



Archaeological Investigations Ltd

# Land at Halesowen Street/Canal Road OLDBURY

*Archaeological evaluation.*



**January 2009**  
*Hereford Archaeology Series 813*

Land at  
Halesowen Street/Canal Road  
Oldbury,  
(NGR 39899 28950)

*Archaeological evaluation.*

---

**Text**

Dale Rouse, Kath Crooks

**Illustration**

Dave Doyle, Dale Rouse

**Site work**

D Rouse, D Doyle

---

**Contents**

1. Summary
  2. Introduction
  3. Historical background
  4. Aims and Objectives
  5. Methodology
  6. Results
  7. Discussion
  8. Conclusion.
  9. Bibliography
- Appendix 1. Site archive
- Appendix 2. The pottery and ceramic building material  
from Halesowen Street, Oldbury.  
By K.H. Crooks
- Table of contexts
- Figures & Plates
- 

**January 2009**

©Archaeological Investigations Ltd

This report is produced by

## ARCHAEOLOGICAL INVESTIGATIONS LTD

Manager : A Boucher BSc, MIFA

UNIT 1, PREMIER BUSINESS PARK,  
WESTFIELDS TRADING ESTATE,  
FARADAY ROAD, HEREFORD  
HR4 9NZ

Tel. (01432) 364901

Fax. (01432) 364900

for: Sandwell Metropolitan Borough Council  
Building Services and Urban Design  
Municipal Buildings  
Barrs Road  
Cradley Heath  
West Midlands  
B64 7JX

---

*Archaeological Investigations Ltd is a trading company wholly owned by the Hereford City and County Archaeological Trust Ltd, a registered charity founded in 1997 to further the work of the City of Hereford Archaeological Committee (founded in 1974) throughout Herefordshire. The Company maintains a core staff with a broad range of expertise, whilst also making extensive use of specialist contract personnel. Besides working on the buried archaeology of Hereford and the country in general, the Company specialises in geophysical survey, historical illustration and the archaeological recording and analysis of standing buildings. Work is usually on a commission basis on behalf of organisations such as English Heritage, the National Trust, and the Landmark Trust. The Company also accepts commissions from local authorities and private developers and provides specialist consultancy advice in relation to archaeology in the planning process and general environmental issues.*

---

### **Cover picture**

Looking across the site from Trench 4 towards the north-east, Trench 3 in the foreground, in the background is Christ Church which was founded in 1841.

---

Hereford City & County Archaeological Trust: Company Number 3283805  
Registered Charity Number 1060840  
Archaeological Investigations Ltd: Company Number 3356931 VAT 692 1750 23

# Land at Halesowen Street/Canal Road Oldbury, (NGR 39899 28950)

*Archaeological evaluation.*

---

## 1. Summary

---

*Archaeological Investigations Ltd was commissioned by Sandwell M.B.C to carry out an archaeological evaluation on open ground (the former bus station site) at the junction of Halesowen Street and Canal Street, close to Oldbury Town centre.*

*The proposed works were considered likely to affect a site of archaeological significance. In line with Planning Policy Guidelines Note 16, paragraphs 21, 22 (DoE 1990), the Planning Authority advised that archaeological work involving trial trenching would be required.*

*The main aim of the project was to locate and identify any surviving archaeological remains within the development area and to establish extent, date, nature and state of preservation of any archaeological deposits which may be present under the area proposed for development. Further aims were to assess the relative importance of any archaeological deposits and/or features found, and the potential loss of archaeological information caused by the development.*

*The findings from the archaeological evaluation showed that building remains (truncated by a concrete pile foundation) and garden soil related to terraced houses formerly fronting Halesowen Street survive close to the present frontage. Most of the site had been machine stripped to the level of the natural clay deposits as part of the preparations for the building of a bus station, but in the area of Trench 2 an earlier soil horizon survived in at least that area of the site. A sample was taken from this deposit but was not processed as there was no secure dating evidence associated with the deposit.*

*The earliest finds from the site included a piece of late medieval pottery (Cistercian ware 15<sup>th</sup>-16<sup>th</sup> c.) and a piece of Post-Medieval roof tile.*

---

---

## **2. Introduction**

---

Archaeological Investigations Ltd was commissioned by Sandwell M.B.C to carry out an archaeological evaluation on open ground (the former bus station site) at the junction of Halesowen Street and Canal Street, close to Oldbury Town centre (Figure 1).

The fieldwork was conducted between 16/12/08 and 18/12/09.

The archaeological evaluation was carried out because Sandwell MBC intended to develop the site for a new library and offices. No archaeological work had been previously undertaken on the site as far as is presently known.

The site was open ground comprising concrete surfaces and a soft area adjacent to the frontage where offices used to stand. It had formerly been used as a bus station.

The underlying drift geology consists mainly of washed gravel and sands overlying clay, the solid geology is Carboniferous Lower Westphalian (A and B) Productive Coal Measures.

The site is on fairly level ground, the temporary bench mark (T.B.M.1) lies at 149.378m O.D. at NGR 39899-28950. The site comprises an area of 0.3 hectares although the overall development affects an area of 0.8 acres.

---

---

## **3. Historical Background.**

---

### **General**

A large number of Roman coins have been discovered in the Oldbury area together with finds relating to the prehistoric period. The name Oldbury is Saxon in origin, but may suggest the presence of an Iron Age hillfort (SMR 4020). The town was referred to as Aldeberia in the 12<sup>th</sup> century. Oldbury may not have been extensively settled in either the medieval or early post-medieval period, with excavation (SMR 6205) to the east of the site discovering cultivation soils in the centre of the settlement. Intercutting pits, which would suggest pressure on space and more intensive settlement, were not present.

Oldbury was Crown property from 1102 to 1166 when it became part of the lands of Halesowen Abbey where it remained until the dissolution. It was first called a manor in 1557 when it was the property of Sir Robert Dudley. In 1633 it was owned by the Cornwallis family where it remained until the 18<sup>th</sup> century.

A chapel, built in 1529, stood near to Halesowen Street (See Figure 2, the 1845 Tithe map). This was demolished in 1840 when Oldbury became a separate parish and was replaced by Christ Church in Church Square (see Figure 4, 1904 Ordnance Survey map). Part of the graveyard for the 16<sup>th</sup> century chapel was later taken up by road improvements.

The area was rural with some small scale industries including nail making until the end of the 18<sup>th</sup> century. The availability of coal meant that mining and ironworking began at Rounds Green.

The first factory was at The Brades, and made industrial and farming tools. Parker's furnaces were erected to extract iron while coal mining spread south to the edge of the coalfield near Langley.

From the 1770s onwards new industries were encouraged into the area by the availability of transport on the canals. The backfilled Oldbury Loop (SMR 13157) runs to the east of the site. In addition to coal mining and ironworking (there were some 70 puddling furnaces at the industry's height) brickworks (See Figure 3, the 1883 First Edition Ordnance Survey map) were established and the spoilheaps for these added to the unhealthy nature of the area.

The increase in industry led to a massive and sudden increase in the population – between the censuses of 1841 and 1851 the population had increased from 7,374 to 11,640. A similar increase was also happening nationally. This sudden increase in population was not without problems.

The attention of the authorities was drawn to the squalid conditions of the new industrial towns mainly as a result of the cholera outbreak of 1831-2 which killed around 30,000 people. A Board of Health was set up and reported in 1844, the Public Health Act of 1848 (probably hastened as the result of a further cholera outbreak) was meant to rectify some of the problems.

However, little action seems to have been taken in Oldbury - the alarming rate of mortality led a number of prominent ratepayers to petition for an enquiry (in 1846 the death rate for male infants under the age of one year in west Bromwich was 1 in 20.4 and it seems unlikely that Oldbury would have been much different). The enquiry took place in November 1855, and found that there were no public lights and no sewerage or sewers and no proper privies. The roads had no gutters and were used as receptacles for rubbish as there was no refuse collection. There were no paved footpaths and many of the cellars were full of stagnant water. Inhabitants bought water from a carrier at the cost of 1s for 100gallons. All water for the town came from Rounds Green springs despite the fact that the act of 1848 had made boroughs responsible for water supply.

