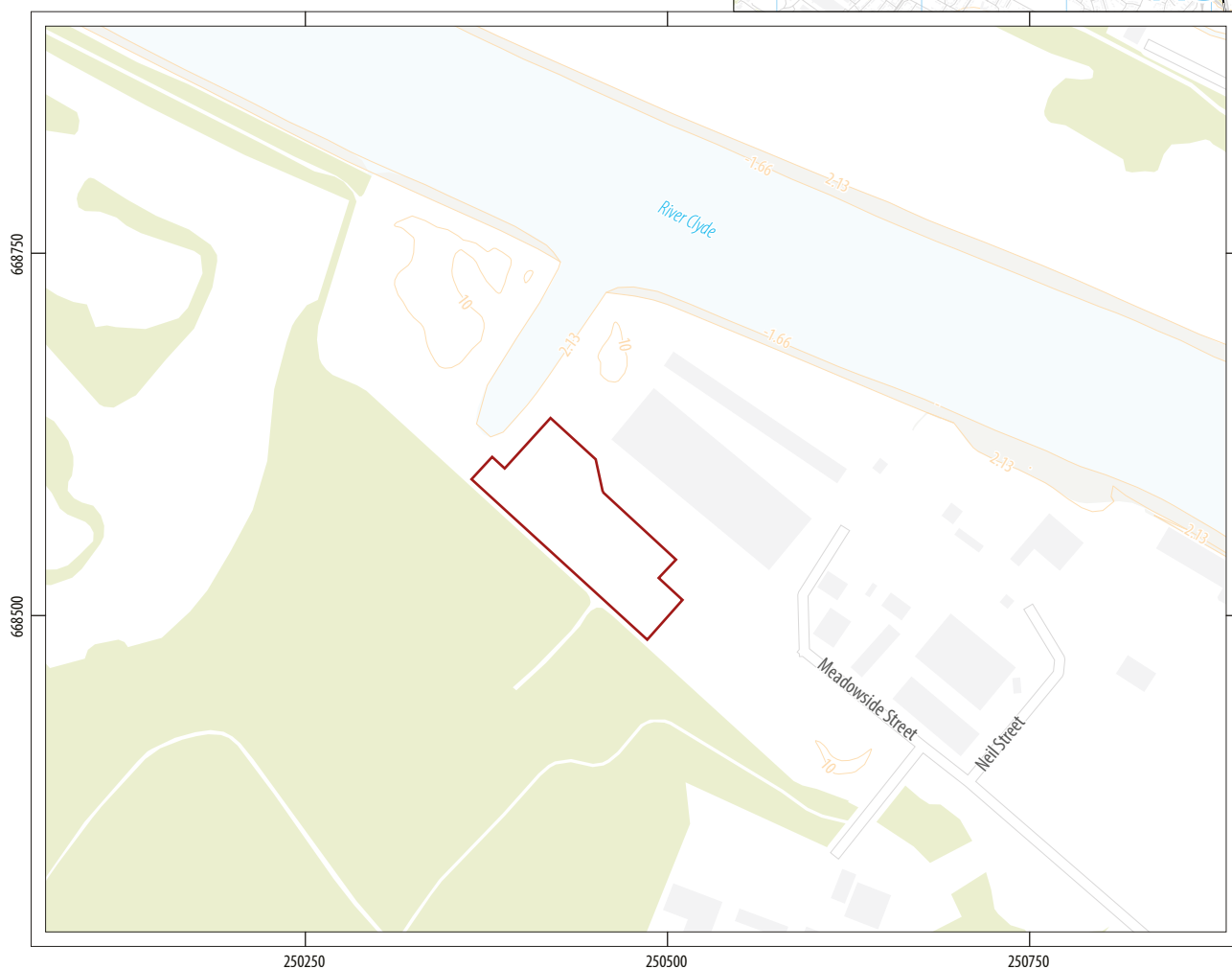
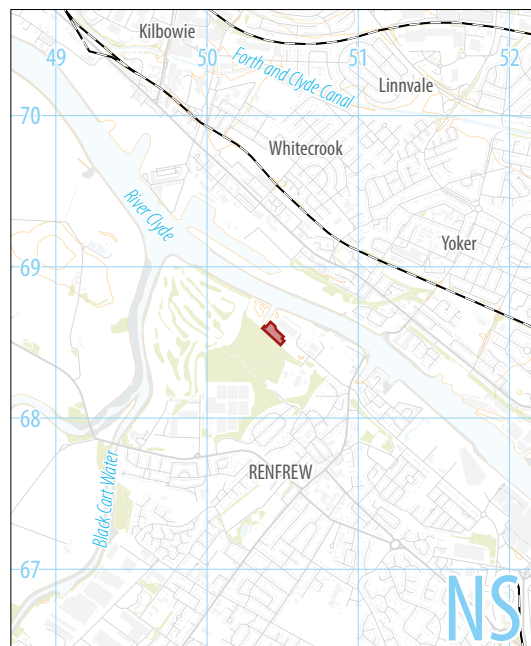
LIST OF ILLUSTRATIONS

ILLUS 1 SITE LOCATION	VIII
ILLUS 2 EXTRACT FROM 1857 OS	2
ILLUS 3 EXTRACT FROM 1897 OS	2
ILLUS 4 DETAILED PLAN OF SITE	5
ILLUS 5 PHOTOGRAMMETRIC PLAN OF THE BUILDING FACING SOUTH-WEST	7
ILLUS 6 VIEW OF THE CHANGES IN HEIGHT OF THE ROOF	9
ILLUS 7 DETAIL OF THE DESIGN OF THE ARCHED OPENINGS	9
ILLUS 8 PLAN OF EAST ELEVATION	11
ILLUS 9 VIEW OF THE EAST ELEVATION	14
ILLUS 10 DETAIL OF THE EAST ELEVATION OF THE CENTRAL BAY	14
ILLUS 11 VIEW OF THE NORTH SIDE OF THE CENTRAL BAY	15
ILLUS 12 THE EAST ELEVATION OF THE NORTH BAY	15
ILLUS 13 PLAN OF NORTH ELEVATION	17
ILLUS 14 GENERAL VIEW OF THE NORTH ELEVATION OF SHED 1 SHOWING EXTENSION 1	19
ILLUS 15 DETAIL OF EXTENTION 2	19
ILLUS 16 VIEW OF THE NORTH FACING ELEVATION OF SHED 2	19
ILLUS 17 GENERAL VIEW OF THE WEST ELEVATION	19
ILLUS 18 DETAIL OF EXTENSION 3	19
ILLUS 19 OBLIQUE VIEW OF THE SOUTH ELEVATION	20
ILLUS 20 DETAIL OF THE SOUTH ELEVATION BAY	20
ILLUS 21 NORTH/SOUTH SECTION THROUGH THE BUILDING	21
ILLUS 22 GENERAL VIEW OF THE SOUTH BAY OF SHED 1	23
ILLUS 23 DETAIL OF THE OVERHEAD CRANE IN THE SOUTH BAY	23
ILLUS 24 THE NORTH WALL OF THE SOUTH BAY	23
ILLUS 25 EAST FACING VIEW OF THE CENTRAL BAY OF SHED 1	24
ILLUS 26 SOUTH WALL OF THE CENTRAL BAY	24
ILLUS 27 GENERAL VIEW OF THE EAST WALL OF THE CENTRAL BAY	24
ILLUS 28 VIEW OF THE STEEL COLUMNS AT THE WEST END OF THE CENTRAL BAY	24
ILLUS 29 DETAIL OF THE STEEL PLATFORM AT THE WEST END OF THE CENTRAL BAY	24
ILLUS 30 EAST FACING VIEW OF THE NORTH BAY OF SHED 1	25

ILLUS 31 GENERAL INTERNAL VIEW OF SHED 2	25
ILLUS 32 VIEW OF THE SOUTH WALL OF SHED 2	25
ILLUS 33 DETAIL FO THE OVERHEAD CRANE IN SHED 2	25
ILLUS 34 DETAIL OF THE FIREPLACE ON THE EAST WALL OF SHED 2	25

Clyde Waterfront
Renfrew Riverside
Glasgow

0 200km
1:12,500,000 @ A4



0 100m
1:5,000 @ A4

development boundary

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ILLUS 1 Site location

CLYDE WATERFRONT AND RENFREWSHIRE RIVERSIDE

HISTORIC BUILDING RECORDING

1 INTRODUCTION

Headland Archaeology (UK) Ltd was commissioned to undertake a Level 3 historic building record (Historic England 2016) of a shipbuilding fabrication shed as part of the Clyde Waterfront and Renfrewshire Riverside (CWRR) project, aimed at delivering infrastructure and environmental improvements in order to facilitate the development of vacant land, improving opportunities for new and existing businesses to grow through improved access to suppliers, customers and employees.

The work was carried out in accordance with a Written Scheme of Investigation prepared by Headland Archaeology (UK) Ltd (2019) and approved by West of Scotland Archaeology Service (WoSAS). Fieldwork was undertaken in April 2019.

2 SITE LOCATION AND DESCRIPTION

The proposed development site (Illus 1) is located south of the River Clyde at Meadowside Industrial Estate, Meadowside Street, Blythswood, Renfrewshire (Centred on NGR: NS 5044 6856). At present the site is in use as a metal recycling facility and includes the large 19th century fabrication shed and several attached extensions, the majority of which are 20th century in origin. The programme of historic building recording is centred on the brick-built fabrication shed.

