

P21-126 and P21-155

NEWMA 2021.2



Military Road and 11a Military Road, Heddon-on-the-Wall, Northumberland

Archaeological Watching Brief

Headland Archaeology North West
RSK Group Ltd | Fourways House | 57 Hilton Street | Manchester M1 2EJ

for Northern Gas Networks

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PROJECT INFORMATION:

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PROJECT CODE	P21-126 and P21-155
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LOCAL AUTHORITY	Northumberland
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PROJECT TEAM:

PROJECT MANAGER	Emma Ings
AUTHOR	Emma Ings
FIELDWORK	Terry Frain
GRAPHICS	Emma Ings

PROJECT SUMMARY

Headland Archaeology (UK) Ltd was commissioned by Northern Gas Networks to undertake a programme of archaeological monitoring (archaeological watching brief) during groundworks for the laying of a replacement gas main along Military Road, Heddon-on-the-Wall, Northumberland (Headland Archaeology Project Code P21-126). The total length of pipe replaced measured 236m and was centred on NGR NZ 413020, 567095, post code NE15 0HA. During the monitoring of this work, the connection of a gas pipe, a total of 7m in length, from Military Road to 11a Military Road, Heddon-on-the-Wall, NE15 0BQ (centred on NGR NZ 413277, 567020) was also monitored (Headland Archaeology Project Code P21-155). It was agreed with Historic England that the two programmes of work would be monitored concurrently and reported on in a single combined report.

The sites are located within the boundary of the Scheduled Ancient Monument of Hadrian's Wall. Scheduled Monument Consent was granted for the replacement gas main along Military Road on the 13th May 2021 under Historic England reference S00241208, and for 11a Military Road on the 17th May 2021 under Historic England reference S00241329.

Archaeological monitoring along Military Road took place on the 17th to 25th May 2021, whilst monitoring of the works at 11a Military Road took place on the 28th May 2021.

Remains of Hadrian's Wall were encountered in three locations in the main excavations on Military Road. Following discussions between Historic England, Headland Archaeology, and Northern Gas Networks, it was decided to adapt the excavation methodology and lay the new gas pipe along the line of the old gas main.

These wall sections had been disturbed and truncated by activity of the 18th – 21st centuries, including possibly being re-used in the construction of the 18th century Military Road, which may have utilised the faced Wall stones as kerbstones. A probable cut and fill of archaeologist Frank Simpson's excavation trenches of 1930. Other disturbance was noted from modern services and the 20th century surfaces of Military Road.

During the monitoring of the works at 11a Military Road, modern surfaces and made ground were identified. These likely relate to the raising of the ground level at 11a Military Road as part of the construction of that property.

No finds were recovered, and no deposits were deemed suitable for environmental sampling.

Both projects have been entered into the OASIS database under a single entry, headland1-501952. A copy of this report shall be deposited with the Great North Museum: Hancock under accession number NEWMA 2021: 2 and the digital archive uploaded onto the ADS within 1 year of publication.

TABLE OF CONTENTS

1.	<u>INTRODUCTION</u>	1
	1.1. SITE LOCATION AND DESCRIPTION	1
	1.2. ARCHAEOLOGICAL BACKGROUND	2
	1.3. AIMS AND OBJECTIVES	4
2.	<u>METHODOLOGY</u>	4
	2.1. RECORDING	4
	2.2. SITE WORKS	4
3.	<u>RESULTS</u>	5
	3.1. MILITARY ROAD GAS MAIN REPLACEMENT (HEADLAND ARCHAEOLOGY PROJECT CODE P21-126)	5
	3.2. 11A MILITARY ROAD NEW GAS PIPE (HEADLAND ARCHAEOLOGY PROJECT CODE P21-155)	16
4.	<u>CONCLUSION</u>	19
5.	<u>REFERENCES</u>	19
6.	<u>APPENDICES</u>	20
	APPENDIX 1 SITE AND CONTEXT REGISTERS	20
	APPENDIX 2 OASIS ENTRY	25

LIST OF FIGURES

Figure 1: Site locations.....	vi
Figure 2: Overall site plan, showing trench locations.....	i
Figure 3: Pre-excavation view along Military Road from the eastern end of the excavation, looking west-south-west	5
Figure 4: Pre-excavation view along Military Road from the western end of the excavation, looking east-south-east	6
Figure 5: Context (105) in plan. Gas main [103] can be seen in the left of the frame. Looking west. 1 x 1m scale.....	7
Figure 6: Context (105) in plan, looking north. 2 x 1m scale.....	7
Figure 7: Plan of Trench 1. Coordinates of points: Point A = 413152.74, 567057.68; Point B = 413154.09, 567057.27; Point C = 413152.14, 567057.20; Point D = 413154.00, 567056.64.....	8
Figure 8: South-facing section of Trench 1	8
Figure 9: East-facing section of Trench 2, showing context (205) and possible overlying mortar in the right of the section and the cut of [203] in the left of the section. Looking west. 2 x 1m scale.....	9
Figure 10: Plan of Trench 2. Coordinates of points: Point E = 413053.73, 567085.07; Point F = 413057.67, 567083.98	10
Figure 11: Representative fragments of whinstone found in (304).....	11
Figure 12: Context (305) as exposed, looking west.....	12
Figure 13: Context (305) once cleaned, looking east-south-east. 2 x 1m scale	12
Figure 14: North-facing section of contexts (305 and (306)), looking south. 2 x 1m scale.....	12
Figure 16: Contexts (306) (foreground) and (305) background), looking east. 2 x 1m scale	13
Figure 15: Context (306), looking west. 1 x 1m scale	13

Figure 17 (left): Replacement gas pipe in Trench 3, demonstrating that it does not disturb the remains of the Wall. Looking west..... 13

Figure 18: Plan of Trench 3. Coordinates of points: Point G 413001.16, 567095.41; Point H = 412996.58, 567096.27 14

Figure 19: North facing section of Trench 3..... 14

Figure 20: Post-excavation shot of Trench 3 from its eastern end, looking west..... 15

Figure 21: Post-excavation shot of Trench 3 from its western end, looking east 15

Figure 22: Post-excavation shot of southern half of the new gas pipe, looking south..... 16

Figure 23: Post-excavation shot of northern half of the new gas pipe, looking east..... 16

Figure 24: Overall view of new gas pipe, looking south-west. 2 x 1m scale..... 17

Figure 25: Differentiation in height between the garden of 11a Military Road (left of frame) and Military Road (right of frame), looking west..... 17

