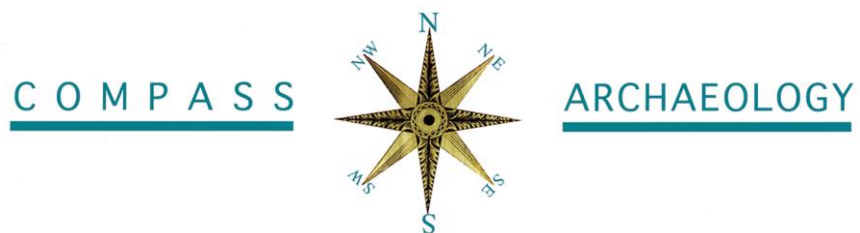


**HIGHWAY IMPROVEMENT WORKS
THE MALL/MARLBOROUGH ROAD, CITY OF
WESTMINSTER, SW1A 1BG**

AN ARCHAEOLOGICAL WATCHING BRIEF



April 2017

HIGHWAY IMPROVEMENT WORKS
THE MALL/MARLBOROUGH ROAD, CITY OF
WESTMINSTER, SW1A 1BG

An Archaeological Watching Brief

Site code: MBH17
Site Location NGR: TQ 29465 79960

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April 2017

Abstract

Between the 23rd January and 15th February 2017, Compass Archaeology conducted an archaeological watching brief on the junction of The Mall and Marlborough Road, SW1A 1BG, during groundwork associated with highway improvement works. The watching brief was commissioned by Peter Brett Associates LLP on behalf of The Royal Parks.

The programme of archaeological works entailed the monitoring and recording of the installation of lighting columns and traffic signal poles, as well as the excavation of trenches for cable ducting installation. The lengths of trench excavated across the site made up c. 125m. They were generally c.0.60m wide and c. 0.20-0.75m deep.

The stratigraphy was homogeneous across the majority of the trenching on site, comprising red asphalt (1) c. 0.10m thick, overlying black asphalt (2) c. 0.08m thick. This lay above a concrete layer (3) c. 0.22-0.26m thick with a mid-brown silty sand layer (4) beneath that. This layer was c. 0.28-0.30m, though it's full extent is unknown. Rare inclusions of ceramic building material (CBM) were found in this layer across the site. Excavations on the southwest traffic island uncovered a concrete slab at a depth of 0.45m below the surface. This was taken to be a previous road surface or similar. The observable portion was 0.70m wide, and 0.18m thick. A layer of gravelly sand backfill (9) was excavated from above the slab on the southern side. Several moulded decorative stone pieces were recovered from this backfill, their date and origin is unknown, some post-medieval pottery was also recovered.

The sequence was interpreted as post-medieval or modern backfill, potentially laid down when The Mall was remodelled into the road that it is today, around 1911. Some of the pottery finds were post-medieval, though they were present in imported backfill which means they cannot be used to date the site activity. The small quantity of CBM that was recovered is most likely modern, as are the moulded decorative stones. These were probably a result of 20th century demolition or bomb damage from the Second World War.

No features of archaeological interest were found, probably due to the limited depth of excavation which did not reach below late post-medieval/modern levels. The lowest level recorded was 5.75m OD.

At this stage, no further groundworks are proposed for The Mall/ Marlborough Road therefore no further archaeological mitigation is required. If this changes, an updated/new proposal, produced in consultation with the relevant parties shall be produced.

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1 INTRODUCTION

- 1.1 The following document forms a summary of the results of an archaeological watching brief conducted on junction of The Mall and Marlborough Road, City of Westminster, SW1A 1BG by Compass Archaeology between 23rd January and 15th February 2017 (fig. 1).



Figure 1: Site location, marked in red. Reproduced from OS data with the permission of the Ordnance Survey on behalf of The Controller of HMSO

- 1.2 The watching brief was commissioned by Peter Brett Associates LLP on behalf of The Royal Parks to oversee highway improvement works in this historically significant and archaeologically sensitive area. The site spans the conservation areas of *Royal Parks* and *St James's* as designated by the City of Westminster Council (fig. 2).
- 1.3 The programme of archaeological works entailed the monitoring and recording of highway improvement works, including groundworks associated with the installation of lighting columns and traffic signal poles, as well as the excavation of new cable ducting below the carriageway and footpaths.