The building subject to historic building recording was bounded to the south by an area of woodland and to the north by a modern industrial unit. A deep-water dock was located to the west side of

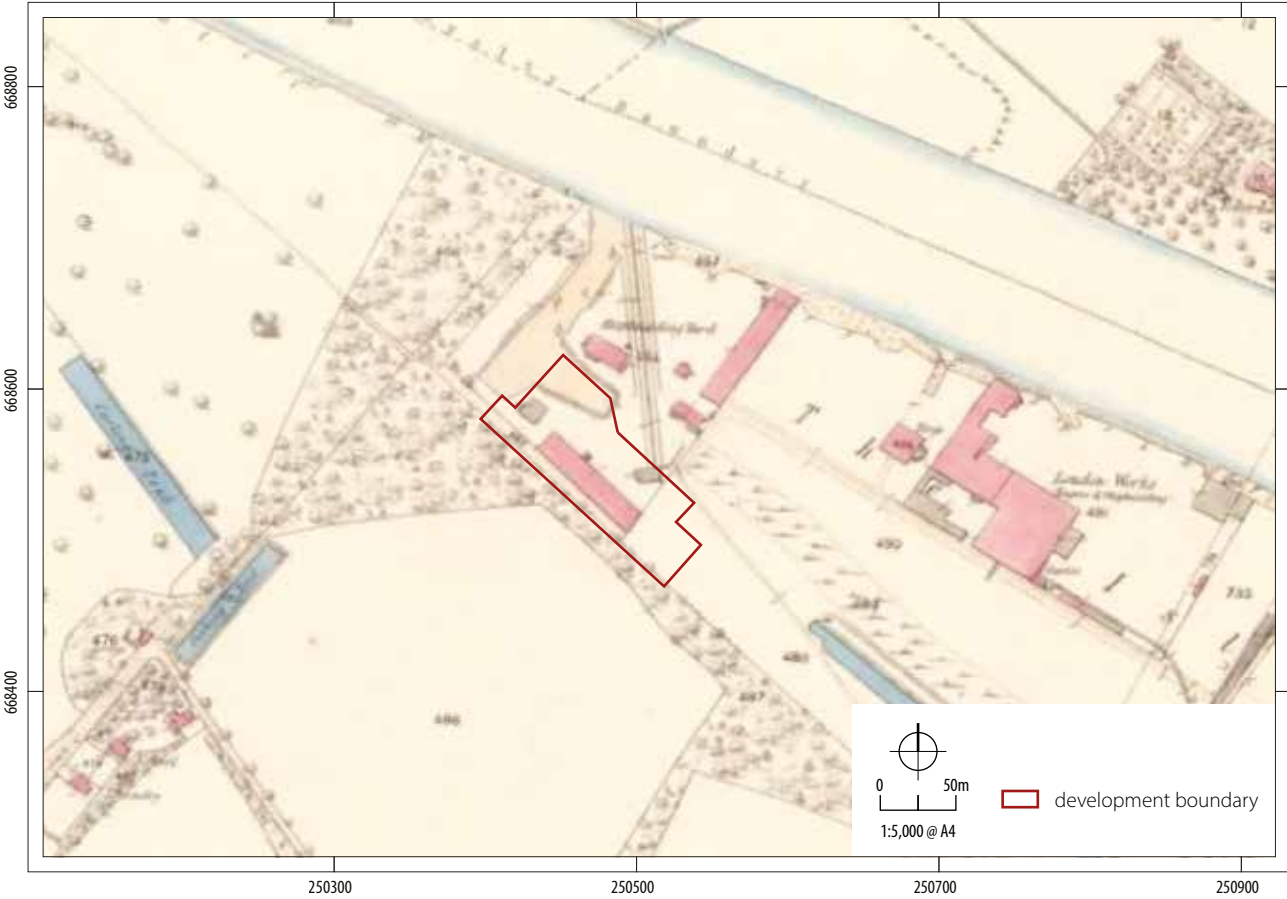
the building and an area of open ground to the east, which has concrete slab surfacing was in use as a preparatory area for large metal items before recycling.

3 ARCHAEOLOGICAL BACKGROUND

A comprehensive Desk Based Assessment was produced by Headland Archaeology as part of the Environmental Statement for the CWRR infrastructure works (Appendix 7.1 to ES Volume 2). This provided a detailed assessment of the known archaeology and built heritage of the wider study area. This identified that although the building is not listed it does have an entry in the National Record for the Historical Environment (HER 42718; CANMORE 202280). The HER states that the building was part of the 'London Works', an engineering and shipbuilding works that, from the mid-20th century, was operated by Simons-Lobnitz Ltd.

4 HISTORICAL OVERVIEW

The River Clyde has been a centre for shipbuilding for hundreds of years, with boats being built in the area possibly as early as the 15th century (clydewaterfront.com). However, it was during the 19th century that shipbuilding became a real source of commerce for Glasgow. With the advent of the steam engine during this period, the opportunities for Glasgow to expand its heavy industry were massive. This was also a period of substantial change in the shipbuilding industry as traditional timber ships were now being replaced with iron-built ships. Accordingly, by 1876 Glasgow was



ILLUS 2 Extract from 1857 OS ILLUS 3 Extract from 1897 OS

building more iron ships than the rest of the world combined (Jones 2010). It is estimated that over 22,000 vessels were built on the Clyde over the past 200 years (ibid).

The first edition Ordnance Survey (surveyed 1857) (Illus 2) depicts two of these early shipbuilding yards at Renfrew, both to the south side of the River Clyde. The eastern yard on this map is named as the 'London Works'. This comprised a series of large interlinked buildings and included a separate forge to the west side of the site. The second yard to the west is not named although may also have been known as the London Works. The west yard comprised two large sheds, several smaller buildings, a wet dock and a slipway.

By the mid-1800's, it was known that several companies were building ships and engines in the area. James Henderson and Son (Est 1847) at the west yard was one of the earliest known boat builders here. Henry Lobnitz joined James Henderson in 1857, which by 1866 had become Henderson, Coulborn and Co (online at archivehub, a). By 1874 Lobnitz had taken charge of the company which was now named Lobnitz, Coulborn and Co. He later strengthened the management of the firm in 1890 by assuming as partners his son Fredrick and William A Young. He subsequently converted the partnership into a private limited company, Lobnitz and Co Ltd in 1895.

The east yard at London road was run by JW Hoby. They specialised in manufacturing tugs and dredgers between 1850 and 1854. One of Hoby's partners was Fox, Henderson and Co. who ran a foundry at the London Works (online at archivehub, b). This company also had iron works operations in Birmingham, also named the London Works. Hoby and Co were bankrupt by 1854 and the company was eventually taken over by Joseph Russell, a former apprentice with Hoby's. Joseph Russell was born in London and this may be where the London Road name is derived. Russell later became the first builder to introduce mass and batch production (Walker 2010). He eventually left Renfrew in 1859 to set up a yard at Ardrossan and later Port Glasgow. It seems the yard was then taken over in 1860 by William Simons, who had previously been based at another yard at Whiteinch, Glasgow. Over the first ten years at Renfrew Simons built passenger vessels, cargo vessels and river steamers but eventually specialised in building dredgers, barges and tugs. When William Simons retired in 1886 Andrew Brown took over the firm and in 1895 with the help of his sons, they established the private limited company of William Simons and Co. Ltd. They continued to manufacture dredging machinery and hopper barges at the site with orders across the globe for their products.

The 1897 Ordnance Survey (Illus 3) seems to depict the two yards as a single entity with several large buildings, trackways and associated buildings spread across the site. This included the infilling of part of the wet dock and removing the slipway. This may have been a result of a change in construction techniques or a change in the styles of ships being built here by the turn of the century. At this time the two companies of Simons (east yard) and Lobnitz (west yard) were still separate entities. This map also shows that a further shipbuilding yard belonging to J and G Thompson had been established (1871) to the north side of the Clyde at this time. It was not apparent from this map if the original shed to the south of the site, as depicted on the 1857 Ordnance Survey, had been demolished or incorporated into the larger shed now depicted as occupying the area. Over the

following 100 years further alterations and buildings were added piecemeal to the site, although from the end of the 19th century onwards the main plan of the site remained static.

William Simons and Co. was eventually sold to the Weir Group in 1957. Two years later the group also bought out Lobnitz and Co. forming Simons-Lobnitz Ltd in 1959. The shipbuilders at Renfrew traded until 1964. By this time the site had produced over 1300 dredgers along with numerous barges and tugs (online source at lobnitz.com). The final launch from Renfrew took place on 27 December 1963 when Hopper Barge No. 3 was put into the water under the supervision of Alexander Stephen & Sons Ltd. All activity ceased following the trials of that vessel and her departure on delivery to Calcutta, India, at the close of April 1964. The assets of Simons-Lobnitz were acquired by Alexander Stephen and Sons Ltd in 1968 and the company's shipbuilding assets were transferred to Upper Clyde Shipbuilders Ltd (lobnitz.com). The site was left abandoned until it was transformed into Christies Scrap Merchants in the 1980's. The company was able to make use of the dock for loading ships with scrap metal for recycling.

5 AIMS AND OBJECTIVES

The objective of the programme of Historic Building Recording (Level 3) (Historic England 2016) was to provide a record of the structure in its present state prior to any demolition or alteration. This will provide an informed report that details the potential for further work or provide an adequate record of the structure in its present surroundings.

The resulting archive will be organised and deposited with National Record of the Historic Environment (NHRE) to facilitate access for future research and interpretation for public benefit.

6 METHOD

The building survey was carried in line with the requirements of a Level 3 record (Historic England 2016) This survey included a:

- › programme of desk-based research to place the development site in its historic and archaeological context. This consisted of map regression based on the Ordnance Survey maps and the title map(s) and apportionments. An examination was also made of records and aerial photographs available from WoSAS and HES;
- › compilation of drawings showing the building layouts, elevations and cross-sections using a total station and a drone survey (to produce a photogrammetric 3d model);
- › compilation of adequate photographic record of the historic building. This includes photographs illustrating the principal buildings, architectural features discovered, in detail and in context. All photographs of archaeological detail feature an appropriately-sized scale;
- › general photographic views of the structures in context;

- › written account providing the date the record was made and the name of the recorders and a summary of the structure's form, materials and historical development.

The recording followed standards and guidance for the archaeological investigation and recording of standing buildings or structures set out by the Chartered Institute for Archaeologists (CIfA 2014a). A full photographic record and detailed descriptions can be found in Appendix 1.

6.1 REPORTING AND ARCHIVES

The results of the works are presented below. A summary report has been prepared for submission to Discovery & Excavation in Scotland (Appendix 2) and the OASIS database (headland1-349863).