Sewerage was installed at a cost of £38,000 but the continuing growth of the population in the area meant that sanitation continued to be a problem.

A further part of the problem in Oldbury consisted of the lack of burial space. It was apparently possible to see coffins 'two or three deep' more or less floating in the graveyard at the old church and for this reason many people were carried to Halesowen, Dudley or West Bromwich for burial - with relays of pallbearers. The state of the graveyard led to victims of cholera – cholera first arrived in the town in 1832 - being buried at a new cholera cemetery in Parsonage Street. Many of the street names in the town such, as New Meeting House Street, attest to the late 18<sup>th</sup> and 19<sup>th</sup>-century date of the expansion.

As with physical health spiritual health was in peril, as a result of which Christ church was founded in 1841 with space for 1400 worshippers – half the places being free and the remainder subject to pew rent.

In 1894 the urban district of Oldbury and Warley was formed with a population of 23000. By 1927 this had risen to 43000 (see Figure 4, 1904 Ordnance Survey map).

### **Site specific**

The Tithe Map (1845, Figure 2) does not show Canal Street though a few buildings are present at the western end – on the site, at the junction of Canal Street and Halesowen Street. The history of Oldbury (1800-1875) (unpublished typescript Smethwick library) states that the cutting of the canal affected the development of the town, with land between Halesowen Street and Low Town being set out for building. The route of the canal meant that there was only one way into and out of Canal Street, as no bridge was built the canal cut off the route into Low Town. This meant that until the construction of Church Square – which had an opening into Canal Street there was only one way into and out of Canal Street, which gained the name ‘ by which it was best known for many years “Pudding Bag Street”’. The proper name of the street was restored to public usage once this route was built.

The subsequent maps show the site as built over, with the building on the corner shown on the 1904 map (Figure 4) as a public house.

---

---

## **4. Aims and Objectives**

---

The project was considered likely to produce results that would be of local or regional archaeological importance.

The main aim of the project was to establish the presence or absence of archaeological remains in the development area and provide further information on the development of Oldbury.

The main objectives of the work were to:

- a. Excavate five trenches of 10m length.
- b. Identify the date and nature of features being investigated.
- c. Assess survival, quality, condition and relative significance of any archaeological features, deposits and structures within the study area. Where budget allowed provide further analysis and dating of recovered materials.
- d. Produce a record of the features
- e. Produce a report
- f. Deposit an archive with the local museum.

---

---

## 5. Methodology

---

Research was carried out using the following repositories; Information held in the company library, Historic Environment Record, Local reference library.

Sources of background information included; Historic maps and published reports on nearby sites.

A site visit was made by Graham Eyre-Morgan.

The trench position and survey grid will be tied into features shown on the Ordnance Survey 1:2500 mapping using G.P.S.

A temporary site bench mark (TBM 1, temporary value of 100, Ordnance value of 149.378m O.D) was set up on site, the O.S value was gained using G.P.S.

Five 10m long trenches were excavated as per the proposed plan. All trenches were 1.6m wide. Trenches were positioned to provide full coverage of the site. Locations of trenches were adjusted to avoid undue disruption to identified services, obstacles within the site, or to target features that were not previously identified.

Trenches or excavation areas were reduced in shallow spits using a mechanical excavator with a toothless bucket under archaeological supervision.

The stratigraphic sequence of any archaeological deposits within a trench were to be ascertained by excavating later features (by machine where they are not archaeologically significant and this would not damage adjacent deposits/features) and recording/sampling the sections exposed. In the case that potential archaeological deposits extend over large areas of the trench/site, then sondages would have been excavated into the base of the excavation area using a combination of hand excavation and the mechanical excavator (where appropriate).

The base and sections of trenches were cleaned where this was necessary for the purpose of identifying archaeological deposits/features, or to assist with recording strata exposed. This was undertaken through a combination of the use of shovels, draw hoes, and pointing trowels.

An auger was available to excavating stratified deposits to minimise damage to the archaeological resource whilst at the same time ascertaining the vertical extent and nature of preservation (specifically environmental) within the archaeological stratigraphy. The auger has a head diameter of 100mm/25mm. The former is usually sufficient for the collection of soil samples, and identification of relatively fine changes in stratigraphy.

Archaeological features would have been recorded in plan at a scale of 1:20, trench plans were produced at 1:100.

Selected sections of trenches were drawn at 1:50. Archaeological features would have been drawn at 1:10 or 1:20.

35mm B&W negative and colour print film – archival record. Digital was also used – useful for emails/reports.



Films were processed as soon after having been exposed as practical to ensure that the record was adequate and complete.

A system of context records was kept and numbered independently by trench. Recording was in accordance with Archaeological Investigations Ltd's site manual. Registers were kept for context records, photographs, drawings, small finds and samples on site.

A general biological sample (c.20 litres) was taken from a deposit considered to be at least possibly Medieval and therefore potentially archaeologically valuable.

All finds were collected from deposits predating 1700. A selection of later finds were retained, the percentage sample dependant on the significance of the deposit and specialist advice (where relevant). All pottery, metal artefacts and stratified bone were retained for further analysis. Ceramic building material (bricks) was retained as a representative sample.

---

---

## 6. Results

---

A total of five trenches were excavated as part of the evaluation on the site of the former bus station at Halesowen Street, Oldbury (Figure 5). The trenches were excavated using a "rubber duck" mechanical excavator with a toothless bucket measuring 1.60m wide, after the concrete surface had been cut, broken up and removed. The depth of trenches did not exceed 1.65m (2m deep in the case of Trench 1) and the concrete that had been removed from the trenches was piled ready for removal from the site.

Two of the trenches (Trenches 1 and 2) were excavated in the earth and tarmac surfaced area of the former bus station offices, the other three (Trenches 3 to 5) were excavated through the concrete surface that surrounds the former office location.

### **Trench 1.** (Figure 6)

Trench 1 was excavated in the northern half of the area of the former offices, adjacent to the roadside. The trench was aligned roughly north-south, and measured almost 11m long. As the original position set out for this trench came straight down onto a modern concrete pile wall, it was moved slightly to the east. Modern ceramic drains close to the surface also restricted where the trench could be positioned.

The excavation of the trench revealed modern drains aligned west-east at both the north and south ends of the trench, and that the present ground level had been made up with earth and rubble, including substantial lumps of concrete. The made ground (101) measured up to 1.20m deep and overlay a truncated, north-south aligned brick wall [104], on the west side of which was fill material (106), suggesting that side of the wall was a backfilled cellar, no doubt part of the range of terraces that used to front onto Halesowen Road (See Figures 3 and 4).

The fill of the cellar had been cut by the concrete pile wall [105] (foundation for the bus station offices) and there was obvious disturbance to the buried remains of the terraced house that was a result of the piling. On the east side of the wall were remnants of buried topsoil mixed with the underlying natural clay (102) that contained modern finds, probably from garden soil from the terraced houses. Below this level (1.20-1.55m below present ground level) was natural yellow-brown clay, sand and washed gravel (103).

### **Trench 2. (Figure 7)**

Trench 2 was excavated in the southern half of the area of the former offices. The trench was aligned roughly east-west, and measured around 11.5m long. The upper 0.30m of the trench consisted of made ground and topsoil (200).

The concrete pile wall that had been observed in Trench 1 [105] crossed Trench 2 in its western half [208]. Along the west edge of the concrete pile wall and on the same alignment was a brick wall bonded with pinkish-grey flecked mortar [206]. The wall lay within a cut [207] and had no foundation.

The wall predated the concrete structure and appeared to have been re-used as revetment for the pour of the concrete. A modern ceramic drain crossed the trench to the east of the concrete pile close to the surface and aligned northeast-southwest.