Figure 26: Plan of Trench 4..... 18

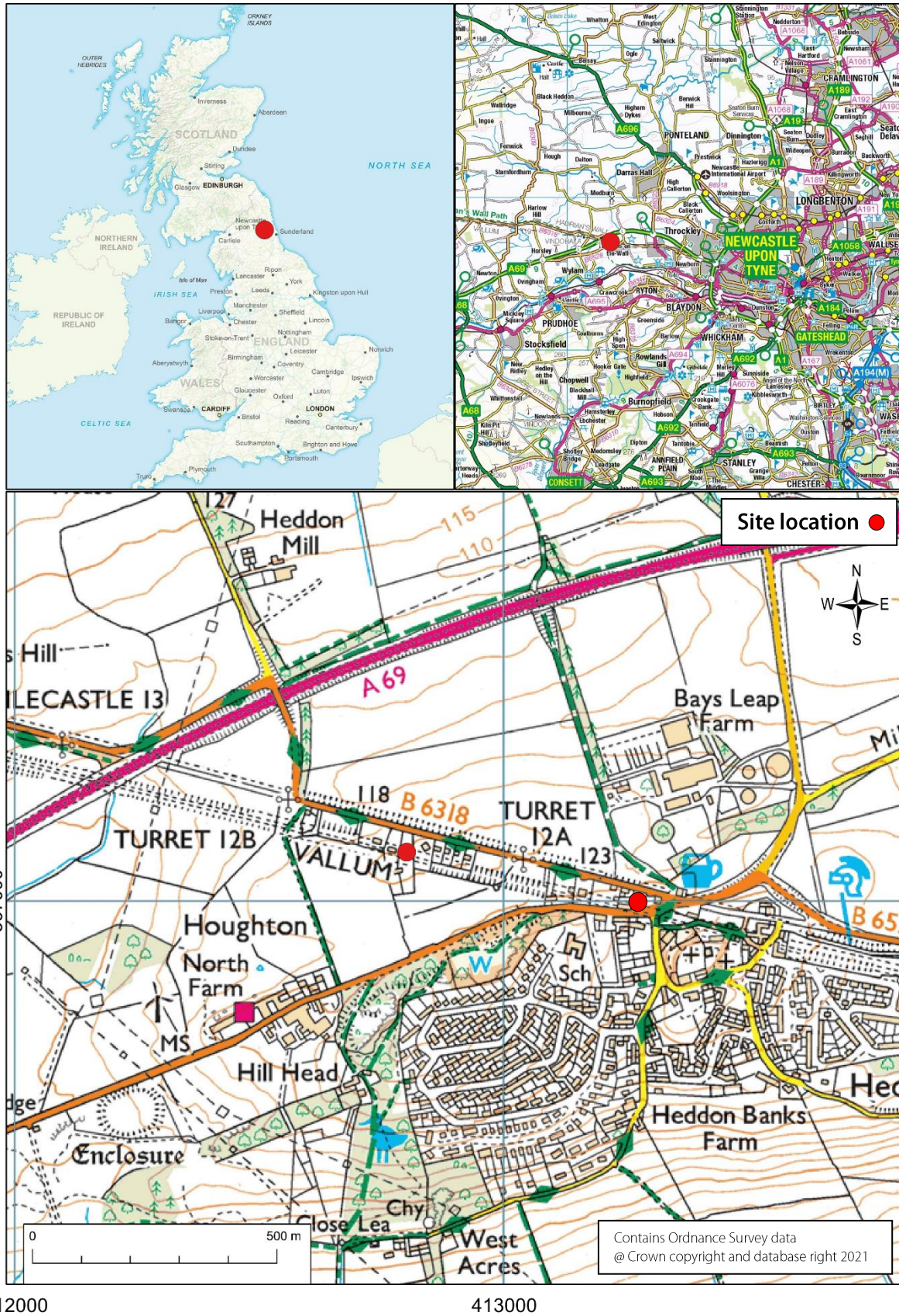


Figure 1: Site locations

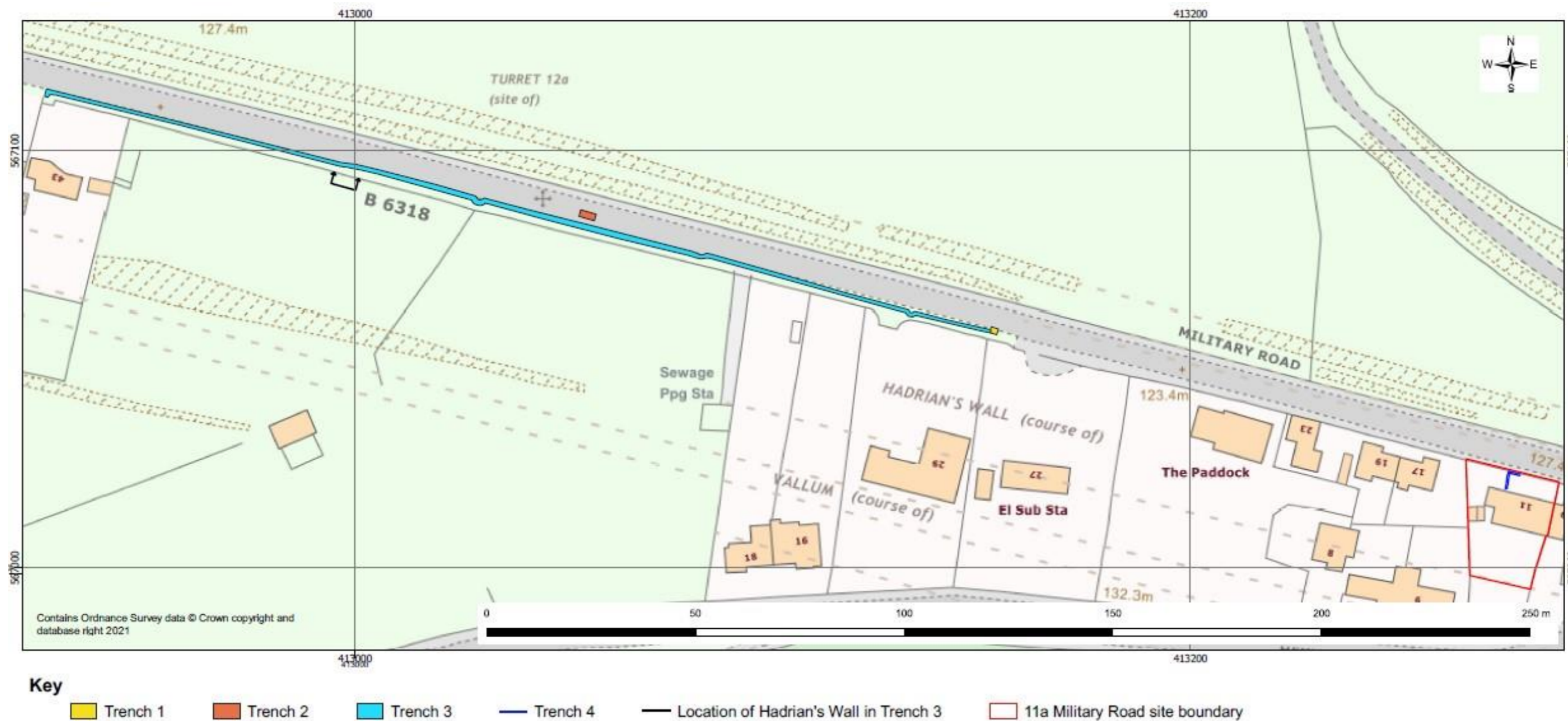


Figure 2: Overall site plan, showing trench locations

Military Road and 11a Military Road, Heddon-on-the-Wall, Northumberland

Archaeological Watching Brief

1. INTRODUCTION

Headland Archaeology (UK) Ltd was commissioned by Northern Gas Networks to undertake a programme of archaeological monitoring (archaeological watching brief) during groundworks for the laying of a replacement gas main along Military Road, Heddon-on-the-Wall, Northumberland (Headland Archaeology Project Code P21-126). The total length of pipe replaced measured 236m and was centred on NGR NZ 413020, 567095, post code NE15 0HA. During the monitoring of this work, the connection of a gas pipe, a total of 7m in length, from Military Road to 11a Military Road, Heddon-on-the-Wall, NE15 0BQ (centred on NGR NZ 413277, 567020) was also monitored (Headland Archaeology Project Code P21-155). It was agreed at the outset with the Historic England advisor, Don O'Meara, that the two programmes of work would be monitored concurrently and reported on in a single combined report (Don O'Meara pers. comm., 6th May 2021).

Although the works fell under permitted development, the sites are located within the boundary of the Scheduled Ancient Monument of Hadrian's Wall. Therefore, Scheduled Monument Consent for the works was sought from Historic England. Consent was granted for Military Road on the 13th May 2021 under Historic England reference S00241208, and for 11a Military Road on the 17th May 2021 under Historic England reference S00241329.

Archaeological monitoring along Military Road took place on the 17th to 25th May 2021, whilst monitoring of the works at 11a Military Road took place on the 28th May 2021.

The two projects have been entered onto the OASIS database under the single entry of headland1-501952. A copy of this report shall be deposited with the Great North Museum: Hancock under the accession number NEWMA 2021: 2 and the digital archive uploaded onto the ADS within 1 year of the date of this report.

1.1. SITE LOCATION AND DESCRIPTION

The site comprised a section of Military Road which lies between the properties of 29 and 45 Military Road. The site sits within the far north-western corner of the village of Heddon-on-the-Wall, Northumberland and is located c. 400m – 500m to the south of the A69. It is centred on NGR NZ 413020, 567095 (Figures 1 and 2).

The section of Military Road that was subject to the works is orientated west-north-west to east-south-east. It sits on a gentle slope, lying at around 127m AOD at its western end and 124m AOD at its eastern end. The site boundary measures 253m in length and 2m in width and has a total area of 1400m². It is bounded to the north and south by open fields, to the east by the properties of 16 and 18 Hexham Road, and to the west by the properties of 43 and 45 Military Road.

The bedrock within the site is Stainmore Formation - Sandstone and the Pennine Lower Coal Measures Formation - Mudstone, Siltstone and Sandstone. Stainmore Formation is identified by the BGS as a sedimentary bedrock, formed approximately 319 to 329 million years ago in the Carboniferous Period in an environment dominated by swamps, estuaries and deltas. It is described by the BGS as a 'cyclical repetition of sandstones, siltstones, mudstones, thin

limestones and some coals'. The Pennine Lower Coal Measures Formation - Mudstone, Siltstone and Sandstone, meanwhile, comprises sedimentary bedrock formed approximately 318 to 319 million years ago, also in the Carboniferous Period when the local environment was dominated by swamps, estuaries and deltas. It is described by the BGS as 'Interbedded grey mudstone, siltstone and pale grey sandstone, commonly with mudstones containing marine fossils in the lower part, and more numerous and thicker coal seams in the upper part'. Overlying the bedrock, the superficial geology of the area is Devensian Diamicton Till, formed up to 2 million years ago in the Quaternary Period when the local environment was dominated by ice age conditions.

