

**THAMES WATER
MAINS REHABILITATION WORKS,
VANBRUGH PARK ROAD, GREENWICH SE3 7NJ
DMA ZOXLWD13**

An Archaeological Watching Brief



September 2017

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VANBRUGH PARK ROAD, GREENWICH SE3 7NJ
DMA ZOXLWD13

An Archaeological Watching Brief

Site code: VPR17

Approximate site centre: NGR TQ 39848 77339

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September 2017

Abstract

On the 15th September 2017 Compass Archaeology conducted an Archaeological Watching Brief located on Vanbrugh Park Road, London Borough of Greenwich SE3 7NJ, during groundworks associated with the abandonment of an old 3" water main and connection to a new 8" PE pipe. The work was commissioned by Thames Water Utilities, based on advice from Compass Archaeology due to the site being located within the buffer zone of the UNESCO World Heritage Site 'Maritime Greenwich'.

The programme of archaeological works entailed the monitoring of the completion of 4 trenches located along the north-south length of Vanbrugh Park Road. The trenches were located in the main carriageway, measuring between 2.2 and 5.6m in length (E-W) x 0.64 – 1.5m in width x a maximum of 1.1m in depth.

The stratigraphy recorded across all four trenches was broadly similar, comprising the existing road surface (1) overlying a series of gravels (5), sandy soil (3) and in some cases natural sand (4). This stratigraphy was truncated in all four trenches by an N-S main cutting through the eastern side of the trench [7], backfilled with a homogenous deposit of MOT Type 1 (8).

A small deposit of rubble, (12) was recorded in the north and south facing sections of Trench 4, which was interpreted as discrete backfilling, most likely using material from the Victorian villas occupying Vanbrugh Park Road until their destruction during the Second World War.

What is taken to be natural sand was encountered in Trench 1 at a depth of 0.75m (c40.85mOD). No further features of archaeological interest were recorded and no finds were recovered.

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1 INTRODUCTION

1.1 This document forms a summary of the results of an archaeological watching brief conducted at Vanbrugh Park Road, London Borough of Greenwich SE3 7NJ by Compass Archaeology on the 15th September 2017 (fig.1).

