



Archaeological
Research
Services Ltd

Watching Brief at Makeney Garage, Derbyshire



Makeney Garage, Makeney, Milford (facing south)

ARS Ltd Report 2007/33
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Executive Summary

An archaeological watching brief was undertaken on the 16th May 2007 at Makeney Garage, Makeney, Milford during excavation of a trench for the proposed redevelopment of the garage. The trench was to accommodate the footings for the extension of the garage for the provision of a dwelling. The site was originally in the vicinity of Makeney Old Hall, demolished in the 1890's and later the Coach House associated with the Hall which had been located across the road. No archaeological features, such as cellars or foundations, were identified during the watching brief.

1. Introduction

- 1.1 This report describes the archaeological investigation undertaken at Makeney Garage, Makeney, Milford on the 16th May 2007 by Alex Thornton of Archaeological Research Services Ltd, on behalf of Blue Line Homes Ltd. The investigation consisted of a watching brief upon the excavation of trenches dug in order to place footings into the ground for the extension to Makeney Garage. A watching brief was also maintained on an area of soil removal due to hydrocarbon contamination.

2. Location and Geology

- 2.1 Makeney Garage is located at SK 3513 4450 on Makeney Road in Makeney, near Milford, Derbyshire (Figs. 1 and 2). Makeney is approximately 8km north of Derby and 7km south-west of Ripley (Fig. 1). The site falls within both the Belper and Milford Conservation Area and the Derwent Valley Mills World Heritage Site.
- 2.2 The watching brief was undertaken at the site of Makeney Garage where the underlying geology is the carboniferous mudstone of the Chatsworth Grit group (BGS 1993).



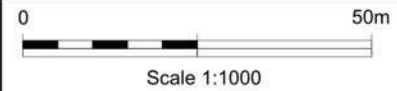
Figure 1. Location map of Makeney Garage.



Fig. 2
Location of Archaeological Watching Brief at Makeney
Garage

Drawn by: AT

On behalf of
Blue Line Homes Ltd



3. Background

- 3.1 The site at Makeney Garage was formally occupied by Makeney House, entered in the Sites and Monuments Record (SMR 17065) as 'Makeney Old Hall'. It was the home of the banker John Heath until his bankruptcy in 1779. Heath owned various properties in and around Makeney and Milford, including a water-powered bone and flint mill at Darley Abbey (Gifford 2007)
- 3.2 The house was purchased in 1818 by Anthony Radford Strutt (Eaton & Everett 2006) and was the family home until the building was demolished in the 1890's. The Strutt's made their fortune from Jedediah Strutt, the creator of the 'Derby Rib Machine' which was used to produce ribbed stockings. Strutt was also the business partner of Richard Arkwright and the founder of a silk stocking firm in Derby and cotton-spinning mills at Belper and Milford.
- 3.3 Makeney House was demolished when it was decided that the hall should be relocated across the road onto a larger site, currently in use as a hotel (<http://www.foliohotels.com/makeneyhall/>). The site was re-built as a farm house and coach house for the Hall. White's Directory (1857) notes that the Old Hall 'is noted for the birth-place of John Bradshaw, president of the council who passed sentence on Charles I., and son of Anthony Bradshaw, Esq.'. It is also noted from the Directory that Anthony Strutt relocated the Hall to its present position (White's Directory 1857).

4. Aims of the Project

- 4.1 The project was an archaeological watching brief and the aims of the project were as follows:
- To supervise the excavations of any services for the building or any other ground works on the site.
 - To ensure any archaeological features were fully recorded and interpreted.
 - To alert all interested parties to the possible destruction of archaeological features.

5. Method Statement

- 5.1 Planning permission was granted to Blue Line Homes Ltd for the redevelopment of Makeney Garage, Milford. The proposed plans consist of constructing an extension onto Makeney Garage and converting the building into a two-storey dwelling. The excavation of footings was required for the extension and an archaeological watching brief was requested by the Developmental Control Archaeologist for Derbyshire County Council for the duration of these works.
- 5.2 It was determined by the contractor that levelling the area for the footings would be required before excavation could begin. Therefore, the representative from Archaeological Research Services Ltd also undertook a watching brief on these initial groundworks. The trench was excavated by machine with a toothless ditching bucket to the required depth of 0.6 metres. At this depth, the naturally occurring substratum had been reached and the field archaeologist determined

that the excavations of the footings would not require any further watching brief to be undertaken.