1.2. ARCHAEOLOGICAL BACKGROUND

The following has been reproduced from the project's WSI (Headland Archaeology 2021):

There is little evidence of prehistoric activity within 500m of the site, although the HER records several prehistoric hut circles on Heddon Common in the approximate location of modern Trajan Walk and Remus Avenue. The site is now built over and there are no remains to be seen. These lay around 280m to the south-south-east of the proposed development site. Heddon-on-the-Wall is perhaps most famous for lying within the Scheduled Ancient Monument of Mile 12 of Hadrian's Wall, defined by Historic England as 'Hadrian's Wall and vallum from East Town House, Heddon-on-the-Wall to the A69 trunk road in wall mile 12' (NHLE list number: 1010617). Hadrian's Wall was constructed in the early 2nd century AD and, apart from a short period during the mid-2nd century when Emperor Antonine attempted to expand the Roman territory further to the north, was the northerly frontier of Roman occupation in Britain until the early 5th century.

The Wall spanned approximately 70 miles from the Cumbrian to the Northumbrian coastline and was constructed of stone and turf, being 8 – 10 Roman feet wide. For most of its length a substantial ditch on the northern side provided additional defence. Small fortlets known as milecastles were constructed at regularly spaced intervals of about a mile and served as controlled crossing points; between these were two equally spaced towers known as turrets. Together the milecastles and turrets provided bases from which the curtain wall could be watched and patrolled. Subsequently, forts were built along the wall, some overlying former milecastles and turrets. At the same time, the vallum, a broad flat-bottomed ditch flanked by a pair of linear banks, was also added to the defensive system to the south of the Wall. Its main function was to act as a barrier to restrict access to the Wall from the south, but it also served as the communication link between the forts. However, no

road per se was constructed on the vallum to enable this communication, and therefore a metalled track was later established in certain sections of its length. In the later 2nd century various sections of the wall were refurbished and a new linear road, the 'Military Way', was added. This road linked all elements of the Wall defence and ran from fort to fort within the area bounded by the Wall and the vallum.

Historic England's list entry for the Scheduled Ancient Monument of Wall Mile 12, in which the proposed development site is located, is as follows:

The monument includes a section of Hadrian's Wall, its external ditch and the vallum from Heddon-on-the-Wall in the east to the A69 trunk road in the west. This section of the Wall corridor has wide views overlooking the Tyne valley to the south and east, whereas to the north and west the ground rises gently giving a less extensive view. The Wall survives as a buried feature for the whole of this section of its course. It lies beneath the B6318 road except at the west end of this section where it continues in a field up to the A69 trunk road. The wall ditch survives as an earthwork for much of this section, to a maximum depth of 1.5m. The upcast counterscarp, usually known as the 'glacis', also survives up to 0.3m high in the area from north of the Three Tuns Inn to the north of North Lodge. Turret 12a survives as a buried feature below the B6318 road. The location of the turret is known from part excavation by Simpson in 1930. Turret 12b also survives as a buried feature below the B6318 road, probably extending south into the area of woodland to the west of North Lodge. As with turret 12a, part excavation by Simpson in 1930 located the site of this turret. The exact location of milecastle 12 has not yet been confirmed. In relation to the known sites of turret 12a and turret 11b, the site of milecastle 12 is expected to lie in the vicinity of Keeper's Cottage at Heddon-on-the-Wall. The vallum survives intermittently as an upstanding earthwork throughout this section. The mounds have been reduced by ploughing and the central ditch has silted up for most of its length. The south mound in this section reaches a maximum height of 1.5m, while the ditch has a maximum depth of 0.6m. The course of the Roman road known as the Military Way, which ran along the corridor between the Wall and the vallum linking the turrets, milecastles and forts, has not yet been confirmed in this section of the corridor. The property known as Tank House, North Lodge and an area east of it, between the Wall and the vallum, are totally excluded from the scheduling as are the houses and gardens numbered 43 to 57 Military Road. All other buildings, road surfaces and property and field boundaries within the protected area are excluded from the scheduling, but the ground beneath them is included.

Little intensive excavation has been carried on Mile 12 of the wall, and for the most part its survival rate, form, and construction history is known from modern planning-related development. However, Turrets 12a

and 12b, thought to be located within the modern Military Road and in the approximate centre of the line of the replacement gas main, were examined and recorded during excavations carried out in 1930 by F. G. Simpson (Simpson 1931). The results are described thus:

The Turrets - Nos. 12a and b could not be excavated completely. At 12a the walls were reduced to ground level beyond the edge of the roadway, and 12b lay largely below the road, which at that point was too narrow for the whole of the interior to be opened up. At none of the turrets was the north face of the Wall exposed, but a thickness of 9 feet 3 inches was measured between 12b and [turret] 13, and again between [turrets] 13 and 13a, and may be taken as the standard for this sector. The three turrets are nearly uniform in plan, their external measurement being the normal 20 Roman feet square: a new feature in turret construction is the finished thickness of 4 feet throughout the side walls, as opposed to the normal thickness of 3 feet or thereabouts. One turret (29a, Blackcarts) has walls 3 feet 7 inches thick; otherwise 3 feet 3 inches is the greatest thickness. The doorways are in the normal position; the jambs have not been faced with stone slabs such as were found at 26b. In 12b and 13a rectangular platforms occupied the south side of the interior; 12a was too badly robbed for any trace of a platform to survive. The masonry of all three turrets is of similar quality, roughly finished, but solidly built with an abundance of good mortar.

Simpson 1931, 322-324

Several archaeological investigations have been carried out within 500m of the site during the last two decades. Surprisingly, Roman remains have been rare, with no in situ remains of that date found during works at 29, 45 and 57 Military Road, and only demolition material and cobbled areas recorded during an evaluation at 23 Military Road (ARS 2011; Cooper, Dougherty, and Henderson 2017; Garrett 2005; Proctor 2004; PCA 2005). In addition to these, watching briefs have been carried out at 47 Military Road (TWM 2002), 23 Military Road (ASDU 2008), and 45 Military Road (AAG Archaeology 2017), but although listed as events on Keys to the Past, could not be found online. The Historic Environment Record also records the findspots of several Roman artefacts within 500m of the site. These include a Roman carved stone, found in 1932, around 300m to the west-north-west of the site; the remains of a Roman drain roughly 500m to its west-north-west; and two reused carved stones found in 1807, approximately 50m – 100m to the east of the site's eastern end.

Settlement at Heddon-on-the-Wall continued into the Medieval period, and the Church of St. Andrew is recorded as being the place of baptism in 653 AD for Peada, prince of the Middle Angles, and Sigebert, King of

the East Saxons, by Bede (Cawallader and Bates 1886, 244). Indeed, some Anglo-Saxon masonry lies within the fabric of the church today, as detailed in its Historic England list entry (NHLE list number 1042770), as well as pieces of carved Roman stone. The church lies approximately 200m south-east of the site and is a Grade I listed building.

Although Heddon-on-the-Wall was not included in the Domesday Survey of 1086, documentary sources attest that it and five surrounding townships (East Heddon, West Heddon, Houghton, Whitchester, and Eachwick) were granted by Henry I to Hugh de Bolbec in the early 12th century. Together, this land became known as the Bolbec Barony (Cawallader and Bates 1886, 245). In the mid-14th century, its lands were divided between two co-heiresses, with the manor of Heddon falling to Philippa, from whom it descended through the Dacres to the Howards, Earls of Carlisle (Cawallader and Bates 1886, 252).

Between 100-250m to the south-east of the site, recent excavations uncovered medieval industrial waste deposits within the Vallum ditch. Analysis and dating of the deposits showed that the ditch had been a large open feature between the 10th and 11th centuries and had been used to dump both domestic and industrial waste. The deposits showed that grain processing and possibly metalworking were being carried out nearby and that Heddon-on-the-Wall was a settlement of some importance in the early medieval period.

Heddon-on-the-Wall was an important mining location from at least the early 17th century onwards, and collieries and quarries were in use in the area until the 19th century. In 1717, the parish of Heddon-on-the-Wall was subject to inclosure of common lands and in 1752 the Military Road was constructed. During the early 19th century, services such as Poor Houses, schools and the railway came to Heddon-on-the-Wall, and the village underwent some development and expansion during the 19th and 20th centuries. Several of the buildings dating to this period of expansion are recorded under the HER as Grade II listed buildings. Those located within 500m of the site include: South Houghton farmhouse; North Noughton farmhouse; Merci Beaucoup cottage; No. 2 Cottage; and the school building, garden wall, gate, and railings of the Old Schoolhouse. These listed buildings form a small cluster roughly 400m to the south-west of the site. Bays Leap farmstead, around 250m to the north-east of the site, first appears on maps during this period.

Readily available historic maps depict the site as undeveloped farmland from at least the mid-19th century until 1951, when 43 – 57, 16 and 18 Hexham Road, and 17 – 29 Military Road were constructed. Maps produced from 1859 until at least 1980 depict the line of the vallum, indicating that this earthwork was still visible during that period.

1.3. AIMS AND OBJECTIVES

The purpose of the investigation was to identify and assess the significance of any element of the historic environment that may have been affected by the utilities works. This would be achieved by determining and understanding the nature, function and character of any remains on the site, in their cultural and environmental setting.