Underlying the drain, and cut by both 207 and 208 (which both cut all the way down into the natural deposits) was a thin (0.10m) of sooty charcoal and ash waste (201), below which was 0.21m of re-deposited clay and mortar rubble (202). All these deposits appeared to relate to the demolition of the terraces and probably represent demolition and levelling of the site.

Underlying these deposits was dark brown garden soil up to 0.40m deep (203), containing modern finds and a single piece of Medieval (Cistercian ware) pottery. The garden soil overlay a thin buried topsoil horizon (204), grey-brown in colour, silty clay textured and around 0.10m deep. The deposit contained a few pieces of modern pottery, glass and a piece of clay pipe, which may have been intrusive from the layer above, or may have been at the interface between the two deposits. A single piece of Post-Medieval roof tile was found within the deposit. The level of the natural clay layer in Trench 2 (205) rose towards the west side of the site.

### **Trenches 3, 4 and 5. (Figure 8)**

The remaining three trenches were all excavated through the concreted areas of the former bus station. The contents of the trenches were virtually identical in composition.

The concrete surface (contexts 300, 400 and 500) over all three trenches averaged 0.20m thick and overlay a sub base consisting of two layers (each 0.10-0.15m thick) of scalpins separated by a polythene membrane (contexts 301, 401 and 501).

Underlying the scalpin sub base in all three trenches was a deep deposit (average between 0.55m-0.85m deep) of bright red building sand, used to fill and level the site (contexts 302, 402, 502).

The sand deposit overlay a few scrappy traces of dark soil and mixed rubbish that gave off an acrid, industrial/chemical smell when first exposed by the machine (contexts 303, 403 and 503). Exclusively modern finds were present within the deposit.

The majority of the deposit had been machine stripped off using a toothed bucket, as was evident from the toothed bucket marks cutting into the underlying natural clay layer (contexts 304, 404 and 504) in all three trenches. The deposits appeared to have been mechanically stripped in preparation for the construction of the bus station.

---

---

## **7. Discussion**

---

It appears from the results of the five evaluation trenches excavated that remains of terraced houses that formerly fronted onto Halesowen Street survive, though much truncated along the western edge of the site, and that some of the soil from gardens related to those buildings is also still present within the area of the former bus station offices.

In Trench 2 a single piece of Medieval pottery was found in garden soil (203) that related to the terraces. An earlier soil horizon (204) survives at least in the immediate area of Trench 2, though its extent and date is not known. The deposit contained a piece of Post-Medieval roof tile. It appears, given the apparently thorough stripping of the deposits from the site at the time of the construction of the bus station that this deposit is unlikely to survive over a very large area beyond Trench 2.

---

---

## **8. Conclusion**

---

In conclusion it appears that there is very little of archaeological significance surviving on the site, although it is possible that a few features could be present in the area around Trench 2, possibly below or within the earliest identified soil horizon (204).

The Cistercian ware pottery found in Trench 2 dated to the 15<sup>th</sup> or 16<sup>th</sup> centuries and along with the Post-Medieval tile from the earliest deposit (204), could imply that there was some activity on or near the site in the later Medieval, or Post-Medieval Period.

The aims of the project can be said to have been satisfied and the methods used were appropriate in this case.

---

---

## 9. Bibliography.

---

- Anon. 2008, "ARCHAEOLOGICAL EVALUATION, LAND AT HALESOWEN STREET/CANAL STREET, OLDBURY". *Sandwell Metropolitan Borough Council. (Brief)*.
- Anon. Undated, "The Borough of Oldbury, Worcestershire". *(Published by Oldbury Corporation)*.
- Boucher, A. 2008, "Land at Halesowen Street/Canal Street, Oldbury. Project 1341". *(Project design)*.
- Daniels, T. 2002, "Britain in photographs, Oldbury, Langley and Warley". *Sutton Publishing*.
- Round, J. Undated, "History of Oldbury 1800-1875, Talks and Recollections by John Round". *(Unpublished typescript in Smethwick Community History & Archives Service)*.

---

---

**Appendix 1. Site archive. (Site Code ODHSCS08)**

---

The site archive includes everything from the evaluation and will be held by Smethwick museum.

1. 2 context registers
2. 29 context sheets
3. 1 environmental sample register
4. 1 environmental sample (204, Sample 1)
5. 1 drawing register
6. 8 scale drawings on drafting film
7. 3 photographic registers
8. 3 x 35mm films, (2x black and white, 1x Colour)
9. 70 digital photographs
10. copy of the project design
11. copy of this report
12. 1 correspondence file
13. 3 Pages of site notebook notes including a page of sketch notes

---



---

**Appendix 2. The pottery and ceramic building material from Halesowen Street, Oldbury.**  
By K.H. Crooks

---

**Summary: the pottery and ceramic building material**

A total of eight sherds of pottery were recovered from the site at Halesowen Street in Oldbury. In addition three fragments of ceramic building material were recovered, with two pieces of drainpipe and a piece of linoleum. Bricks were sampled from contexts 206 and 104 - two examples from 104 and one from 206 (Plate 20). The material was all of later post medieval date, with the exception of a single sherd of 'Cistercian'-type ware, dating to the later medieval to early post-medieval period.

A brief summary of ceramic material from the site is given below (Table 1)

Table 1: Summary of ceramic material from the site at Halesowen Street, Oldbury

Context	weight	Description/comments
104	850g	Brick: mortar adhering; whitewash on one edge
104	1000g	Brick – mortar adhering – ridge 20mm wide on one of long edges. Whitewash on one face
203	8.9g	Machine made, yellow and brown decoration; probably a jug
203	2.8g	Machine made whiteware; wavy edge, plate or saucer
203	4.7g	Machine made whiteware, probably from a plate or saucer
203	9.6g	Stoneware; bowl or dish
203	1.4g	Cistercian-type ware – sandy fabric and black internal and external glaze. Cup?
203	34.2g	Flat ceramic roof tile; unglazed – sanded on one side
203/204	62.6	Flat ceramic tile- sanded one side. Probably roof tile but fairly thick (17mm).
206	1080g	Brick: mortar adhering. Shallow ridge 20mm wide on one of long edges
404	1.2g	Plain white china – cup or bowl
404	3.1g	Transfer printed ware, blue transfer – cup or bowl
404	3.2g	China – plate or saucer
404	113g	Flat ceramic sanded one side – roof tile

**The ceramic building material**

Introduction

Brick making has a long history in the woods of the West Midlands and was recorded as early as the 15<sup>th</sup> century in Yardley (Whitehead 1981). It seems likely that the early brick and tile makers combined this occupation with farming. However, the Industrial Revolution, with both the increasing demand for brick and the greater ease of transporting it by rail led to increasing mechanisation in the industry, though in remoter areas of the West Midlands small scale brick and tile making continued in the woodlands. A number of brickworks were established in the West Midlands during the 19<sup>th</sup> century (e.g. SMR13049 – MBL4918, and the Albion Brick and Tile Works SMR 13048 – MBL4917).

Description

Bricks were sampled from contexts 206 and 104 - two examples from 104 and one from 206 (Plate 20). The bricks were substantially intact but all had suffered a degree of damage.

They were of similar fabric, a red iron rich clay with occasional marbling of yellow less ferrous clay, suggesting that they originate in the same area.

The dimensions of the three bricks from the site were identical at 230mm long, by 110mm wide and 70mm deep – slightly larger than the present U.K. standard of 215mm by 102.5mm by 65mm (<http://en.wikipedia.org/wiki/Brick>). None was frogged. Two out of the three examples from the site, that from 206 and one of those from 104 had a shallow raised band 2cm wide along one face, considerably more pronounced on the specimen from context 104. It does not seem to have any structural use (the specimen from context 104 was whitewashed on the side with the ridge) making it likely that the ridge is an accidental result of the manufacturing or drying process.

