

ROCH14

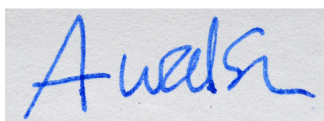


Rochester New Station, Kent
Archaeological Watching Brief
Prepared for C Spencer Ltd

PROJECT SUMMARY SHEET

<i>Client</i>	C Spencer Ltd
<i>National Grid Reference</i>	TQ 745 685
<i>Parish:</i>	Rochester
<i>Council:</i>	Kent County Council
<i>OASIS ref.:</i>	headland4-193978
<i>Archive will be deposited with:</i>	Headland Archaeology, until Guildhall Museum (Medway) will accept archives
<i>Project Manager:</i>	Joe Abrams
<i>Author:</i>	Emma Jeffery
<i>Fieldwork:</i>	Julian Newman, Jake Streatfeild-James, Emma Jeffery, Rhiannon Gardiner
<i>Graphics:</i>	Rafael Maya-Torcely
<i>Schedule</i>	
<i>Fieldwork</i>	December 2014 – March 2015
<i>Report</i>	August 2015

Signed off by



Anthony Walsh

Date: 11/08/2015

CONTENTS

1.....	INTRODUCTION.....	4
2.....	SITE LOCATION AND DESCRIPTION.....	4
3.....	ARCHAEOLOGICAL BACKGROUND.....	4
4.....	METHODOLOGY.....	5
4.1	Field Methodology.....	5
4.2	Recording.....	5
4.3	Reporting and Archives.....	6
5.....	RESULTS.....	6
6.....	DESCRIPTION OF HERITAGE ASSETS.....	7
7.....	CONCLUSION.....	7
8.....	REFERENCES.....	8
8.1	Bibliographic sources.....	8
	APPENDICES.....	9
Appendix 1	– Site registers.....	9

LIST OF ILLUSTRATIONS

Illus 1 – Site location plan

Illus 2 – Site plan showing areas monitored

Illus 3 – W-facing section of construction pit 1

Illus 4 – Construction Pit 4, looking south-east

Illus 5 – Alluvium at south-western end of Construction Pit 8

Illus 6 – East-west beams, possibly part of a riverside revetment, from Construction Pit 1

Illus 7 – Oak log pile recovered from Construction Pit 1

Illus 8 – Railway sleeper recovered from Construction Pit 1

Illus 9 – Halved oak logs recovered from Construction Pit 1

Illus 10 – Brick culvert [605], looking north-west

Illus 11 – Man-hole [804], looking north-east

ROCHESTER NEW STATION, KENT

Archaeological Monitoring

Headland Archaeology (UK) Ltd undertook archaeological observation of groundworks associated with the redevelopment of Rochester Train Station. The works identified the remains of a probable post-medieval riverside revetment and evidence for the modern development of the area (modern drainage system and levelling / made-ground deposits). This represents the periods when the DA was marshland and the later 19-20th century development of the area. It is considered to have low significance of local interest.

1 INTRODUCTION

Planning permission has been granted for the redevelopment of Rochester Train Station, as part of the East Kent Resignalling Phase 2 Project (Ref: C8281/SA0138). The redevelopment of Rochester Station involves the demolition of parts of the old station in order to construct the Rochester New Station, alongside various ancillary works (drainage, services, removal of contaminated ground, etc).

A condition was placed on this planning permission which stated that a programme of archaeological work was required. This resulted in an 'Archaeological Management Plan' being compiled and agreed with the Heritage Conservation Group at Kent County Council (C Spencer Ltd, 2013). This outlined the need for archaeological monitoring of any groundworks which may have an impact on archaeological remains.

The client commissioned Headland Archaeology (UK) Ltd to prepare a Written Scheme of Investigation (WSI) for the monitoring (Headland Archaeology 2014), based on the requirements outlined in the Archaeological Management Plan. Headland Archaeology then undertook the site works and prepared a report (this document) on the results. The WSI was approved by the Heritage Conservation Group at Kent County Council prior to commencement of the project.

2 SITE LOCATION AND DESCRIPTION

The site (henceforth known as the 'Development Area', DA) is located in north Kent, within the centre of the historic town of Rochester to the west of the River Medway and east of Rochester Castle and Cathedral. It is centred on National Grid Reference TQ 74354 68585.

The DA comprises a sub-rectangular parcel of land, approximately 1.7ha in size. It includes the Corporation Street Car Park and the adjacent section of railway track.

It is bounded by Blue Boar Lane to the south, Corporation Street to the west, Haywards House to the north, and the Rochester riverside to the east (Illus 1).

The land on which the DA sits is at approximately 5m AOD. There is some variation in the ground-level within the DA, with a higher bank where the railway sits and the lower flat area of the Corporation Street car park.

The DA lies on the northern periphery of the North Downs, a series of chalk hills. The underlying geology of the DA is described as Lewes Nodular Chalk formed in the Cretaceous Period, with superficial deposits of silty peaty sandy clay alluvium formed in the Quaternary Period (British Geological Survey).

The DA lies at the southern (inland) end of an extensive marshland area within the Medway Estuary. This consists of saltmarsh and intertidal mud / sand flats. The DA lies immediately upstream of a meander of the river – the river here is tidal and the DA has probably been influenced by estuarine conditions throughout much of the past.

3 ARCHAEOLOGICAL BACKGROUND

The archaeological and historical background of the DA has been covered in a pre-application desk-based assessment (Wessex Archaeology 2013). The conclusions of this will be summarised here.