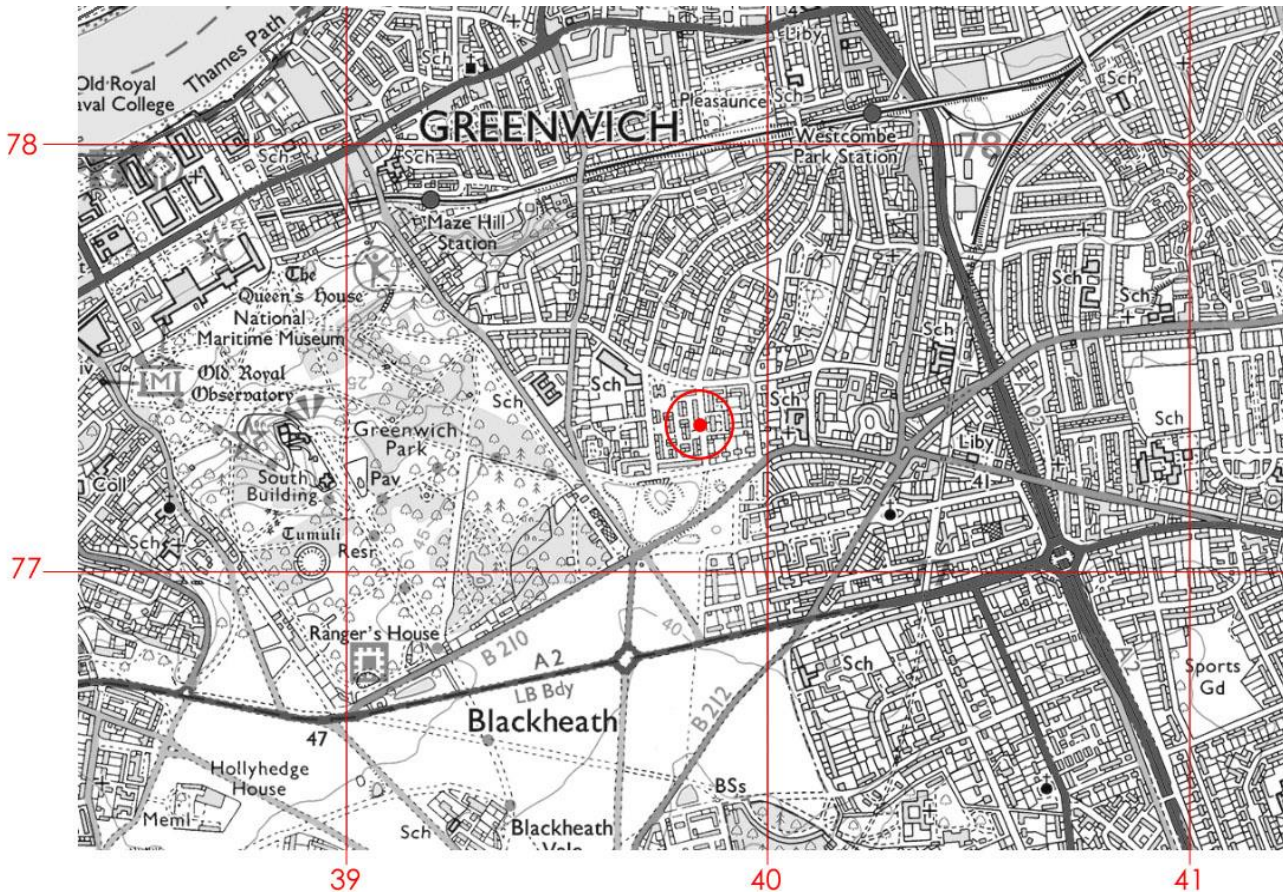


Figure 1: Site location, marked in red.

1.2 The watching brief was commissioned by Thames Water Utilities, following advice from Compass Archaeology identifying the site as being situated within the buffer zone of the UNESCO World Heritage Site of 'Maritime Greenwich', and a locally designated Archaeological Priority Area based around the Watling Street Roman road (fig.2).

1.3 The programme of archaeological works entailed the monitoring of the completion of 4 trenches located along the length of Vanbrugh Park Road to expose the existing utilities network in preparation for replacement works.