Both bricks from context 104 had whitewash on one face, possibly suggesting use in a cellar or outhouse. The bonding material on all three bricks appeared more or less identical – a medium grey matrix with black and white (lime) flecks.

The bricks were of post-medieval date and, given the proximity of a number of brickworks to the site, were probably made locally. They seem likely to derive from the buildings shown on the earlier editions of the Ordnance Survey maps (Figs 4 and 5).

None of the fragments of roof tile was diagnostic. That recovered from the interface between layers 203 and 204, was fairly thick at 17mm but not sufficiently so to suggest another purpose. There was no evidence for glaze on any of the fragments from the site.

### **Discussion**

With the exception of the sherd of Cistercian-type ware the pottery and building material from the site suggests a date in the 19<sup>th</sup> or 20<sup>th</sup> centuries. Cistercian type ware is dated to the later 15<sup>th</sup> to 16<sup>th</sup> centuries and may have been produced locally – production waste including wasters and saggars of this ware was found during a watching brief close to the Oak House (SMR 8015 – MBL 3179) in nearby West Bromwich. The fragment from the site at Oldbury was recovered from a deposit which also contained a piece of a linoleum tile and modern machine made ceramics.

### **Conclusion**

Except for one sherd of pottery the ceramic material from the site dated to after the period when intensive occupation of the Oldbury area began. However, the amount of pottery even from this period was comparatively small and it is likely that much pottery was removed with other material from the site during stripping for the construction of the bus station.

### **References**

Sandwell Sites and Monuments Record

Whitehead, D, 1981, Brick and Tile making in the woodlands of the West Midlands in the 16<sup>th</sup> and 17<sup>th</sup> centuries, *Vernacular Architecture, Vol 12*

<http://en.wikipedia.org/wiki/Brick>

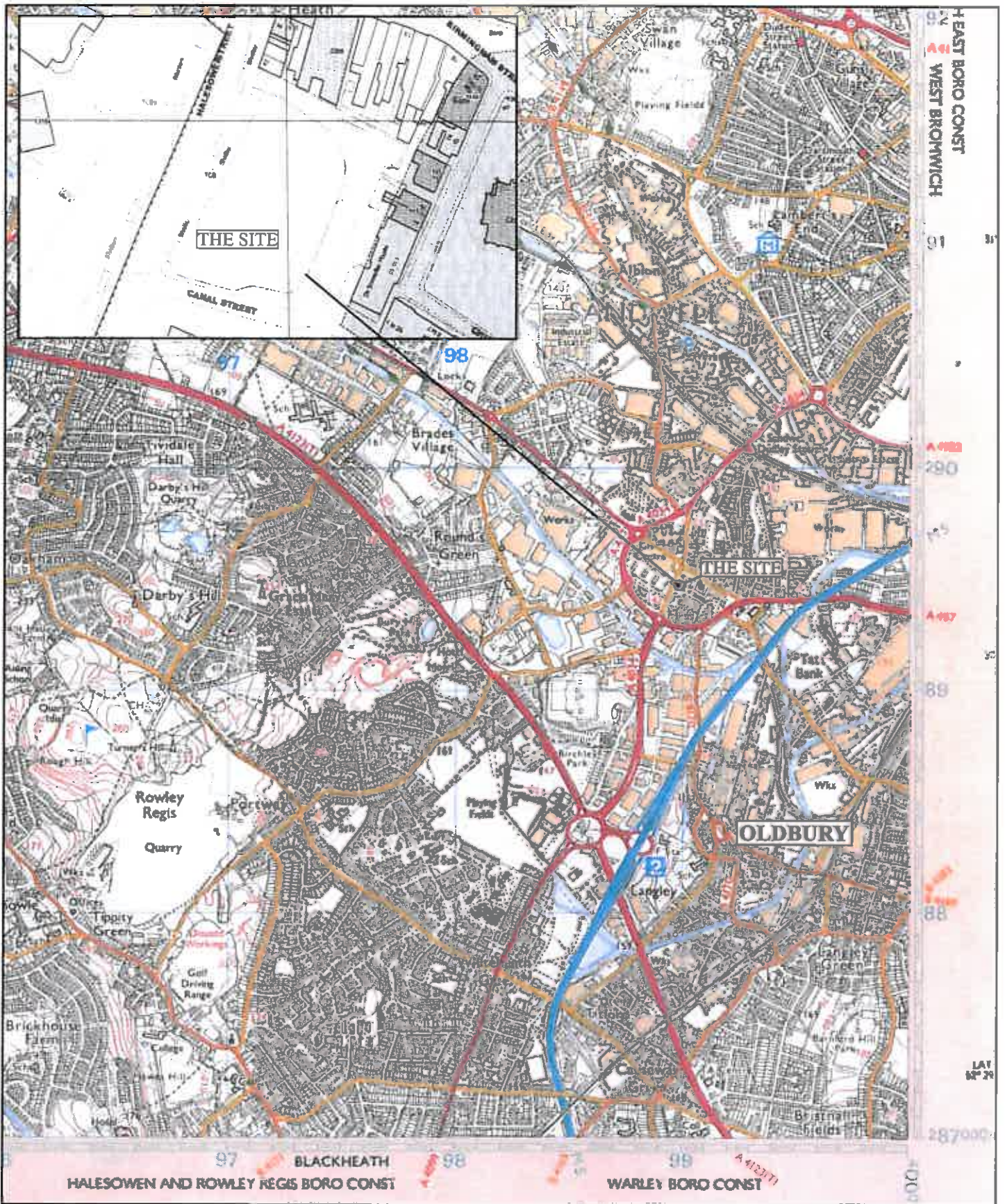
**Table of contexts, Halesowen Street/Canal Street, Oldbury.**

Context	Description	Dimensions	interpretation
100	Layer of gravel	10m +long by 0.05m deep	Modern surface covering over former bus station office location.
101	Layer, mixed deposit containing modern building rubble etc. Metal objects and large concrete bases are present within the deposit	10m+long by 3.30m wide by 1.20-1.65m deep	Made ground, made up from demolition of the former bus station offices.
102	Layer, yellow-brown clay. Gravel, sand and small cobbles at the top of the deposit.	0.47-0.55m deep	Natural deposits.
103	Layer, mid/dark brown silty clay, moderate/firm compaction.	0.55m deep	Natural deposits.
104	Linear structure, Red brick, bonded with greyish white, flecked mortar. Disturbed by the later building of the bus station.	0.25m wide by 0.20m	Building wall, back (east) cellar wall of one of the terraced houses that used to front Halesowen Street.
105	Linear structure, Concrete structure, substantial dimensions. Steel sheet piles still in situ on east side of the structure.	4.60m+ long by 0.70m wide by 2m+ deep	Concrete pile wall, part of the foundation for the former bus station. (same as 208).
106	Mixed deposit, contains lots of brick, rubble, mixed earth. Modern finds.	0.20m+ deep	Fill of the cellar of a terraced house (see 104), located on the west side of 104.
200	Modern mixed deposit, equals (101).	0.20-0.30m deep	Made ground, made up from demolition of the former bus station offices.
201	Deposit/layer, of black, sooty waste, silty sand texture.	0.10-0.30m deep	Levelling/demolition layer.
202	Layer, contains mortar, rubble and re-deposited yellow-brown clay.	N/A	Mixed levelling/demolition deposit.
203	Layer, dark brown sand and silty clay, mod-firm compaction, modern finds, except for a single piece of Cistercian ware pottery (15 <sup>th</sup> -16 <sup>th</sup> century).	0.40m deep	Garden soil, related to the terraced houses.
204	Layer, mid grey-brown silty clay. Mod-firm compaction, frequent charcoal flecks. Single piece of Post-Med roof tile in deposit.	0.10m deep	Earliest deposit located on the site. Buried soil horizon, possibly related to Post-med activity.
205	Layer, yellow-brown clay. Gravel, sand and small cobbles at the top of the deposit.	N/A	Natural deposits.