There was an Iron Age and Romano-British settlement at Rochester, with Watling Street (a Roman road) c125m to the west of the DA, and the city walls to the west of the DA having Roman foundations. Various Romano-British buildings have been found close to the DA. The DA itself is positioned opposite the North Gate of the City and it has been suggested that this would have been the location of the harbour, although there has been no archaeological evidence to support this.

There is little archaeological evidence for Anglo-Saxon activity within the DA, although charters record parcels

of land that were given to the Bishop of Rochester, and there is known to have been an Anglo-Saxon church within the footprint of Rochester Cathedral (which was constructed itself in 604). Rochester is mentioned in the Domesday Book as a medium-sized settlement.

Evidence for the medieval town of Rochester includes the 11th century cathedral, the 12th and 13th century City Walls, and the early 12th century castle. Remains of medieval houses and rubbish pits have been found in archaeological work.

The remains of post-medieval buildings, pits, and other features have been found in Rochester. Historic maps provide an indication of the usage of the DA in the post-medieval period – Baker's 1772 Map shows the northern part of the DA within an area of land called 'The Common', and the southern part of the DA within marshland. Sale's 1816 Map shows the DA to lie predominantly within marshland and the 1842 Tithe Map records the land as being marshland used as pasture. Greater development is shown on the 1869-70 OS Map, which shows the railway line having been constructed. The 1897 OS Map shows part of the DA in use as a cattle market – a market continued to be positioned there until the later 20th century.

Previous archaeological work has provided an indication of the types of archaeological deposits which may survive within the DA. Roman flood and marsh deposits are overlain by medieval marsh reclamation deposits (an attempt to consolidate the level of the ground for cultivation). These were subsequently sealed by a cultivation horizon with post-medieval pottery. These archaeological horizons appear c.3.1m AOD (c.1.9m beneath the present ground-surface).

4 AIMS & OBJECTIVES

The aims and objectives are outlined in the WSI (Headland Archaeology 2014). In general the purpose of the investigation was to record and enhance understanding of the significance of heritage assets before they are lost (NPPF para 141). The general aims of the investigations were:

- Establishing the location, extent, nature, function, character, and date of any archaeological features or deposits that may be present.
- Establishing the integrity and state of preservation of any archaeological features or deposits that may be present.
- Securing where appropriate, the assessment, analysis, conservation, and long-term storage of any artefactual / ecofactual material recovered from the site.

The local and regional research contexts are provided by *The South East Research Framework Conference agenda documents* (KCC 2012). Any evidence retrieved during the works was analysed in light of the objectives contained in these frameworks.

In particular the site had the potential to uncover Roman or medieval remains (related to marsh reclamation and cultivation), and post-medieval remains associated with cultivation and the later cattle market. Contributing to the understanding of the sedimentary and palaeo-environmental history of this area of the Medway valley was another research aim, although this will mainly be addressed in the separate geoarchaeological investigation.

4 METHODOLOGY

4.1 Field Methodology

Archaeological monitoring was undertaken in a series of visits made between the 16th December 2014 and the 29th March 2015. This involved the observation of the excavation of drainage / sewage trenches. These took place in four main areas, consisting of eight separate pits (see Illus 2). These were all positioned towards the southern part of the DA, to the east and west of the railway line around Blue Boar Lane.

The dimensions of the monitored areas are outlined in Appendix 1. In total, an area of approximately 131m² of groundworks was observed. Excavations were to a maximum depth of 4.5m beneath the existing ground-surface (c.0.5mOD), the average was 1.2m deep (3.8mOD).

Monitoring was carried out in accordance with the WSI and the Archaeological Management Plan (Headland Archaeology 2014; C Spencer 2014). This involved continuous archaeological observation, investigation and recording during all groundworks likely to have an impact on archaeological deposits or remains; those impacts generally greater than 1m. The groundworks were undertaken by a mechanical excavator using a flat-bladed bucket.

All works were conducted in accordance with standards set out by the Chartered Institute for Archaeologists (CIfA) (CIfA 2014a) and adheres to English Heritage's (2009) *Management of Research Projects in the Historic Environment*.

4.2 Recording

All recording was in accordance with the Code of Practice of the Chartered Institute for Archaeologists

(ClfA 2014a). The trench and contexts were given unique numbers. All recording was undertaken on pro forma record cards that conform to accepted archaeological standards. All stratigraphic relationships were recorded.

An overall site plan at an appropriate scale and relative to the National Grid was compiled. The site plan was accurately tied in to the National Grid and a scale version is shown in Illus 2.

A digital photographic record was taken and a metric scale was clearly visible in record photographs.

4.3 Reporting and Archives

The results of the works are presented below. A summary report has been prepared for submission to the OASIS database (headland4-193978).

The project archive will eventually be deposited at Guildhall Museum, Medway. However, the Museum are not currently accepting archaeological archives, and so the archive will be held by Headland Archaeology until Guildhall Museum, or another repository, is able to accept it. All archive preparation will be undertaken in accordance with guidelines published by the ClfA on behalf of the Archaeological Archives Forum (ClfA 2014b).

5 RESULTS

5.1 General stratigraphy

The majority of the trenches and pits revealed an upper deposit of brick rubble layers ((101), (201), (301), (501), (601), (602), (701), (801)), with some concrete surfaces [401] and brick bases [405]. These represent the modern surfaces found within the DA today.