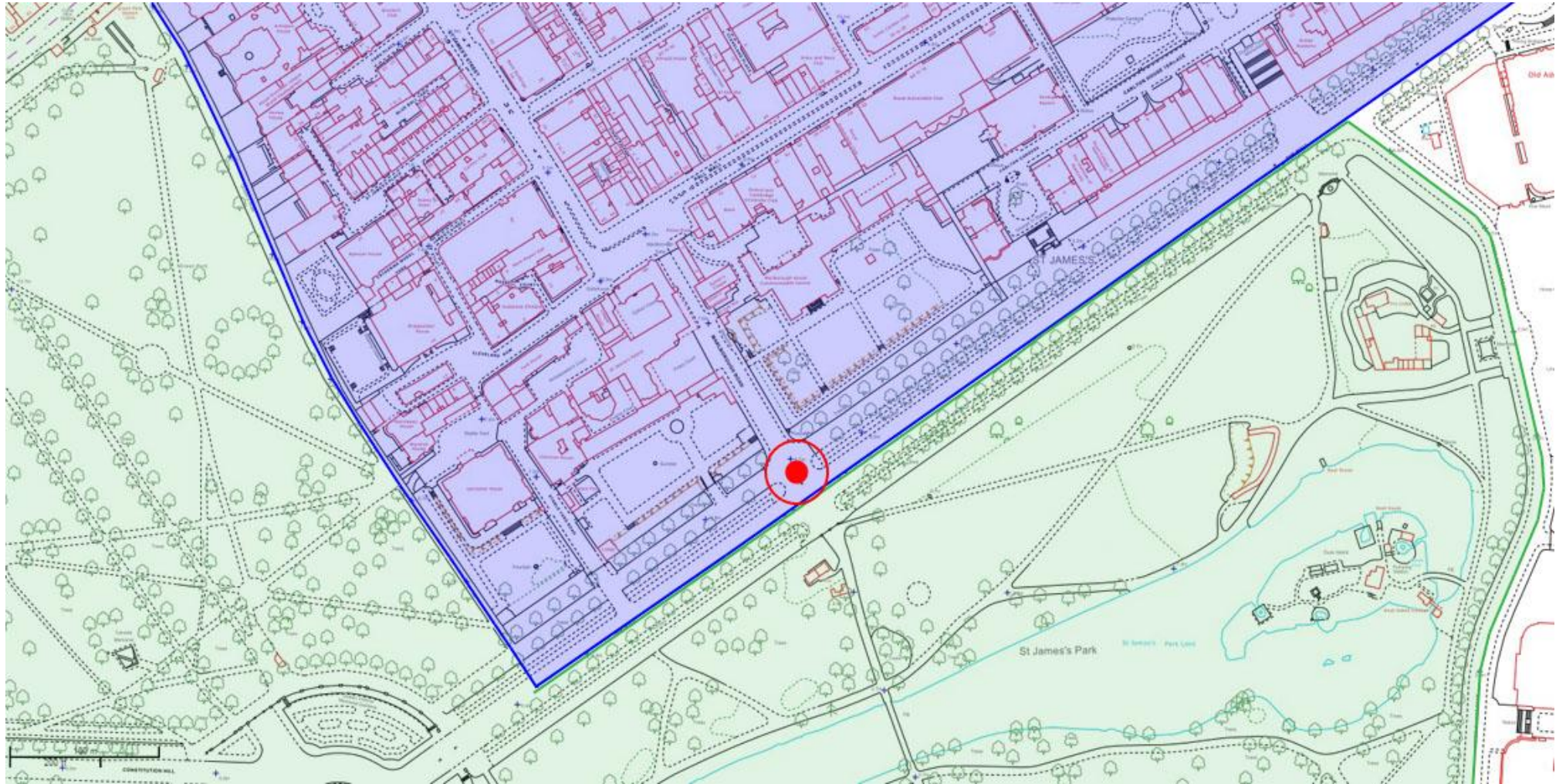


Figure 2: Site location (red) in relation to the Conservation Areas of Royal Parks (green) and St James (blue). Extract adapted from the 'Conservation Area Map', Westminster City Council 2017. Reproduced from OS data with the permission of the Ordnance Survey on behalf of The Controller of HMSO

2 ACKNOWLEDGEMENTS

- 2.1** Compass Archaeology would like to thank Peter Brett Associates LLP and the Royal Parks for commissioning Compass to undertake the archaeological watching brief, and also Conway's for ensuring accessibility and support on site during their groundworks.

3 SITE LOCATION, GEOLOGY AND TOPOGRAPHY

- 3.1** The groundworks were situated towards the western end of The Mall, at the junction with Marlborough Road in the main carriageway, including North Horse Ride. The works extended south to Marlborough Gate, bounded by St James's Palace to the north-west and St James's Park to the south-east.
- 3.2** According to the British Geological Survey, (Sheets 256, *North London* & 271, *South London*) the site overlies an outcrop of Kempton Park Gravel, bounded to the west and south by a deposit of Langley Silt overlying a band of alluvium, following the course of the River Thames. The site lies to the south of a large outcrop of Lynch Hill Gravel overlying Hackney Gravel and London Clay.
- 3.3** The site is level, situated at approximately 6.7mOD, with the junction sitting at the bottom of a slight slope northwards up Marlborough Road, the northern end being at 7.6mOD.