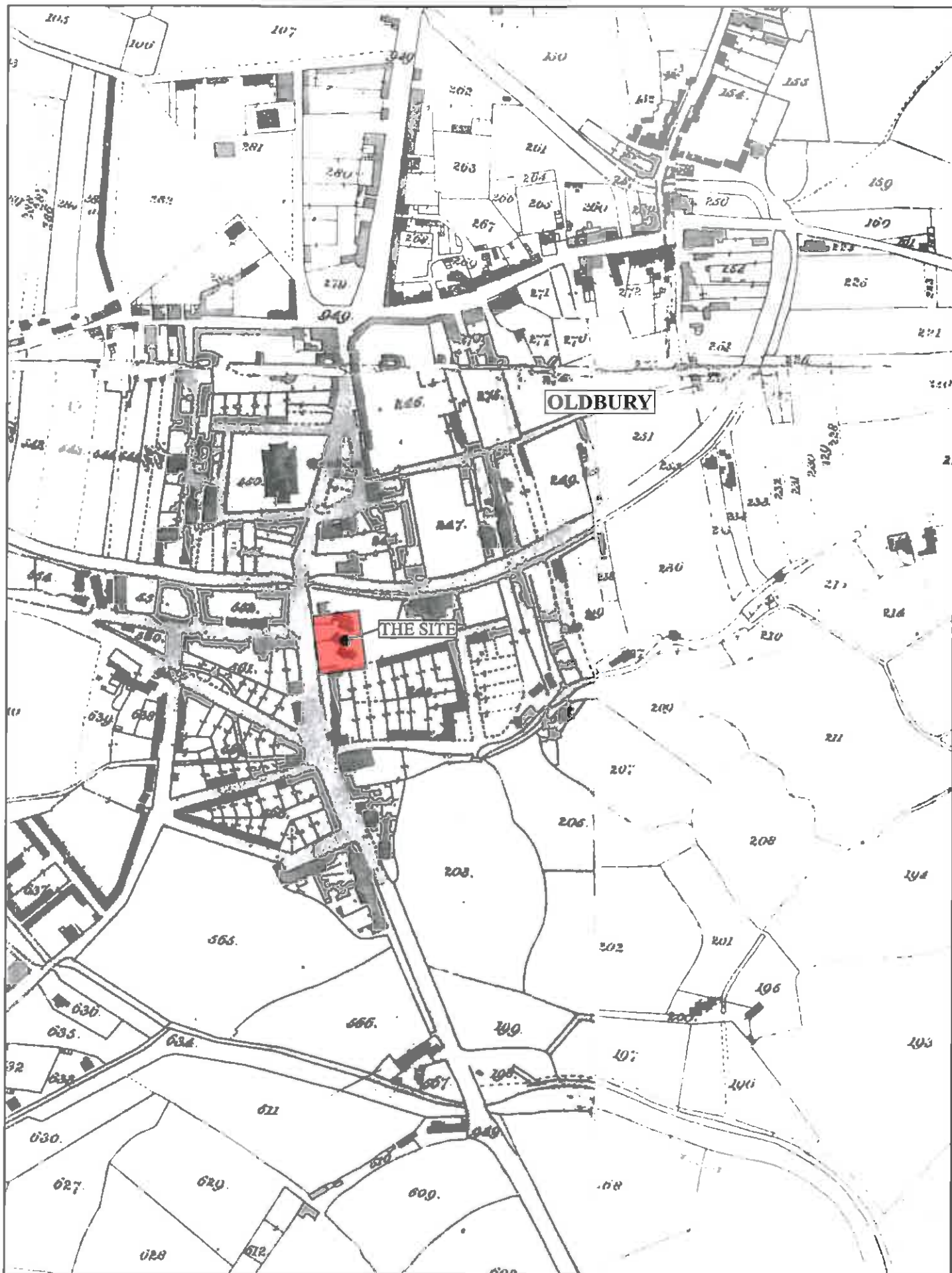
206	Linear structure, Red brick, bonded with pinkish-grey, flecked mortar. Possibly re-used as revetting wall for the concrete pile wall for the later building of the bus station.	1.60m+ long by 0.40m wide by 0.50m deep	Probably a garden/outbuilding wall or similar structure as the wall had no foundation.
207	Cut, containing wall (206). Shape truncated by later concrete pile wall. Appears to be steep sided, slightly "U" shaped to flat base.	0.60m wide by 0.65m deep	Linear wall trench.
208	Linear structure, Concrete structure, substantial dimensions. Steel sheet piles still in situ on east side of the structure.	0.70m wide by + deep	Concrete pile wall, part of the foundation for the former bus station. (same as 105.
300	Layer/surface, slab of modern concrete with rebar.	0.20m deep	Modern surface of former bus station.
301	Layer(s), two crushed grey stone layers with a polythene membrane between the layers. Both layers same depth.	0.20-0.30m deep	Modern scalpin sub base for concrete surface.
302	Layer, bright red sand, clean, mostly free of finds and inclusions.	0.55-0.85m deep	Layer of builders sand used to fill and level the site prior to construction of the bus station.
303	Layer, mixed deposit of black (industrial ?) waste and garden soil.	Varies-ave 0.15m deep	Traces of possible industrial waste (acid smell when first exposed), mixed with garden soil, mostly machine stripped from the site prior to construction of the bus station.
304	Layer, yellow-brown clay. Gravel, sand and small cobbles at the top of the deposit.	N/A	Natural deposits.
400	Layer/surface, slab of modern concrete with rebar.	0.20m deep	Modern surface of former bus station.
401	Layer(s), two crushed grey stone layers with a polythene membrane between the layers. Both layers same depth.	0.20-0.30m deep	Modern scalpin sub base for concrete surface.
402	Layer, bright red sand, clean, mostly free of finds and inclusions.	1m deep	Layer of builders sand used to fill and level the site prior to construction of the bus station.
403	Layer, mixed deposit of black (industrial ?) waste and garden soil.	0.60m deep max	Traces of possible industrial waste (acid smell when first exposed), mixed with garden soil, mostly machine stripped from the site prior to

			construction of the bus station.
404	Layer, yellow-brown clay. Gravel, sand and small cobbles at the top of the deposit.	N/A	Natural deposits.
500	Layer/surface, slab of modern concrete with rebar.	0.20m deep	Modern surface of former bus station.
501	Layer(s), two crushed grey stone layers with a polythene membrane between the layers. Both layers same depth.	0.30-0.40m deep	Modern scalpin sub base for concrete surface.
502	Layer, bright red sand, clean, mostly free of finds and inclusions.	0.60-0.70m deep	Layer of builders sand used to fill and level the site prior to construction of the bus station.
503	Layer, mixed deposit of black (industrial ?) waste and garden soil.	0.10m deep ave	Traces of possible industrial waste (acrid smell when first exposed), mixed with garden soil, mostly machine stripped from the site prior to construction of the bus station.
504	Layer, yellow-brown clay. Gravel, sand and small cobbles at the top of the deposit.	N/A	Natural deposits.