More specifically, the aims of the investigation included:

- to ensure that archaeological monitoring was undertaken on all aspects of the groundworks associated with the scheme;
- to ensure the adequate recording of any archaeological remains that were revealed by the scheme;
- to ensure the analysis, conservation and long-term storage of any artefactual/ecofactual material recovered from the site;
- to integrate the results of the works into the wider historic and archaeological context of the landscape; and
- to ensure that an accurate and comprehensive record and report of any archaeological deposits found during works was produced and disseminated to the appropriate organisations.

2. METHODOLOGY

Archaeological monitoring was carried out in accordance with the two projects' WSIs (Headland Archaeology 2021a and b) and was undertaken according to the standards set out in the relevant Chartered Institute for Archaeologists 'Codes of Conduct' and 'Standards and Guidance' documents (CIfA 2019; CIfA 2020).

All potentially significant features and deposits were cleaned by hand, planned and recorded in accordance with CIfA standards and guidance (CIfA 2020).

2.1. RECORDING

Contexts were given unique numbers and all recording was undertaken on *pro forma* record sheets that conform to accepted archaeological standards. All stratigraphic relationships were recorded.

Within this report, deposits are shown in round brackets and cuts in square brackets.

An overall location and site plan at appropriate scales is shown in Figures 1 and 2. Detailed plans of each trench and trench sections are shown in Figures 7, 10, 18, 19, and 26.

A photographic record was taken, and selected images are presented in Figures 3 – 6, 8, 9, 11 – 17, and 20 – 25.

Trench, context, drawing, and photographic registers are presented in Appendix 1.

2.2. SITE WORKS

Monitoring of the gas main replacement along Military Road was carried out between the 17th to the 25th May 2021. Monitoring of the new gas pipe trench at 11a Military Road was carried out on the 28th May 2021.

The original methodology for the laying of the replacement gas main was to feed it through small pilot trenches along the length of its planned route. However, remains of Hadrian's Wall were encountered in both the first pilot trench (Trench 1) and the second (Trench 2) on May 17th. Works were halted and these remains fully exposed by hand by the attending archaeologist. Don O'Meara of Historic England was immediately informed and carried out a site inspection later that day. Following discussions between Historic England, Headland Archaeology, and Northern Gas Networks, it was decided to adapt the excavation methodology and lay the new gas pipe along the line of the old gas main. During the excavation of the old gas main trench (Trench 3), a short section of Wall, which had been disturbed during the installation of the old main, was exposed and recorded.

Trench 1 measured 1.8m x 1.4m x 1.7m, Trench 2 measured 3.85m x 1.65m x 1.1m, and the remainder of the replacement gas main trench (Trench 3) measured 233m x 0.8m – 1.3m x 1.7m (Figures 2, 7, 10, 18 and 19). All of the trenches were orientated west-north-west to east-south-east and were located along the southern side of Military Road.

The excavation of the new gas pipe trench at 11a Military Road, Trench 4, was undertaken by hand. The L-shaped trench measured 7m x 0.3m and 0.4m x 1m (Figure 26).

3. RESULTS

During the excavation of the replacement gas main along Military Road, a total of 20 deposits, layers, and features were identified. Five of these contexts, (105), (106), (205), (305), (306), were related to Hadrian's Wall. Three contexts, [203], (204), and (303), were interpreted as the probable cut and fill of one of Simpson's archaeological trenches of 1930, discussed in the Archaeological Background. One context, road surface (304), was thought to be the remains of the 18th century Military Road. All other deposits, cuts and features were 20th century in date.

During the excavation of the new gas pipe trench at 11a Military Road, a total of five deposits and layers were identified. All of these dated to the modern period and were related to the construction and use of the property.

The dimensions of these deposits, layers, and features and their stratigraphic relationships are detailed in Tables 1.1 and 1.2 of the Appendix. Coordinates of the planning and section points used in site drawings are given in selected figures, and the coordinates for the centre point of each of the exposed sections of wall are given in text.

3.1. MILITARY ROAD GAS MAIN REPLACEMENT (HEADLAND ARCHAEOLOGY PROJECT CODE P21-126)

The modern tarmac road surface of Military Road, contexts (101), (201), and (301), was seen in all trenches and had an average thickness of 0.1m – 0.2m. The make up for this surface and previous 20th century road surfaces were assigned the contexts

(102), (202), and (203). The thickness of these contexts differed, with (102) averaging 0.6m thickness, (202) averaging 0.4m thickness, and (302) averaging 0.2m thickness.



Figure 3: Pre-excavation view along Military Road from the eastern end of the excavation, looking west-south-west



Figure 4: Pre-excavation view along Military Road from the western end of the excavation, looking east-south-east

Trench 1

In Trench 1 (Figures 7 and 8), a modern gas main [103] and water main [104] were recorded. Cut [103] ran north-south at the eastern end of the trench before turning to run east-west along its southern side, and truncated (105). Cut [104] also ran east-west along the southern side of the trench, truncating (105) and overlying [103], therefore being of later date than [103].

Trench 1 contained context (105), surviving core material of Hadrian's Wall (Figures 5 - 8). This was encountered at around 0.6m below modern ground surface at a height of 125.66m – 125.70m AOD. The maximum dimensions of the exposed Wall core were 0.6m x 0.6m x 0.2m. It was centred on NGR NZ 13153, 67057.

It incorporated a single course of at least 6 small – medium sub-angular sandstone blocks laid

horizontally and bonded with clay packing (106), a compacted yellowish orange sandy clay containing lenses of medium – coarse sand and interpreted as redeposited natural.

These two contexts were truncated to the east by [103] and to the south by [104], with their upper levels being removed and/or disturbed by the 18th century road (304). This small section of Hadrian's Wall appeared to continue to the north and west beyond Trench 1; no cut was seen related to the truncations by modern services.

Trench 1 was the only trench containing natural geological strata, (107). This comprised compacted yellowish orange sandy clay containing occasional small – medium sub-rounded sandstone and lenses of coarse sand, underlying (105).

The coordinates of the section and planning points for Trench 1 are detailed in Figure 7.



Figure 5: Context (105) in plan. Gas main [103] can be seen in the left of the frame. Looking west. 1 x 1m scale.



Figure 6: Context (105) in plan, looking north. 2 x 1m scale

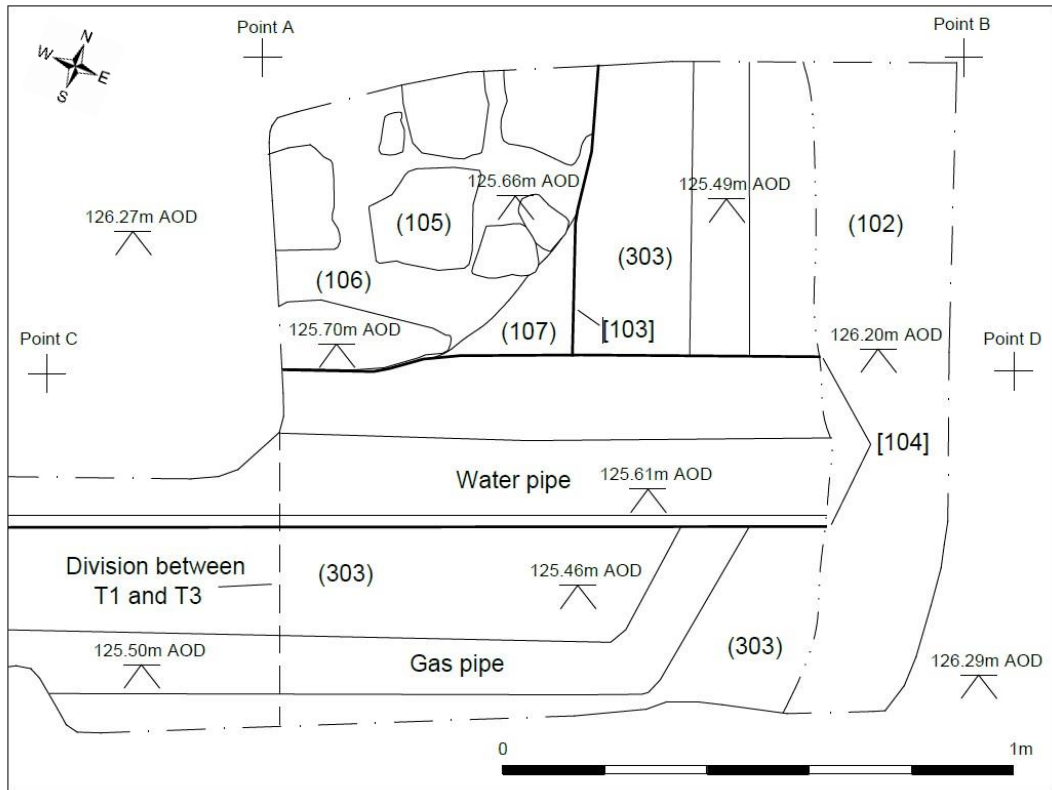


Figure 7: Plan of Trench 1. Coordinates of points: Point A = 413152.74, 567057.68; Point B = 413154.09, 567057.27; Point C = 413152.14, 567057.20; Point D = 413154.00, 567056.64