4 ARCHAEOLOGICAL AND HISTORIC BACKGROUND

4.1 Prehistoric

During the prehistoric period, much of the archaeological evidence for activity and population is concentrated around the River Thames, where gravel islands would have provided relief from the swampy landscape. A hoard of prehistoric Potin, or billon coins (an alloy of bronze, tin and lead), was found in St James's Park during groundworks for the canal (MLO 11016).

It is likely that intensive post-medieval activity in the area of the site would have most likely partially or wholly truncated any additional prehistoric archaeological remains.

4.2 Roman

There is limited archaeological evidence of Roman activity in the vicinity of the site. The Roman settlement of *Londinium* was centred on the present day City of London, extending from Ludgate in the west to Aldgate in the east and as such there is little evidence for Roman occupation outside this area.

Two excavations in St James's Park (MLO 2863; MLO 73682) recovered isolated Roman finds, but these were likely residual, a representation of Roman activity rather than specific evidence for the occupation of the site itself.

4.3 Saxon

There has been no evidence of Saxon activity in the vicinity of the site. This is most likely due to the lack of extensive Saxon occupation in the area. The main Saxon settlement, *Ludenwic*, was located further west in what is now Covent Garden where substantial remains have been excavated to corroborate this.

4.4 Medieval

A hospital for leprous women, dedicated to St James, was established in the 12th century on the land immediately north of the site. Subsequent excavations (MLO 60714) uncovered the foundations of a timber framed building interpreted to be the earliest phase of this hospital. Further work uncovered architectural and ceramic fragments dated to 1150-1250 providing more evidence of the early hospital (MLO 39781). Several other investigations (MLO 63503; MLO 60714) found evidence of demolition and robber cuts from the 13th century, indicating that this early building was short-lived.

The hospital and grounds survived until the 15th century, when it was donated to Eton College. Henry VIII took the land back in the early 16th century and re-established it as St James's Palace.

4.5 Post-Medieval

The most significant post-medieval development in the vicinity of the site was the creation of St James's Palace between 1531 and 1536. Previous archaeological investigations have found evidence of this early phase, including the remains of a Tudor garderobe (MLO63502) and postholes, believed to be associated with the initial construction. 16th century walls and an extensive 17th century drainage system linking St James to the adjacent Marlborough House (MLO98817; MLO76316), indicate the site was a significant structure, even in its earlier phases. Plans from 1720 and 1761 (figs. 3 & 4) show the development of the palace, the western ranges in particular. The palace remained the principal royal residence until the purchase of Buckingham House to the west in 1762 by George III. In 1809, a fire destroyed part of the palace, including the monarch's private apartments at the south east corner. These apartments were not replaced, leaving the Queen's Chapel in isolation. Marlborough Road now runs between the two buildings.

5 PLANNING AND OBJECTIVES

- 5.1 The groundworks at this site entailed the excavation of trenching for lighting columns, traffic signal poles and new cable ducting underneath the existing carriageway and pavement.

The installation of new lighting columns at the junction of The Mall with Marlborough Road required five pits to be excavated, not exceeding a maximum depth of 700mm.

The installation of the new traffic signal poles, including lighting systems and pedestrian crossing measures comprised several small trenches around the junction, situated on raised kerbs or protected by existing plastic bollards. The maximum depth of these excavations was also 700mm. The groundworks outlined above were connected by a series of new cable ducts. The majority of this trenching measured 500mm wide X 600mm deep (fig. 5).