The complete project archive will be deposited with Historic Environment Scotland within six months of the completion of the project. The records (paper and digital) will be archived according to best practice guidelines set out by the Archaeological Archiving Forum (2011).

7 RESULTS

7.1 HBR SURVEY INTRODUCTION

The survey work was undertaken in April 2019. The building that formed the focus of the historic building recording was in a moderate to poor condition. It was a large sub-rectangular brick constructed building forming the only remaining part of the 19th century shipbuilding works in the area. The overall alignment of the building was south-east/north-west, and the building was approximately 170m long and 60m wide. For brevity only cardinal points will be used elsewhere in the report, with the north-eastern elevation as north, this being the long axis of the building closest to the river. The building was divided into two large sheds and three attached extensions. The sheds were recorded as Shed 1 (the eastern shed) and Shed 2 (the western shed) and the extensions were recorded as Extension 1, 2 and 3 (Illus 4).

Although the building presented four main elevations, the east side of the building was uneven with the central part of the building projecting further east than the southern part. The north elevation of Shed 1 included two later extensions and the east side of the north elevation of Shed 2 was set at an angle. Therefore, each individual elevation was recorded separately (Elevation A to O) (as shown on Illus 4 and 5). The building had a saw-tooth roof presenting multiple gables to the north and south elevations.

Much of the building presented a series of repetitive architectural features, typical of 19th century design, making it appear as if most of the building presented a single phase of construction. In fact, the building had been added to several times with multiple phases of construction recorded. This was most clearly represented by extensions to the north and west and varying roof heights as seen

on Illus 5. The building also included several 20th century additions including two extensions to the north side of the building.

In general, the 19th century constructed elements of the building were of red brick in an English Garden Bond with a cement render. All the original window and door openings were round-headed with pale yellow brick voussoirs making the upper part of the openings more prominent. The large round-headed entrances included four rows of brick work across the arches with two bands of pale yellow bricks and two bands of red brick (Illus 7).

7.2 EXTERNAL EAST END (ILLUS 8)

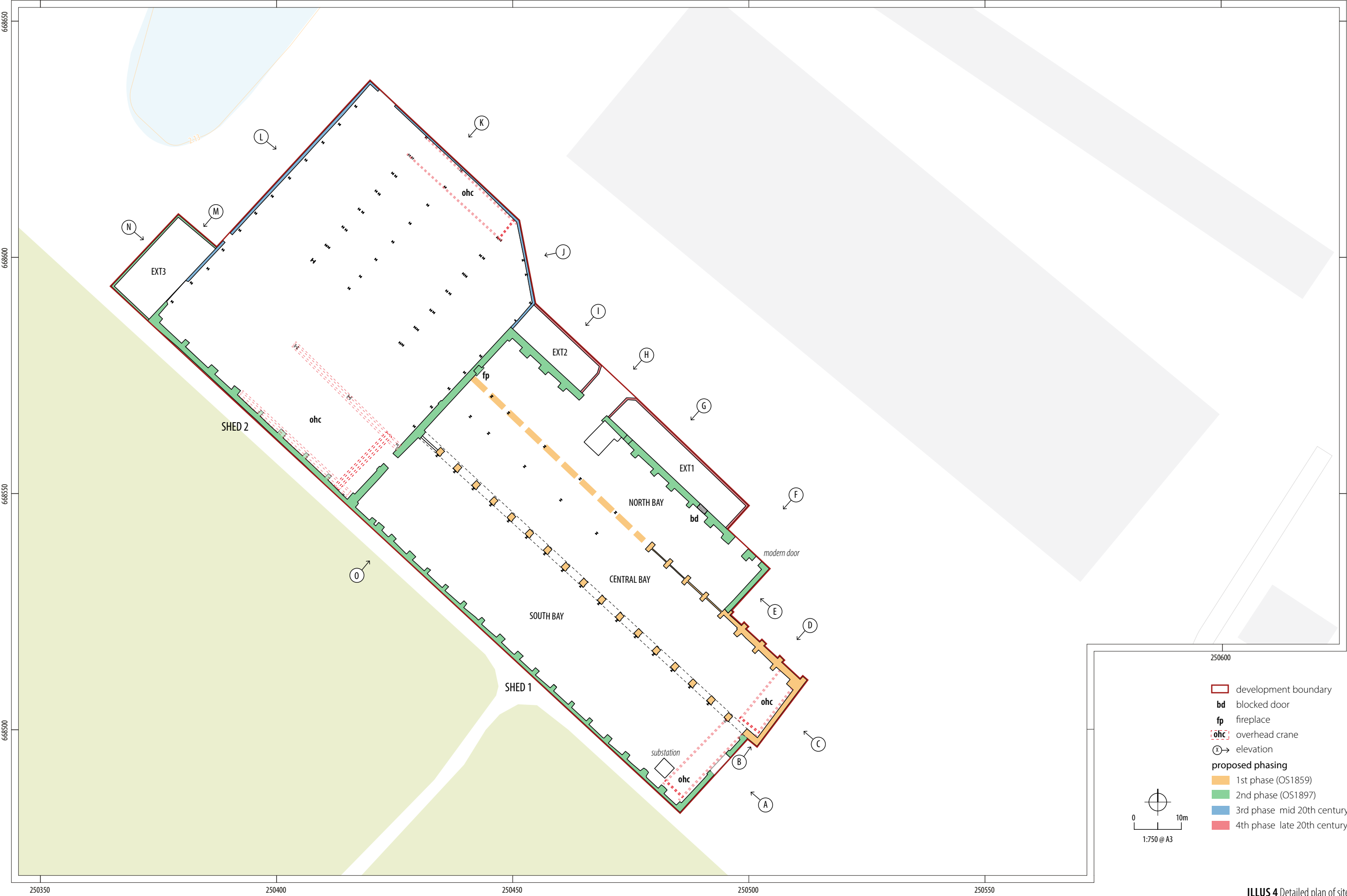
The east end of the building presented the most interesting face of the structure due to the projection of the central part of the wall and the recessed wall to the north side which altogether offered three separate bays to the elevation. The stepped plan of the elevation required the recording of five separate faces of wall (Elevation A – E). To the south side was Elevation A which included a large round-headed entrance and three long round-headed windows (Illus 9). The door included a modern steel roller door and the three windows had been brick-blocked. The north side of this wall abutted the south-facing elevation (Elevation B) of the central bay. Only a narrow width of this elevation was visible with no features identified although the roof line was higher than the north and south bays and a small round-headed window was just visible in the south facing gable.

The central bay (Elevation C) was symmetrical displaying three identical rows of five round-headed windows. The lower two rows had all been brick-blocked with only the upper row of windows still incorporating their casement frames (Illus 10).

The north side of the central bay (Elevation D) presented the most complicated elevation of the whole building due to the insertion and subsequent blocking of several openings within the original blocked openings (Illus 11). This was also the only external elevation to include brick buttresses, with five visible. The elevation included two gable ends each with a small round-headed window inserted. Below this were two rows of eight windows to the top and middle of the wall and six at the base along with a large round-headed entrance. All the openings had been brick-blocked with later square windows inserted into the middle and lower blocked windows to the west side of the elevation and two round-headed windows and a square window inserted into the entrance. These had also all been brick-blocked. A construction line high up on this elevation indicated that the external area to the north of this elevation had once been roofed although this would not have been part of the original construction.

The final bay to the north side of the east elevation (Elevation E) Included two brick-blocked doors at ground floor level and a single square opening higher on the wall (Illus 12). The size of the opening may suggest it was a door (possible leading to an external staircase or extant floor). The wall also included two rows of small brick-blocked recesses (one set being just below the upper window/door). These are likely to have been inserted when internal flooring was inserted (see below).

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ILLUS 4 Detailed plan of site



ILLUS 5 Photogrammetric plan of the building facing south-west



ILLUS 6 View of the changes in height of the roof **ILLUS 7** Detail of the design of the arched openings

7.3 EXTERNAL NORTH ELEVATION (ILLUS 13)

The north elevation included the elevations of Shed 1 (elevation F and H) and Shed 2 (elevation J and K) plus two later extensions (elevations G and I) to the front of Shed 1. The elevation of Shed 1 (Illus 14) comprised seven bays, each with an identical gable end incorporating a small round-headed window and brick voussoir. Below the gables four of the bays included sets of four round-headed windows. The bay at the east end had no windows and the following bay along with the bay at the west end of the elevation included three windows. The two extensions masked much of the lower half of the remainder of the elevation. Where the full extent of the elevation was visible it exposed two of the tall round-headed entrances. The entrance close to the west end included a cobble floor and rail tracks leading into the building (see Illus 6).

Both extensions to the north side of the elevation were constructed of brick over two floors and represent 20th century additions to the fabrication shed. Extension 1 to the east masked 3 ½ bays of the earlier building. It included casement windows to ground and first floor levels with small entrances at both ends and a flat roof. The corners at each end of the extension were angled and it included a series of slightly projecting brick buttresses.

Extension 2 to the west masked the final two bays of the main elevation. This extension had been altered and partially covered in a cement render (Illus 15). Again, it was over two floors and had a flat roof. Markings on the north-facing wall indicated that it originally included an outer staircase. This led to a door on the upper floor that had been brick-blocked and subsequently partly turned into a large window. Three large entrances on the ground floor had also been brick-blocked.

The north elevation of Shed 2 comprised of two components (elevation J and K) with the eastern half (J) set at an angle facing north-east and the remainder facing north, although they were constructed of the same materials which also continued across the west elevation (Illus 16). The elevation comprised a brick wall base (2.5m high) with a corrugated steel upper wall. The brick wall included three large doors and three windows. This elevation along with the west elevation (L) were the most recently constructed and date to the 20th century.

7.4 EXTERNAL WEST ELEVATION)

The west elevation of Shed 2 was constructed in the same materials as the north elevation of Shed 2 with a brick wall base and corrugated steel upper wall (Illus 17). A series of casement windows were positioned at the top of the brick wall with a further continuous long window at the top of the corrugated wall. A single large entrance was located close to the south side. At the south end of the elevation was the third extension. This was a small two storey brick-built office (Illus 18) with a double gable end facing west (Elevation N). The building had been altered with a brick-blocked double door on the north face, new windows inserted and an upper floor office added to the north-west corner. This building was probably originally part of the shipbuilding yard but had been modified for use as offices for the scrap yard.