These overlay significant depths of made-ground / levelling deposits, comprising a mixture of chalk, clayey-silts, sands, and brick rubble ((102), (103), (202), (302), (402), (403), (404), (603), (604), (702), (802)) (Illus 3 and 4). These were observed to depths of at least 3.7m (1.3mOD) (Trench 8), and 1.2-1.4m beneath the ground-surface (3.6-3.8mOD) in Trenches 1 and 2. The quantities of made-ground deposits represent modern land reclamation associated with the construction of the railway line and associated development, which took place from the later 19th century (shown on the 1869-70 OS Map).

The natural geological deposit, the alluvium, was observed in Construction Pit 1, 2, and 8. In Construction Pit 1 this comprised an organic rich grey black silt (103) observed at 1.2m beneath the present ground-surface (3.8mOD). In Construction Pit 2 this comprised a light grey brown silty clay (203) observed 1m beneath the

present ground-surface (4m AOD). Mid grey blue clay / silts (808) were observed at 3.7m beneath the present ground-surface in Trench 8 (1.3m AOD) (Illus 5). These natural deposits represent the alluvial deposits which formed the marshland, and which overlay the Chalk formation (which was not observed in any of the monitoring).

Peat deposits (809) were observed in Construction Pit 8 around 4m beneath the ground-surface (c.1m AOD).

5.2 Worked wood

Six pieces of worked wood were recorded within Construction Pit 1. Two of these were in situ and were parallel to each other, aligned east-west, and projecting out of the eastern section for 0.8m (Illus 6). They were broken at their western ends, although certainly were not present in the western section. They measured 0.1m in width and were observed 1.7m beneath the present ground-surface (3.3mOD). They may have formed part of a structure to the east.

Two upright posts were observed c.0.4m to the north of the east-west beams. They were positioned approximately 0.3m apart, and may have been associated with the east-west beams. One was an oak log pile, 4.25m long and 170mm in diameter (Illus 7). Its tip had been neatly hewn with a fine blade metal axe, and several side branches lopped off. The other was a tarred softwood railway sleeper, 2.1m long, 0.18m x 0.18m across with one end sawn to a point (Illus 8). These two timbers were analysed by Damian Goodburn during an on-site meeting, who stated that the fresh condition of the oak log pile indicated a late post-medieval to early 19th century date (probably 17th-18th century) and that it was probably originally a foundation pile set under a plank sill bearing a brick foundation for a building. He stated that the railway sleeper was probably of very recent date.

Two small halved oak logs were recorded within Construction Pit 1, at a higher level than those discussed above. These are not thought to have been in situ. The logs were, just over 1m long by 160-200mm wide and 100mm thick (Illus 9). One had an oblique treenail or peg hole c.25mm in diameter, and the other an axe cut joint. Damian Goodburn suggested they may have been left-overs from a shipyard where such timbers were used for boat and barge frames, scaffolds, and shores. Alternatively, they may have been used for heavy stock fencing or temporary farm enclosures. Either of these options are possible considering the proximity of the site to the River Medway and the ship building industries (e.g. Frindsbury), and the presence of farms shown on historic maps close to the site. The condition of the pieces of timber suggests a later post-medieval to 19th century date.

Historic mapping shows that this part of the DA was marshland until the second half of the 19th century. It is therefore likely that the timbers, particularly the in situ parallel east-west beams, formed part of riverside revetments pre-dating the construction of the railway. This is supported by the three groups of timbers uncovered in the AOC work around Blue Boar Creek (Damian Goodburn pers comm), thought to have been part of a partially-destroyed revetment.

The mixed character of the displaced timbers suggests that they were most likely dumped here during the construction of the railway (including the railway sleeper.)

5.3 Modern features

The remains of the modern drainage system were observed within Construction Pits 6-8. This comprised two brick culverts [605] and [804].

Culvert [605] was aligned N-S, and measured 1.8m wide and 1m deep. It was made of London stock brick (Illus 10). It had a flat top and three stepped sides, above a concrete casing containing the drain. The brick top was visible c0.25m below ground-level (BGL), with the concrete casing at c1.2m BGL.

Culvert [804] was aligned E-W connecting two man holes. It was an arched brick construction, and measured 13m+ long by 1.2m wide and 0.8m deep (Illus 11). It was visible 3.1m BGL.

The culverts formed part of the same drainage system. This was presumably constructed in the later 19th century, alongside the construction of the railway and development of this area.

Other features associated with the drainage system were also recorded, including the wooden shoring for the man hole at the south-western end of Construction Pit 8 [805], and the levelling deposit (807) for the concrete blinding beneath the man-hole [806].

In Construction Pit 1 was a modern cut [105] that may relate to earlier drains or services. The cut extended N-S of the northern section of the pit for c1.2m and was 0.75m wide and 1.85m deep. It was filled with brick rubble (104).

6 Description of heritage assets

The table below outlines the heritage assets identified during the archaeological monitoring, and their significance. The Regional Research Frameworks, and the questions raised in these, are considered.

Table 1: Significance of Heritage Assets

Description of Heritage Asset	Feature Number/s	Significance of heritage asset on Local, Regional, National, International scale
HA1: Post-medieval timbers	107	Low significance of local interest
HA2: Evidence for modern development of the DA – drainage features and made-ground deposits	102, 103, 106, 202, 302, 402, 403, 404, 603, 604, 605, 702, 802, , 804, 805, 806	None

Heritage Asset 1 comprises the post-medieval timbers recorded in Trench 1. Some of these may represent part of a post-medieval riverside revetment, with the others reflecting the modern development associated with the railway. They are therefore representative of the general development of this area in the post-medieval period and later 19th century and are considered to have low significance of local interest.

Heritage Asset 2 comprises the evidence for modern development identified within the DA. The Asset includes the drainage features identified in Pits 6 and 8, the modern cut in Construction Pit 1, and the made-ground and levelling deposits observed across the DA. These are considered to have no heritage significance.