- 5.3 Prior to initial ground leveling, an area of hydrocarbon contamination had to be removed. A representative from Ground Sense Ltd had previously evaluated the site with regard to the contamination. It was determined that a small area to the north-east of Makeney Garage was contaminated from an oil spillage and required removal. The soil removal was observed by the Archaeological Research Services Ltd representative in order to ensure any archaeological features were recorded and interpreted.
- 5.4 The machine, using a toothless digging bucket, excavated in level spits around the contaminated region to establish the size of the affected area (Figs. 3 and 4). This uncovered the remains of a sandstone floor possibly associated with the site's use as a Coach House for the new Makeney Hall. The floor was recorded and photographed and upon confirmation from the Development Control Archaeologist, was removed so that the contaminated area could be eliminated.



Fig. 3. Excavation of the contaminated area (facing east)



Fig. 4. Representative from Ground Sense Ltd.
evaluating the soil for hydrocarbon contamination (facing east)

6. Description of Archaeological Investigations

- 6.1 The sandstone flags (02) encountered during the excavation of the contaminated area were positioned in a relatively regular pattern (Fig. 5 and Appendix 1),

suggesting they may be the remains of a floor level. Although the floor level appeared to exist in two separate areas, it had probably been disturbed during previous work undertaken to establish the extent of the contamination. The flagged floor level continued in Trench 1, revealing the extent of the floor across the site. The flags varied in shape and size and are most likely to date from the 19th century when the site was the location of a Coach House. No archaeological finds were found in association with this context.



Fig. 5. Sandstone blocks reminiscent of a floor (scale = 0.25m) (facing north-east)

- 6.2 Upon deeper excavation of the contaminated area, a sandstone culvert (04) was uncovered (Fig. 6 and Appendix 1). The culvert walls were constructed from sandstone bricks, capped by large sandstone blocks. It measured 0.56m in width and 0.58m in depth and was exposed for a length of 4m. The bricks were not mortared into position. The culvert probably dates from the same period as the sandstone floor level although this could not be confirmed. As this area of the site was still contaminated the culvert had to be fully recorded and then demolished. Further excavation revealed the lower regions of the culvert were constructed of red brick (Figs. 7 and 8).



Fig. 6. Sandstone culvert uncovered during excavation of the hydrocarbon contamination (scale = 0.25m) (facing north-west).



Fig. 7. Excavation of the area of contamination and the sandstone culvert (facing east)



Fig. 8. Post-excavation of the sandstone culvert showing the ed brick footings (facing east)

- 6.3 On complete removal of the hydrocarbon contamination trench 1 was opened to the north-east of Makeney Garage, abutting the wall (Fig. 9 and 10). The trench was excavated in level spits of a maximum depth of 0.2m and measured 10.6m x 8.12m, with a depth of 0.58m.