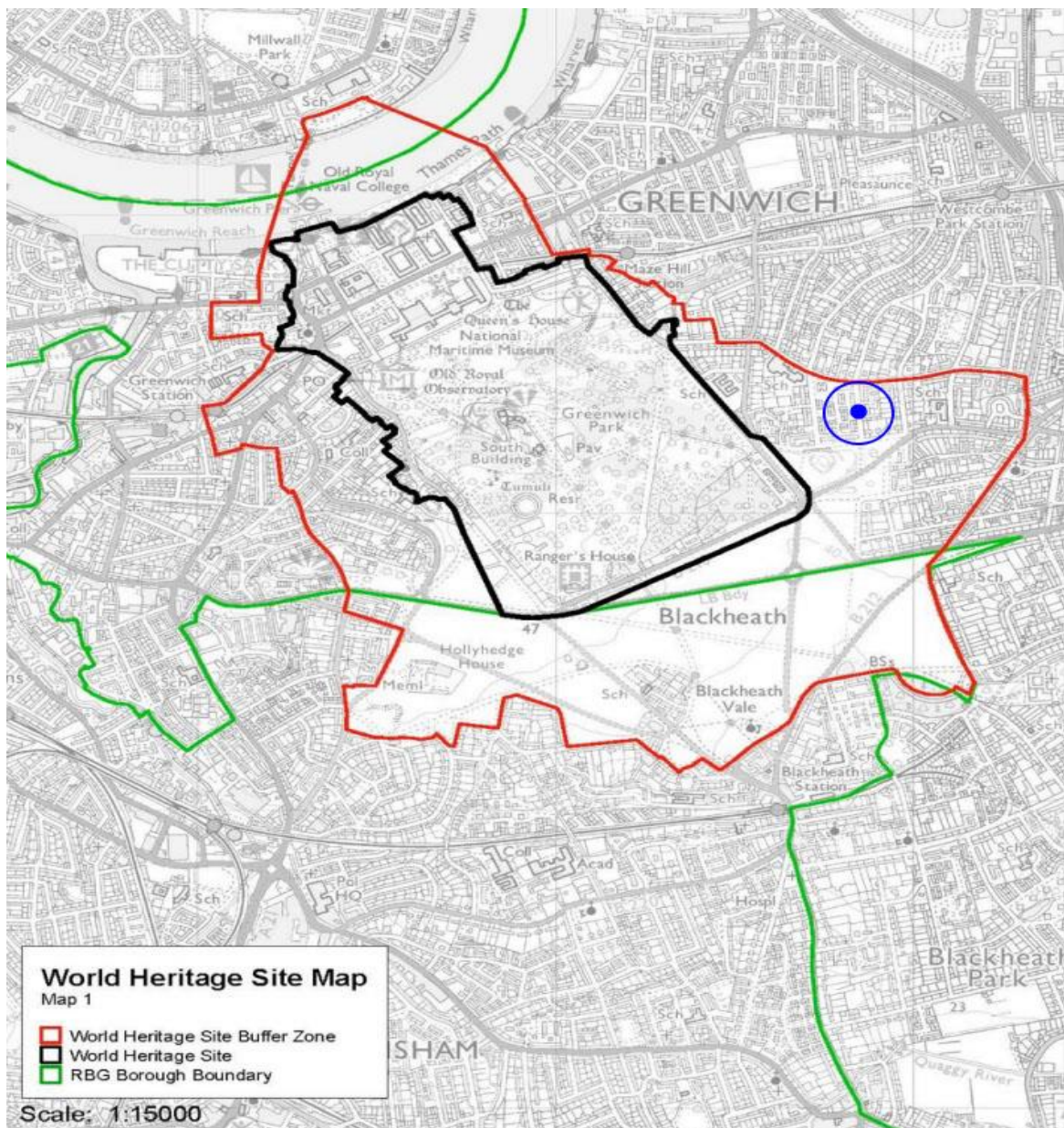


Figure 2: Site location (blue roundel) in relation to the UNESCO World Heritage Site of 'Maritime Greenwich'. Adapted from the Maritime Greenwich World Heritage Site Management Plan Third Review 2014.

2 ACKNOWLEDGEMENTS

2.1 Compass Archaeology would like to thank Thames Water Utilities for commissioning Compass to undertake the archaeological watching brief and for ensuring access to, and support on, site during the groundworks.

3 SITE LOCATION, GEOLOGY AND TOPOGRAPHY

- 3.1** The site covers the length of Vanbrugh Park Road, ending at the junction with Vanbrugh Park to the south and Westcombe Park Road to the north. The road is lined either side by residential properties set back from the pavement, comprised of a mixture of low-rise flats on the west side of the road and two-storey property blocks on the east side, most of which were built in the early 1960s.
- 3.2** The British Geological Survey, Sheets 270: *South London* and 271: *Dartford*, indicates that the works area lies over a large area of the Harwich formation; predominantly sand containing black flints and locally shelly. The ground 1.5km to the north is dominated by River Terrace gravels and alluvium alongside the Thames, and to the south by a wide expanse of London Clay.
- 3.3** The site is relatively level with only a shallow, but noticeable slope downwards situated at approximately 40.9mOD at the southern end of the road and 41.8mOD at the northern end. The ground drops to the east to 40.1mOD.

4 ARCHAEOLOGICAL AND HISTORIC BACKGROUND

- 4.1** Included here is a summary of the archaeological and historical background of the site, which was discussed in depth in the accompanying Written Scheme of Investigation (Compass Archaeology September 2017).

4.2 *Prehistoric*

- 4.2.1** Little evidence for prehistoric activity has been uncovered in the surrounding area; although the location of the site on a prominent hilltop seems likely to have been favoured by early settlers. Two flint flakes were found in the front garden of No.15 Vanbrugh Park Road but only broadly dated to the prehistoric period.

4.3 *Roman*

- 4.3.1** Soon after the Claudian invasion of AD43 the Romans founded *Londinium* and its suburb of Southwark either side of the Thames, linked by the earlier London Bridge. Several of the link roads passed through Greenwich. One of these, Watling Street, formed the road from London to the Kent coast, and is now preserved in the course of Old Dover Road / Shooter Hill Road, and continuing west as Vanbrugh Park Road which passes east-west across the southern end of the works area.

- 4.3.2** There is some evidence of activity in the vicinity of the site, included several coins found on Westcombe Park Road, and in Greenwich Park.

4.4 *Medieval*

- 4.4.1** Greenwich remained something of a rural backwater for much of the medieval period. It was not until the early-15th century when a royal palace was built for the Duke of Gloucester (protector of the minor Henry VI), that Greenwich developed from a small fishing village to a more prestigious location. The works area lies approximately 1.3km south-east of the Palace site.