7 CONCLUSION

The archaeological monitoring of the groundworks at Rochester Station revealed the remains of a probable post-medieval riverside revetment, and evidence for the modern development of the area (modern drainage systems and levelling / made-ground deposits). This represents both the period when it was marshland and the later 19-20th century development of the area.

Trenches 3, 4, 5, 6, and 7 did not extend beneath the modern made-ground deposits.

No evidence for earlier archaeological activity was present.

The results from the watching brief therefore contribute to questions concerning the post-medieval and modern development of the area. The probable post-medieval riverside revetment is considered to have low

significance of local interest, with the evidence for the modern development of the DA having no significance.

As no further ground works are required as part of the development the potential damage to any unidentified heritage assets is negligible.

8 REFERENCES

8.1 Bibliographic sources

Communities and Local Government, 2012, *National Planning Policy Framework, Government National Planning Policy*.

English Heritage, 2006, *Management of Research Projects in the Historic Environment: The MoRPHE Project Managers' Guide*. London: Historic England.

CIfA, 2014a, *CIfA Standards and guidance for an archaeological watching brief*.
<http://www.archaeologists.net/codes/ifa> accessed 09/07/2015.

CIfA, 2014b *CIfA Standards and guidance for the creation, compilation, transfer and deposition of archaeological archives*.
<http://www.archaeologists.net/codes/ifa>, accessed 09/07/2015.

C Spencer Ltd, 19/09/2013, *Archaeological Management Plan. Rochester Station Construction Phase*.

Headland Archaeology 2014, *Programme of Archaeological Works at Rochester New Station: Written Scheme of Investigation*

KCC 2012, *South East Research Framework Conference agenda documents*
http://www.kent.gov.uk/leisure_and_culture/heritage/south_east_research_framework/serf_agenda_conference_documents.aspx (accessed 03/10/2014)

Wessex Archaeology, 2013. *Rochester Station Redevelopment, Corporation Street, Rochester, Kent. An Archaeological desk-based assessment and Impact Assessment*.

APPENDICES

Appendix 1 – Site registers

Trench Register

Trench	Orientation	Length (m)	Width (m)	Max. depth (m)	Ave. depth (m)	Description
1	E-W	12	2	2	1.5	Sewer pipe
2	N-S	4.9	2.1	1.75	1.4	Drainage
3	N-S	2	2	1.05	1	Drainage
4	E-W	3	1.05	1	0.95	Drainage
5	NW-SE	12	1.5	0.5	0.5	Drainage
6	NE-SW	16.85	0.6	1.2	1.2	Trial trench for drainage
7	NE-SW	16.85	0.6	1.2	1.2	Trial trench for drainage
8	NE-SW	18.5	4.1	4.5	4.1	Drainage

Context Register

Context number	Description	Length (m)	Width (m)	Depth (m)
(101)	Modern brick rubble levelling in grey silty sand layer.			0-0.45
(102)	Re-deposited chalk with soil band in. Re-deposited build-up layer.			0.45-0.8
(103)	Light grey clayey silt with lenses of dark brown green clayey silt. Re-deposited build-up layer.			0.8-1.2
(104)	Dark black grey silts and sand/ clayey silt, organic rich. Natural geological deposit (alluvium).			1.2-2m (LOE)
(105)	Post-Medieval / Modern brick rubble backfill of cut [106].	1.2m+	0.75	1.85m
[106]	Post-Medieval / Modern cut. Filled with rubble backfill (105).	1.2m+	0.75m	1.85m
Structure [107]	In situ wood found at depth of 1.7m. Two parallel E-W beams run out of E section, possibly forming part of a structure extending to the east. Two other posts removed before arrived, and two upright posts positioned to the north of the E-W beams (these are thought to have been disturbed).	0.8	0.1	Observed at 1.7 BGL.
(201)	Dark grey black silt sand mixed with frequent brick rubble. Modern made-ground deposit.			0-0.4
(202)	Mix of chalk, pale brown grey sand, dark silty sand lenses and brick rubble. Made ground, levelling build up layer.			0.4-1.4
(203)	Light grey brown silty clay. Natural geological deposit (alluvium).			1+ (LOE)