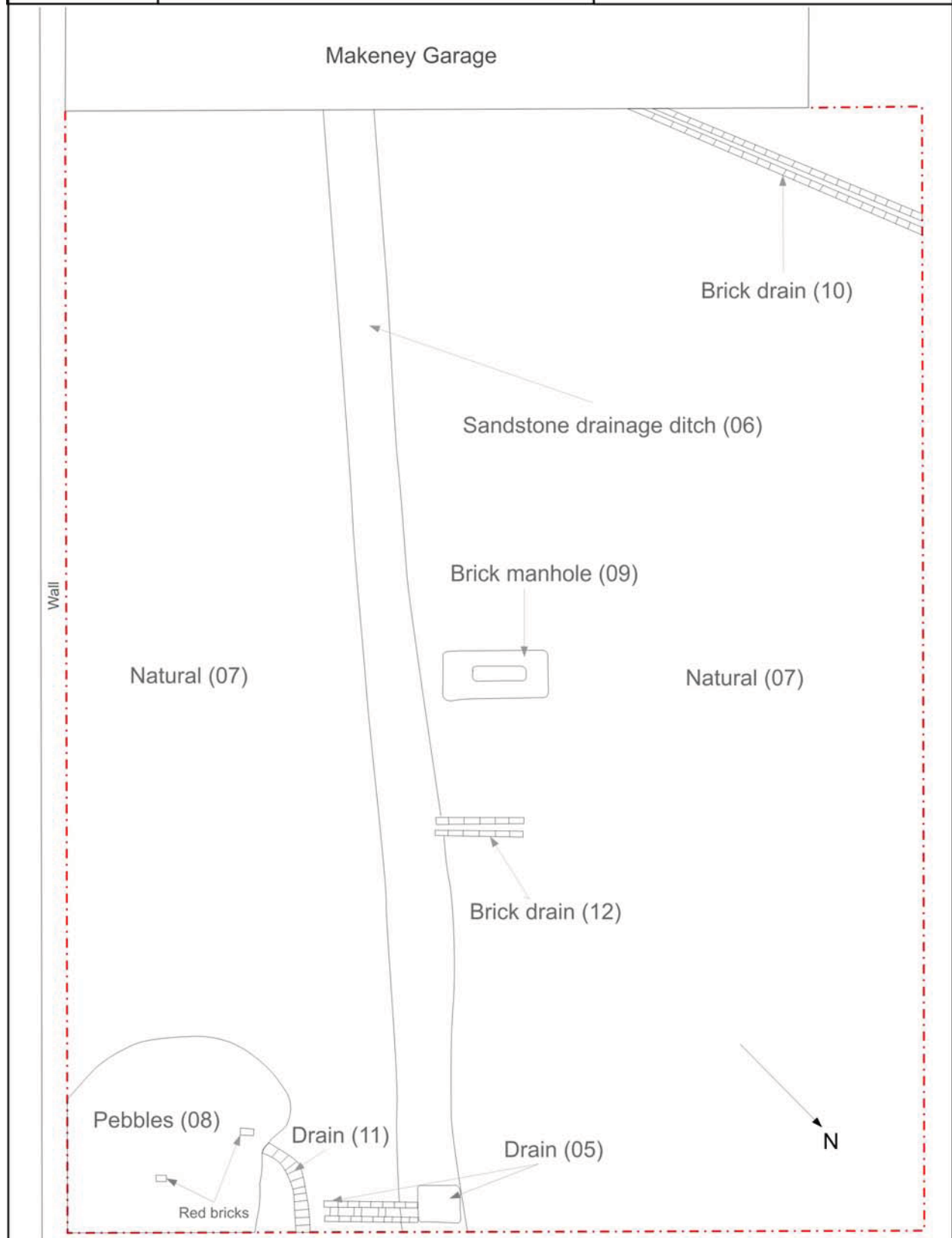
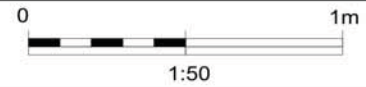