4.5 *Post-medieval*

- 4.5.1 During the early post-medieval period development was primarily focused on the site of the old Greenwich Palace which fell into disrepair by the middle of the time of the Civil War, and was later demolished and re-occupied by the Royal Naval Hospital designed by Sir Christopher Wren. This collection of buildings and the park to the southeast form the UNESCO World Heritage site of 'Maritime Greenwich', with the works area lying within the buffer zone, put in place to control development and preserve the fabric and setting of the World Heritage Site.
- 4.5.2 The works area lay on the periphery of the main settlement and was not developed until the mid-19th century when Vanbrugh Park Road and the surrounding streets were laid out in c1865, comprising a number of large detached villa properties (fig.3).

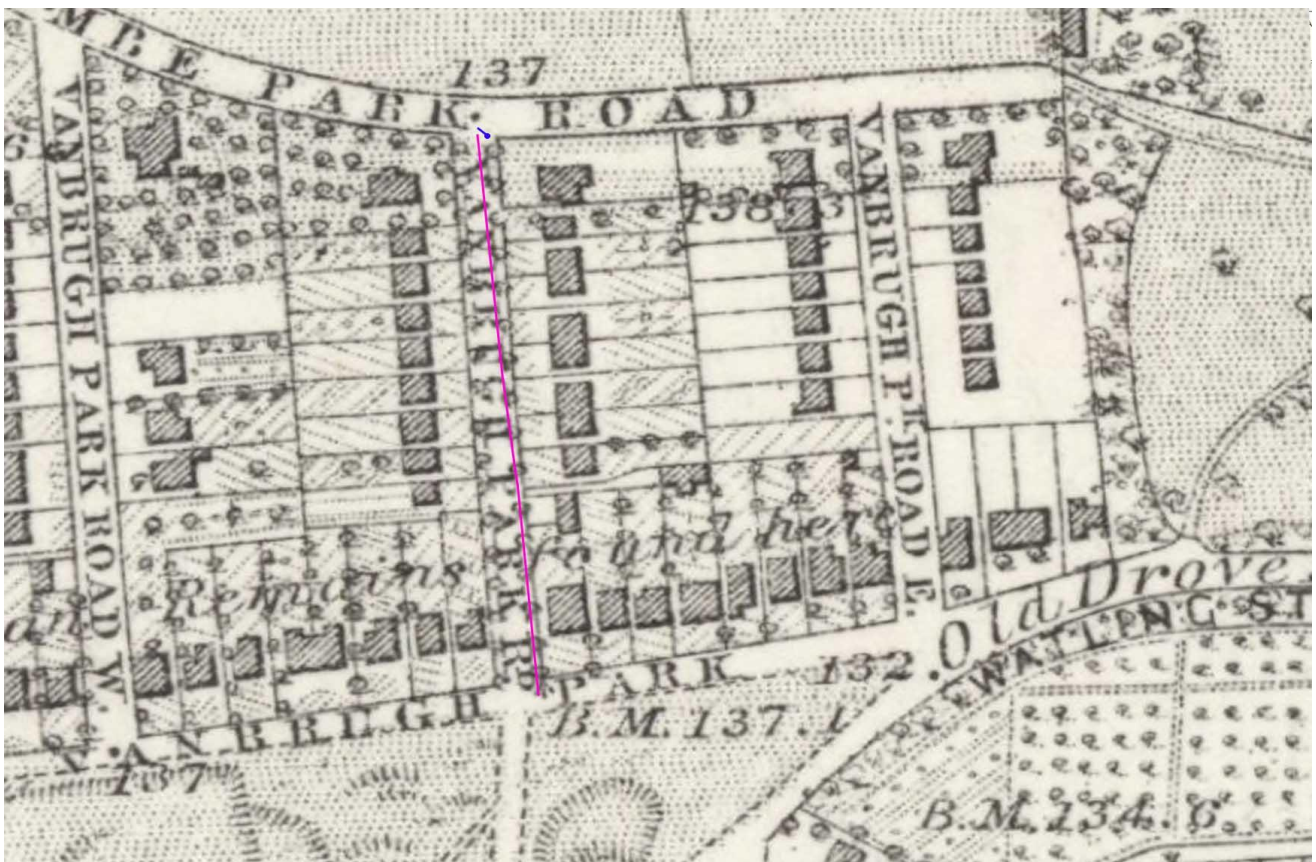


Figure 3: Vanbrugh Park Road (pink) as shown on the 1870 OS map. The road was characterised by a series of detached Villas set back from the road, with extensive rear gardens.

4.5.3 These buildings survived until the Second World War when several of the properties sustained considerable damage as the result of a V1 flying bomb, which hit the west side of the street (fig.4). Most of the properties were subsequently cleared to make way for the Vanbrugh Park Estate, laid out in 1963 by Chamberlain Powell and Bon, which still occupy the street today.