(301)	Dark grey black silty sand with frequent brick inclusions. Water pipes cut through. Modern made ground.			0-0.45
(302)	Mixture of chalk, brown-grey sand, dark sand, and brick rubble. Modern levelling/make up deposit.			0.45-1+ (LOE)
[401]	Concrete base in southern section.			0-0.1
(402)	Dark grey-black silty deposit with frequent brick rubble. Modern make-up deposit.			0.1-0.6
(403)	Chalk layer. Levelling deposit.			0.6-0.7
(404)	Brown grey sand. Levelling deposit.			0.7-0.95+ (LOE)
(405)	Brick base in northern section. Black bricks (measured 0.23mX 0.075m), 7 courses (0.5m) deep.			0-0.5
(501)	Mixture of yellow sands and dark grey black silty sand with chunks of brick rubble and concrete. Modern construction/make up deposit.			0-0.5
(601)	Embankment full of modern rubbish, sleeper fragments, modern brick. Organic rich topsoil, dark grey brown sandy silt.			0-1
(602)	Brick inclusions, blocks of chalk, and general building detritus of bricks and concrete. Modern made ground.			0-0.4
(603)	Mixture of chalk, sand, and mudbrick. Made ground / levelling deposit.			0.4-1.2
(604)	Yellow sand. Redeposited. Made ground.			0.4-0.1
[605]	London stock brickwork on top of a substantial concrete casing for drainage pipe. Likely built this way to protect drains from the train movement above (culvert). Brickwork has a flat top with 3 stepped sides. Crosses the trench on a N-S alignment.	1.63+ (LOE)	1.8	0-1
(701)	Brick inclusions, blocks of chalk, and general building detritus of bricks and concrete. Modern made ground.			0-0.35
(702)	Mixture of brick rubble, chalk, mudstone, sand, and general building waste. Addition of various services cause patchy areas. Modern made-ground.			0.35-1.2+ (LOE)
(801)	Brick inclusions, blocks of chalk, and general building detritus of bricks and concrete. Modern made ground.			0-0.3
(802)	Mixture of bricks, chalk, patches of sand, mud brick and backfilled rubbish, mainly relating to the railway. Modern made-ground.			0.3-2.75+ (LOE)
(803)	Alluvium rich backfill deposit packed around drainage pipe (structure [804]). Dark brown grey, dark grey blue at NW end. Occasional chalk, brick and mudbrick fragments.			2.75-3.5
[804]	Man-hole for drainage pipe connecting manholes. 3-4 bricks deep, London stock brick.	13	1.2	3.1-3.9
[805]	6 wooden beams/ 0.45m-1.3m in length and 0.03m-0.1m in thickness. Protruded from SW section of TR 8, parallel to manhole 6450. Original shoring for the construction of the man hole.			3.1-3.9

(806)	Concrete blinding beneath structure [804]. Stretches the length of the structure	13	2.5	3.9-4.4
(807)	Gravels and sands. Levelling deposit for (806).			4.4-4.6
(808)	Mid grey blue clay / silts with frequent pockets of mid brown peats which quickly oxidise to a dark brown. Disturbed by previous building works. Natural alluvium deposit.			3.7-4
(809)	Mid brown peat deposit. Reed or sedge peat. No archaeology observed. The level varies across TR 8. Disturbed in places by previous building works - example structure [605].			4+ (LOE)

Photographic Register

Photo number	Digital photo number	Direction facing	Description
1	1	S	Excavation area as found (Trench 1)
2	2	N	Excavation area as found (Trench 1)
3	3	N	Excavation area as found (Trench 1)
4	4		Two halved oak logs, Trench 1, not in situ
5	5		Two halved oak logs, Trench 1, not in situ
6	6		Two halved oak logs, Trench 1, not in situ
7	7		Two halved oak logs, Trench 1, not in situ
8	8	N	Modern cut through made ground, [106] (Trench 1)
9	9	N	Modern cut through made ground, [106] (Trench 1)
10	10	N	Modern cut through made ground, [106] (Trench 1)
11	11	N	Trench 1
12	12	N	Trench 1
13	13	N	Trench 1 showing made ground
14	14	N	Trench 1
15	15		Site with Rochester Castle and Cathedral in background
16	16		Site with Rochester Castle and Cathedral in background
17	17	N	Excavated soakaway (Trench 1) with in situ wood
18	18	E	Excavated soakaway (Trench 1) with in situ wood
19	19		E-W wood 'beams' (Trench 1)
20	20		E-W wood 'beams' (Trench 1)
21	21		E-W wood 'beams' and post (Trench 1)
22	22	N	E-W wood 'beams' and post (Trench 1)

23	23		E-W wood 'beams' zoomed in (Trench 1)
24	24		Railway sleeper
25	25		Railway sleeper
26	26		Railway sleeper
27	27		Detail of point on railway sleeper
28	28		Detail of notch on railway sleeper
29	29		Railway sleeper
30	30		Detail of point on railway sleeper
31	31		Detail of notch on railway sleeper
32	32	W	Original run of Trench 2 subsequently back filled
33	33	W	Original run of Trench 2 subsequently back filled
34	34	N	Example of trench section and soil profile in Trench 2
35	35	S	Trench 2 general shot
36	36	S	Trench 2 general shot
37	37	SE	Trench 2 general shot
38	38	SE	Trench 2 general shot
39	1		Oak log pile
40	2		Detail of point – oak log pile
41	3		Detail of point – oak log pile
42	4		Detail of oak log pile
43	5		Detail of oak log pile
44	6		Detail of oak log pile
45	5401	N	Pit 4, W half, N section
46	5402	S	Pit 4, W half, S section
47	5403	SE	General shot of Pit 4, W half
48	5404	SW	Pit 4, E half
49	5405		Railway sleeper
50	5406		Railway sleeper
51	5407		Railway sleeper
52	5408		Railway sleeper
53	5409		Railway sleeper
54	5410		Railway sleeper
55	5411		Railway sleeper
56	5412		Railway sleeper
57	5413		Railway sleeper
58	5414	NW	Trench 5 general shot
59	5415	SE	Trench 5 general shot
60	1	W	Signal box location sign, co-ordinates for Trench 6, 7 and 8
61	2	SE	Trench 6 - first day of excavation
62	3	NW	Trench 6 - first day of excavation
63	4		Brick culvert [605]
64	5		Brick culvert [605]
65	6	SE	NW facing section of Trench 6 SW end
66	7	SE	NW facing section of Trench 6