Reproduced from the Ordnance Survey mapping, by permission of the Controller of Her Majesty's Stationery Office. Crown Copyright Archaeological Investigations Ltd Unit 1 Premier Business Park, Faraday Road, Hereford HR4 9NZ. License No. AL100015338

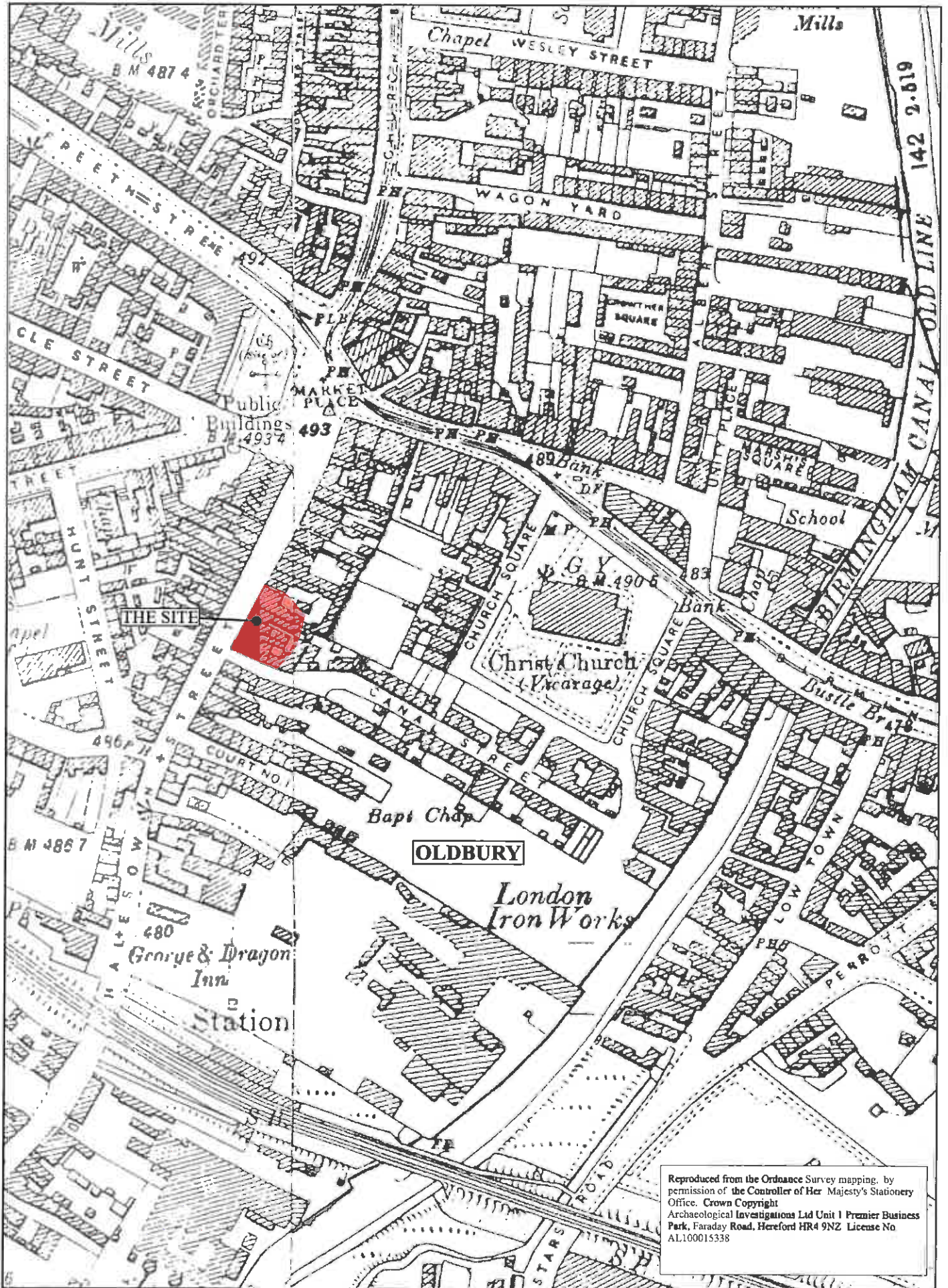
Location of the site of the former bus station in Oldbury, Figure 1.



Location of the site in Oldbury as it was shown on the 1845 Tithe map, Figure 2.



The site in Oldbury as it appears on the 1883 First Edition Ordnance Survey map, Figure 3.



Location of the site on the 1904 Ordnance Survey map, showing the terraced houses and a public house at the junction of Canal Street and Halesowen Street in Oldbury, Figure 4.



**Notes:**

**TBM**  
 398991.3165 E  
 289506.9331 N  
 149.378m

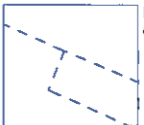
**Trench 1**  
 1 398988.222 E / 289491.217 N  
 2 398989.673 E / 289490.449 N  
 3 398984.480 E / 289482.858 N  
 4 398986.197 E / 289481.964 N  
 L1= 149.10m  
 L2= 149.14m

**Trench 2**  
 1 398977.126 E / 289477.474 N  
 2 398977.899 E / 289479.046 N  
 3 398987.473 E / 289472.440 N  
 4 398988.133 E / 289474.071 N  
 L1= 149.28m  
 L2= 148.65m

**Trench 3**  
 1 399004.261 E / 289474.883 N  
 2 399005.903 E / 289474.162 N  
 3 398999.506 E / 289464.566 N  
 4 399001.153 E / 289463.825 N  
 L1= 148.67m  
 L2= 148.63m

**Trench 4**  
 1 398974.211 E / 289461.005 N  
 2 398974.610 E / 289462.922 N  
 3 398983.585 E / 289456.544 N  
 4 398984.252 E / 289460.474 N  
 L1= 148.75m  
 L2= 148.63m

**Trench 5**  
 1 399002.750 E / 289494.998 N  
 2 399004.490 E / 289495.837 N  
 3 399068.611 E / 289486.650 N  
 4 399010.070 E / 289487.757 N  
 L1= 149.04m  
 L2= 148.85m

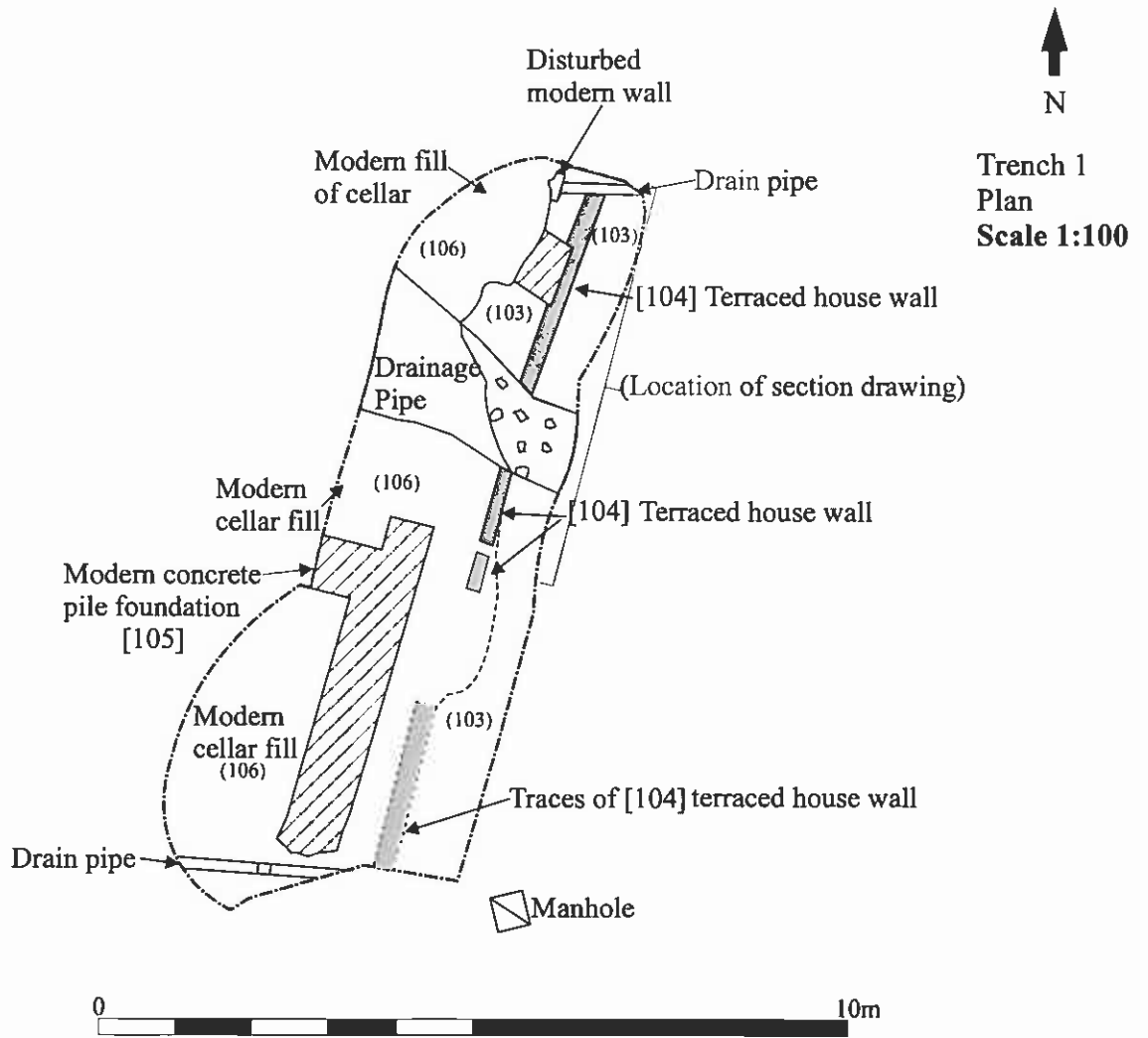
 Property boundaries based on the 1883 first edition OS map