7.5 EXTERNAL SOUTH ELEVATION)

Access to the rear of the building (Elevation N) was limited due to the proximity of the woodland to this side of the building. A public track runs between the building and the woodland, although the trees limited the position from which a photographic record could be made (Illus 19). The elevation comprised 13 tall gabled bays. Each bay included four tall round-headed windows that had been brock-blocked (Illus 20). These bays were identical apart from the final bay at the west end. This was wider and taller with flat squared ends at the base of the gable. The similarity of the windows across all 13 bays indicate that this elevation was part of a single phase of construction. The only additional features were several tie-bar ends that had been inserted to support the wall.

7.6 INTERNAL SHED 1 (ILLUS 21)

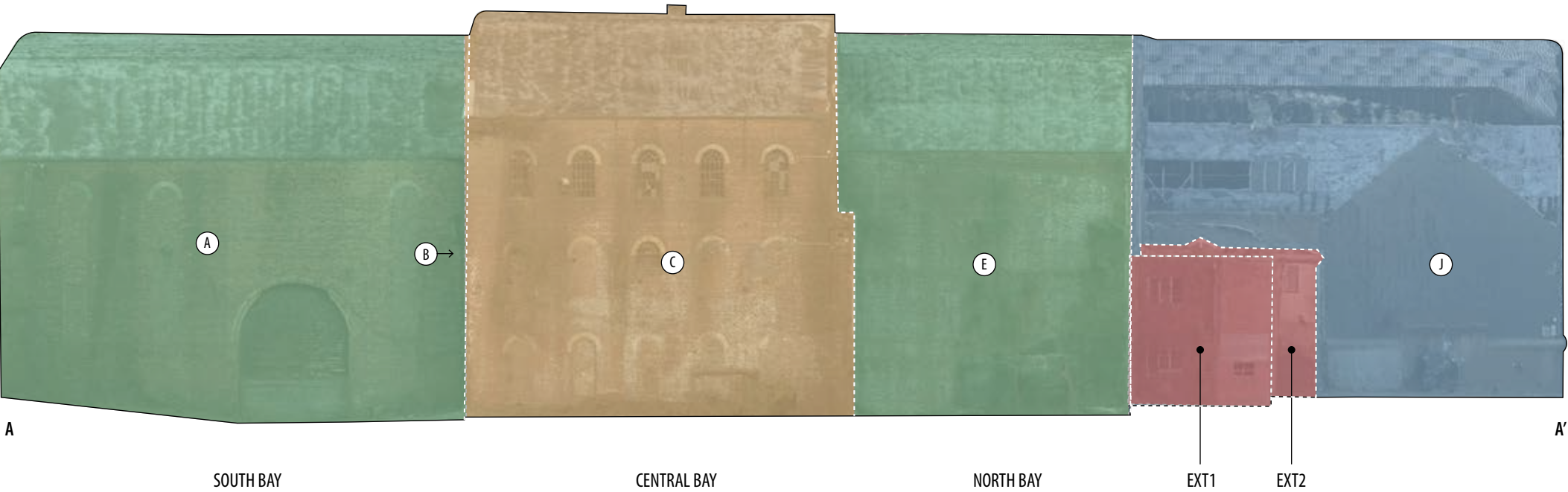
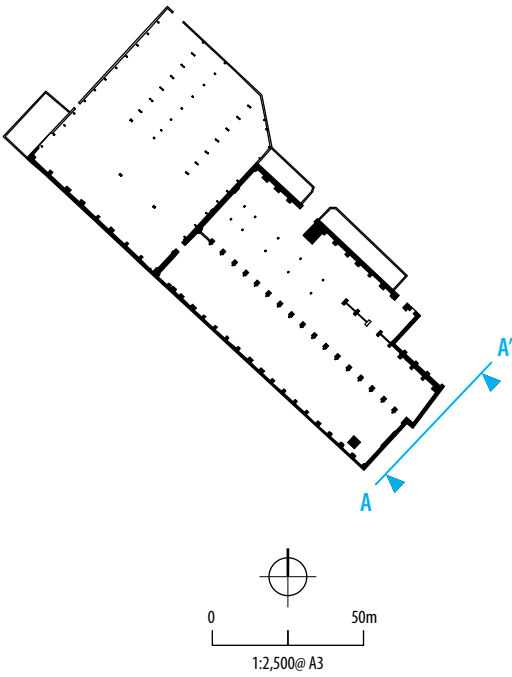
The internal space of Shed 1 was divided into three long east/west aligned bays by a mixture of brick walls, brick pillars and steel roof support columns. The building had a concrete floor with two sets of north/south aligned rails running through. A modern WC had been built in the north bay and an electricity substation was located in the south bay.

South bay

The south bay (Illus 22) was formed of the south and east external walls of the building along with a series of 17 brick pillars supporting an upper wall to the north side and a brick partition wall to the west. The south wall included 17 internal buttresses 10.5m high that supported a long steel beam. The beam was mirrored on the north internal wall although on this side it was supported on a series of steel columns. This indicated that the two walls were not contemporary. The beams carried an overhead crane that was still present. This crane had a name plate indicating it had been supplied by 'Royce Ltd of Manchester' and was capable of holding '25 tons' (Illus 23). The north wall was a continuous wall along the whole length of the shed (Illus 24). It comprised nine gabled bays which, although very similar in design, displayed several differences. The first four bays at the east end included two large openings at the base with steel lintels. Above the openings were two rows of four round-headed windows and within each gable was a further small round-headed window. The remaining five bays to the west were lower and only included a single set of four windows. In addition to this the lintels of three of the openings at the base of the wall were raised higher. Two of these raised openings aligned with entrances on the north side of the building. The west wall formed the partition wall between buildings 1 and 2. It was brick built with a single large brick-blocked door. The east wall mirrored what was recorded on the east external elevation.

Central bay

The walls of the central bay of Shed 1 were not continuous on both sides, the north wall was only present in the eastern half of the building. The area to the west was more open with a double row of steel pillars replacing the wall here. The south wall (also the north wall of the south bay) replicated the design described above with the addition of a series of projecting brick buttresses. From the east end the initial eight of these were set higher than those to the west



- ① elevation
- proposed phasing**
- 1st phase (OS1859)
 - 2nd phase (OS1897)
 - 3rd phase mid 20th century
 - 4th phase late 20th century

0 5m
1:250 @ A3

ILLUS 8 Plan of east elevation

and corresponded to the change in height of the wall. A steel beam was placed on the top of the buttresses with a lower beam set on the western group (Illus 25).

The north wall mirrored much of the design of the south wall at the east end with four gabled bays, each incorporating a small round-headed window plus two rows of four windows below. Where it altered from this design was at ground level. Here a further series of windows were present along with two large round-headed entrances in the second and third bay from the east (Illus 26). The first of these had been brick blocked and a square window inserted which had also subsequently been brick-blocked. As with opposite wall the projecting buttresses carried a steel beam, and these were used to support a second overhead crane. The wall ended abruptly at the west end of the fourth bay and no visible evidence was identified to suggest the wall had originally continued, although the walls were covered in a thick white paint that may have masked any alteration. The east wall of the central bay was of a similar design and formed the external east elevation to the building (Illus 27). A double row of steel columns was present to the western half of the bay (Illus 28) and at the west end a large raised steel platform was present (Illus 29).

North bay

The north bay of Shed 1 was shorter than the other two bays and was formed of the gabled north external wall of the building and two gabled bays of the south wall (Illus 30). At the east end the back wall was masked by a series of three mezzanine floors supported on steel cross beams. At ground floor level below the mezzanine floors was a breeze-block constructed store room. The north wall included projecting brick buttresses and three round-headed entrances, one of which had been brick-blocked due to the addition of the extension to the outside of the building. The west wall included a blocked fireplace which was identified more clearly from Shed 2. Along with the rail tracks and the cranes, a series of markings and brick outlines on the floor of the north bay provided evidence of machinery associated with the shipbuilding works in this building.

7.7 INTERNAL SHED 2

Shed 2 was still in use for sorting metal waste at the time of the site visit and this restricted access to some areas of the space. The shed was one large room (Illus 31) with rows of steel pillars supporting the roof. The north and west walls were relatively modern and only the south and east wall represented earlier 19th century parts of the building. The south wall formed the external south wall of the building and comprised four gabled bays each with four long brick-blocked round-headed windows (Illus 32). A series of projecting buttresses again supported a long steel beam for carrying an overhead crane (Illus 33). The steel beam carrying the other side of the crane was supported on steel columns. A steel double sliding door was recorded on the east wall leading to Shed 1. A faint outline of a brick-blocked round-headed entrance was visible under the painted wall. A small round-headed fireplace or flue was also recorded to the north side of the east wall (Illus 34).

8 DISCUSSION

The HBR survey identified that the fabrication shed had four main phases of development. The 1857 Ordnance Survey map depicted a single long shed in the location of the present building. By the publication of the 1897 Ordnance Survey a much larger building is present. It was not apparent at first if the original building had been demolished as part of the later expansion or if it had been incorporated into the later shed. A cursory examination of the building suggested it had all be rebuilt as the general decorative design of the features, such as the round-headed windows and entrances and the use of yellow brick voussoirs, were mirrored across all the main walls. Closer inspection revealed a somewhat more complex phasing plan.

The 1857 Ordnance Survey map depicts the east end of the original building set at a slight angle. This angled end is also visible on later maps depicting the enlarged building. The central part of the east elevation of the present building is also set at an angle. It was therefore presumed this elevation of the building represented the remains of the original fabrication shed. Unfortunately, this does not accurately mirror the cartographic evidence (see Illus 2). This may be a result of poor accuracy of the earlier mapping although this is unlikely. Therefore, it seems the original building had been demolished prior to the expansion.

