

P21-063

NEWMA 2021.2



53 Military Road, Heddon-on-the-Wall, Northumberland

Archaeological Watching Brief

Headland Archaeology North West
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for Northern Gas Networks

v.1.0
03/06/2021

PROJECT INFORMATION:

PROJECT NAME	53 Military Road, Heddon-on-the-Wall, Northumberland
TYPE OF WORK	Archaeological Watching Brief
CLIENT	Northern Gas Networks
PROJECT CODE	P21-063
NGR	NZ 413020 567095
PARISH	Heddon-on-the-Wall
LOCAL AUTHORITY	Northumberland
FIELDWORK DATE	11/05/2021 – 12/05/2021
OASIS REF.	headland1-501832
ARCHIVE REPOSITORY	Great North Museum: Hancock

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PROJECT SUMMARY

Headland Archaeology (UK) Ltd was commissioned by RSK on behalf of Northern Gas Networks to undertake a programme of archaeological monitoring (archaeological watching brief) during groundworks for the connection of 53 Military Way, Heddon-on-the-Wall, NE15 0HA to the local gas network running along Military Road, the B6318 (NGR NZ 412803, 567075).

Although the works fell under permitted development, the site is located within the boundary of the Scheduled Ancient Monument of Hadrian's Wall. Therefore, Scheduled Monument Consent for the works was sought by Historic England, and approved on the 2nd March 2021 under Historic England reference S00241026.

Archaeological monitoring took place on the 11th and 12th May 2021 and involved the monitoring of the excavation of 9 small trenches which ran along the line of the new gas pipe connection.

All trenches apart from Trench 4 contained modern deposits relating to two iterations of the current driveway, which directly overlay natural deposits. Two modern service trenches were also seen. The only archaeological features relating to Hadrian's Wall that were identified were two probable fills of the Roman vallum in Trench 4. This trench was located in the base of the vallum ditch as seen to its west within the garden of 53 Military Road. These results also demonstrate that the existing OS data for the vallum is incorrect, and that it lies around 7m to the north of its recorded location.

The laying of the earliest driveway for 53 Military Road clearly entailed substantial terracing, truncation, and levelling of the ground, thereby removing any in situ archaeological remains. Trench 4 was outside of this disturbance as the ground surface was already reduced.

No finds were recovered and no environmental sampling was undertaken.

The project has been entered into the OASIS database under the entry headland1-501832. A copy of this report shall be deposited with the Great North Museum: Hancock under the accession number NEWMA 2021.2, and the digital archive uploaded onto the ADS within 1 year of the date of this report.

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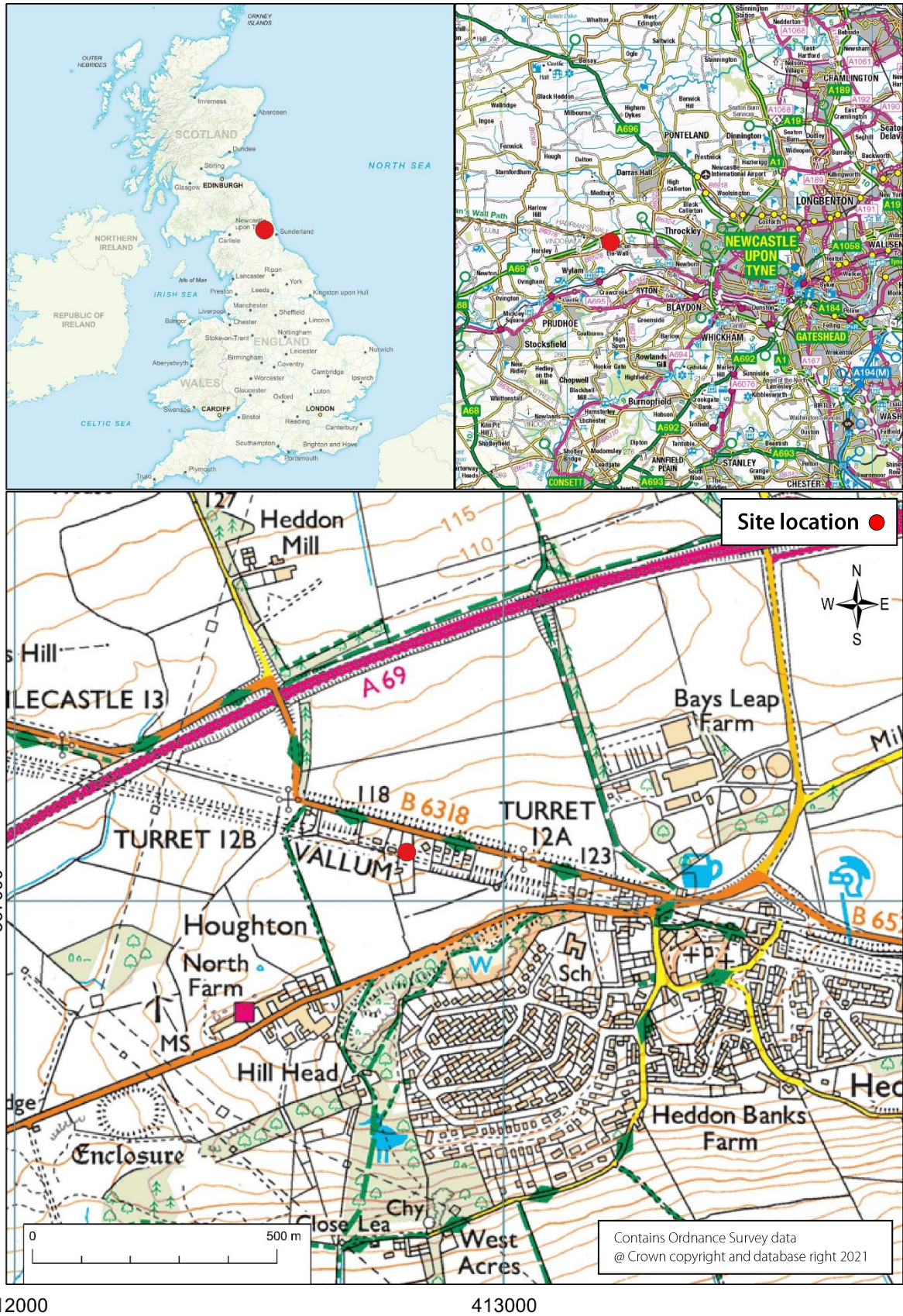
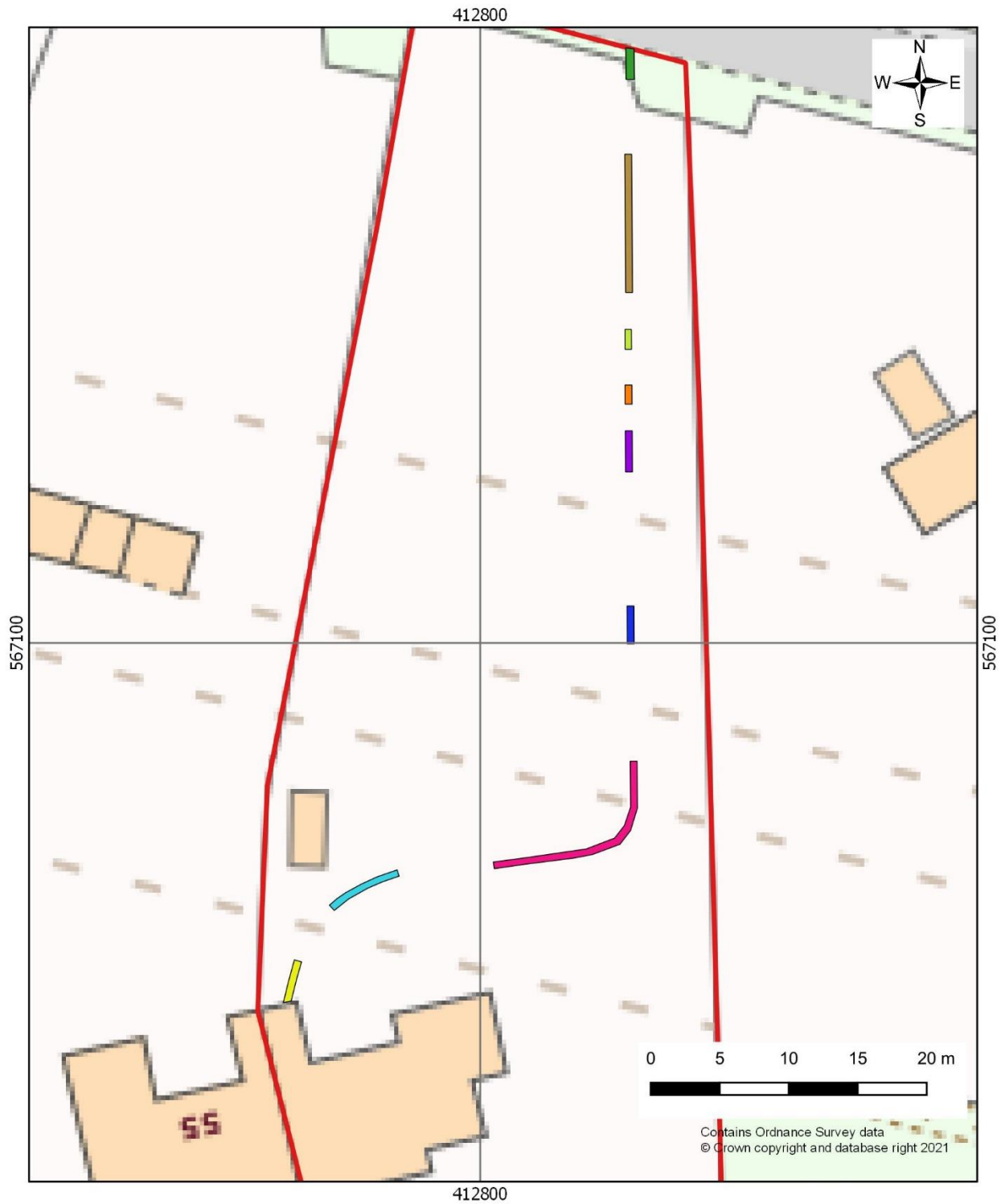