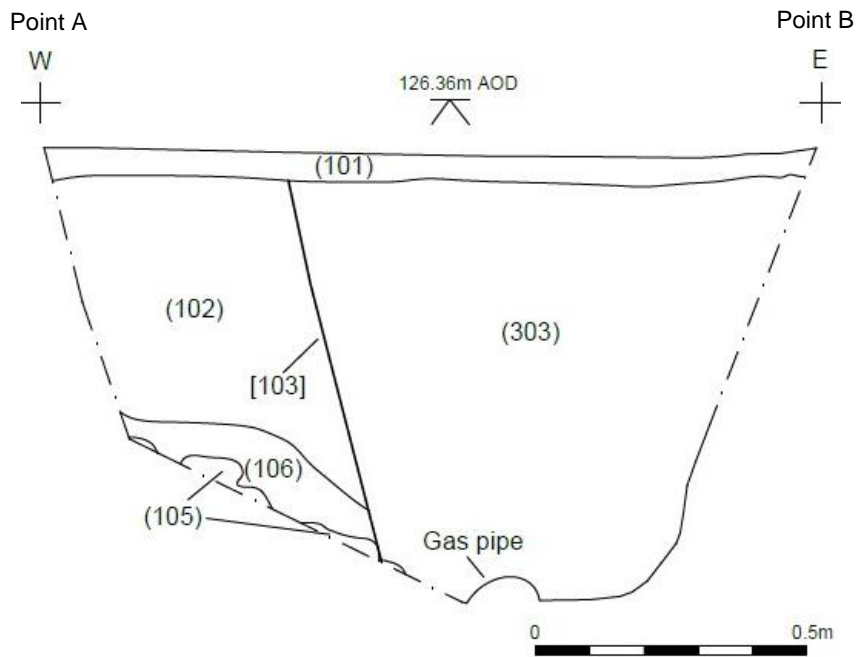


Figure 8: South-facing section of Trench 1

Trench 2

In addition to the modern surfaces and make up layers of Military Road outlined above, Trench 2 (Figures 9 and 10) also contained context [203]. This cut filled most of Trench 2 and ran north-south, turning in the south-western corner of the trench to run east - west. In so doing, it truncated the remains of the Wall, context (205), which ran north-south along the western side of the trench.

Cut [203] had almost vertical sides and a width of 0.5m; its base was not reached. It was filled with (204), a loose, friable brownish grey sandy clay containing quite frequent blocks of whinstone and redeposited Wall stone, mostly from the core; frequent patches of very dark grey ashy silt; frequent medium – large tarmac fragments and rounded pebbles; and frequent small fragments of charcoal and coal. The almost vertical sides of [203] and its location, in line with Hadrian's Wall and close

to the site of Turret 12a, implies that it was an archaeological trench and is thought to relate to Simpson's 1930 investigations.

A small section of probable Wall core, (205), ran north-south along the western side of Trench 2 and continued to the north and south. The remains were encountered at around 25.25m AOD and were truncated by [203]. The exposed area of Wall measured 0.95m long by 0.3m wide, being exposed to a depth of 0.25m. It continued into section to the west and north, and was centred on NGR NZ 13054, 67085. The coordinates of the planning points for Trench 2 are detailed in Figure 10.

Wall remains (205) were constructed of large sub-angular stone blocks in a compacted yellowish orange sandy clay packing. A small deposit of probable compacted whiteish pink limestone mortar overlay the stones was seen in section only (Figure 9).



Figure 9: East-facing section of Trench 2, showing context (205) and possible overlying mortar in the right of the section and the cut of [203] in the left of the section. Looking west. 2 x 1m scale

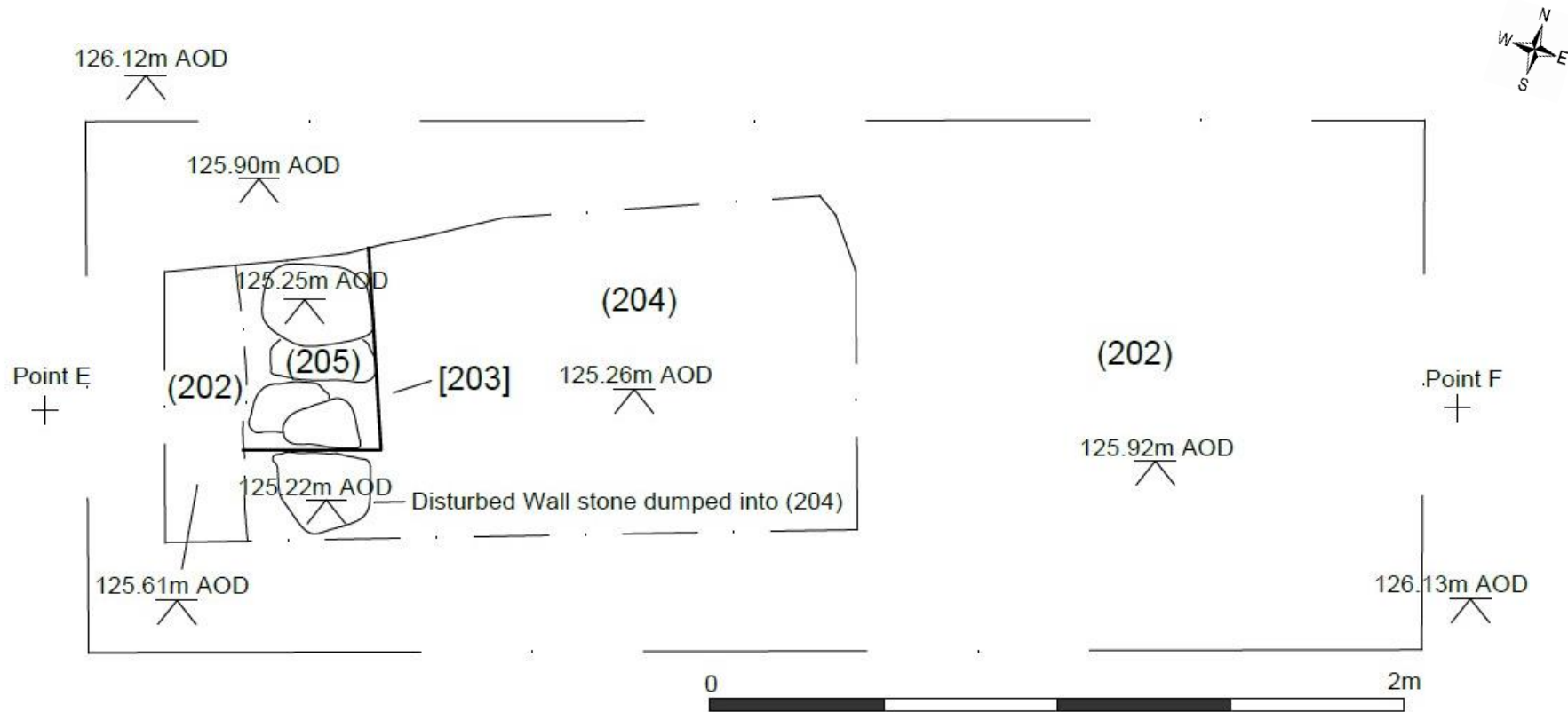


Figure 10: Plan of Trench 2. Coordinates of points: Point E = 413053.73, 567085.07; Point F = 413057.67, 567083.98

Trench 3

Trench 3 (Figures 18 and 19), the remaining length of the replacement gas pipe trench, contained the cut and fill of the existing gas main, [103] and (303), which ran east – west along the southern side of the trench. This truncated the probable remnants of the 18th century Military Road surface, context (304). This surface consisted of well-packed and frequent angular blocks of whinstone from the Wall of between 0.2m and 0.6 in size (Figure 11); frequent fragments of sandstone facing Wall stones; and frequent lumps of Wall core stones in a friable very dark greyish brown ash-rich clayey silt which contained occasional small sherds of Post-Medieval pottery (discarded on site). The cut for this surface could be seen along the very edge of the base of the trench, abutting the protruding, in situ Wall stones (305) and (306). The thickness of this surface ranged from 0.4m to 0.8m and was seen throughout the length of the trench.

As noted above, a small section of in situ Hadrian's Wall, (305) and (306), was encountered in the north-facing section of the trench, running along the southern side of Trench 3 (Figures 2 and 12 - 16). In total, this section of Wall measured 6m in length, protruding into the trench by between 0.25m and 0.3m. It was seen at a height of 25.90m AOD and reached an exposed maximum depth of 0.5m. Context (305), facing stones, was centred on NGR NZ 12999.62, 67095.74, and context (306), Wall core, was centred on NGR NZ 12996.37, 67096.46. The coordinates of their section and planning points are given in Figure 18.

Context (305) consisted of a line of faced stones, measuring on average 0.3m x 0.2m x 0.3m and incorporating at least three surviving courses; the lowest course contained eight or nine stones, the middle course totalling seven stones, and the upper course containing a single stone. These stones exhibited some wear on their faces, suggesting either that they were used as kerbstones for the 18th century road, or formed part of the Broad Wall

lower courses, onto which the narrower upper wall was built. The exposed section of this context measured 4.2m x 0.3m x 0.5m and was abutted on its western side by (306).