67	8	SE	NW facing section of Trench 6
68	9	SE	NW facing section of Trench 6 NE end
69	10	NW	SE facing section of Trench 6 SW end
70	11	NW	SE facing section of Trench 6
71	12	NW	SE facing section of Trench 6
72	13	NW	SE facing section of Trench 6
73	14	NW	SE facing section of Trench 6
74	15	NW	SE facing section of Trench 6
75	16	NW	SE facing section of Trench 6 NE end
76	17	SE	NW facing section of Trench 6 NE end
77	18	SE	NW facing section of Trench 6 NE end
78	19	SW	General shot Trench 6
79	20	NW	SE facing section of Trench 7 SW end
80	21	NW	SE facing section of Trench 7
81	22	NW	SE facing section of Trench 7
82	23	NW	SE facing section of Trench 7 NE end
83	24	SE	Brick culvert [605]
84	25	SW	Brick culvert [605]
85	26	NE	Brick culvert [605]
86	27	SW	General shot of Trench 7
87	28	SW	General shot of Trench 6
88	29	SE	Full extent of the yellow brickwork of brick culvert [605] within excavation area
89	30	SE	Full extent of the yellow brickwork of brick culvert [605] within excavation area
90	31	NW	Full extent of the yellow brickwork of brick culvert [605] within excavation area
91	32	NW	Full extent of the yellow brickwork of brick culvert [605] within excavation area
92	33	NE	Full extent of the yellow brickwork of brick culvert [605] within excavation area
93	34	S	Full extent of the yellow brickwork of brick culvert [605] within excavation area
94	35	SE	Brick culvert [605]
95	36	NW	Section of Trench 6 including brick culvert [605]
96	37	SW	Trench shot Trench 6, brick culvert [605] being excavated
97	38	SW	Excavation of brick culvert [605]
98	39	SW	Excavation of brick culvert [605]
99	40		Brick culvert [605] during excavation
100	41	NE	SW facing stepped edge of brick culvert [605]
101	42	NW	Brick culvert [605] during excavation
102	43	NW	SE facing section of brick culvert [605] during excavation
103	44	NW	Section of brick culvert [605] SE facing section of Trench 6
104	45	SE	NW facing section of Trench 7 SW end

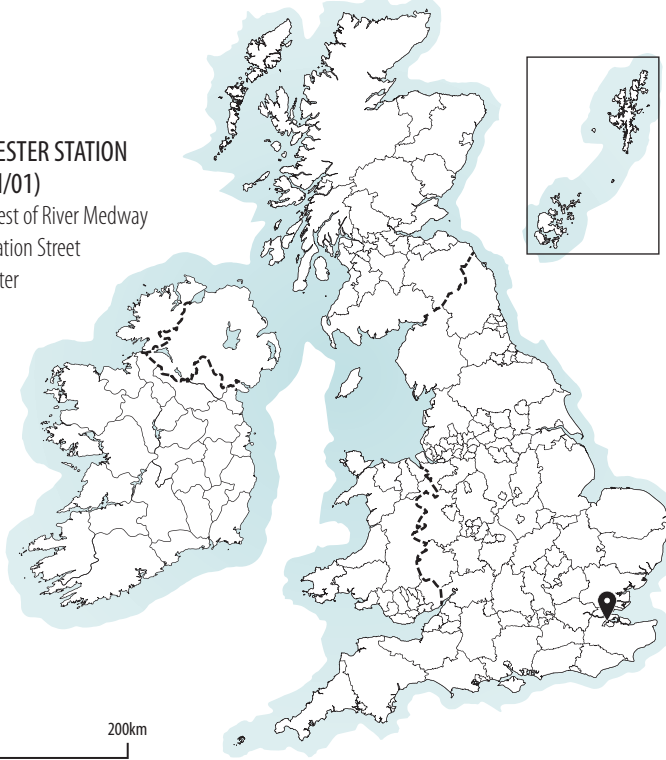
105	46	SE	NW facing section of Trench 7
106	47	SE	NW facing section of Trench 7
107	48	SE	NW facing section of Trench 7
108	49	SE	NW facing section of Trench 7 NE end
109	50		Investigation into depth of concrete base of brick culvert [605]
110	51		Investigation into depth of concrete base of brick culvert [605]
111	52	NE	Storage of wooden pile previously found on site
112	53		Storage of wooden pile previously found on site
113	54	SW	NE facing section of Trenches 6,7 and 8
114	55	SW	General shot of Trench 8
115	56	SW	General shot of Trench 8
116	57	N	General shot of Trench 8, soakaway in centre visible
117	58		SW end of Trench 8, area excavated over the weekend
118	59	SE	SW end of Trench 8, chalk packing around man-hole [804]
119	60	SW	SW end of Trench 8, alluvium deposit (803) visible
120	61	NW	Man-hole [804] broken through
121	62		Man-hole [804] broken through, close up
122	63	NW	Excavation of man-hole [804] NE of structure [605]
123	64	NW	Excavation of man-hole [804] NE of structure [605]
124	65	NE	View into man-hole [804] during excavation
125	66	NE	General trench shot Trench 8
126	67	SW	Position of wood in (803)
127	68	SW	General trench shot Trench 8
128	69	NW	General shot of Trench 8 after reducing concrete of culvert [605]
129	70	NW	(803) alluvium deposit at SW end of Trench 8
130	71	SSW	Position of wood structure [805] next to manhole, SW end of Trench 8
131	72		Wood 1 from structure [805] with scale
132	73		Wood 1 from structure [805] without scale
133	74		Wood 2 from structure [805] with scale
134	75		Wood 2 from structure [805] without scale
135	76		Wood 3 from structure [805] with scale
136	77		Wood 3 from structure [805] with scale
137	78		Wood 4 from structure [805] with scale
138	79		Wood 4 from structure [805] without scale
139	80		Wood 4 from structure [805] close up of metal fitting
140	81		Wood 4 from structure [805] close up of metal fitting reverse