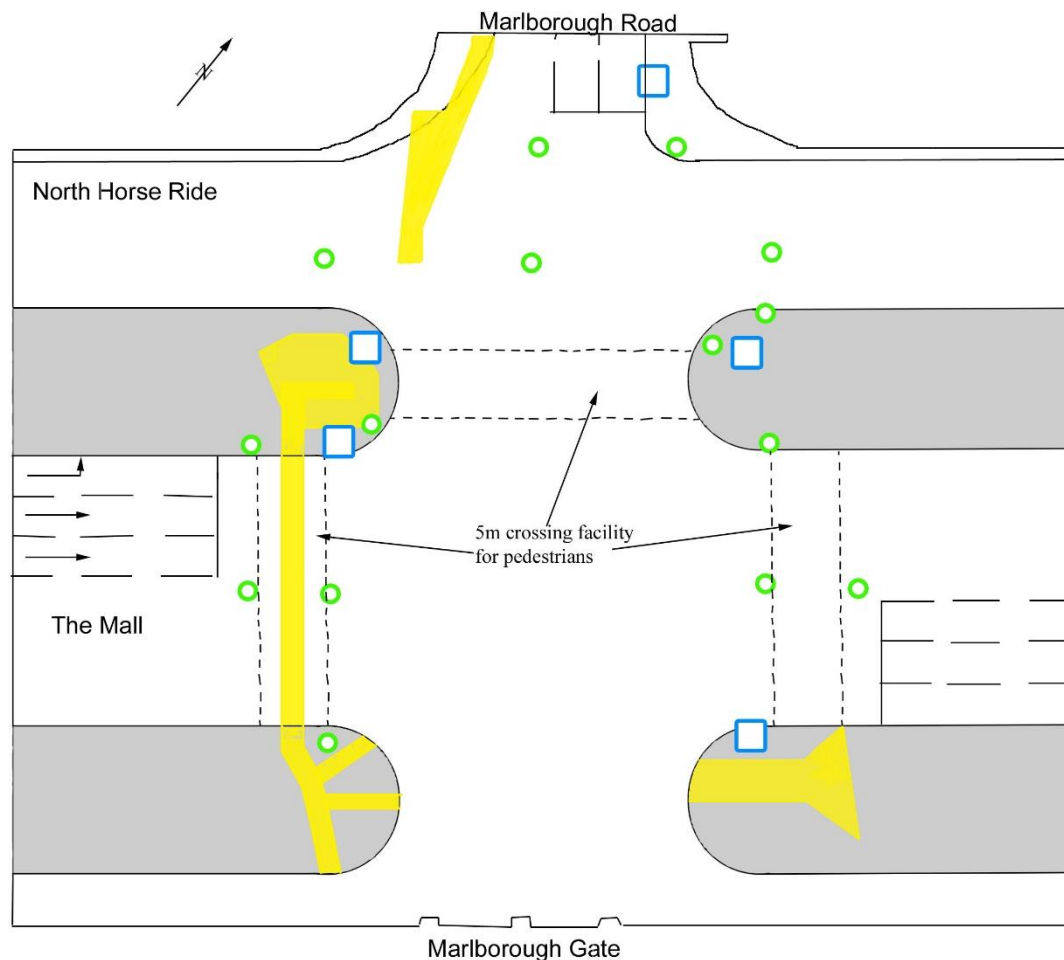


Figure 5: Plan of the works carried out on The Mall/Marlborough Road. Trenching in yellow, new lighting columns: blue squares and new traffic signals: green circles. Adapted from a plan provided by Peter Brett Associates.

- 5.2 The City of Westminster has its own policies concerning archaeological remains, in addition to those set out by the National Planning Policy Framework (NPPF).

Adopted in 2016, *Policy S25 Heritage*, and *Policy DES11* laid out in the Unitary Development Plan (2010) are most relevant to this site:

Policy S25 Heritage

Recognising Westminster's wider historic environment, its extensive heritage assets will be conserved, including its listed buildings, conservation areas, Westminster's World Heritage Site, its historic parks including five Royal Parks, squares, gardens and other open spaces, their settings, and its archaeological heritage. Historic and other important buildings should be upgraded sensitively, to improve their environmental performance and make them easily accessible.

Policy DES 11: Scheduled Ancient Monuments, Areas and Sites of Archaeological Priority and Potential

10.150

In considering applications for development of land with archaeological potential, the City Council will require an archaeological assessment detailing the potential impact of development upon surviving archaeological remains. Should archaeological evaluation and investigations be required, it must be undertaken in accordance with a written scheme of investigation approved by the City Council. The Greater London Archaeology Advisory Service provides guidance papers detailing these procedures. With respect to policy DES 11 B (3), investigation may include a watching brief and, or, a full excavation.

- 5.3** As the site is also situated in the Conservation Areas of *Royal Parks* and *St James's*, the following policy extracts were also applicable (taken from the *Unitary Development Plan Chapter 10: Urban Design and Conservation (2010)*).

Policy DES 9 Conservation Areas

- A) *Applications for outline planning permission in conservation areas.*

In the case of outline planning applications within designated conservation areas it may be necessary to require additional details to be produced in order that the physical impact of the proposed development may be fully assessed.

- (F) *Setting of conservation areas.*

Development will not be permitted which, although not wholly or partly located within a designated conservation area, might nevertheless have a visibly adverse effect upon the area's recognised special character or appearance, including intrusiveness with respect to any recognised and recorded familiar local views into, out of, within or across the area.