A section of surviving Wall core, (306), was associated with (305). Its dimensions as seen were 1.8m x 0.25m x 0.27m. The deposit consisted of at least two courses of small stones measuring, on average, 0.15m x 0.15m x 0.15m. There were aligned so as to present a 'face' against the backs of facing stones (305). Very little packing material was evident.

Contexts (305) and (306) had been disturbed by the excavation of the old gas main. As the new gas pipe followed the same route, they were not further disturbed (Figure 17).

Trench 3 also contained a modern water main, [307], and was bounded on its southern side by context (308), a modern turf and topsoil bank of up to 0.5m in height.

No finds were recovered, although small sherds of Post-Medieval pottery were noted in (304), and no deposits were suitable for environmental sampling.



Figure 11: Representative fragments of whinstone found in (304)



Figure 12: Context (305) as exposed, looking west.



Figure 13: Context (305) once cleaned, looking east-south-east. 2 x 1m scale



Figure 14: North-facing section of contexts (305 and (306)), looking south. 2 x 1m scale



Figure 16: Context (306), looking west. 1 x 1m scale



Figure 15: Contexts (306) (foreground) and (305) (background), looking east. 2 x 1m scale



Figure 17 (left): Replacement gas pipe in Trench 3, demonstrating that it does not disturb the remains of the Wall. Looking west

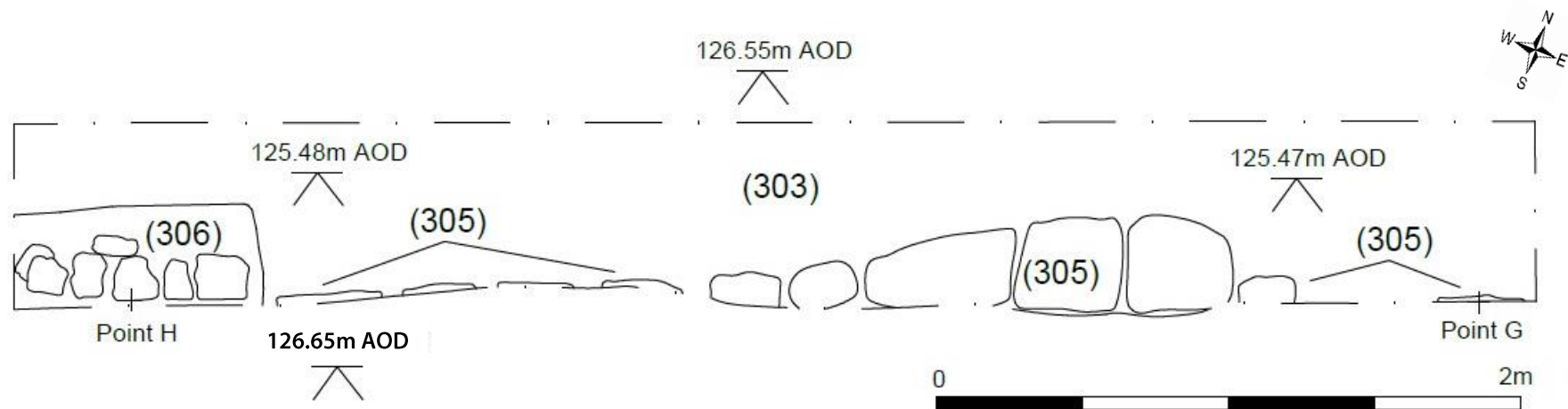


Figure 18: Plan of Trench 3. Coordinates of points: Point G 413001.16, 567095.41; Point H = 412996.58, 567096.27

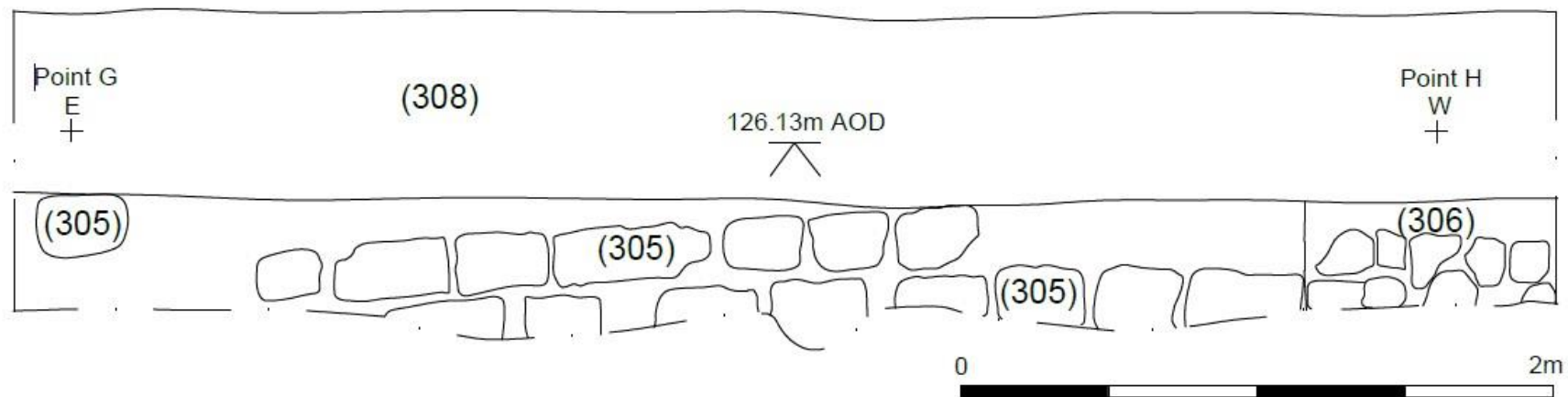


Figure 19: North facing section of Trench 3



Figure 20: Post-excitation shot of Trench 3 from its eastern end, looking west



Figure 21: Post-excitation shot of Trench 3 from its western end, looking east

3.2. 11A MILITARY ROAD GAS PIPE (HEADLAND ARCHAEOLOGY PROJECT CODE P21-155)

Monitoring of the excavation and laying of the new gas pipe at 11a Military Road took place on the 28th May 2021. The dimensions of the excavated pipe trench were 7m x 0.3m – 0.4m x 1m. The trench ran east-west at its northern end and then turned to run north-south for the rest of its length (Figure 22 – 26). This trench is identified as Trench 4 on the overall site plan (Figure 2).

Five layers and deposits were identified, all of Modern date. The upper two layers, (401) and (402), formed the current paved brick surface of 11a Military Road's garden; together, these two layers were 0.34m thick. They overlaid the modern, concrete-capped east-west orientated service trench [405], which was 0.6m in width and 0.4m in depth.

Beneath these three contexts lay a thick deposit made ground formed of quite compact, mid-orangish brown coarse sandy clay, (404), which contained fairly frequent medium rounded stones. This was overlain by a thin ashy layer of material (403), formed of loose, dark greyish black silty ash. Context (403) had an average thickness of 60mm, but context (404) continued past the base of the pipe trench; therefore, its full depth is not known. These two made ground deposits must relate to the raising of the ground level at 11a Military Road by at least a metre above that of Military Road, presumably as part of the construction of the property (Figure 25).

No finds were recovered and no environmental samples were taken.



Figure 22: Post-excavation shot of southern half of the new gas pipe, looking south



Figure 23: Post-excavation shot of northern half of the new gas pipe, looking east



Figure 24: Overall view of new gas pipe, looking south-west. 2 x 1m scale



Figure 25: Differentiation in height between the garden of 11a Military Road (left of frame) and Military Road (right of frame), looking west