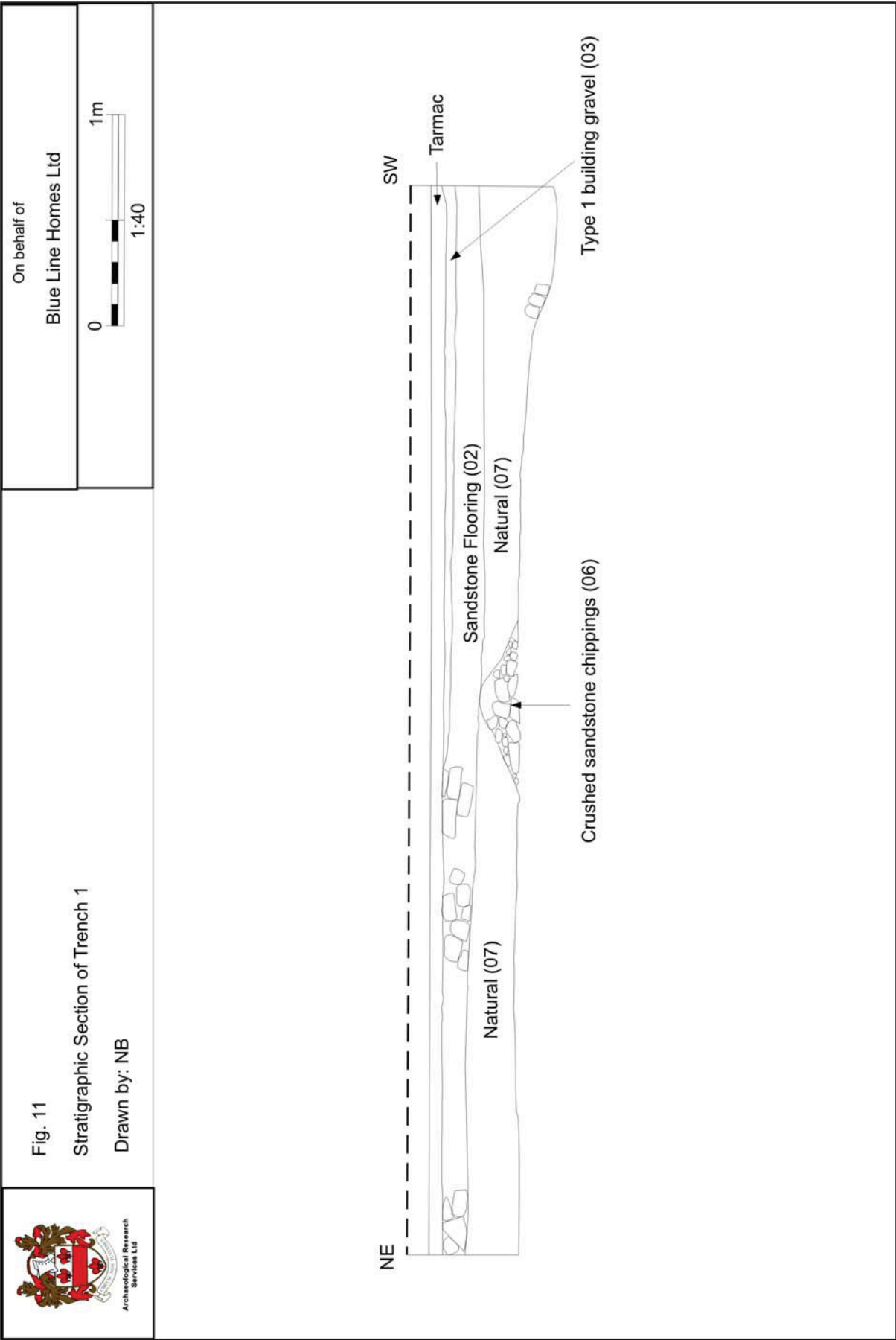
Fig. 9. Trench 1 which abuts Makeney Garage at the top of the photograph (scale = 2m) (facing south-west)



Fig. 10
Plan of Trench 1

On behalf of
Blue Line Homes Ltd





- 6.4 The majority of the stratigraphy within trench 1 remained the same throughout (Fig. 11). A shallow layer of tarmac (01) overlaid a silty clay matrix (02) which contained areas of the sandstone floor uncovered in the area of contamination (Figs. 5 and 12). This layer existed to a depth of 0.26m below which was the reddish brown (7.5YR 4/4) alluvial clay (07) (Figs. 12, 13 and Appendix 1). In certain areas across the trench the tarmac overlaid a deposit of type 1 building gravel (03) which was pale brown in colour (10YR 6/3) (Figs. 12 and 14). A deposit of small, dark yellowish brown (10YR 3/4), crushed sandstone chippings (06) had been cut into the alluvial clay, most likely to represent the cut of a previous drain (Figs. 12 and 13).