141	82		Wood 5 from structure [805] with scale
142	83		Wood 5 from structure [805] without scale
143	84		Wood 6 from structure [805] with scale
144	85		Wood 6 from structure [805] without scale
145	86	N	All wood recovered from structure [805] Wood 1 at top - Wood 6 at bottom
146	87	N	All wood recovered from structure [805] Wood 1 at top - Wood 6 at bottom
147	3932	SE	Concrete reinforcing of man-hole [804]
148	3933	S	Wood in SE facing section SW end of trench 8
149	3935	NW	Aspects of wooden shoring for concrete of man-hole [804]
150	3936	NW	Wooden shoring for man-hole [804] visible in section
151	3937	NW	Wooden shoring for man-hole [804] visible in section
152	3938	NW	Wooden shoring for man-hole [804] visible in section
153	3939	NW	Wooden shoring for man-hole [804] visible in section
154	3940	NW	Wooden shoring for man-hole [804]
155	3942	ENE	NE end of trench after Spit 3 excavation
156	3944		Wooden shoring for man-hole [804] shoring
157	3945		Wooden shoring for man-hole [804] shoring
158	3949	S	Wood along SW edge of TR8 - support for manhole
159	3951	NW	Wooden shoring for man-hole [804]
160	3952	NW	Interface between (808) and (809) and from wooden shoring from man-hole [804]
161	3955		Dark peat (809)
162	3956		Example of the base after the excavation of the final spit

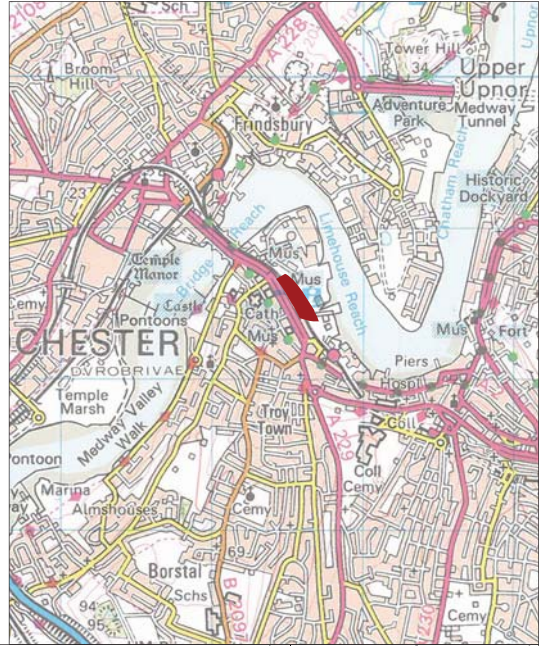
Drawings Register

Drawing number	Scale	Description
1	1:20	Oak log pile
2	1:10	Halved oak log
3	1:10	Halved oak log

**ROCHESTER STATION
(ROCH/01)**
land west of River Medway
Corporation Street
Rochester
Kent



0 200km



Ordnance Survey © Crown copyright 2014. All rights reserved. Licence no. AL 100013329

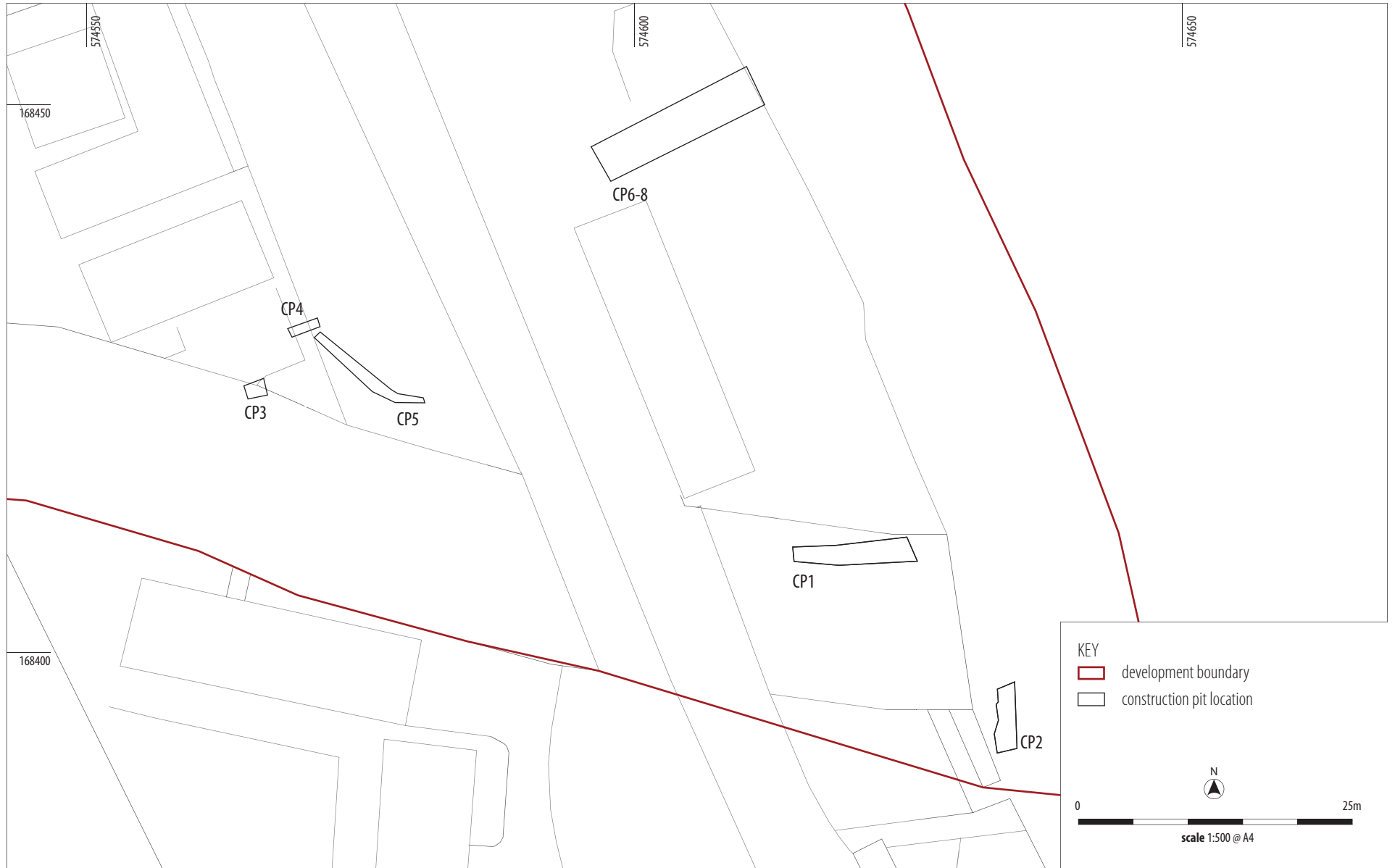
KEY
 development boundary
 construction pit location