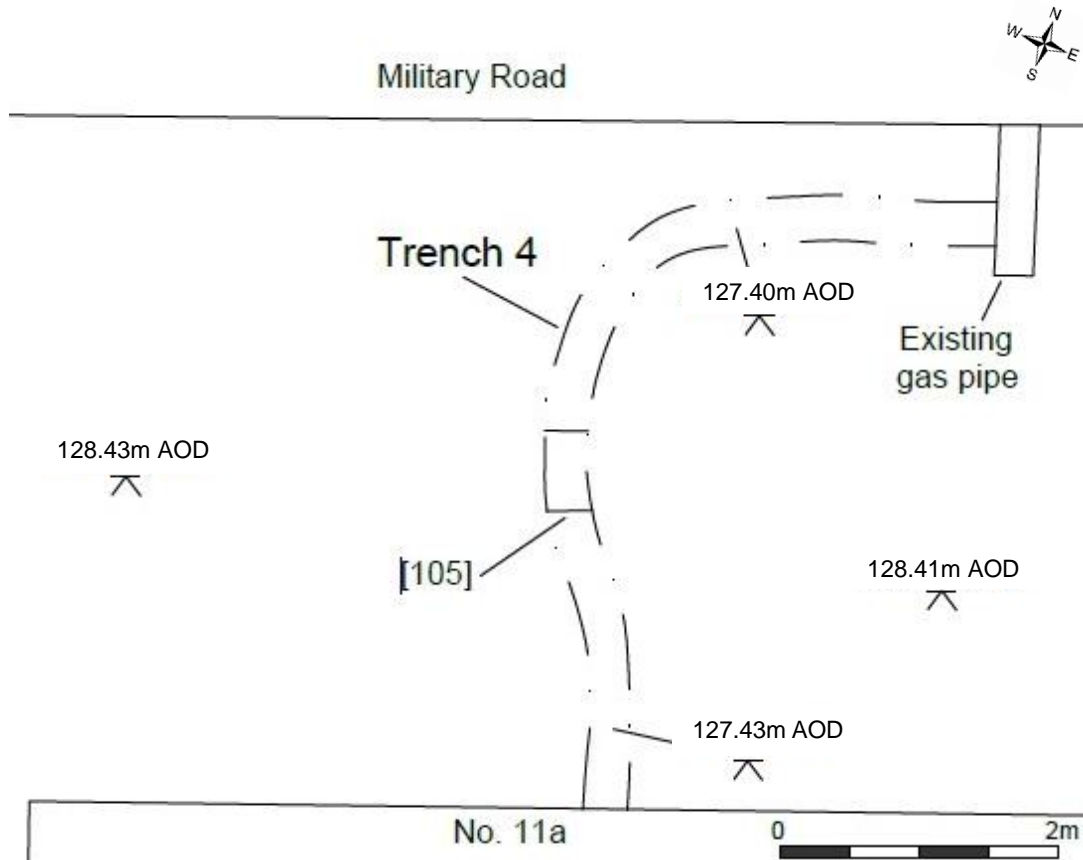


Figure 26: Plan of Trench 4

4. CONCLUSION

A total of 20 deposits, layers, and features were identified during groundworks relating to the laying of a replacement gas main along Military Road. Five of these contexts, (105), (106), (205), (305), (306), were related to Hadrian's Wall and were either *in situ* facing stones or wall core. These were centred on NGR NZ 13153, 67057 ((105) and (106)), NGR NZ 13054, 67085 ((205)), NGR NZ 12999.62, 67095.74, ((305)), and NGR NZ 12996.37, 67096.46 ((306)). They lay at between 125.25m AOD and 126.00m AOD, approximately 0.6m – 0.8m below current ground level. They were situated along the southern side of Military Road.

These Wall remains were disturbed and truncated by activity of the 18th – 21st centuries. This activity included context (304), thought to be the remains of the 18th century Military Road, which may have utilised the faced Wall stones as kerbstones; contexts [203], (204), and (303), which were interpreted as the probable cut and fill of Simpson's archaeological trench of 1930; modern services [103], [104] and [107]; and the 20th century surfaces of Military Road.

Due to *in situ* remains of Hadrian's Wall being present, the planned excavation methodology for the new gas main was adapted, in consultation with Historic England, on the first day of groundworks to avoid harm to the monument. Rather than the pipe being laid between pilot trenches, as planned, it was laid along the same route as the old gas main which was to be replaced. Excavations did not further disturb the Wall.

During the concurrent monitoring of the works at 11a Military Road, five layers and deposits were identified, all of which were of Modern date. The upper two layers pertained to the current paved brick surface of 11a Military Road's garden, which overlaid an east-west orientated service trench. Beneath these three contexts was a thick deposit of made ground overlain by a thin ashy layer of material. These two made ground contexts must relate to the raising of the ground level of 11a Military Road by at least a metre above that of Military Road,

presumably as part of the construction of that property.

5. REFERENCES

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Military Road, Heddon-on-the-Wall: Written Scheme of Investigation for an Archaeological Watching Brief

Headland Archaeology 2021b
11a Military Road, Heddon-on-the-Wall: Written Scheme of Investigation for an Archaeological Watching Brief

Historic England SMC
S00241208: *Hadrian's Wall and vallum from East Town House, Heddon-on-the-Wall to the A69 trunk road in wall mile 12*. Granted 13th May 2021

Historic England SMC
S00241329: *Hadrian's Wall and vallum from East Town House, Heddon-on-the-Wall to the A69 trunk road in wall mile 12*. Granted 17th May 2021

6. APPENDICES

APPENDIX 1 SITE AND CONTEXT REGISTERS

1.1 Trench register and matrices

Trench No	Matrix	Orientation	Dimensions (as appropriate)		
			Length	Width	Depth (max)
1	(101) (102) [104] [103] (106) (105) (107)	ESE – WNW	1.8m	1.4m	1.7m
2	(201) (202) (204) [203] (205)	ESE – WNW	3.85m	1.65m	1.1m
3	(308) (301) (302) [307] (303) [103] (305) (306)	ESE – WNW	233m	0.8m – 1.3m	1.7m
4	(401) (402) [405] (403) (404)	Varying	7m	0.3m – 0.4m	1m

1.2 Context register

Context No	Trench No	Description	Dimensions (as appropriate)		
			Length	Width	Thickness
(101)	T1	Modern tarmac	1.8m	1.4m	0.1m
(102)	T1	Make up for (101) and disturbed previous road surfaces. Occasional displaced blocks from the 18 th century Military Road surface, tarmac and concrete fragments.	1.8m	1.4m	0.6m
[103]	T1	Existing gas main of Military Road. Runs north-south at the eastern end of the trench, truncating (105), then turns to run east-west along the southern side of the trench. Made of yellow plastic and hard foam concrete. Filled by (303).	2.5m	1m	0.8m
[104]	T1	Water main. Runs east-west along the southern side of the trench, truncating (105) and overlying [103].	1.8m	0.7m	0.7m
(105)	T1	Surviving wall core, seen 0.6m below modern ground surface, comprising at least 6 small – medium sub-angular sandstone blocks laid horizontally and with (106). Truncated to east by [103] and to south by [104]. Upper levels removed and/or disturbed by (304). Continues to north and west of the trench. No cut seen due to truncations.	0.6m	0.6m	0.1m
(106)	T1	Clay packing around stones of (105). Compacted yellowish orange sandy clay (redeposited natural) containing lenses of medium – coarse sand.	0.6m	0.6m	0.2m
(107)	T1	Natural. Compacted yellowish orange sandy clay with occasional small – medium sub-rounded sandstone and lenses of coarse sand. Seen underlying [103] in Trench 1.	N/A	N/A	N/A
(201)	T2	Modern tarmac	3.85m	1.65m	0.2m
(202)	T2	Make up for (201) and previous road surfaces	3.85m	1.65m	0.4m
[203]	T2	Cut of trench, maybe relating to Simpson's 1930 investigations. Runs north-south, truncating wall (205), and has almost vertical sides and a fairly flat base. Turns to run east-west within Trench 2. Base not seen. The location of this cut, in line with Hadrian's Wall and close to the site of Turret 12a, implies this is an archaeological trench.	1.5m	1.2m	0.5m
(204)	T2	Fill of trench [203]. Loose, friable brownish grey sandy clay containing quite frequent blocks of whin stone and redeposited Wall stone (most from the core); frequent patches of very dark grey ashy silt; frequent medium – large tarmac fragments and rounded pebbles; and frequent small fragments of charcoal and coal.	1.5m	1.2m	0.5m
(205)	T2	A small section of probable Wall core running north-south along the western side of Trench 2. Constructed of large sub-angular stone blocks in a compacted yellowish orange sandy clay packing. A small deposit of probable compacted whiteish pink limestone mortar overlay the stones in section only.	0.95m	0.3m	0.25m

(301)	T3	Modern tarmac	233m	0.8m – 1.3m	0.2m
(302)	T3	Make up for (301) and previous road surfaces	233m	0.8m – 1.3m	0.2m
(303)	T3	Mixed back fill of old Military Road gas pipe [103]. Loose, friable brownish grey sandy clay containing quite frequent blocks of whin stone and redeposited Wall stone (most from the core); frequent patches of very dark grey ashy silt; frequent medium – large tarmac fragments and rounded pebbles; and frequent small fragments of charcoal and coal.	235m	0.8m – 1.3m	0.4m – 0.8m
(304)	T3	18 th century Military Road surface. Constructed of frequent angular blocks of whinstone between 0.2m and 0.6 in size, frequent fragments of sandstone facing stones and frequent lumps of core stones in a friable very dark greyish brown ash-rich clayey silt containing occasional small sherds of Post-Medieval pottery (discarded on site). All of the stone was reused from Hadrian's Wall.	233m	0.8m – 1.3m	0.4m – 0.8m
(305)	T3	Line of south-facing Hadrian Wall stones, seen in section only, measuring on average 0.3m x 0.2m x 0.3m. As excavated, only the tails of the stones were seen, but two of the stones were fully excavated to investigate their construction by undercutting the section. The portion of the wall exposed contained at least three surviving courses, with the lowest course containing eight or nine stones, the middle course totalling seven stones, and the upper course containing a single stone.	4.2m	0.3m	0.5m
(306)	T3	Surviving area of wall core associated with (305). Present at the western end of (305). Consists of at least two courses of small (average 0.15m x 0.15m x 0.15m) stones, aligned so as to present a 'face' against the rears of the facing stones. Very little packing material evident.	1.8m	0.25m	0.27m
[307]	T3	Water main backfilled with compacted sand and silty clay which contained occasional fragments of Wall core and facing stones.	Not recorded	0.5m	0.8m – 0.9m
(308)	T3	Modern turf and topsoil bank running along the southern side of Military Road.	233m	N/A	0.2m – 0.5m (height)
(401)	T4	Modern brick paving in front garden of 11a Military Road	7m	0.4m	80mm
(402)	T4	Concrete base and levelling layer for (401)	7m	0.4m	0.26m
(403)	T4	Loose, dark greyish black silty ash layer beneath (402) and above (404)	7m	0.4m	60mm
(404)	T4	Quite compact mid-orangish brown coarse sandy clay with fairly frequent medium rounded stones	7m	0.4m	0.6m
[405]	T4	Concrete-capped modern east – west service trench	Unknown	0.6m	0.4m