Figure 1: Site location



Key

- | | | | |
|---|--|--|--|
|  Site boundary |  Trench 3 |  Trench 6 |  Trench 9 |
|  Trench 1 |  Trench 4 |  Trench 7 | |
|  Trench 2 |  Trench 5 |  Trench 8 | |

Figure 2: Trench locations

53 Military Road, Heddon-on-the-Wall, Northumberland

Archaeological Watching Brief

1. INTRODUCTION

Headland Archaeology (UK) Ltd was commissioned by RSK on behalf of Northern Gas Networks to undertake a programme of archaeological monitoring (archaeological watching brief) during groundworks for the connection of 53 Military Way, Heddon-on-the-Wall, NE15 0HA to the local gas network running along Military Road, the B6318 (NGR NZ 412803, 567075).

Although the works fell under permitted development, the site is located within the boundary of the Scheduled Ancient Monument of Hadrian's Wall. Therefore, Scheduled Monument Consent for the works was sought by Historic England, and approved on the 2nd March 2021 under Historic England reference S00241026. This consent outlined the requirement for the archaeological work undertaken.

Archaeological monitoring took place on the 11th and 12th May 2021 and involved the monitoring of the excavation of 9 small trenches which ran along the line of the new gas pipe connection.

The project has been entered into the OASIS database under the entry headland1-501832. A copy of this report shall be deposited with the Great North Museum: Hancock under the accession number NEWMA 2021.2, and the digital archive uploaded onto the ADS within 1 year of the date of this report.

1.1. SITE LOCATION AND DESCRIPTION

The site is located to the immediate south of Military Road and 300m to the west-north-west of the village of Heddon-on-the-Wall, Northumberland. It lies c.

430m to the south of the A69 and is centred on NGR NZ 412803, 567075 (Figures 1 and 2).

The site comprises a large garden fronting onto Military Road to the north, with the property itself and a small, tarmacked car parking area located at the southern end of the plot. The property is accessed from the road by means of a tarmacked access track, the line of which the development followed. The site sits on a gentle slope, lying at around 125m AOD at its northern end and 127m AOD at its southern end, and has a total area of just over 3400m².

The bedrock within the site is Stainmore Formation - Sandstone. This is sedimentary bedrock formed approximately 319 to 329 million years ago in the Carboniferous Period in an environment dominated by swamps, estuaries and deltas. It is described by the BGS as a 'cyclical repetition of sandstones, siltstones, mudstones, thin limestones and some coals'. Overlying the bedrock, the superficial geology of the area is Devensian Diamicton Till, formed up to 2 million years ago in the Quaternary Period when the local environment was dominated by ice age conditions.

1.2. ARCHAEOLOGICAL BACKGROUND

The following has been reproduced from the project's WSI (Headland Archaeology 2021):

There is little evidence of prehistoric activity within 500m of the proposed development site, although the HER records several prehistoric hut circles on Heddon Common in the approximate location of modern Trajan Walk and Remus Avenue. The site is now built over and there are no remains to be seen. These lay

around 280m to the south-south-east of the proposed development site.