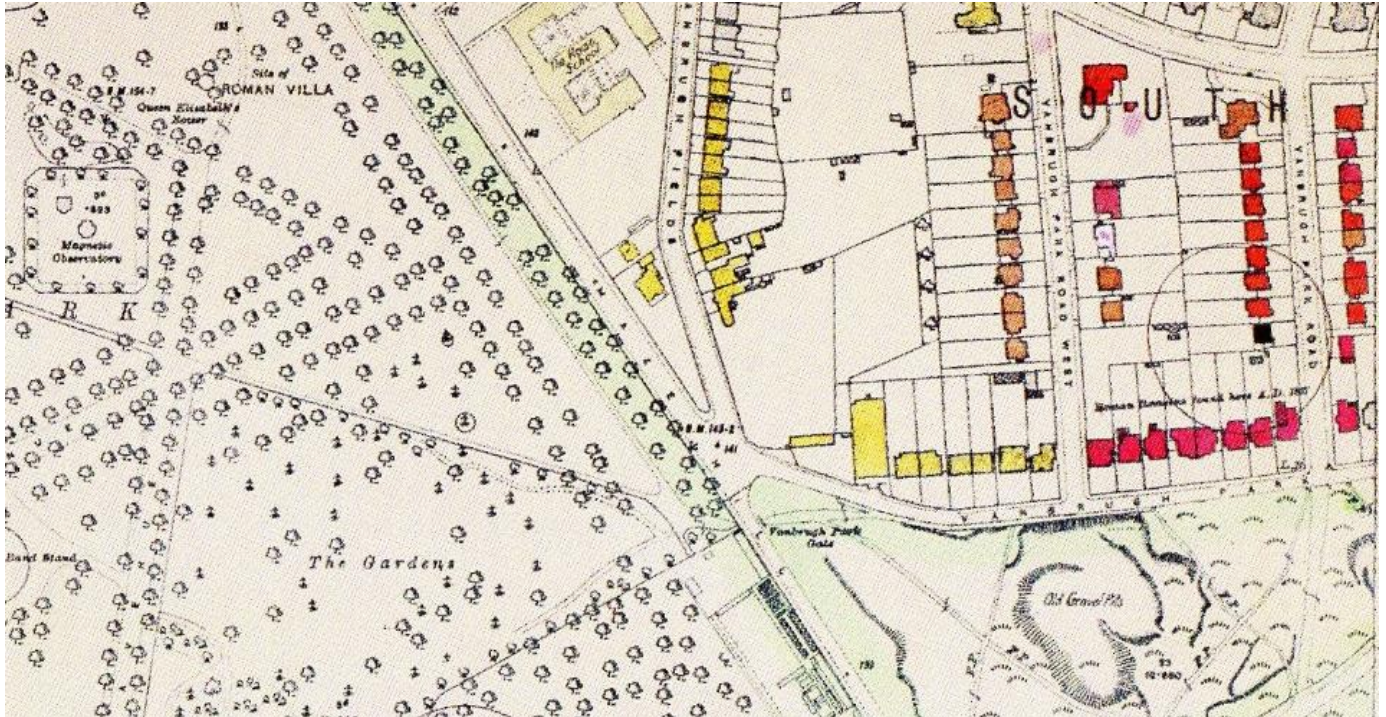


Figure 4: Extract from the LCC Bomb Damage Maps (Ward 2015) showing the extensive damage caused to the site by a V1 flying bomb.

5 PLANNING AND OBJECTIVES

- 5.1** The groundworks followed the standards set out in the London Plan (*Chapter Seven: London's Living Spaces and Places*) which states that new developments are expected to align with the following procedures:

Historic Environment and Landscapes

Policy 7.8 *Heritage assets and archaeology*

Strategic

- A** *London's heritage assets and historic environment, including listed buildings, registered historic parks and gardens and other natural and historic landscapes, conservation areas, World Heritage Sites, registered battlefields, scheduled monuments, archaeological remains and memorials should be identified, so that the desirability of sustaining and enhancing their significance and of utilising their positive role in place shaping can be taken into account.*
- B** *Development should incorporate measures that identify, record, interpret, protect and, where appropriate, present the site's archaeology.*