5.4 Whilst it was anticipated that the groundworks would mainly expose recent made ground, and some post-medieval activity, the fieldwork presented the opportunity to answer the following research questions:

- Is there any evidence for the early medieval activity on the site, particularly in reference to the hospital of St James the Less?
- Is there any evidence of the demolition / rebuilding of this structure?
- Are there any surviving elements of the Post-medieval playing field prior to its redevelopment in the 17th and 18th centuries?
- At what level does archaeology survive across the site as a whole?
- If encountered, what is the natural geology and at what level does it exist across the site?

6 METHODOLOGY

6.1 Standards

6.1.1 The field and post-excavation work was carried out in accordance with Historic England guidelines (*Greater London Archaeology Advisory Service: Standards for Archaeological Work, 2015*). Works also conformed to the standards of the Chartered Institute for Archaeologists (*Standard and guidance for archaeological field evaluation, 2014*). Overall management of the project was undertaken by a full member of the Chartered Institute.

6.1.2 Fieldwork was carried out in accordance with the Construction (Health, Safety & Welfare) Regulations. All members of the fieldwork team held valid CSCS (Construction Skills Certificate Scheme) cards, and wore hi-vis jackets, hard-hats, steel-toe-capped boots, etc., as required. All members of the fieldwork team also followed the contractors' health and safety guidelines.

6.1.3 The City of London and Historic England were kept informed of the progress of fieldwork and any finds recovered.

6.2 Fieldwork

6.2.1 The archaeological watching brief took place during the groundworks as outlined above. All excavation work was carried out by machine, and backfilled after the installation of the new systems.

6.2.2 Archaeological contexts were recorded as appropriate on *pro-forma* sheets by written and measured description, and drawn in plan and/or section, at scales of 1:20 and 1:10 respectively. The investigations were recorded on a general site plan and related to the Ordnance Survey grid. Levels were taken on deposits, transferred from the nearest Spot Height, situated on the pavement on the NE side of The Mall, E of Marlborough Road at 6.50m. The fieldwork record was supplemented by digital photography, in .jpeg and RAW formats.

6.2.3 The recording system followed the procedures set out in the Museum of London recording manual. By agreement the recording and drawing sheets used are directly compatible with those developed by the Museum.

6.3 Post-Excavation

The fieldwork was followed by off-site assessment and compilation of a report, and by ordering and deposition of the site archive with the Museum of London Archaeological Archive.

6.3.1 Assessment of finds was undertaken by appropriately qualified staff, (see Appendix II). Finds and samples were treated in accordance with the appropriate guidelines, including the Museum of London's 'Standards for the Preparation of Finds to be permanently retained by the Museum of London'. All identified finds and artefacts have been retained and bagged with unique numbers related to the context record, although certain classes of building material and modern finds will be discarded once an appropriate record has been made.

6.4 Report Procedure

6.4.1 This report contains a description of the fieldwork plus details of any archaeological remains or finds, and an interpretation of the associated deposits. Illustrations have been included as appropriate, including a site plan located to the OS grid. A short summary of the project has been appended using the OASIS Data Collection Form (Appendix III).

6.4.2 Copies of this report will be supplied to the Client and Historic England.

6.4.3 There is no provision for further analysis or publication of significant findings. Should these be made the requirements would need to be discussed and agreed with the Client.

6.5 The Site Archive

Assuming that no further work is required, an ordered indexed and internally consistent archive of the evaluation will be compiled in line with Museum of London Guidelines for the Preparation of Archaeological Archives, and will be deposited in the Museum of London Archaeological Archive under site code MBH17. The integrity of the site archive should be maintained, and the landowner will be urged to donate any archaeological finds to the Museum.

7 RESULTS

7.1 What follows is a written description of the observations made during the watching brief. Deposits are shown in round brackets: (x), and cuts and structures in square brackets: [x]. The majority of the excavations revealed homogenous stratigraphy and nothing in the way of finds or features of archaeological interest. Three of the more unique sections are presented below, along with a general overview of the site stratigraphy.

7.2 In total the lengths of trench excavated across the site made up c. 125m. They were generally c.0.60m wide and c. 0.20-0.75m deep. The stratigraphy was homogenous across the majority of the trenching on site, comprising red asphalt (1) c. 0.10m thick, overlying black asphalt (2) c. 0.08m thick. This lay above a concrete layer (3) c. 0.22-0.26m thick with a mid-brown silty sand layer (4), beneath that. This layer was c. 0.28-0.30m, though its full extent is unknown (fig. 6). Rare inclusions of ceramic building material (CBM) were found in this layer across the site.