Although this means the angled east elevation of the central bay does not represent the remains of the original building, it does equate that the upstanding building was not constructed as a single phase. Although the overall design of the openings are similar across the building the spread of windows on the east elevation of the central bay were particular to that elevation. This spread of windows was also repeated within the building on the walls of the central bay of Shed 1. The north wall of the central bay continued the length of the building although it was noted that it changed height along its length. No construction line or change in style was identified in the wall at this point and it seems likely that the whole wall was part of a single construction. The crane support beam on this side was also supported on steel beams rather than a brick buttress indicating that the central bay may have formed the first phase of construction of the expansion of the sheds.

By 1897 the building had been enlarged forming a much wider and longer building. This would have included the addition of the south and north bays, constructed to the same designs used in the central bay such as the round-headed windows and coloured brick voussoirs, and the addition of the large second shed. The building to the west end of the shed (extension 3) is also depicted on the later Ordnance Survey map and although it had been altered, it clearly represents part of this second phase of construction. An additional building is also depicted to the north-east corner of the fabrication shed on the 1897 Ordnance Survey. The north facing elevation here (Elevation D) did show evidence of a roofline implying the elevation had originally been an internal wall.

No dating evidence was identified for the overhead cranes that were present although they were probably installed as part of the expansion of the yard in the late 19th century. The design of the buttresses on these buildings imply they were purposely

9



10



ILLUS 9 View of the east elevation ILLUS 10 Detail of the east elevation of the central bay



ILLUS 11 View of the north side of the central bay **ILLUS 12** The east elevation of the north bay

constructed to support the crane crossbeams rather than just to support the walls.

The original building depicted on the 1857 Ordnance Survey was probably established by James Henderson and Co. in 1847. The initial expansion of the yard probably started under Henderson, Coulborn and Co. after 1866 with further expansion carried out after 1874 under the management of Lobnitz Coulborn and Co.

It is presumed the north and west walls of Shed 2 were constructed much later but on the same alignment as the building depicted on the 1897 Ordnance Survey and these formed part of Phase 3 of the development.

The two extensions to the north elevation represent a fourth phase of construction, the styles representing 20th century design. The layout of these two buildings suggest they may have been offices rather than part of the industrial use of the sheds.

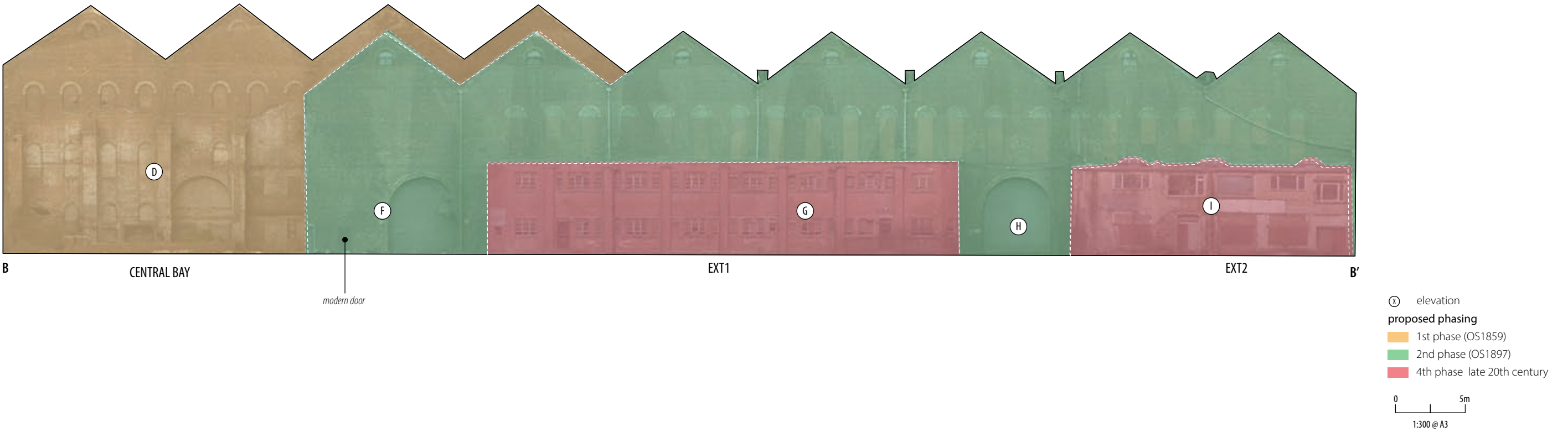
9 CONCLUSION

The survey has identified four general phases of construction of the upstanding fabrication shed with. The earliest phase saw the demolition of the original shed and the construction of a single long bay in the mid-19th century. This was quickly followed by the construction of additional bays to the north and south in the late 19th century. The final two phases were a bit more piecemeal with new walls added to the western shed and the addition of offices buildings to the north side of the building in the 20th century. Once the yard had ceased trading much of the infrastructure was removed and eventually it was transformed for use as a metal recycling centre.

The building is due to be demolished and the area prepared for modern development. If groundworks are carried out across the site, it may be possible to identify the footprint of the original buildings and the earlier infrastructure of the yard, giving insight into how the early yard would have operated. Therefore a targeted watching brief, during the demolition foundation grubbing out, on the footprint of the original building should be considered.

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ILLUS 13 Plan of North elevation

14



15



16



17



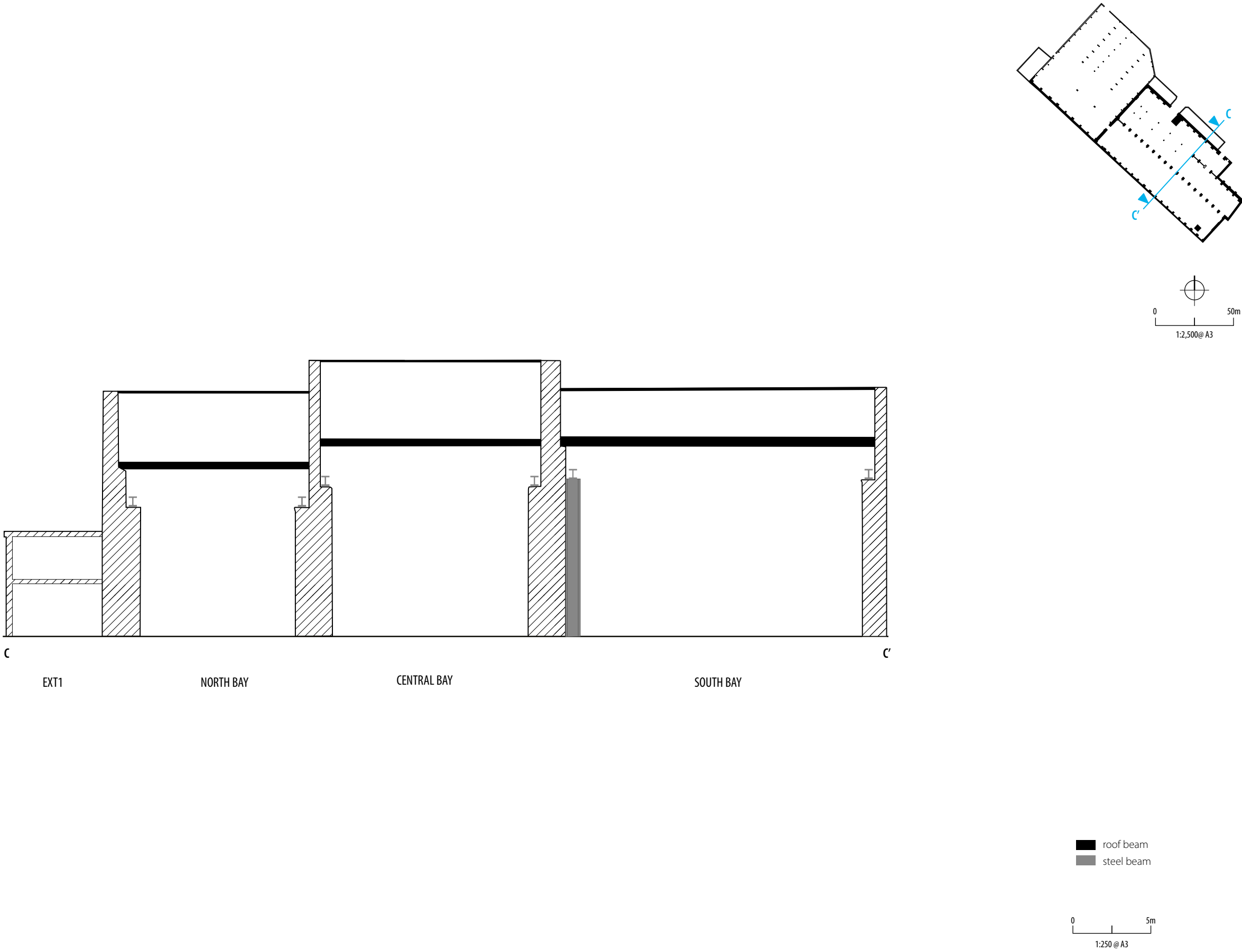
18



ILLUS 14 General view of the north elevation of Shed 1 showing Extension 1 **ILLUS 15** Detail of Extension 2 **ILLUS 16** View of the north facing elevation of Shed 2 **ILLUS 17** General view of the west elevation **ILLUS 18** Detail of Extension 3



ILLUS 19 Oblique view of the south elevation **ILLUS 20** Detail of the south elevation bay



ILLUS 21 North/South section through the building



22



23



24

ILLUS 22 General view of the south bay of Shed 1 **ILLUS 23** Detail of the overhead crane in the south bay **ILLUS 24** The north wall of the south bay



ILLUS 25 East facing view of the central bay of Shed 1 **ILLUS 26** South wall of the central bay **ILLUS 27** General view of the east wall of the central bay **ILLUS 28** View of the steel columns at the west end of the central bay **ILLUS 29** Detail of the steel platform at the west end of the central bay



ILLUS 30 East facing view of the north bay of Shed 1 **ILLUS 31** General internal view of Shed 2 **ILLUS 32** View of the south wall of Shed 2
ILLUS 33 Detail of the overhead crane in Shed 2 **ILLUS 34** Detail of the fireplace on the east wall of Shed 2