Heddon-on-the-Wall is perhaps most famous for lying within the Scheduled Ancient Monument of Mile 12 of Hadrian's Wall, defined by Historic England as 'Hadrian's Wall and vallum from East Town House, Heddon-on-the-Wall to the A69 trunk road in wall mile 12' (NHLE list number: 1010617). Hadrian's Wall was constructed in the early 2nd century AD and, apart from a short period during the mid-2nd century when Emperor Antonine attempted to expand the Roman territory further to the north, was the northerly frontier of Roman occupation in Britain until the early 5th century.

The Wall spanned approximately 70 miles from the Cumbrian to the Northumbrian coastline and was constructed of stone and turf, being 8 – 10 Roman feet wide. For most of its length a substantial ditch on the northern side provided additional defence. Small fortlets known as milecastles were constructed at regularly spaced intervals of about a mile and served as controlled crossing points; between these were two equally spaced towers known as turrets. Together the milecastles and turrets provided bases from which the curtain wall could be watched and patrolled. Subsequently, forts were built along the wall, some overlying former milecastles and turrets. At the same time, the vallum, a broad flat-bottomed ditch flanked by a pair of linear banks, was also added to the defensive system to the south of the Wall. Its main function was to act as a barrier to restrict access to the Wall from the south, but it also served as the communication link between the forts. However, no road per se was constructed on the vallum to enable this communication, and therefore a metalled track was later established in certain sections of its length.

In the later 2nd century various sections of the wall were refurbished and a new linear road, the 'Military Way', was added. This road linked all elements of the Wall defence and ran from fort to fort within the area bounded by the Wall and the vallum.

Historic England's list entry for the Scheduled Ancient Monument of Wall Mile 12, in which the proposed development site is located, is as follows:

The monument includes a section of Hadrian's Wall, its external ditch and the vallum from Heddon-on-the-Wall in the east to the A69 trunk road in the west. This section of the Wall corridor has wide views overlooking the Tyne valley to the south and east, whereas to the north and west the ground rises gently giving a less extensive view. The Wall survives as a buried feature for the whole of this section of its course. It lies beneath the B6318 road except at the west end of this section where it continues in a field up to the A69 trunk road. The wall

ditch survives as an earthwork for much of this section, to a maximum depth of 1.5m. The upcast counterscarp, usually known as the 'glacis', also survives up to 0.3m high in the area from north of the Three Tuns Inn to the north of North Lodge. Turret 12a survives as a buried feature below the B6318 road. The location of the turret is known from part excavation by Simpson in 1930. Turret 12b also survives as a buried feature below the B6318 road, probably extending south into the area of woodland to the west of North Lodge. As with turret 12a, part excavation by Simpson in 1930 located the site of this turret. The exact location of milecastle 12 has not yet been confirmed. In relation to the known sites of turret 12a and turret 11b, the site of milecastle 12 is expected to lie in the vicinity of Keeper's Cottage at Heddon-on-the-Wall. The vallum survives intermittently as an upstanding earthwork throughout this section. The mounds have been reduced by ploughing and the central ditch has silted up for most of its length. The south mound in this section reaches a maximum height of 1.5m, while the ditch has a maximum depth of 0.6m. The course of the Roman road known as the Military Way, which ran along the corridor between the Wall and the vallum linking the turrets, milecastles and forts, has not yet been confirmed in this section of the corridor. The property known as Tank House, North Lodge and an area east of it, between the Wall and the vallum, are totally excluded from the scheduling as are the houses and gardens numbered 43 to 57 Military Road. All other buildings, road surfaces and property and field boundaries within the protected area are excluded from the scheduling, but the ground beneath them is included.

Little intensive excavation has been carried on Mile 12 of the wall, and for the most part its survival rate, form, and construction history is known from modern planning-related development. However, Turrets 12a and 12b, approximately 200m to the west-north-west and east-south-east of 53 Military Road respectively, were examined and recorded during excavations carried out in 1930 by F. G. Simpson (Simpson 1931). The results are described thus:

The Turrets - Nos. 12a and b could not be excavated completely. At 12a the walls were reduced to ground level beyond the edge of the roadway, and 12b lay largely below the road, which at that point was too narrow for the whole of the interior to be opened up. At none of the turrets was the north face of the Wall exposed, but a thickness of 9 feet 3 inches was measured between 12b and [turret] 13, and again between [turrets] 13 and 13a, and may be taken as the standard for this sector. The three turrets are nearly uniform in plan, their external measurement being the normal 20 Roman feet square: a new feature in turret construction is the

finished thickness of 4 feet throughout the side walls, as opposed to the normal thickness of 3 feet or thereabouts. One turret (29a, Blackcarts) has walls 3 feet 7 inches thick ; otherwise 3 feet 3 inches is the greatest thickness. The doorways are in the normal position; the jambs have not been faced with stone slabs such as were found at 26b. In 12b and 13a rectangular platforms occupied the south side of the interior; 12a was too badly robbed for any trace of a platform to survive. The masonry of all three turrets is of similar quality, roughly finished, but solidly built with an abundance of good mortar.

Simpson 1931, 322-324

Several archaeological investigations have been carried out within 500m of the proposed development site during the last two decades. Surprisingly, Roman remains have been rare, with no in situ remains of that date found during works at 29, 45 and 57 Military Road, and only demolition material and cobbled areas recorded during an evaluation at 23 Military Road (ARS 2011; Cooper, Dougherty, and Henderson 2017; Garrett 2005; Proctor 2004; PCA 2005). In addition to these, watching briefs have been carried out at 47 Military Road (TWM 2002), 23 Military Road (ASDU 2008), and 45 Military Road (AAG Archaeology 2017), but although listed as events on Keys to the Past, could not be found online.

The Historic Environment Record also records the findspots of several Roman artefacts within 500m of the site. These include a Roman carved stone, found in 1932, around 170m to the west-north-west of 53 Military Road; the remains of a Roman drain roughly 400m to its west-north-west; and two reused carved stones found in 1807, approximately 450m – 500m to the property's east-south-east.

Settlement at Heddon-on-the-Wall continued into the Medieval period, and the Church of St. Andrew is recorded as being the place of baptism in 653 AD for Peada, prince of the Middle Angles, and Sigebert, King of the East Saxons, by Bede (Cadwallader and Bates 1886, 244). Indeed, some Anglo-Saxon masonry lies within the fabric of the church today, as detailed in its Historic England list entry (NHLE list number 1042770), as well as pieces of carved Roman stone. The church lies approximately 500m east-south-east of the development site and is a Grade I listed building.