Project 1341  
 Scale 1:400  
 Halesowen Street, Oldbury.

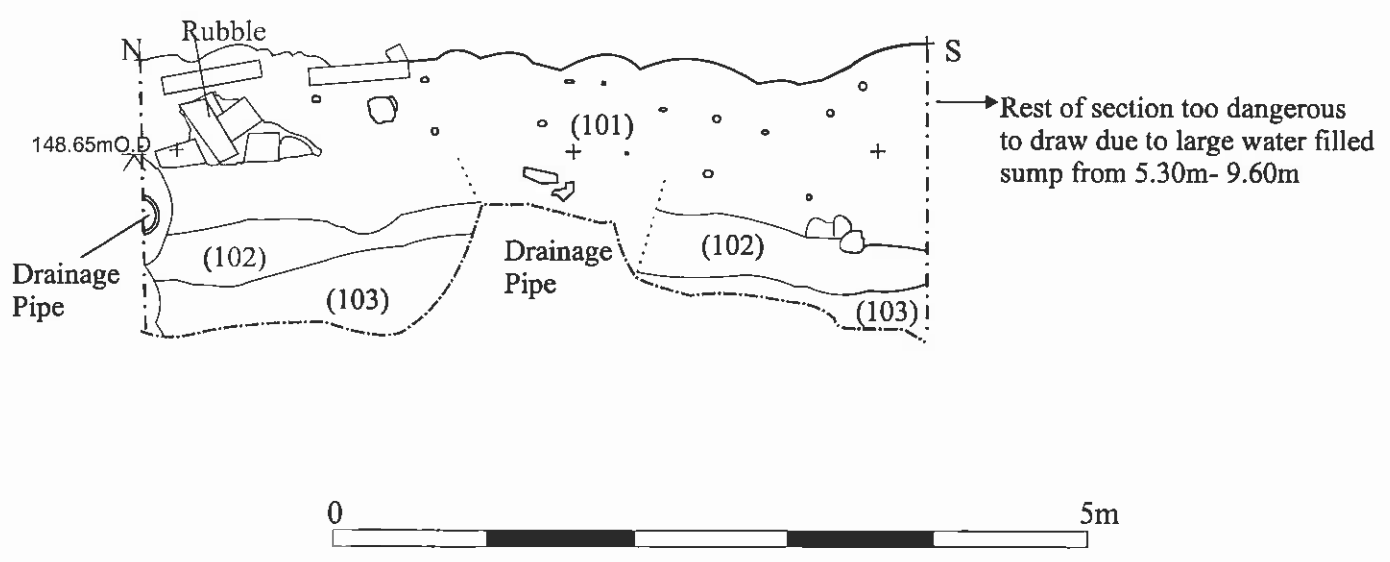
**Trench Location Plan.**



Figure 5

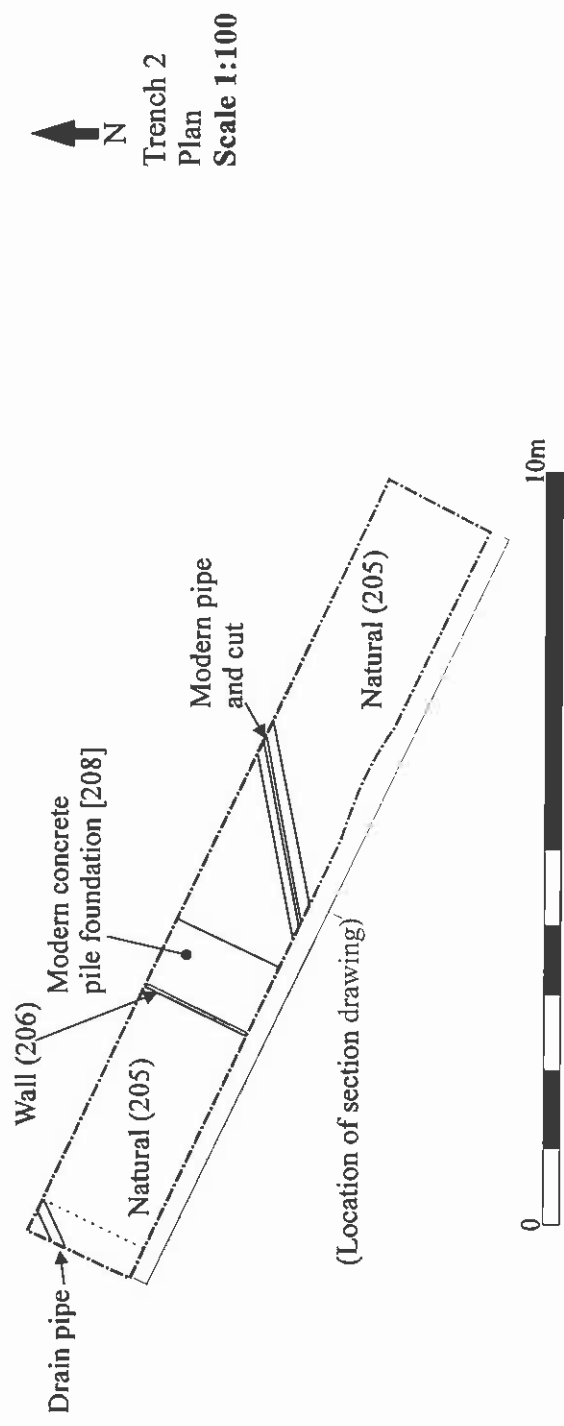


**Trench 1**  
**West facing section**  
**Scale 1:50**

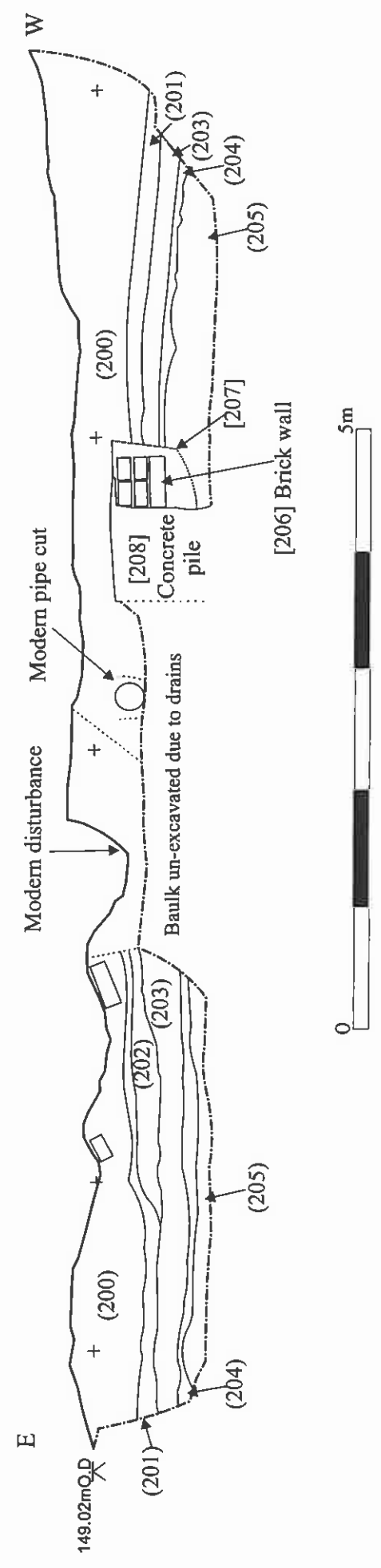


Plan and section of Trench 1, Figure 6.