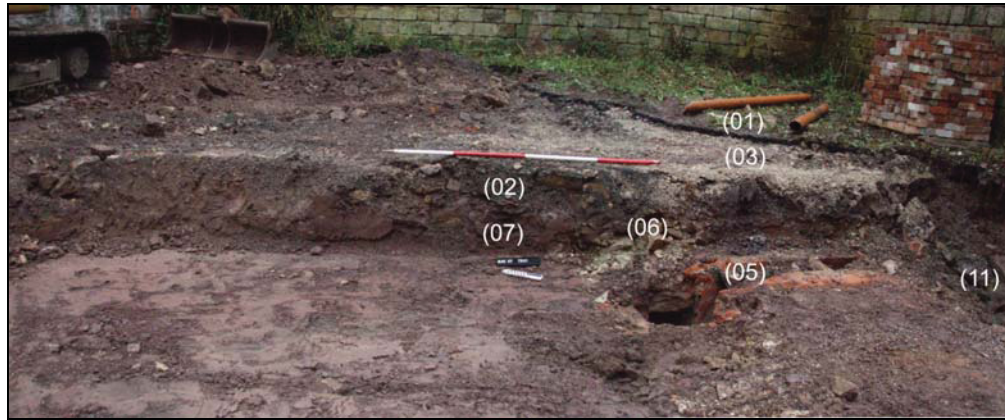


Fig. 12. Stratigraphic section of Trench 1 showing the contexts (scale = 2m) (facing north-east)



Fig. 13. Photograph showing the contexts within Trench 1 (scale = 2m) (facing south-west)

- 6.5 During the excavation of trench 1, four brick drains and a brick manhole cover were exposed. These features had all been cut into the alluvial clay (07) and existed within, or partially within, the area containing the sandstone floor (02). In

the north-east section of the trench (Figs. 9 and 14), a drain was uncovered which was built from red brick and capped by a sandstone block in the same fashion as with the sandstone culvert (04) found in the contaminated area. The similarity in construction material suggests that this brick drain was added to the site at a similar date to the sandstone culvert. The drain appears to lie within the trench of crushed sandstone chippings (06) suggesting that the trench may have been used as a drainage ditch. Furthermore, this drain seems to be in line with the sandstone culvert and is therefore possibly connected via the drainage ditch (06). A ceramic bottle which still contains its original cork was recovered from within the drain.



Fig. 14. Brick drain in the north-east section of Trench 1 (scale = 1m) (facing north-east)

- 6.6 In the east of the trench (Fig. 7) a red brick culvert was uncovered (Fig. 13). It was bedded in with a deposit of pebbles (08) and capped by a modern manhole, almost certainly at a later date. This culvert existed to a depth of at least 0.7m and was cut into the natural substratum (07).
- 6.7 In the west corner of trench 1 (Fig. 7) a modern red brick drain (10) which supplied Makeney Garage was exposed for an area of 2.62m (Fig. 13).
- 6.8 In the centre of trench 1, a rectangular red brick manhole cover (09) was excavated (Fig. 13). The manhole is most likely to be of modern construction as it was a different shape and style of construction to the other drains. Furthermore, a piece of modern metal pipe was recovered from this region.

7. Conclusions

- 7.1 In conclusion, the groundworks undertaken for the proposed Makeney Garage extension revealed some of the drainage system for the garage and possibly the previous 19th century Coach House. A flagged sandstone floor level was also uncovered, also possibly associated with the old Coach House.

8. Publicity, Confidentiality and Copyright

- 8.1 Any Publicity will be handled by the client.

- 8.2 Archaeological Research Services will retain the copyright of all documentary and photographic material under the Copyright, Designs and Patent Act, 1988.

9. Statement of Indemnity

- 9.1 All statements and opinions contained within this report arising from the works undertaken are offered in good faith and compiled according to professional standards. No responsibility can be accepted by the author/s of the report for any errors of fact or opinion resulting from data supplied by any third party, or for loss or other consequence arising from decisions or actions made upon the basis of facts or opinions expressed in any such report(s), howsoever such facts and opinions may have been derived.

10. Acknowledgements

- 10.1 Archaeological Research Services Ltd would like to thank the Derbyshire County Council for their support in this work. We are particularly grateful to the client Tim Tomlinson of Blue Line Homes Ltd for ensuring the smooth running of operations on the ground and Andy Myers for his advice and assistance.

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APPENDIX 1