Although Heddon-on-the-Wall was not included in the Domesday Survey of 1086, documentary sources attest that it and five surrounding townships (East Heddon, West Heddon, Houghton, Whitchester, and Eachwick) were granted by Henry I to Hugh de Bolbec in the early 12th century. Together, this land became known as the Bolbec Barony (Cawallader and Bates 1886, 245). In the

mid-14th century, its lands were divided between two co-heiresses, with the manor of Heddon falling to Philippa, from whom it descended through the Dacres to the Howards, Earls of Carlisle (Cawallader and Bates 1886, 252).

Between 350-400m to the south-east of 53 Military Road, recent excavations uncovered medieval industrial waste deposits within the Vallum ditch. Analysis and dating of the deposits showed that the ditch had been a large open feature between the 10th and 11th centuries and had been used to dump both domestic and industrial waste. The deposits showed that grain processing and possibly metalworking were being carried out nearby and that Heddon-on-the-Wall was a settlement of some importance in the early medieval period.

Just over 300m to the south-south-west of 53 Military Road lies the deserted medieval village of Houghton. It was first recorded in 1291 and is depicted on maps until the 19th century. However, little can be seen today except for a boundary bank to the south of the farmhouses.

Heddon-on-the-Wall was an important mining location from at least the early 17th century onwards, and collieries and quarries were in use in the area until the 19th century. In 1717, the parish of Heddon-on-the-Wall was subject to inclosure of common lands and in 1752 the Military Road was constructed. During the early 19th century, services such as Poor Houses, schools and the railway came to Heddon-on-the-Wall, and the village underwent some development and expansion during the 19th and 20th centuries. Several of the buildings dating to this period of expansion are recorded under the HER as Grade II listed buildings. Those located within 500m of the proposed development site include: South Houghton farmhouse; North Noughton farmhouse; Merci Beaucoup cottage; No. 2 Cottage; and the school building, garden wall, gate, and railings of the Old Schoolhouse. These listed buildings form a small cluster roughly 250 – 300m to the south-south-west of the development site. Bays Leap farmstead, around 350m to the north-north-east of the development, first appears on maps during this period.

A shallow, undated earthwork recorded during a watching brief and sited around 450m to the south-west of 53 Military Road is recorded on the HER. The nature and date of this feature is uncertain, but it may be the remains of a pond or bellpit.

Readily available historic maps depict the site as undeveloped farmland from at least the mid-19th century until 1951, when 43 – 57 Military Road were constructed. Whereas numbers 43 – 51 and 57 Military Road lie to the north of the vallum, 53 and 55 Military Road lie to its south. Maps produced from 1859 until at

least 1980 depict the line of the vallum, indicating that this earthwork was still visible during that period. It is important to note here that the line of the proposed gas pipe will span the full width of the vallum as it is depicted on historic mapping.

1.3. AIMS AND OBJECTIVES

The purpose of the investigation was to identify and assess the significance of any element of the historic environment that may have been affected by the utilities works. This would be achieved by determining and understanding the nature, function and character of any remains on the site, in their cultural and environmental setting.

More specifically, the aims of the investigation included:

- to ensure that archaeological monitoring was undertaken on all aspects of the groundworks associated with the scheme;
- to ensure the adequate recording of any archaeological remains that were revealed by the scheme;
- to ensure the analysis, conservation and long-term storage of any artefactual/ecofactual material recovered from the site;
- to integrate the results of the works into the wider historic and archaeological context of the landscape; and
- to ensure that an accurate and comprehensive record and report of any archaeological deposits found during works was produced and disseminated to the appropriate organisations.

2. METHODOLOGY

Archaeological monitoring was carried out in accordance with the WSI (Headland Archaeology 2021) and was undertaken according to the standards set out in the relevant Chartered Institute for Archaeologists 'Codes of Conduct' and 'Standards and Guidance' documents (CIfA 2019; CIfA 2020).

All potentially significant features and deposits were subsequently be cleaned by hand, planned and recorded in accordance with CIfA standards and guidance (CIfA 2020). The spatial extent of the trenches recorded.

2.1. RECORDING

Contexts were given unique numbers and all recording was undertaken on *pro forma* record sheets that conform to accepted archaeological standards. All stratigraphic relationships were recorded. Once the required depth (0.7m – 0.8m) was reached for the utilities works, two representative sections were drawn at a scale of 1:20, identifying individual contexts and the underlying natural deposits. However, as these sections are shown clearly on the presented photographs, they are not reproduced in this report.

Within this report, deposits are shown in round brackets and cuts in square brackets.

An overall location and site plan at appropriate scales is shown in Figures 1 and 2. A section showing probable vallum deposits is shown in Figure 10.

A photographic record was taken, and selected images are presented in Figures 3 – 19.

Trench, context, drawing, and photographic registers are presented in Appendix 1.

2.2. SITE WORKS

Monitoring of the work was carried out on the 11th and 12th May 2021.

Excavation of the new gas pipe was carried out using a combination of hand and machine digging, with a total of 9 small pilot trenches excavated along its length (Figure 2). A pneumatic mole was used to join each of these pilot trenches so that the new pipe could be inserted between them.

The pilot trenches varied from 1.3m to 15m in length but were all of 0.5m width. Apart from Trench 9, which reached a maximum depth of 1m, the trenches were dug to a depth of 0.7m – 0.8m.

3. RESULTS

A total of twelve deposits, layers, and features were identified. Of these, only two, (004) and (007), were thought to pre-date the modern era. The dimensions of these deposits, layers, and features and their stratigraphic relationships are detailed in Tables 1.1 and 1.2 of the Appendix.

Context (001), the modern gravel drive, was seen in all trenches apart from Trench 9, and exhibited an average thickness of 0.1m. According to the landowner, this drive was laid during the late 1990s.

Context (001) was underlain by context (002), the earlier gravel drive, present in all trenches apart from Trench 1 (Figures 6 – 10 and 14 – 17). It comprised small – medium-sized cobbles in a very dark greyish silty matrix and is likely to be the original driveway associated with the construction of the property. Natural deposit (003), a friable medium-fine grained yellowish white sandstone and yellowish orange clay, was seen in all trenches but Trench 4 (Figures 5 – 8 and 14 – 17).