1.2 Drawing register

Drawing no	Sheet No	Plan	Section	Scale	Description
0001	01	✓		1:20	Plan of Trench 1
0002	01		✓	1:10	Section of Trench 1
0003	01	✓		1:20	Plan of Trench 2
0004	01		✓	1:10	Section of (305)
0005	01	✓		1:20	Site plan (Military Road)

1.3 Photographic register

*Photo numbers 0001 – 0030 are of Military Road gas main replacement, photo numbers 0031 – 0039 are of 11a Military Road gas man connection

Photo no	Digital No	Direction	Description
0001	IMG-2546	ESE	Pre-excavation shot of site from its western end, looking along Military Road
0002	IMG-2548	WSW	Pre-excavation shot of site from its eastern end, looking along Military Road
0003	IMG-2549	S	Trench 1: location
0004	IMG-2552	N	Wall (105) in plan, showing its truncation by modern services. 2 x 1m scale
0005 + 0006	IMG-2555 + IMG-2557	W	Wall (105) in plan. 1 x 1m scale
0007	IMG-2562	N	Wall (105) in plan. 2 x 1m scale
0008	IMG-2567	E	Trench 2: location. 2 x 1m scale
0009	IMG-2572	W	Section of (205) in Trench 2, showing pinkish mortar to the right of the frame. 2 x 1m scale
0010	IMG-2578	W	Groundworks, showing Trench 1 in bottom right of the frame (working shot)
0011	IMG-2582	W	Groundworks in front of no. 29 Military Road, showing curve in line of trench to avoid an existing drain (working shot)
0012	IMG-2593	W	Working shot of Trench 3, taken from the approximate centre of the trench and looking along the trench
0013	IMG-2595	W	General shot over Trench 3, taken from the approximate centre of the trench
0014	IMG-2598	E	General shot over Trench 3, taken from the approximate centre of the trench and looking along trench

0015	IMG-2599	W	(305) as exposed during groundworks (working shot)
0016	IMG-2603	W	Collection of whinstone from (304)
0017	IMG-2608	W	(305) in plan. 2 x 1m scale
0018	IMG-2611	ESE	(305) in plan. 2 x 1m scale
0019	IMG-2617	SE	Location shot of (305), which begins by the telegraph pole in the centre of the frame and continues to the west past the right of the frame
0020	IMG-2620	W	(305) in plan. 2 x 1m scale
0021	IMG-2621	N	(305) in section. 2 x 1m scale
0022	IMG-2624	W	Shot over (305) (bottom left of frame) and (306) (top left of frame). 2 x 1m scale
0023	IMG-2625	WSW	(306) in plan. 1 x 1m scale
0024	IMG-2626	W	(306) in plan. 1 x 1m scale
0025	IMG-2627	E	Shot over (306) (bottom right of frame) and (305) (top right of frame). 2 x 1m scale
0026	IMG-2630 + 2643	E	Post-excavation shot of Trench 3 from its western end. 2 x 1m scale
0027	IMG-2643	E	Post-excavation shot of Trench 3 from its western end.
0028	IMG-2644	ESE	Post-excavation shot of Trench 3 from its western end.
0029 + 30	IMG-2647 + 2649	W	New gas pipe laid against (305) and (306)
0031	IMG_2658	W	11a Military Road site location showing height of garden above surrounding land
0032	IMG_2659	E	Brick surface (401) over north of area
0033	IMG_2661	E	Working shot of pipe trench excavation
0034	IMG_2664	E	Context [405] (in section only)
0035	IMG_2668	S	North-south line of pipe trench cutting through made ground (404)
0036	IMG_2669	E	East-west line of pipe trench cutting through brick surface (401) and made ground (404)
0037	IMG_2673	SW	Post-excavation shot of pipe trench. 2 x 1m scale
0038	IMG_2674	NW	Post-excavation shot of pipe trench. 2 x 1m scale
0039	IMG_2676	W	Difference in height between ground level of 11a's garden and Military Road

APPENDIX 2 OASIS ENTRY

Summary for headland1-501952

OASIS ID (UID)	headland1-501952
Project Name	Watching Brief at Military Road and 11a Military Road, Heddton-on-the-Wall
Activity type	Watching Brief
Project Identifier(s)	P21-126, P21-155
Planning Id	
Reason For Investigation	Scheduled monument consent
Organisation Responsible for work	Headland Archaeology Ltd
Project Dates	17-May-2021 - 28-May-2021
Location	Military Road, Heddton-on-the-Wall NGR : NZ 12894 67123 LL : 54.998519, -1.7999783 12 Fig : 412894,567123 11a Military Road, Heddton-on-the-Wall, Northumberland NGR : NZ 13149 67010 LL : 54.9974932, -1.7959977 12 Fig : 413149,567010
Administrative Areas	Country : England County : Northumberland District : Northumberland Parish : Heddton-on-the-Wall
Project Methodology	<p>Archaeological monitoring was carried out in accordance with the WSI (Headland Archaeology 2021) and was undertaken according to the standards set out in the relevant Chartered Institute for Archaeologists 'Codes of Conduct' and 'Standards and Guidance' documents (CIfA 2019; CIfA 2020). All potentially significant features and deposits were subsequently be cleaned by hand, planned and recorded in accordance with CIfA standards and guidance (CIfA 2020). The spatial extent of the trenches recorded. Contexts were given unique numbers and all recording was undertaken on pro forma record sheets that conform to accepted archaeological standards. All stratigraphic relationships were recorded. A photographic record was taken.</p> <p>Monitoring of the gas main replacement along Military Road was carried out between the 17th to the 25th May 2021. Monitoring of the new gas pipe trench at 11a Military Road was carried out on the 28th May 2021.</p> <p>The original methodology for the laying of the replacement gas main was to feed it through small pilot trenches along the length of its planned route. However, remains of Hadrian's Wall were encountered in both the first pilot trench (Trench 1) and the second (Trench 2) on May 17th. Works were halted and these remains fully exposed by hand by the attending archaeologist. Don O'Meara of Historic England was immediately informed and carried out a site inspection later that day. Following discussions between Historic England, Headland Archaeology, and Northern Gas Networks, it was decided to adapt the excavation methodology and lay the new gas pipe along the line of the old gas main.</p> <p>The excavation of the new gas pipe trench at 11a Military Road, Trench 4, was undertaken by hand.</p>

Project Results	<p>A total of 20 deposits, layers, and features were identified during the laying of the 236m long new gas main along Military Road. Five of these contexts, (105), (106), (205), (305), (306), were the faced stones and wall core of Hadrian's Wall. These were centred on NGR NZ 13153, 67057 ((105) and (106)), NGR NZ 13054, 67085 ((205)), NGR NZ 12999.62, 67095.74, ((305)), and NGR NZ 12996.37, 67096.46 ((306)). They lay at between 125.25m AOD and 126.00m AOD, approximately 0.6m – 0.8m below current ground level. They were situated along the southern side of Military Road.</p> <p>These deposits had been disturbed and truncated by activity of the 18th – 21st centuries. This activity included context (304), thought to be the remains of the 18th century Military Road, which may have utilised the faced Wall stones as kerbstones; contexts [203], (204), and (303), which were interpreted as the probable cut and fill of one of archaeologist Frank Simpson's excavation trenches of 1930; modern services [103], [104] and [107]; and the 20th century surfaces of Military Road.</p> <p>During the monitoring of the works at 11a Military Road, five layers and deposits were identified. The upper two layers pertained to the current paved brick surface of 11a Military Road's garden, which overlaid a modern east-west orientated service trench. Beneath these three contexts was a thick deposit of made ground overlain by a thin ashy layer of material. These two made ground contexts must relate to the raising of the ground level at 11a Military Road by at least a metre above that of Military Road, presumably as part of the construction of that property. No finds were recovered, and no deposits were deemed suitable for environmental sampling.</p>
Keywords	Wall - ROMAN - FISH Thesaurus of Monument Types
HER	Northumberland HER - unRev - STANDARD
HER Identifiers	
Archives	PAPER - to be deposited with Great North Museum: Hancock