Planning decisions

- C** *Development should identify, value, conserve, restore, re-use and incorporate heritage assets, where appropriate.*
- D** *Development affecting heritage assets and their settings should conserve their significance, by being sympathetic to their form, scale, materials and architectural design.*
- E** *New development should make provision for the protection of archaeological resources, landscapes, and significant memorials. The physical assets should, where possible, be made available to the public on-site. Where the archaeological asset or memorial cannot be preserved or managed on-site, provision must be made for the investigation, understanding, recording, dissemination and archiving of that asset.*

LDF Preparation

- F** *Boroughs should, in LDF policies, seek to maintain and enhance the contribution of built, landscaped and buried heritage to London's environmental quality, cultural identity and economy as part of managing London's ability to accommodate change and regeneration.*
- G** *Boroughs, in consultation with English Heritage, Natural England and other relevant statutory organisations, should include appropriate policies in their LDFs for identifying, protecting, enhancing and improving access to the historic environment and heritage assets and their settings where appropriate, and to archaeological assets, memorials and historic and natural landscape character within their area.*

- 5.2 Additionally, as the site lies within the buffer zone surrounding ‘Maritime Greenwich’ the site is subject to a Management Plan as protected by various legislation to preserve the heritage assets.
- 5.3 Further, the works also conformed to the London Borough of Greenwich’s Core Strategy (adopted July 2014), specifically policies DH3, DH(h) and DH(m), laid out below:

Policy DH3 Heritage Assets

The Royal Borough will protect and enhance the heritage assets and settings of Royal Greenwich, including the Maritime Greenwich World Heritage Site, preserving or enhancing the character or appearance of the 20 Conservation Areas, applying a presumption in favour of the preservation of statutory listed buildings and their settings, giving substantial weight to protecting and conserving locally listed buildings, protecting the three registered parks and gardens, as well as Royal Greenwich’s archaeological remains and areas of special character.

Policy DH(h) Conservation Areas

i) Character and Setting

Planning permission will only be granted for proposals which pay special attention to preserving or enhancing the character or appearance of the Conservation Area. The local scale, the established pattern of development and landscape, building form and materials will all be taken into account. Development on sites in the vicinity of a Conservation Area and which would have a visual effect of its character or appearance, should respect the setting of the area.

Policy DH(m) Archaeology

The Royal Borough will expect applicants to properly assess and plan for the impact of proposed developments on archaeological remains where they fall within ‘Areas of High Archaeological Potential (AHAPs)’ [as shown in figure 5]. In certain instances preliminary archaeological site investigations may be required before proposals are considered. The Royal Borough will seek to secure the co-operation of developers in the excavation, recording and publication of archaeological finds before development takes place by use of planning conditions/legal agreements as appropriate.

At identified sites of known archaeological remains of national importance, including scheduled monuments, there will be a presumption in favour of physical preservation of the remains in situ and to allow for public access and display and to preserve their settings. For sites of lesser importance the Royal Borough will seek to preserve the remains in situ, but where this is not feasible the remains should either be excavated and removed from the site, or investigated, excavated and recorded before destruction. Appropriate conditions/legal agreements may be used to ensure this is satisfied.

5.4 Archaeological research questions

5.4.1 The fieldwork presented the opportunity to answer the following general and more specific research questions:

- Is there any evidence for prehistoric activity on the site?
- Is there any evidence for development &/or land-use in the medieval and early post-medieval periods?
- What is the evidence for post-medieval activity on the site? What form does the evidence for such activity take?
- At what levels do archaeological and/or geological deposits survive across the site?

6 METHODOLOGY

6.1 Standards

6.1.1 The field and post-excavation work was carried out in accordance with Historic England guidelines (*Greater London Archaeology Advisory Service: Standards for Archaeological Work, 2015*). Works also conformed to the standards of the Chartered Institute for Archaeologists (*Standard and guidance for an archaeological watching brief 2015*). Overall management of the project was undertaken by a full member of the Chartered Institute.

6.1.2 Fieldwork was carried out in accordance with the Construction (Health, Safety & Welfare) Regulations. All members of the fieldwork team held valid CSCS (Construction Skills Certificate Scheme) cards, and wore hi-vis jackets, hard-hats, steel-toe-capped boots, etc., as required. All members of the fieldwork team also followed the contractors' health and safety guidelines.

6.1.3 The Client and Historic England were kept informed of the progress of fieldwork and any finds recovered.

6.2 Fieldwork

6.2.1 The fieldwork entailed the monitoring of 4 trenches located along the north-south length of Vanbrugh Park Road. The trenches were located in the main carriageway, measuring between 2.2m and 5.6m in length, (E - W), by up to 0.64m and 1.5m in width, (N - S), and a maximum of 1.1m in depth (fig.5).