Trenches 1 and 2 contained modern service trenches, which underlay (001) and cut into (002) (Figures 5 and 6). Trench 9 contained the modern cobble driveway deposits (008) and (009), which overlay the earlier driveway of (002). It also contained two phases of footpath for Military Road, the earlier phase, (011), truncated by modern gas main [012] running east – west along the road. Trench 9's section suggested that the driveway deposits abutted the footpath deposits, although their installation may have been contemporary (Figure 19).

The only two deposits of a probable archaeological nature were (004) and (007), both seen in Trench 4 (Figures 9 and 19). (004), overlying (007), was a compact mid-greyish brown sandy silt with no tip lines or inclusions, suggesting a natural silting

depositional process. (007) was a very compact light orangish yellow coarse sandy clay, seen at the very base of Trench 4 and interpreted as redeposited natural. No cut relating to these deposits was seen in section or plan. The deposits encountered are considered to be fills within the vallum ditch, the cut of which is likely to be deeper and wider than the boundaries of Trench 4.

Figures 11 – 13 show that the banks and ditch of Hadrian's Wall's vallum survive in very good condition in the garden of 53 Military Road, to the immediate west of the excavated trenches. Trench 4 was clearly located in the base of the vallum ditch (see Figure 13 in particular), and therefore contexts (004) and (007) are thought to be vallum ditch fill deposits.

The deposits of Trench 4 and the elevated level of Trench 5, to Trench 4's north, indicate that the OS data for the position of the vallum ditch and banks is incorrect; these actually lie approximately 7m to the north of their recorded locations (see Figure 20).

The present elevation of the driveway of 53 Military Road is clearly lower than the property's garden to its west, and slopes downwards towards Military Road in a gradual and consistent manner. This, and the recorded sections of the pilot trenches, suggests that substantial terracing, truncation, and levelling occurred during the laying of the original driveway. On site, this was estimated to reach depths of up to 1.5m, thereby removing any archaeological remains and cutting into natural deposits. It is believed that the remains of the vallum ditch, however, being lower in elevation, avoided the worst of this disturbance and are therefore it is still present within the area of Trench 4.

No finds were recovered and (004) and (007) were deemed by Historic England to be too sterile to undergo environmental sampling (pers. comm Don O'Meara, 13th May 2021).



Figure 3: View of 53 Military Road from Military Road, looking south

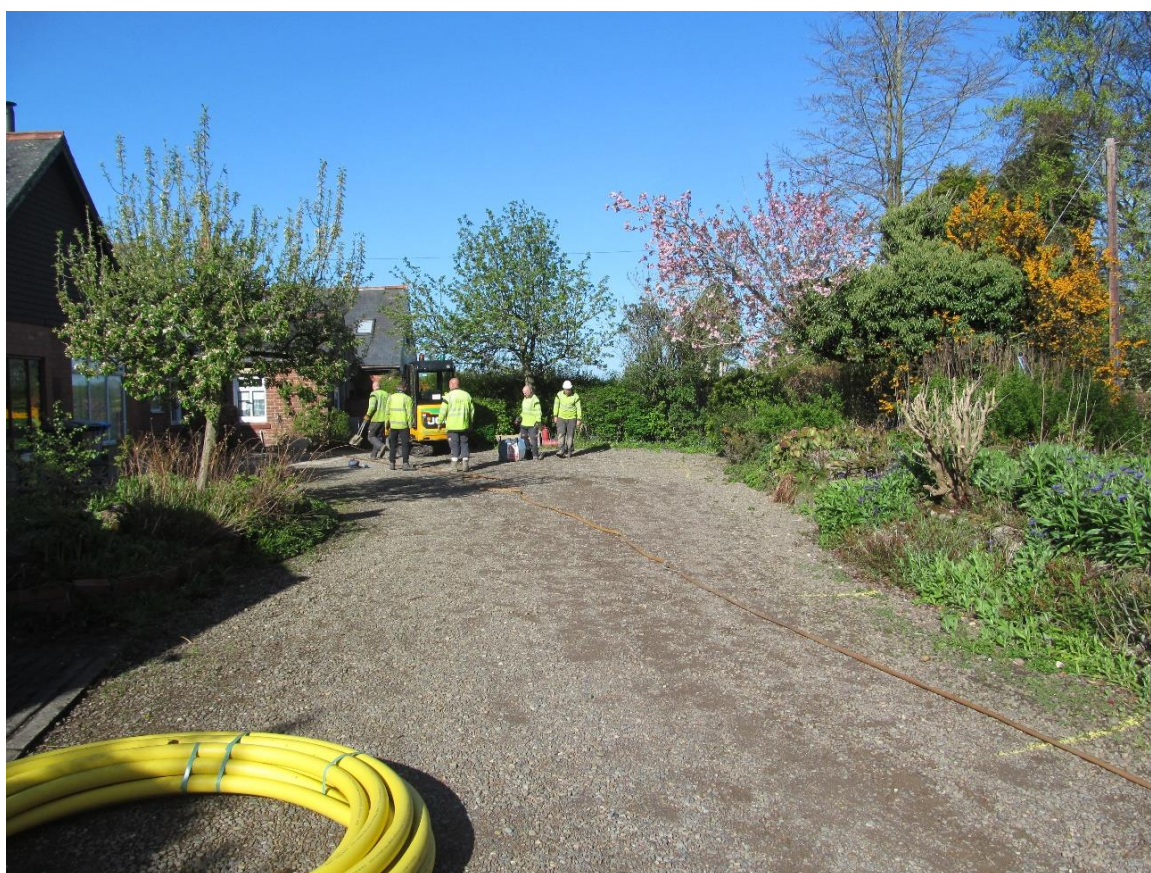


Figure 4: View of 53 Military Road property and car parking area, looking south-west



Figure 5: Trench 1, looking south-west. Unscaled.