Figure 6: Stratigraphy of N-S trench at N end. Facing W. Scale 1m.

7.2.1 No finds or features of an archaeological nature were recovered from the trenching.

7.3 A short trench c. 2m long and c. 0.67m deep was dug to the southeast of the N-S running trench which comprised of made ground (5) c. 0.06m thick, overlying a whitish lens (6) 0.05m thick, followed by another layer of made ground (5) c. 0.12m thick. A grey lens (7) lay under this, c. 0.04m thick, followed by the mid-brown silty sand (4), c. 0.40m thick, though its full extent is unknown (fig. 7).



Figure 7: Stratigraphy in small trench SE of N-S trench. Facing SW. Scale 1m.

- 7.3** The excavation of a pole on the northwest traffic island created a slight depression of 0.20-0.30m deep. This was filled with a mixed backfill (8) which contained a small quantity of post-medieval finds, CBM and pottery (appendix II). These were taken to have been imported from somewhere else and were not *in situ* (fig. 8).



Figure 8: Backfill from small post-hole on NW traffic island. Facing S. No scale.

- 7.4** Excavations on the southwest traffic island uncovered a concrete slab at a depth of 0.45m below the surface. This was taken to be a previous road surface or similar. The slab was 0.70m wide, and 0.18m thick. The total length was unknown as it fell outside the limit of excavation. A layer of gravelly sand backfill (9) was excavated from above the slab on the southern side (fig. 9). Several moulded decorative stone pieces were recovered from this backfill, their date and origin is unknown (fig. 10, 11, 12), some post-medieval pottery was also recovered.



Figure 9: Slab beneath walkway on southern side of The Mall. Taken to be a previous road surface or similar. Facing SW, scale 0.5m.



Figure 10: Moulded decorative pieces, mostly broken, found above the road / slab in fig. 9. Scale 0.5m



Figure 11: Side detail of moulded decoration. Scale 0.20m



Figure 12: Top detail of moulded decoration. Scale 0.20m.

8 DISCUSSION

- 8.1 Stratigraphy observed across the site is almost certainly of modern origin, probably deposited when The Mall was remodelled into its current appearance, first in 1911. The red asphalt was added in the mid-1950s, it is unlikely though that this addition would have disturbed the underlying deposits.
- 8.2 The finds that were recovered from this site were post-medieval, though they were present in imported backfill which means they cannot be used to date the site activity. The small quantity of CBM that was recovered are most likely modern, as are the moulded decorative stones. These were probably a result of 20th century demolition or bomb damage from the Second World War. Several high explosive bombs are recorded to have been dropped around The Mall and in the vicinity of the site.

9 CONCLUSION

The following provides a summary of the work undertaken with reference to the original research questions.

- 9.1 *Is there any evidence for the early medieval activity on the site, particularly in reference to the hospital of St James the Less?*

There was no archaeological evidence found for early medieval activity on site, in reference to the hospital of St James the Less or otherwise.

9.2 *Is there any evidence of the demolition / rebuilding of this structure?*

No evidence was found for either the existence of the early hospital building or its demolition.

9.3 *Are there any surviving elements of the post-medieval playing field prior to its redevelopment in the 17th and 18th centuries?*

The excavation did not uncover any surviving elements of the post-medieval playing field that was redeveloped into The Mall.

9.4 *At what level does archaeology survive across the site as a whole?*

Since the excavation did not uncover any archaeological remains, it is clear that the level at which they occur, if there are any present would be more than 0.85m below the surface.

9.5 *If encountered, what is the natural geology and at what level does it exist across the site?*

The level of excavation did not reach the natural geology. The lowest level recorded was 5.75m OD.

9.6 The nature of the site and the shallow extent of the excavation meant that there was very little recovered in the way of archaeological remains. The widely used and remodelled roadway of The Mall means that any archaeological finds would have been disturbed or truncated; potentially several times.