11 APPENDICES

APPENDIX 1 PHOTOGRAPHIC REGISTER

PHOTO	FACING	DESCRIPTION
CWRR-01	W	External east elevation
CWRR-02	W	External east elevation
CWRR-03	W	External east elevation
CWRR-04	W	External east elevation
CWRR-05	W	External east elevation
CWRR-06	W	Detail of entrance on east elevation
CWRR-07	W	Detail of entrance on east elevation
CWRR-08	W	External east elevation
CWRR-09	W	External east elevation
CWRR-10	W	External east elevation
CWRR-11	W	External east elevation
CWRR-12	W	External east elevation
CWRR-13	W	External east elevation
CWRR-14	W	External east elevation
CWRR-15	W	External east elevation
CWRR-16	W	External east elevation
CWRR-17	W	External east elevation
CWRR-18	W	External east elevation
CWRR-19	W	Detail of window of elevation A
CWRR-20	NW	External east elevation
CWRR-21	N	External east elevation
CWRR-22	NW	External east elevation
CWRR-23	NW	View of roof showing change in height between bays
CWRR-24	N	Elevation B
CWRR-25	N	Elevation B
CWRR-26	N	Elevation B
CWRR-27	N	Elevation B
CWRR-28	W	East elevation – C
CWRR-29	W	East elevation – C
CWRR-30	W	East elevation – C
CWRR-31	W	East elevation – C
CWRR-32	W	East elevation – C
CWRR-33	W	East elevation – C
CWRR-34	W	East elevation – C
CWRR-35	W	East elevation – C
CWRR-36	W	East elevation – C

PHOTO	FACING	DESCRIPTION
CWRR-37	W	East elevation – C
CWRR-38	W	East elevation – C
CWRR-39	W	East elevation – C
CWRR-40	W	East elevation – C
CWRR-41	W	East elevation – C
CWRR-42	W	East elevation – C
CWRR-43	W	East elevation – C
CWRR-44	W	East elevation – C
CWRR-45	W	Detail on elevation C
CWRR-46	W	Detail on elevation C
CWRR-47	S	East elevation – D
CWRR-48	S	East elevation – D
CWRR-49	S	East elevation – D
CWRR-50	S	East elevation – D
CWRR-51	S	East elevation – D
CWRR-52	S	East elevation – D
CWRR-53	S	East elevation – D
CWRR-54	S	East elevation – D
CWRR-55	S	East elevation – D
CWRR-56	S	East elevation – D
CWRR-57	S	East elevation – D
CWRR-58	S	East elevation – D
CWRR-59	S	East elevation – D
CWRR-60	S	East elevation – D
CWRR-61	S	East elevation – D
CWRR-62	S	Detail on Elevation D
CWRR-63	S	Detail on Elevation D
CWRR-64	S	Detail on Elevation D
CWRR-65	S	Detail on Elevation D
CWRR-66	S	Detail on Elevation D
CWRR-67	S	Detail on Elevation D
CWRR-68	S	Detail on Elevation D
CWRR-69	S	Detail on Elevation D
CWRR-70	S	Detail on Elevation D
CWRR-71	S	Detail on Elevation D
CWRR-72	S	Detail on Elevation D
CWRR-73	S	Detail on Elevation D
CWRR-74	S	Detail on Elevation D
CWRR-75	S	Detail on Elevation D
CWRR-76	S	Detail on Elevation D

PHOTO	FACING	DESCRIPTION
CWRR-77	S	Detail on Elevation D
CWRR-78	S	Detail on Elevation D
CWRR-79	S	Detail on Elevation D
CWRR-80	S	Detail on Elevation D
CWRR-81	S	Detail on Elevation D
CWRR-82	S	Detail on Elevation D
CWRR-83	S	Detail on Elevation D
CWRR-84	S	Detail on Elevation D
CWRR-85	S	Detail on Elevation D
CWRR-86	S	Detail on Elevation D
CWRR-87	S	Detail on Elevation D
CWRR-88	S	Detail on Elevation D
CWRR-89	S	Detail on Elevation D
CWRR-90	S	Detail on Elevation D
CWRR-91	S	Detail on Elevation D
CWRR-92	S	Detail on Elevation D
CWRR-93	S	Detail on Elevation D
CWRR-94	S	Detail on Elevation D
CWRR-95	S	Detail on Elevation D
CWRR-96	S	Detail on Elevation D
CWRR-97	S	Detail on Elevation D
CWRR-98	S	Detail on Elevation D
CWRR-99	S	Detail on Elevation D
CWRR-100	S	Detail on Elevation D
CWRR-101	W	East elevation E
CWRR-102	W	East elevation E
CWRR-103	W	East elevation E
CWRR-104	W	East elevation E
CWRR-105	W	East elevation E
CWRR-106	W	East elevation E
CWRR-107	W	East elevation E
CWRR-108	W	East elevation E
CWRR-109	W	East elevation E
CWRR-110	W	East elevation E
CWRR-111	W	East elevation E
CWRR-112	W	East elevation E
CWRR-113	S	External north elevation
CWRR-114	S	External north elevation
CWRR-115	S	Detail of entrance Elevation F
CWRR-116	S	Detail of entrance Elevation F

PHOTO	FACING	DESCRIPTION
CWRR-117	S	Detail of small door on Ele
CWRR-118	SW	External north elevation
CWRR-119	SW	External north elevation
CWRR-120	SW	External north elevation
CWRR-121	SE	External north elevation
CWRR-122	E	Detail of extension 1
CWRR-123	SE	Detail of extension 1
CWRR-124	S	Detail of extension 1
CWRR-125	S	Detail of extension 1
CWRR-126	S	Detail of extension 1
CWRR-127	S	Detail of extension 1
CWRR-128	S	Detail of extension 1
CWRR-129	S	Detail of extension 1
CWRR-130	S	Detail of extension 1
CWRR-131	S	Detail of extension 1
CWRR-132	S	Detail of extension 1
CWRR-133	S	Detail of extension 1
CWRR-134	S	Detail of extension 1
CWRR-135	S	Detail of extension 1
CWRR-136	SW	Detail of extension 1
CWRR-137	W	Detail of extension 1
CWRR-138	S	External north elevation
CWRR-139	S	External north elevation
CWRR-140	S	External north elevation
CWRR-141	S	External north elevation
CWRR-142	S	External north elevation
CWRR-143	S	External north elevation
CWRR-144	S	External north elevation
CWRR-145	S	External north elevation
CWRR-146	S	External north elevation
CWRR-147	S	External north elevation
CWRR-148	S	External north elevation
CWRR-149	S	External north elevation
CWRR-150	S	External north elevation
CWRR-151	S	External north elevation
CWRR-152	–	Working shots
CWRR-153	–	Working shots
CWRR-154	–	Working shots
CWRR-155	–	Working shots
CWRR-156	S	External north elevation

PHOTO	FACING	DESCRIPTION
CWRR-157	S	External north elevation
CWRR-158	SE	External north elevation
CWRR-159	SE	External north elevation
CWRR-160	SE	External north elevation
CWRR-161	S	Detail of extension 1
CWRR-162	SW	Detail of extension 1
CWRR-163	SE	Detail of extension 1
CWRR-164	W	Detail of extension 1
CWRR-165	W	Detail of extension 1
CWRR-166	S	External North elevation – H
CWRR-167	S	External North elevation – H
CWRR-168	S	External North elevation – H
CWRR-169	S	External North elevation – H
CWRR-170	S	External North elevation – H
CWRR-171	S	External North elevation – H
CWRR-172	SW	Gen view of north elevation
CWRR-173	SW	Gen view of north elevation
CWRR-174	SW	Gen view of north elevation
CWRR-175	SW	Gen view of north elevation
CWRR-176	SW	Gen view of north elevation
CWRR-177	W	Gen view of east elevation
CWRR-178	W	Gen view of east elevation
CWRR-179	W	Gen view of south elevation
CWRR-180	N	South elevation
CWRR-181	N	South elevation
CWRR-182	N	South elevation
CWRR-183	N	South elevation
CWRR-184	N	South elevation
CWRR-185	N	South elevation
CWRR-186	N	South elevation
CWRR-187	N	South elevation
CWRR-188	N	South elevation
CWRR-189	N	South elevation
CWRR-190	N	South elevation
CWRR-191	N	South wall of Extension 3
CWRR-192	N	South wall of Extension 3
CWRR-193	NE	South wall of Extension 3
CWRR-194	NE	South wall of Extension 3
CWRR-195	NE	South wall of Extension 3
CWRR-196	NE	South wall of Extension 3

PHOTO	FACING	DESCRIPTION
CWRR-197	N	South elevation at the west end
CWRR-198	N	South elevation at the west end
CWRR-199	N	South elevation at the west end
CWRR-200	NE	South elevation at the west end
CWRR-201	N	South elevation
CWRR-202	N	South elevation
CWRR-203	N	South elevation
CWRR-204	N	South elevation
CWRR-205	N	South elevation
CWRR-206	W	South elevation
CWRR-207	S	Door on extension 1
CWRR-208	W	Extension 2
CWRR-209	SW	Extension 2
CWRR-210	SW	Extension 2
CWRR-211	SW	Extension 2
CWRR-212	SW	Extension 2
CWRR-213	S	Extension 2
CWRR-214	S	Extension 2
CWRR-215	S	Extension 2
CWRR-216	S	Extension 2
CWRR-217	S	Entrance on north elevation – H
CWRR-218	S	Entrance on north elevation – H
CWRR-219	S	Entrance on north elevation – H
CWRR-220	S	Entrance on north elevation – H
CWRR-221	S	Entrance on north elevation – H
CWRR-222	S	Entrance on north elevation – H
CWRR-223	S	North elevation – J-K
CWRR-224	S	North elevation – J-K
CWRR-225	SW	North elevation – J-K
CWRR-226	S	North elevation – J-K
CWRR-227	SE	North elevation – J-K
CWRR-228	S	North elevation – J-K
CWRR-229	SW	North elevation – J-K
CWRR-230	SE	North elevation – J-K
CWRR-231	S	North elevation – J-K
CWRR-232	S	North elevation – J-K
CWRR-233	SE	North elevation – J-K
CWRR-234	S	North elevation – J-K
CWRR-235	SW	Detail of wall of North elevation J
CWRR-236	SE	North elevation – J-K