0 250m
 scale 1:5,000 @ A4



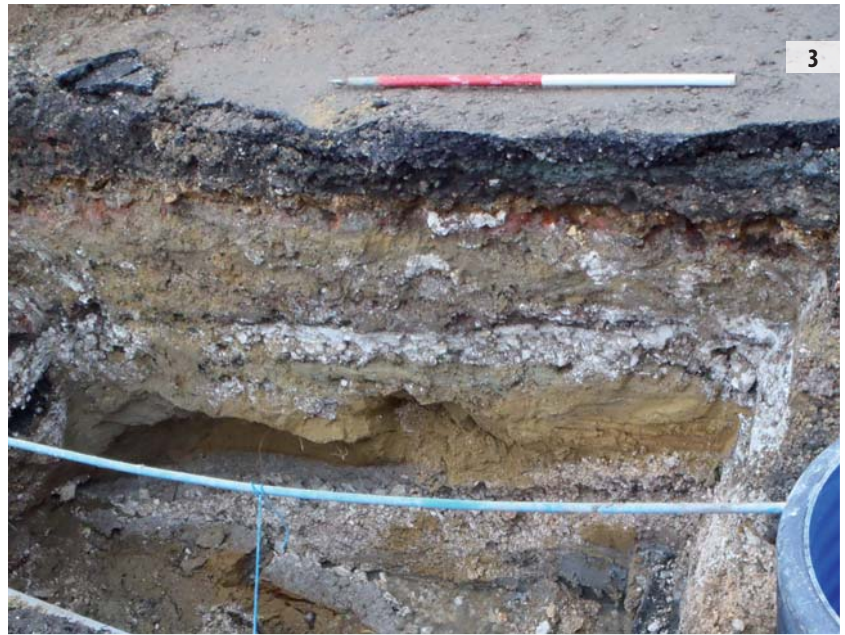
**HEADLAND
ARCHAEOLOGY**
 SOUTH & EAST Building 68C, Wrest Park
 Silsoe Bedfordshire MK45 4HS
 01525 861 578
 www.headlandarchaeology.com

ILLUS 1
Site location



ILLUS 2
Site plan showing areas monitored

ILLUS 3
W facing section of Construction Pit 1



ILLUS 4
Construction Pit 4, looking SE



ILLUS 5
Alluvium at SW end of Construction Pit 8



ILLUS 6

E-W beams, possibly part of riverside revetment, from
Construction Pit 1



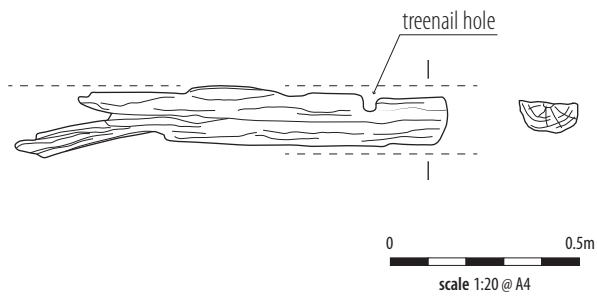
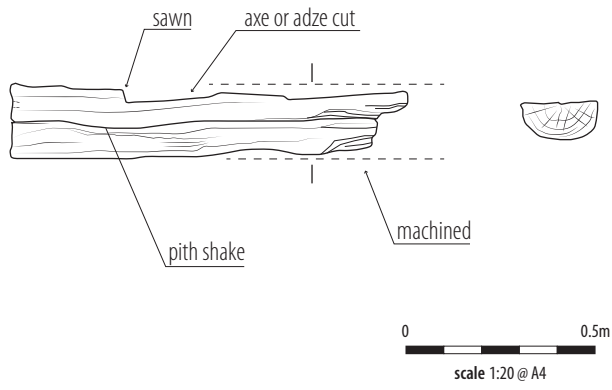


ILLUS 7
Oak log pile recovered from Construction Pit 1

ILLUS 8

Railway sleeper recovered from Construction Pit 1





ILLUS 9
Halved oak logs recovered from Construction Pit 1

ILLUS 10
Brick culvert [605] in Construction Pit 6, looking NW



ILLUS 11
Man-hole [804] in Construction Pit 8, looking NE