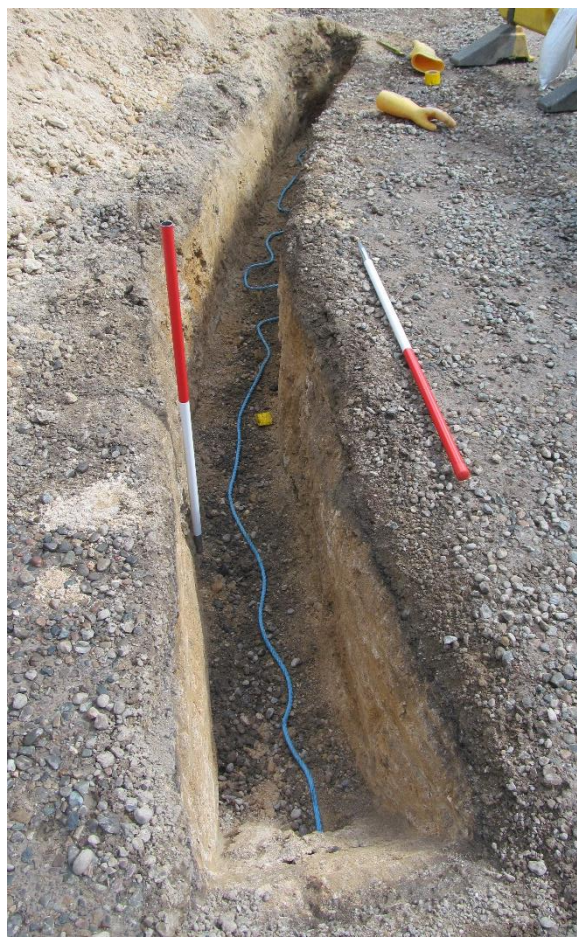


Figure 6: Trench 2, looking east. 2 x 2m scale



Figure 7: Trench 3, western half, looking west-south-west. 2 x 1m scale



Figure 8: Trench 3, looking north-east. 2 x 1m scale



Figure 9: East-facing section of Trench 4, showing probable vallum deposits. 1 x 1m scale



Figure 10: View over garden of 53 Military Road, showing greenhouses sat within the vallum ditch and the vallum banks rising to their left and right. Looking south-south-west to north-west



Figure 11: View over greenhouses sat within the vallum ditch, with 53 Military Road in the background. Looking south-east



Figure 12: View over Trench 4's southern end, showing greenhouses in the centre background and the rise of the vallum bank to their left



Figure 13: Trenches 5 (foreground) to 8 (background), looking north. 1 x 1m scale



Figure 14: East-facing section of Trench 6, representative of Trenches 5 – 8.



Figure 15: Trench 8, looking north. 2 x 1m scale



Figure 16: East-facing section of Trench 9, showing earlier iterations of 53 Military Road's driveway and the footpath along Military Road. The two are demarcated by the protruding brick to the left of the vertical scale



Figure 17: View of southern end of the PDA once excavated, looking south-west



Figure 18: View of northern end of the PDA once excavated, looking south

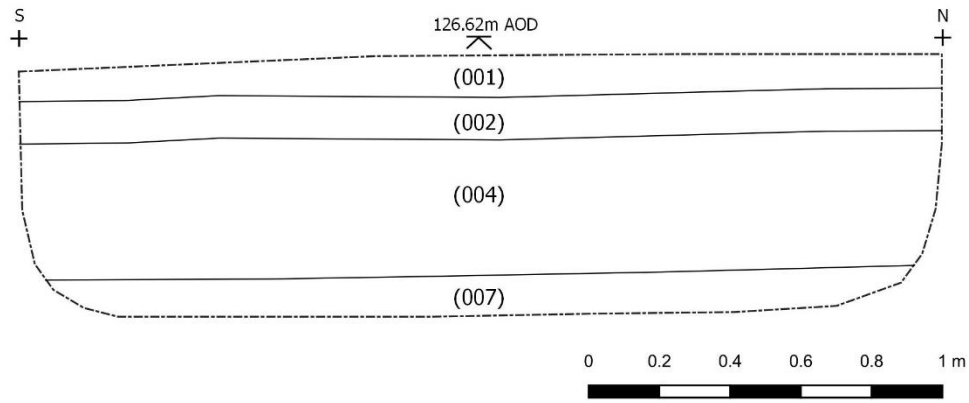
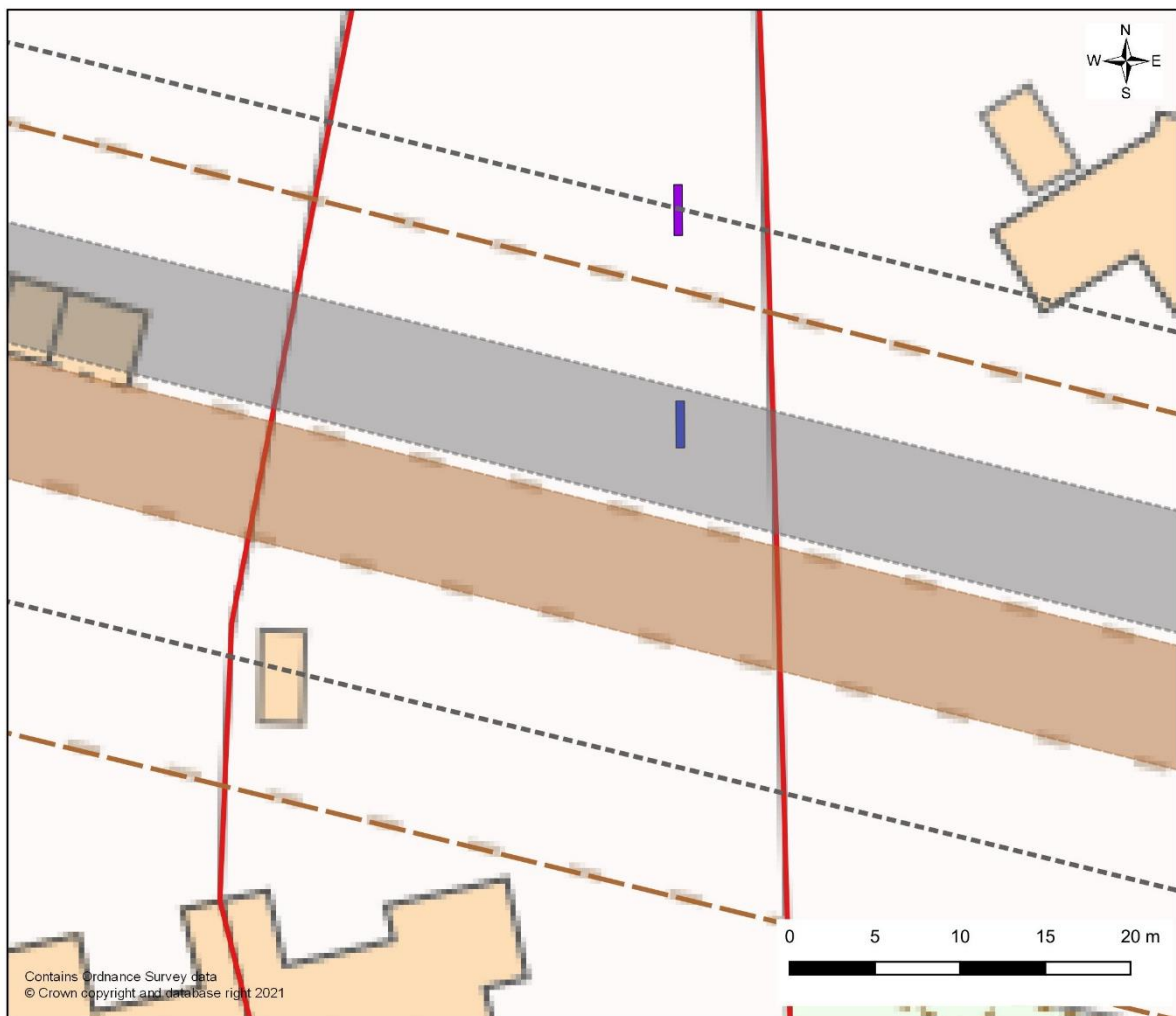