PHOTO	FACING	DESCRIPTION
CWRR-237	SE	North elevation – J-K
CWRR-238	SE	North elevation – J-K
CWRR-239	E	West elevation – L
CWRR-240	E	West elevation – L
CWRR-241	E	Extension 3
CWRR-242	NE	Extension 3
CWRR-243	E	Extension 3
CWRR-244	NE	West elevation – L
CWRR-245	E	Door on west elevation – L
CWRR-246	S	Extension 3
CWRR-247	S	Extension 3
CWRR-248	S	Extension 3
CWRR-249	S	Rail tracks leading in to north elevation entrance
CWRR-250	E	Internal working shots
CWRR-251	E	Internal working shots
CWRR-252	E	Internal working shots
CWRR-253	E	Internal working shots
CWRR-254	E	South bay of Shed 1
CWRR-255	E	South bay of Shed 1
CWRR-256	S	South wall of south bay
CWRR-257	S	South wall of south bay
CWRR-258	SE	South wall of south bay
CWRR-259	SE	South wall of south bay
CWRR-260	S	South wall of south bay
CWRR-261	S	South wall of south bay
CWRR-262	S	South wall of south bay
CWRR-263	S	South wall of south bay
CWRR-264	S	South wall of south bay
CWRR-265	S	South wall of south bay
CWRR-266	E	Detail of roof in Shed 1
CWRR-267	S	South wall of south bay
CWRR-268	–	Detail of machinery on south wall of south bay
CWRR-269	–	Detail of machinery on south wall of south bay
CWRR-270	–	Detail of machinery on south wall of south bay
CWRR-271	W	South wall of the south bay
CWRR-272	W	South wall of the south bay
CWRR-273	W	South bay of Shed 1
CWRR-274	W	South bay of Shed 1
CWRR-275	S	Substation in south bay
CWRR-276	S	Substation in south bay

PHOTO	FACING	DESCRIPTION
CWRR-277	E	East wall of south bay
CWRR-278	E	East wall of south bay
CWRR-279	–	Detail of overhead crane in south bay
CWRR-280	–	Detail of overhead crane in south bay
CWRR-281	–	Detail of overhead crane in south bay
CWRR-282	–	Detail of overhead crane in south bay
CWRR-283	–	Detail of overhead crane in south bay
CWRR-284	NW	North wall of south bay
CWRR-285	NW	North wall of south bay
CWRR-286	N	North wall of south bay
CWRR-287	N	North wall of south bay
CWRR-288	N	North wall of south bay
CWRR-289	N	North wall of south bay
CWRR-290	N	North wall of south bay
CWRR-291	N	North wall of south bay
CWRR-292	NW	Detail of pillar on north wall of South bay
CWRR-293	NE	North wall of south bay
CWRR-294		Detail of overhead crane in south bay
CWRR-295		Detail of overhead crane in south bay
CWRR-296	N	North wall of south bay
CWRR-297	N	North wall of south bay
CWRR-298	NE	North wall of south bay
CWRR-299	NE	North wall of south bay
CWRR-300	N	North wall of south bay
CWRR-301	N	North wall of south bay
CWRR-302	W	West wall of south bay
CWRR-303	W	West wall of south bay
CWRR-304	W	West wall of south bay
CWRR-305	–	Detail of roof in Shed 1
CWRR-306	W	Detail of opening on W wall
CWRR-307	–	Working shot on Shed 1
CWRR-308	–	Detail of crane in central bay
CWRR-309	–	Detail of crane in central bay
CWRR-310	–	Detail of crane in central bay
CWRR-311	–	Detail of crane in central bay
CWRR-312	–	Detail of crane in central bay
CWRR-313	–	Detail of crane in central bay
CWRR-314	E	Central bay of Shed 1
CWRR-315	E	Central bay of Shed 1
CWRR-316	S	South wall of central bay

PHOTO	FACING	DESCRIPTION
CWRR-317	S	South wall of central bay
CWRR-318	S	South wall of central bay
CWRR-319	S	South wall of central bay
CWRR-320	SW	South wall of central bay
CWRR-321	S	South wall of central bay
CWRR-322	S	South wall of central bay
CWRR-323	SW	South wall of central bay
CWRR-324	S	South wall of central bay
CWRR-325	S	South wall of central bay
CWRR-326	SW	South wall of central bay
CWRR-327	W	Central bay of Shed 1
CWRR-328	W	Central bay of Shed 1
CWRR-329	E	East wall of central bay
CWRR-330	E	East wall of central bay
CWRR-331	E	East wall of central bay
CWRR-332	E	East wall of central bay
CWRR-333	E	East wall of central bay
CWRR-334	–	Overhead crane in central bay
CWRR-335	NW	North wall of central bay
CWRR-336	NW	North wall of central bay
CWRR-337	NW	North wall of central bay
CWRR-338	N	North wall of central bay
CWRR-339	N	North wall of central bay
CWRR-340	N	North wall of central bay
CWRR-341	N	North wall of central bay
CWRR-342	NE	North wall of central bay
CWRR-343	NE	North wall of central bay
CWRR-344	NE	North wall of central bay
CWRR-345	NE	North wall of central bay
CWRR-346	NE	North wall of central bay
CWRR-347	N	North wall of central bay
CWRR-348	N	North wall of central bay
CWRR-349	W	West end of central bay
CWRR-350	W	West end of central bay
CWRR-351	W	West end of central bay
CWRR-352	W	West end of central bay
CWRR-353	W	West end of central bay
CWRR-354	SW	Steel platform at west end of Shed 1
CWRR-355	SW	Steel platform at west end of Shed 1
CWRR-356	E	North bay of Shed 1

PHOTO	FACING	DESCRIPTION
CWRR-357	E	North bay of Shed 1
CWRR-358	SE	South wall of north bay
CWRR-359	SE	South wall of north bay
CWRR-360	E	South wall of north bay
CWRR-361	S	South wall of north bay
CWRR-362	S	South wall of north bay
CWRR-363	S	South wall of north bay
CWRR-364	S	South wall of north bay
CWRR-365	S	South wall of north bay
CWRR-366	S	South wall of north bay
CWRR-367	S	South wall of north bay
CWRR-368	S	South wall of north bay
CWRR-369	S	South wall of north bay
CWRR-370	S	South wall of north bay
CWRR-371	E	East wall of north bay
CWRR-372	E	East wall of north bay
CWRR-373	E	East wall of north bay
CWRR-374	–	Detail of top of buttress
CWRR-375	E	Detail of east wall
CWRR-376	NW	North wall of the north bay
CWRR-377	NW	North wall of the north bay
CWRR-378	N	North wall of the north bay
CWRR-379	N	North wall of the north bay
CWRR-380	N	North wall of the north bay
CWRR-381	N	North wall of the north bay
CWRR-382	N	North wall of the north bay
CWRR-383	N	North wall of the north bay
CWRR-384	N	North wall of the north bay
CWRR-385	N	North wall of the north bay
CWRR-386	N	North wall of the north bay
CWRR-387	N	North wall of the north bay
CWRR-388	N	North wall of the north bay
CWRR-389	N	North wall of the north bay
CWRR-390	N	North wall of the north bay
CWRR-391	N	North wall of the north bay
CWRR-392	N	North wall of the north bay
CWRR-393	N	North wall of the north bay
CWRR-394	N	North wall of the north bay
CWRR-395	N	North wall of the north bay
CWRR-396	N	North wall of the north bay

PHOTO	FACING	DESCRIPTION
CWRR-397	NE	North wall of the north bay
CWRR-398	NE	North wall of the north bay
CWRR-399	NE	North wall of the north bay
CWRR-400	E	North and central bay of Shed 1
CWRR-401	E	North and central bay of Shed 1
CWRR-402	E	North and central bay of Shed 1
CWRR-403	SE	South wall of central bay
CWRR-404	N	Detail of north wall of north bay
CWRR-405	NE	North wall of the north bay
CWRR-406	NE	North wall of the north bay
CWRR-407	E	North and central bay of Shed 1
CWRR-408	E	North and central bay of Shed 1
CWRR-409	E	North and central bay of Shed 1
CWRR-410	E	North and central bay of Shed 1
CWRR-411	W	West wall of north bay
CWRR-412	W	West wall of north bay
CWRR-413	E	North bay of Shed 1
CWRR-414	E	North bay of Shed 1
CWRR-415	E	Interior of Extension 1 ground floor
CWRR-416	W	Interior of Extension 1 ground floor
CWRR-417	SE	Detail of
CWRR-418	SE	Detail of
CWRR-419	S	Detail of
CWRR-420	S	Detail of
CWRR-421	S	South wall of central bay
CWRR-422	S	South wall of central bay
CWRR-423	S	South wall of central bay
CWRR-424	SE	South wall of central bay
CWRR-425	W	View of ship in dock
CWRR-426	S	South interior wall of Shed 2
CWRR-427	S	South interior wall of Shed 2
CWRR-428	SE	South interior wall of Shed 2
CWRR-429	S	South interior wall of Shed 2
CWRR-430	S	South interior wall of Shed 2
CWRR-431	S	South interior wall of Shed 2
CWRR-432	SE	View of interior of Shed 2
CWRR-433	S	View of interior of Shed 2
CWRR-434	S	View of interior of Shed 2
CWRR-435	SW	View of interior of Shed 2
CWRR-436	S	South interior wall of Shed 2