10 **SOURCES**

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10.2 Cartographic Sources

Knyff, J. (1708). *Le Palais et Park de St. James.*

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APPENDIX I: LIST OF CONTEXTS

Context	Description
(1)	Red asphalt road surface
(2)	Black asphalt below (1)
(3)	Concrete bedding for (1) & (2)
(4)	Brown silty sand
(5)	Made ground below pedestrian areas
(6)	Whitish lens in (5)
(7)	Grey gravel/stone lens in (5)
(8)	Imported backfill surrounding post
(9)	Backfill above slab [10]- southern side
[10]	Concrete slab

APPENDIX II: THE FINDS

Pottery by Paul Blinkhorn

The pottery assemblage comprised 18 sherds with a total weight of 406g. It is entirely post-medieval, and largely modern. It was recorded using the conventions of the Museum of London Type-Series (eg. Vince 1985), as follows:

ENGS:	English Stoneware , 1700-1900. 2 sherds, 36g.
HORT:	Horticultural Earthenwares , 19 th – 20 th century. 9 sherds, 116g
PMR:	Post-medieval Redware , 1580 – 1900. 2 sherds, 163g.
REFW:	Refined Whiteware , 1800-1900. 1 sherd, 19g.
ROCK:	Rockingham Mottled Brown-glazed Ware , 1800-1900. 1 sherd, 15g.
STSL:	Staffordshire Slipware , 1650 – 1800. 1 sherd, 14g.
SWSG:	Staffordshire White Salt-Glazed Stoneware , 1720-1780. 1 sherd, 16g.
YELL:	Yellow Ware , 1840-1900. 1 sherd, 27g.

The pottery occurrence by number and weight of sherds per context by fabric type is shown in Table 1. Each date should be regarded as a *terminus post quem*. The range of fabric types is typical of sites in the London area.

Cntxt	PMR		STSL		ENGS		SWSG		HORT		ROCK		REFW		YELL		Date
	No	Wt	No	Wt	No	Wt	No	Wt	No	Wt	No	Wt	No	Wt	No	Wt	
8	1	86	1	14	2	36	1	16	9	116	1	15			1	27	M19thC
9	1	77											1	19			19thC
Total	2	163	1	14	2	36	1	16	9	116	1	15	1	19	1	27	

Table 1: Pottery occurrence by number and weight (in g) of sherds per context by fabric type

Bibliography

Vince, AG, 1985 The Saxon and Medieval Pottery of London: A review *Medieval Archaeology* **29**, 25-93

MISCELLANEOUS

Glass: Two fragments of clear, blue/green tinted glass were recovered from (8). They are both fairly unworn with scratched surfaces.

Fragment 1: measures 65mm x 38mm x 5mm thick.

Fragment 2: measures 28mm x 21mm x 10mm thick.

Animal Bone:

Two fragments of animal bone were recovered from (8). They are both fragmented and the species is unidentifiable. One of the bones is a scapula from a large mammal, and the other is a metapodial from a medium mammal. Rates of fusion are unobservable. The scapula shows some evidence of butchery; 2 fairly shallow chop marks.

Shell: One oyster shell was recovered (8).

Horseshoe:

A partial horseshoe was recovered from (8). It is heavily corroded. 60mm x 83mm x 5mm.

Building Material: Composite

5 moulded decorative elements were recovered from context (9). All five pieces appear to be produced from a relatively lean mix concrete poured into a mould, and potentially finished with a finer mix slip, coated with white paint.

Date unknown, although their high position within the stratigraphy and method of production suggests they are late post-medieval in date, if not modern.

Ceramic Building Material: Tile

Context	Number of fragments	Weight	Min & max dimensions (mm)
(4)	1	18g	42 x 35 x 11
(8)	6	318g	34 x 23 x 10 - 85 x 67 x 14

APPENDIX III: DRAWINGS

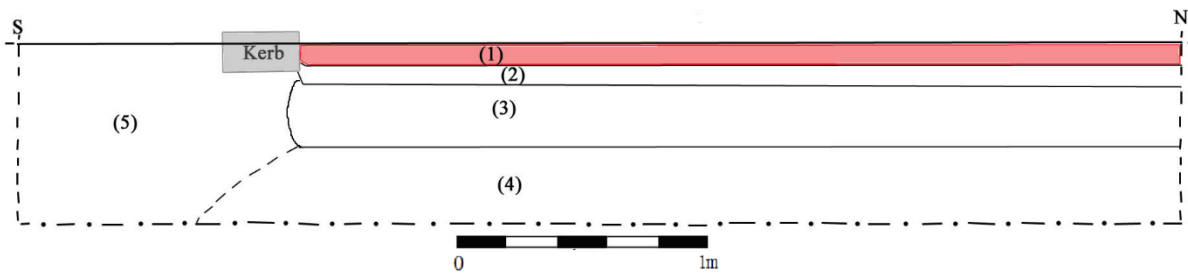


Figure 13: West facing section of N-S oriented trench. Northwest of Marlborough Gate. Scale 1:20