6.2.2 The works were undertaken via a mechanical excavator fitted with a toothless grading bucket to clear the extant road surface, followed by hand excavation and cleaning to expose the existing services. Upon completion of the trenches, the existing 3" main was capped and abandoned, and connected to a new 8" PE pipe.

6.2.3 Archaeological contexts were recorded as appropriate on *pro-forma* sheets by written and measured description, and drawn in plan or section, generally at scales of 1:10 or 1:20. The investigations were recorded on a general site plan and related to the Ordnance Survey grid. Levels were taken on archaeological features or deposits, transferred from the nearest Ordnance Datum Benchmark, two spot heights noted on

Ordnance Survey Plans *Vanbrugh Park Road*, at 41.8mOD (north) and 40.9mOD (south). The fieldwork record was supplemented by digital photography, in .jpeg and RAW formats.

- 6.2.4 The recording system followed the procedures set out in the Museum of London recording manual. By agreement the recording and drawing sheets used are directly compatible with those developed by the Museum.

6.3 Post-excavation

The fieldwork was followed by off-site assessment and compilation of a report, and by ordering and deposition of the site archive.

6.3.1 Finds and samples

No finds or samples were recovered during the archaeological watching brief, from either the trenches or spoil.

6.4 Report procedure

- 6.4.1 This report contains a description of the fieldwork plus details of any archaeological remains or finds, and an interpretation of the associated deposits. Illustrations have been included as appropriate, including a site plan located to the OS grid. A short summary of the project has been appended using the OASIS Data Collection Form.

- 6.4.2 Copies of this report will be supplied to the Client and Historic England.

- 6.4.3 There is no provision for further analysis or publication of significant findings. Should these be made the requirements would need to be discussed and agreed with the Client.

6.5 The site archive

Assuming that no further work is required, an ordered indexed and internally consistent archive of the evaluation will be compiled in line with MoL Guidelines for the Preparation of Archaeological Archives, and will be deposited in the Museum of London Archaeological Archive under site code VPR17.

7 RESULTS

7.1 The following forms a written description of observations made during the watching brief. The works are discussed in order, from Trench 1 towards the southern end of the road, to Trench 4 at the northern end. Deposits are shown as (x), cuts and structures as [x]. The text is supplemented with illustrative photographs. For a full context list refer to Appendix I.