PHOTO	FACING	DESCRIPTION
CWRR-437	S	South interior wall of Shed 2
CWRR-438	S	South interior wall of Shed 2
CWRR-439	S	South interior wall of Shed 2
CWRR-440	S	South interior wall of Shed 2
CWRR-441	S	South interior wall of Shed 2
CWRR-442	S	South interior wall of Shed 2
CWRR-443	N	View of ship in dock
CWRR-444	–	Overhead crane in Shed 2
CWRR-445	–	Overhead crane in Shed 2
CWRR-446	–	Overhead crane in Shed 2
CWRR-447	–	Overhead crane in Shed 2
CWRR-448	–	Overhead crane in Shed 2
CWRR-449	E	East wall of Shed 2
CWRR-450	E	East wall of Shed 2
CWRR-451	N	North wall of Shed 2
CWRR-452	N	North wall of Shed 3
CWRR-453	NE	Interior of Shed 2
CWRR-454	NE	Interior of Shed 2
CWRR-455	–	Detail of steel pillars
CWRR-456	–	Detail of steel pillars
CWRR-457	S	Interior of Shed 2
CWRR-458	SE	Interior of Shed 2
CWRR-459	SE	Interior of Shed 2
CWRR-460	S	Interior of Shed 2
CWRR-461	S	Interior of Shed 2
CWRR-462	E	East wall of Shed 2
CWRR-463	SE	East wall of Shed 2
CWRR-464	SE	Detail of steel pillars in Shed 2
CWRR-465	E	Detail of steel pillars in Shed 3
CWRR-466	E	East wall in Shed 2
CWRR-467	E	East wall in Shed 2
CWRR-468	E	Interior of Shed 2
CWRR-469	N	Interior of Shed 2
CWRR-470	W	Interior of Shed 2
CWRR-471	W	Interior of Shed 2
CWRR-472	W	Interior of Shed 2
CWRR-473	N	Interior of Shed 2
CWRR-474	N	Interior of Shed 2
CWRR-475	S	South wall of Shed 2
CWRR-476	S	South wall of Shed 2

PHOTO	FACING	DESCRIPTION
CWRR-557	N	North wall of Shed 1 in sequence W to E
CWRR-558	N	North wall of Shed 1 in sequence W to E
CWRR-559	N	North wall of Shed 1 in sequence W to E
CWRR-560	N	North wall of Shed 1 in sequence W to E
CWRR-561	N	North wall of Shed 1 in sequence W to E
CWRR-562	N	North wall of Shed 1 in sequence W to E
CWRR-563	N	North wall of Shed 1 in sequence W to E
CWRR-564	N	North wall of Shed 1 in sequence W to E
CWRR-565	N	North wall of Shed 1 in sequence W to E
CWRR-566	N	North wall of Shed 1 in sequence W to E
CWRR-567	N	North wall of Shed 1 in sequence W to E
CWRR-568	N	North wall of Shed 1 in sequence W to E
CWRR-569	N	North wall of central bay W to E
CWRR-570	N	North wall of central bay W to E
CWRR-571	N	North wall of central bay W to E
CWRR-572	N	North wall of central bay W to E
CWRR-573	N	North wall of central bay W to E
CWRR-574	N	North wall of central bay W to E
CWRR-575	N	North wall of central bay W to E
CWRR-576	N	North wall of central bay W to E
CWRR-577	N	North wall of central bay W to E
CWRR-578	N	North wall of central bay W to E
CWRR-579	N	North wall of central bay W to E
CWRR-580	N	North wall of central bay W to E
CWRR-581	N	North wall of central bay W to E
CWRR-582	N	North wall of central bay W to E
CWRR-583	N	North wall of central bay W to E
CWRR-584	N	North wall of central bay W to E
CWRR-585	N	North wall of central bay W to E
CWRR-586	N	North wall of central bay W to E
CWRR-587	N	North wall of central bay W to E
CWRR-588	N	North wall of central bay W to E
CWRR-589	N	North wall of central bay W to E
CWRR-590	N	North wall of central bay W to E
CWRR-591	E	East wall of central bay S to N
CWRR-592	E	East wall of central bay S to N
CWRR-593	E	East wall of central bay S to N
CWRR-594	E	East wall of central bay S to N
CWRR-595	E	East wall of central bay S to N
CWRR-596	E	East wall of central bay S to N

PHOTO	FACING	DESCRIPTION
CWRR-597	E	East wall of central bay S to N
CWRR-598	E	Floor marks in North bay of Shed 1
CWRR-599	S	Floor marks in North bay of Shed 1
CWRR-600	N	Floor marks in North bay of Shed 1
CWRR-601	W	Floor marks in North bay of Shed 1
CWRR-602	S	Floor marks in North bay of Shed 1
CWRR-603	E	Floor marks in North bay of Shed 1
CWRR-604	S	Floor marks in North bay of Shed 1
CWRR-605	W	Floor marks in North bay of Shed 1
CWRR-606	E	Floor marks in North bay of Shed 1
CWRR-607	S	South wall of the north bay W to E
CWRR-608	S	South wall of the north bay W to E
CWRR-609	S	South wall of the north bay W to E
CWRR-610	S	South wall of the north bay W to E
CWRR-611	S	South wall of the north bay W to E
CWRR-612	S	South wall of the north bay W to E
CWRR-613	S	South wall of the north bay W to E
CWRR-614	S	South wall of the north bay W to E
CWRR-615	S	South wall of the north bay W to E
CWRR-616	S	South wall of the north bay W to E
CWRR-617	S	South wall of the north bay W to E
CWRR-618	S	South wall of the north bay W to E
CWRR-619	S	South wall of the north bay W to E
CWRR-620	S	South wall of the north bay W to E
CWRR-621	S	South wall of the north bay W to E
CWRR-622	S	South wall of the north bay W to E
CWRR-623	S	South wall of the north bay W to E
CWRR-624	S	South wall of the north bay W to E
CWRR-625	S	South wall of the north bay W to E
CWRR-626	S	South wall of the north bay W to E
CWRR-627	S	South wall of the north bay W to E
CWRR-628	S	South wall of the north bay W to E
CWRR-629	S	South wall of the north bay W to E
CWRR-630	S	South wall of the north bay W to E
CWRR-631	S	South wall of the north bay W to E
CWRR-632	S	South wall of the north bay W to E
CWRR-633	S	South wall of the north bay W to E
CWRR-634	S	South wall of the north bay W to E
CWRR-635	S	South wall of the north bay W to E
CWRR-636	S	South wall of the north bay W to E

PHOTO	FACING	DESCRIPTION
CWRR-637	S	South wall of the north bay W to E
CWRR-638	SE	South wall of the north bay W to E
CWRR-639	W	Exterior of east wall
CWRR-640	W	Exterior of east wall
CWRR-641	W	Exterior of east wall
CWRR-642	W	Exterior of east wall
CWRR-643	W	Exterior of east wall
CWRR-644	W	Exterior of east wall
CWRR-645	W	Exterior of east wall
CWRR-646	W	Exterior of east wall
CWRR-647	W	Exterior of east wall
CWRR-648	W	Exterior of east wall
CWRR-649	W	Exterior of east wall
CWRR-650	W	Exterior of east wall
CWRR-651	W	Exterior of east wall
CWRR-652	W	Exterior of east wall
CWRR-653	W	Exterior of east wall
CWRR-654	W	Exterior of east wall
CWRR-655	W	Exterior of east wall

PHOTO	FACING	DESCRIPTION
CWRR-656	W	Exterior of east wall
CWRR-657	W	Exterior of east wall
CWRR-658	W	Exterior of east wall
CWRR-659	W	Exterior of east wall
CWRR-660	W	Exterior of east wall
CWRR-661	W	Exterior of east wall
CWRR-662	W	Exterior of east wall
CWRR-663	W	Exterior of east wall
CWRR-664	W	Exterior of east wall
CWRR-665	W	Exterior of east wall
CWRR-666	W	Exterior of east wall
CWRR-667	W	Exterior of east wall
CWRR-668	W	Exterior of east wall
CWRR-669	W	Exterior of east wall
CWRR-670	W	Exterior of east wall
CWRR-671	W	Exterior of east wall
CWRR-672	W	Exterior of east wall

APPENDIX 2 DISCOVERY AND EXCAVATION IN SCOTLAND REPORT

LOCAL AUTHORITY:	Renfrewshire
PROJECT TITLE/SITE NAME:	Clyde Waterfront Renfrewshire Waterfront
PROJECT CODE:	CWRR19
PARISH:	Blythswood
NAME OF CONTRIBUTOR:	Donald Wilson
NAME OF ORGANISATION:	Headland Archaeology (UK) Ltd
TYPE(S) OF PROJECT:	HBR
NMRS NO(S):	–
SITE/MONUMENT TYPE(S):	Fabrication Shed
SIGNIFICANT FINDS:	None
NGR	NS 5044 6856
START DATE	March 2019
END DATE	March 2019
PREVIOUS WORK	None
MAIN (NARRATIVE) DESCRIPTION:	<p>Headland Archaeology (UK) Ltd was commissioned to undertake a Level 3 historic building survey prior to the redevelopment of the area at Clyde Waterfront, Renfrewshire. The property, a fabrication shed, formerly formed part of the Shipbuilding facility operated by Simons-Lobnitz Ltd. The original building was constructed c 1854 although this seems to have been demolished prior to remodelling of the buildings prior to 1895. The extant building comprises one large building separated into two very large roofed sheds. The two sheds incorporated fabric from four phases of construction relating to the 19th and 20th century expansion.</p> <p>The interior of the eastern shed comprised of three long bays separated by brick walls and rows of steel beams. The east end of the central bay provided evidence of the initial expansion phase of the fabrication shed. The bays to the north and south sides of the building had been added later but were constructed to a very similar design with tall round headed doors and windows. The interior still included several overhead cranes supported on large horizontal steel beams. Several later modifications had also been made to the interior of the east shed. The west shed was still in use as part of Christies scrap metal yard although much of the layout and steel roof support beams were of 19th century design.</p>
PROPOSED FUTURE WORK:	Yes
CAPTION(S) FOR ILLUSTRS:	None
SPONSOR OR FUNDING BODY:	Renfrewshire Council
ADDRESS OF MAIN CONTRIBUTOR:	13 Jane Street, Edinburgh. EH6 5HE
EMAIL ADDRESS:	office@headlandarchaeology.com
ARCHIVE LOCATION	Historic Environment Scotland



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