Figure 19: Drawn section of Trench 4, showing vallum deposits



Key

- Site boundary
- Line of vallum north bank (OS data)
- Line of vallum ditch (OS data)
- Line of vallum south bank (OS data)
- Approximate line of vallum north bank (after fieldwork)
- Approximate line of vallum ditch (after fieldwork)
- Trench 4
- Trench 5

Figure 20: Line of vallum banks and ditch, as shown on OS mapping and after fieldwork

4. CONCLUSION

The watching brief was successfully undertaken in line with the provisions of the scheduled monument consent and the specification set out in the WSI.

All trenches apart from Trench 4 contained only modern deposits relating to two iterations of the current driveway, which directly overlay natural deposits. Two modern service trenches were also seen. The only archaeological features relating to Hadrian's Wall that were identified were two probable fills of Hadrian's Wall's vallum (004 and 007) in Trench 4. This trench was located within the base of the vallum ditch, as seen to the west within the garden of 53 Military Road. These results also demonstrate that the existing OS data for the vallum

position is inaccurate, and that it, lies around 7m to the north of its previously recorded location.

The laying of the earliest driveway for 53 Military Road clearly entailed substantial terracing, truncation, and levelling of the ground, thereby removing any in-situ archaeological remains. Trench 4 avoided the worst of this disturbance because the ground's elevation was already low over the backfilled vallum.

No finds were recovered and (004) and (007) were deemed by Historic England to be too sterile to undergo environmental sampling (pers. comm Don O'Meara, 13th May 2021).

5. REFERENCES

British Geological Society
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Chartered Institute for Archaeologists
2019
Code of Conduct

Chartered Institute for Archaeologists
2020
*Standard and Guidance for an
Archaeological Watching Brief*

Headland Archaeology 2021
*53 Military Road, Heddon-on-the-
Wall: Written Scheme of Investigation for
an Archaeological Watching Brief*

Historic England SMC
*S00241026: X1/20429 53 Military
Road, Heddon on the Wall, NE15 0HA –
gas works. Granted 2nd March 2021*

6. APPENDICES

APPENDIX 1 SITE AND CONTEXT REGISTERS

1.1 Trench register

Trench No	Matrix	Orientation	Dimensions (as appropriate)		
			Length	Width	Depth (max)
1	(001) [005] (003)	SW - NE	3m	0.5m	0.7m
2	(001) [006] (002) (003)	SW - NE	5.2m	0.5m	0.8m
3	(001) (002) (003)	E - W to N - S	15m	0.5m	0.8m
4	(001) (002) (004) (007)	N - S	2.6m	0.5m	0.7m
5	(001) (002) (003)	N - S	3m	0.5m	0.8m
6	(001) (002) (003)	N - S	1.3m	0.5m	0.8m
7	(001) (002) (003)	N - S	1.4m	0.5m	0.8m
8	(001) (002) (003)	N - S	10m	0.5m	0.8m
9	(008) (010) (009) [012] (002) (011) (003)	N - S	2.3m	0.6m	1m

1.2 Context register

Context No	Trench No	Description	Dimensions (as appropriate)		
			Length	Width	Thickness / Depth
(001)	T1 - 8	Modern gravel drive	Entire site	0.5m	0 - 0.1m
(002)	T2 - 9	Earlier (mid-20 th century) gravel drive	Entire site	0.5m	0.12m
(003)	T1 - 3; T5 - 9	Natural: Friable medium-fine grained yellowish white sandstone and yellowish orange clay.	N/A	N/A	N/A
(004)	T4	Probable upper fill of Roman vallum	2.6m	0.5m	0.38m
[005]	T1	Electric service trench	3m	0.2m	0.6m
[006]	T2	Sewer trench	0.5m	0.5m	0.1m
(007)	T4	Probable lower fill of Roman vallum	2.6m	0.5m	0.1m
(008)	T9	Modern cobble driveway	1m	0.5m	70mm
(009)	T9	Sandy make-up layer for (008)	1m	0.5m	50mm
(010)	T9	Modern tarmac footpath	1.23m	0.5m	80mm
(011)	T9	Earlier footpath and make-up layer	0.9m	0.5m	0.2m
[012]	T9	Gas main	0.5m	0.5m	1.84m

1.2 Drawing register

Drawing no	Sheet No	Plan	Section	Scale	Description
0001	01	✓		1:1000	Sketch plan of site
0002	01		✓	1:20	Trench 9 east-facing section
0003	01		✓	1:20	Representative section of Trenches 1 - 3 and 5 - 8

1.3 Photographic register

Photo no	Digital No	Direction	Description
0001	IMG-2407	S	Pre-ex shot of access road from Military Road to 53 Military Road
0002	IMG-2408	SW	Pre-ex shot of access road from Military Road to 53 Military Road
0003	IMG-2410	S	Groundworks set up of Trench 1 (working shot)