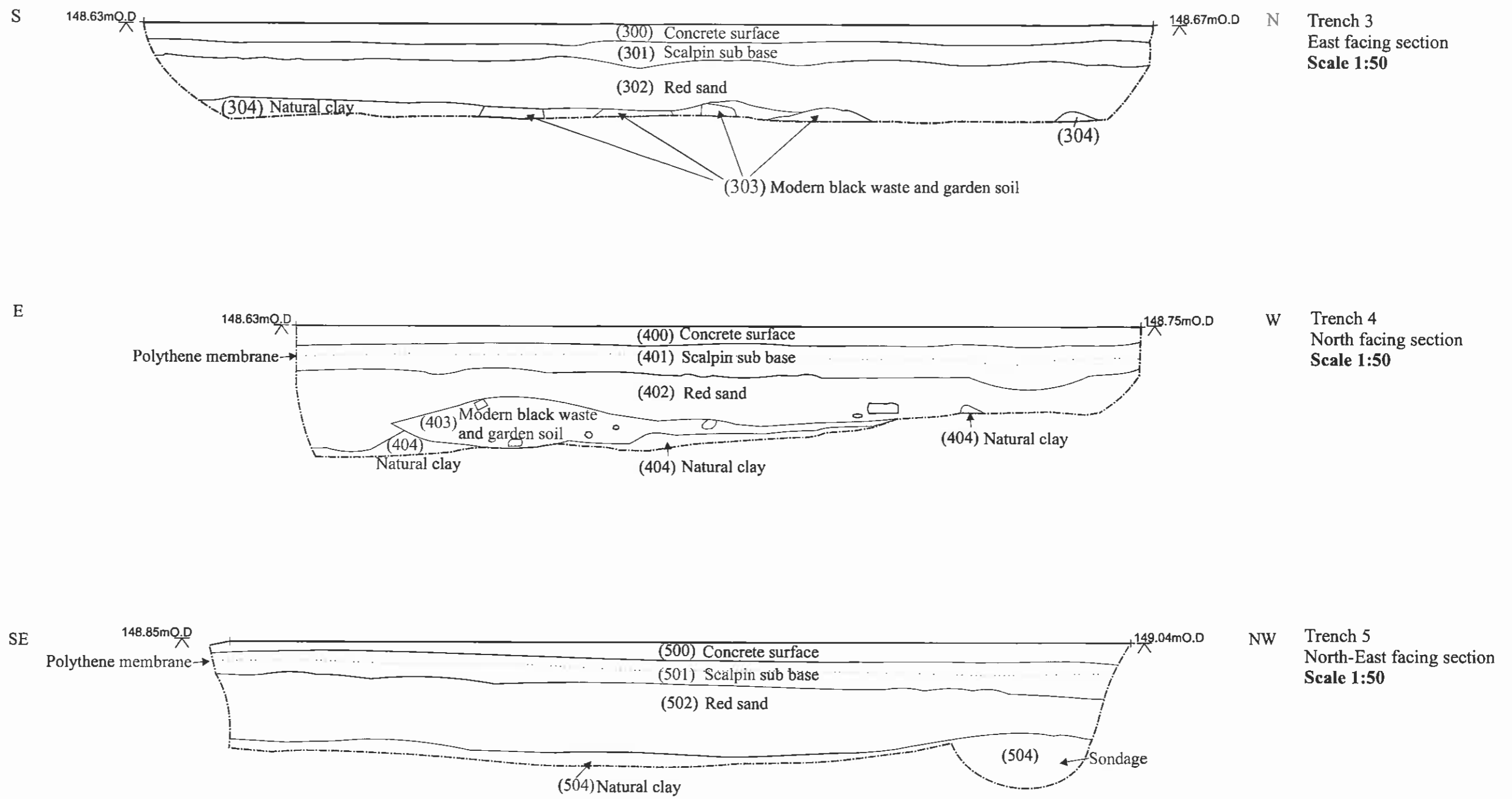


Trench 2  
 North facing section  
 Scale 1:50



Plan and section of Trench 2, Figure 7.

Plan and section of Trench 2, Figure 7.



Long sections of Trenches 3, 4 and 5 at 1:50 scale, Figure 8.



Plate 1, Trench 1, facing south, note the large concrete pile along the western edge of the trench.



Plate 2, Trench 1, facing east, cleaning up rubble in the north end of the trench before cleaning the section for drawing.



Plate 3, Trench 1, facing south-east, the concrete pile foundation is in the foreground, the area behind it is made ground overlying natural clay.



Plate 4, Trench 1 facing the north, the south end filled with water as soon as the trench was opened.



Plate 5, Trench 1, facing west. There are sheet pile impressions on this side of the concrete pile.



Plate 6, Trench 2, facing west from the eastern end of the trench.



Plate 7, Trench 2, facing north, shows the remains of the (collapsed) wall [206], concrete [208] and adjacent stratified deposits.



Plate 8, Trench 2 facing east, shows remains of the wall [206] and that the concrete pile [208] behind.



Plate 9, Trench 2, facing north, shows demolition/levelling layers overlying a layer of garden soil (203) which overlies a buried soil horizon (204).



Plate 10, Trench 3, facing south, shows the red sand deposit below the modern concrete surface and overlying the natural clay below.



Plate 11, Trench 3, facing west, shows the red sand overlying traces of (303), the black waste and garden soil over the natural clay (304) below.



Plate 12, Trench 3, facing west, shows the thickness of the concrete surface, the scalpin sub base and the underlying red building sand used to fill and level the site before the bus station was built.



Plate 13, Trench 4, facing west, looking towards the bus stops on Halesowen Street.



Plate 14, Trench 4, black marks cutting the natural yellow-brown clay, evidence that a machine with a toothed bucket was used to strip the site before the bus station was built.



Plate 15, Trench 4, facing south, shows the red sand below the modern concrete surface and scalpin sub base. Black waste and garden soil visible in section.



Plate 15, Trench 5, facing north-west towards Sainsburys and the bus stop.



Plate 17, Trench 5, facing south, shows thickness of the concrete, the scalpin sub base and the underlying red sand that was directly overlying natural clay.



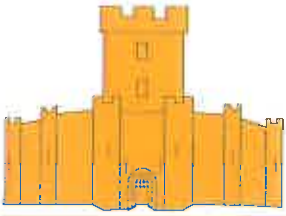
Plate 18, Trench 5, the north-west end facing south, shows the sondage into the natural clay layer.



Plate 19, Trench 2, all the trenches were fenced off each evening until they were backfilled, which was done for the health and safety of the general public.



Plate 20, Sample of bricks from (bottom 2 bricks) context [104] (cellar wall in Trench 1), and (top 1) context [206] (wall in Trench 2). The bricks are of the same fabric and appear to be from the same moulds.



Archaeological Investigations Ltd  
Unit 1. Premiere Business Park  
Westfields Trading Estate  
Faraday Road  
Hereford  
HR4 9NZ  
Tel:01432 364901  
[www.archaeologists.tv](http://www.archaeologists.tv)