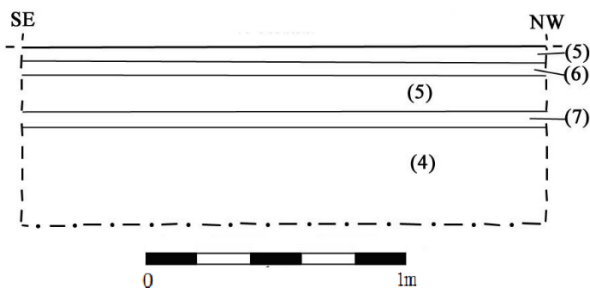


Figure 14: Southwest facing section of trench. Southeast of N-S oriented trench. Scale 1:20

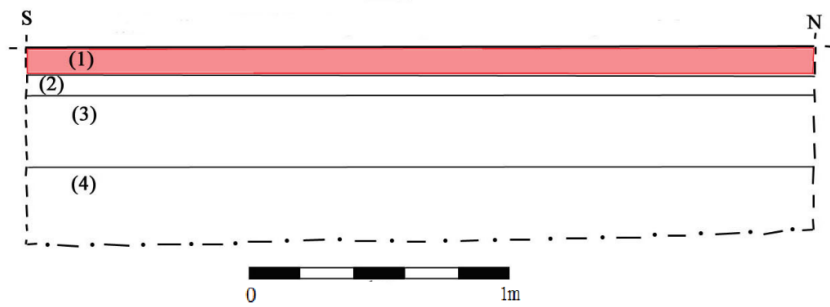


Figure 15: West facing section of N-S trench. Southwest of Marlborough Road. Scale 1:20

APPENDIX IV: OASIS FORM

OASIS ID: [compassa1-283065](#)

Project details

Project name	Highway Improvement Works, The Mall/Marlborough Road, City of Westminster, SW1A 1BG
Short description of the project	Between the 23rd January and 15th February 2017, Compass Archaeology conducted an archaeological watching brief on the junction of The Mall and Marlborough Road, SW1A 1BG during groundwork associated with highway improvement works. The programme of archaeological works entailed the monitoring and recording of excavations for the installation of lighting columns, traffic signal poles and ducting for new cables. The excavations made up c. 125m of trenching, generally 0.60m wide and 0.20-0.75m deep. The stratigraphy was homogeneous, comprising 0.10m thick red asphalt, overlying 0.08m thick black asphalt. This was bedded in a concrete layer 0.22-0.26m thick with a mid-brown silty sand underneath. The extent of this layer is unknown. Some modern ceramic building material and moulded decorative stones were found, along with some post-medieval pottery and other small finds. No features of archaeological interest were found, probably due to the depth of the excavation. The lowest level recorded was 5.75m OD.
Project dates	Start: 23-01-2017 End: 15-02-2017
Previous/future work	No / No
Type of project	Recording project
Site status	Conservation Area
Current Land use	Transport and Utilities 1 - Highways and road transport
Monument type	NONE None
Significant Finds	CERAMIC BUILDING MATERIAL Modern
Significant Finds	MOULDED DECORATIVE STONES Modern
Significant Finds	POTTERY Post Medieval
Significant Finds	GLASS Post Medieval

Significant Finds HORSESHOE Post Medieval

Investigation type "Watching Brief"

Prompt Planning condition

Project location

Country England

Site location GREATER LONDON CITY OF WESTMINSTER CITY OF WESTMINSTER The Mall/Marlborough Road

Postcode SW1A 1BG

Study area 50 Square metres

Site coordinates TQ 529468 179959 50.940517920931 0.177209341484 50 56 25 N 000 10 37 E Point

Project creators

Name of Organisation Compass Archaeology

Project brief originator Peter Brett Associates LLP

Project design originator Compass Archaeology

Project director/manager Geoff Potter

Project supervisor Heidi Archer

Type of sponsor/funding body City of London Corporation

Name of sponsor/funding body The Royal Parks

Project archives

Physical Archive recipient Museum of London archaeological archive

Physical Contents "Ceramics","Glass","Metal"

Digital Archive recipient Museum of London Archaeological Archive

Digital Contents "Ceramics","Glass","Metal"

Digital Media available	"Images raster / digital photography", "Spreadsheets", "Text"
Paper Archive recipient	Museum of London Archaeological Archive
Paper Contents	"Ceramics", "Glass", "Metal"
Paper Media available	"Context sheet", "Photograph", "Report", "Unpublished Text"

Project bibliography 1

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Place of issue or publication	London
Description	Short report summarising the results of the watching brief. Contains relevant background information including reason for commission, site location, historical and archaeological background and site and post-excavation methodology. Results include plans, photographs and text of the monitored work with a short discussion and conclusion. Results are supported by relevant specialist analyses.

Entered by	Miranda Fulbright (mail@compassarchaeology.co.uk)
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