Photo no	Digital No	Direction	Description
0004	IMG-2411	N	Groundworks set up of Trench 1 (working shot)
0005	IMG-2412	SW	Groundworks in Trench 1 (working shot)
0006	IMG-2413	SW	Natural deposits in base and sections of Trench 1
0007	IMG-2415	N	Groundworks in Trench 2 (working shot)
0008	IMG-2416	SW	Trench 1 and 2 working shot
0009	IMG-2417	NE	Groundworks in Trench 2 (working shot)
0010	IMG-2423	W	View over vallum in garden of 53 Military Road
0011	IMG-2424	NE	Groundworks in Trench 3 (working shot)
0012	IMG-2428	SW	Post-excavation shot of Trench 2
0013 - 14	IMG-2430 - 1	NE	Post-excavation shot of Trench 1 (Trench 2 in background)
0015	IMG-2433	NE	Post-excavation shot of Trench 2
0016	IMG-2434	SW	Post-excavation shot of Trench 1
0017	IMG-2435	ENE	Working shot over Trenches 2 and 3
0018 - 20	IMG-36 - 38	NE - ENE	Post-excavation shot of Trench 2, 2 x 1m scale
0021 - 2	IMG-2439 - 40	SW	Post-excavation shot of Trench 1, 2 x 1m scale
0023	IMG-2442	SW	Post-excavation shot of Trench 3
0024 - 5	IMG-2445 - 6	NNW - N	Groundworks in Trench 4 (working shot)
0026 - 7	IMG-2447 - 8	NNW - N	Groundworks in Trench 5 (working shot)
0028 - 9	IMG-2450 - 1	SW	Post-excavation shot of Trench 3, 2 x 1m scale
0030	IMG-2452	WSW	East-facing section of Trench 3, 1 x 1m scale
0031 - 2	IMG-2453 + 5	NE	Post-excavation shot of Trench 3, 2 x 1m scale
0033	IMG-2456	NNE	Working shot over Trenches 4 - 9
0034	IMG-2458	NE	Post-excavation shot of Trench 4, 2 x 1m scale
0035 - 6	IMG-2459 - 60	NW	Post-excavation shot of Trench 4, 2 x 1m scale
0037	IMG-2461	N	Post-excavation shot of Trench 4, 2 x 1m scale
0038 - 9	IMG-2463 - 4	SW	Post-excavation shot of Trench 5
0040	IMG-2467	WNW	View over vallum from Trench 4
0041	IMG-2470	SSW	General working shot

Photo No	Digital No	Direction	Description
0042	IMG-2472	SE	View along Military Road to east, showing that sits on raised ground
0043	IMG-2473	SW	View along Military Road to west, showing that sits on raised ground
0044	IMG-2474	NW	Groundworks in Trench 8 (working shot)
0045	IMG-2476	W	East-facing section of Trench 8, 1 x 1m scale
0046 - 9	IMG-2476 - 80	NW - N	Post-excavation shot of Trench 8, 1 x 1m scale
0050	IMG-2481	N	Post-excavation shot of Trench 5
0051 - 2	IMG-2482 - 3	W	View over southern end of Trench 5 towards vallum in garden
0053 - 4	IMG-2484 - 5	N	Post-excavation shot of Trench 5, 1 x 1m scale
0055 - 6	IMG-2486 - 7	W	East-facing section of Trench 5, 1 x 1m scale
0057	IMG-2488	NW	Post-excavation shot of Trench 6, 1 x 1m scale
0058 - 9	IMG-2489 - 90	W	East-facing section of Trench 6, 1 x 1m scale
0060	IMG-2491	NW	Post-excavation shot of Trench 7, 1 x 1m scale
0061 - 2	IMG-2492 - 3	W	East-facing section of Trench 7, 1 x 1m scale
0063 - 4	IMG-2494 - 5	NNW	Post-excavation shot of Trench 8, 2 x 1m scale
0065 - 6	IMG-2496 + 9	W	East-facing section of Trench 8, 1 x 1m scale
0067	IMG-2500	NNW	Post-excavation shot of Trench 8, 2 x 1m scale
0068 - 9	IMG-2501 + 4	S	Groundworks in Trench 9 (working shot)
0070 - 1	IMG-2505 - 6	W	East-facing section of Trench 9, 2 x 1m scale
0072 - 3	IMG-2507 - 8	SW	Post-excavation shot of Trench 9, 2 x 1m scale
0074 - 5	IMG-2509 - 10	W	Post-excavation shot of Trench 9, 2 x 1m scale
0076 - 7	IMG-2511 - 2	S - SW	Backfilling on Trench 8 (working shot)
0078	091314_2	W	Mid-excavation shot of Trenches 1 and 2
0079	091442_4	W	Laying pipe in Trench 1 (working shot)
0080	101404_4	N	Mid-excavation shot of Trench 9
0081 - 2	104019_8 + 104026_7	S	Working shot of Trenches 8 and 9
0083	092100_3	W	View over vallum in garden of 53 Military Road

Photo No	Digital No	Direction	Description
0084 - 9	112255_9 – 112304_8	SE - NNW	Panorama over vallum in garden of 53 Military Road
0090 - 7	1124099 – 112418_8	SE - N	Panorama over vallum in garden of 53 Military Road
0098	112517_7	SE	View over vallum to south-west corner of 53 Military Road
0099	120557_8	W	Post-excavation shot of T9
0100 - 1	125250_5 + 125254_9	S - E	View over vallum to south-west corner of 53 Military Road
0102	134133_0	SSW	Working short of Trenches 8 and 9
0103	134256_6	SW	Detail of Trench 9's east-facing section
0104 - 5	140026_7 + 140228_1	S	Post-excavation shot of Trenches 8 and 9

APPENDIX 2 OASIS ENTRY

Summary for headland1-501832

OASIS ID (UID)	headland1-501832
Project Name	Watching Brief at 53 Military Road, Heddon-on-the-Wall
Activity type	Watching Brief
Project Identifier(s)	P21-063
Planning Id	
Reason For Investigation	Scheduled monument consent
Organisation Responsible for work	Headland Archaeology Ltd
Project Dates	11-May-2021 - 12-May-2021
Location	53 Military Road, Heddon-on-the-Wall NGR : NZ 12803 67100 LL : 54.998311, -1.801396 12 Fig : 412803,567100
Administrative Areas	Country : England County : Northumberland District : Northumberland Parish : Heddon-on-the-Wall
Project Methodology	Archaeological monitoring during excavation of new gas pipe trench

Project Results	<p>The work involved the monitoring of the excavation of 9 small trenches which ran along the line of the new gas pipe connection. All trenches apart from Trench 4 contained modern deposits relating to two iterations of the current driveway, which directly overlay natural deposits. Two modern service trenches were also seen. The only archaeological features relating to Hadrian's Wall that were identified were two probable fills of the Roman vallum in Trench 4. This trench was located within the base of the vallum ditch as seen to its west within the garden of 53 Military Road. These results also demonstrate that the existing OS data for the vallum is incorrect, and that it lies approximately 7m to the north of its recorded location.</p> <p>The laying of the earliest driveway for 53 Military Road clearly entailed substantial terracing, truncation, and levelling of the ground, thereby removing any in situ archaeological remains. Trench 4 avoided the worst of this disturbance as the ground's elevation was already low. No finds were recovered and no environmental sampling was undertaken.</p>
Keywords	Vallum - ROMAN - FISH Thesaurus of Monument Types
HER	Northumberland HER - unRev - STANDARD
HER Identifiers	
Archives	unknown - to be deposited with Archaeology Data Service Archive unknown - to be deposited with Great North Museum: Hancock