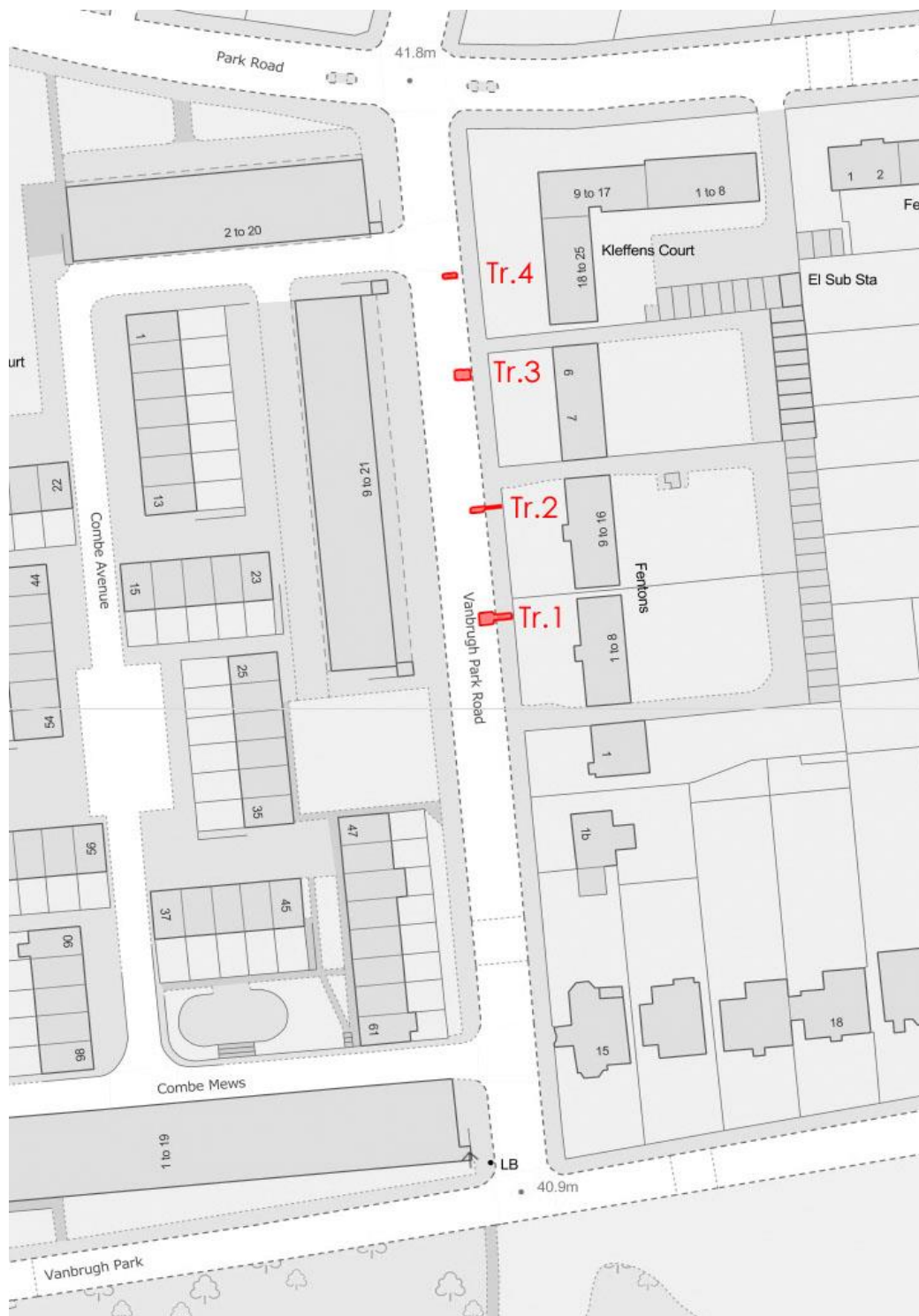


Figure 5: Location of Trenches 1-4 monitored during the archaeological watching brief.

7.2 Trench 1

7.2.1 The first trench was located towards the centre of Vanbrugh Park Road, aligned east-west straddling the carriageway and pavement adjacent to Nos.1-8 Fentons Apartments. The trench was rectangular in plan, measuring 5.6m in length x 0.6 – 1.2m in width x 0.4 – 0.85m in depth (fig.6).



Figure 6: Western part of the trench, within the carriageway. Facing W. Scale 1m.

7.2.2 The stratigraphy within the carriageway comprised 100mm - 160mm of well compacted asphalt (1) overlying 200mm - 350mm of moderately well compacted light brown gravel (5), above 380mm - 700mm of a mid-brown sandy soil (3) and a minimum of 100mm of orange sand (4), continuing below the level of excavation. The orange sand (4) is taken to be the natural geology.

7.2.3 At the eastern end of the trench, contexts (3), (4) and (5) were truncated by a linear service cut, [7], measuring 250mm in width. This cut contained a plastic PE pipe and was backfilled with a homogenous deposit of light pink MOT Type 1 (8) (fig.7).



Figure 7: North facing section of Trench 1, showing the gravels and light sandy soil below the road surface. Orange sand can be seen towards the base of the section, with service cut [7] to the left of frame. Facing SW. Scale 1m.

7.2.4 The stratigraphy in the narrow section beneath the existing pavement comprised 50mm - 60mm of stone paving and tarmac (taken to be a reinstated surface from previous groundworks) above 300mm of light brown sandy soil (3) and a minimum of 100mm of the loosely compacted orange natural sand (4).

7.2.5 No archaeological finds or features were encountered in this trench.

7.3 Trench 2

7.3.1 The second trench was located to the north of the first, aligned east-west, adjacent to Nos. 9-16 Fentons Apartments, again straddling the carriageway and pavement. This trench measured a total of 5.6m in length by 0.5m – 0.65m in width by 0.7m - 0.9m in depth (fig.8).



Figure 8: Western part of Trench 2. The continuation of the main can be seen towards the top of the frame, and service cut [7] towards the bottom. Facing W. Scale 1m.

7.3.2 The stratigraphy recorded in this trench was similar to Trench 1 above, comprising 120mm of asphalt (1) overlying 170mm of compact gravel within a light-brown sandy matrix (5), above 470mm of light-brown, moderately loose, sandy-soil (3), The orange sand (4) recorded in Trench 1 however was not observed.

7.3.3 Similarly, the stratigraphy was interrupted at the eastern end of the trench by service cut [7], backfilled with MOT Type 1 (8), which measured 300mm in width. A lead pipe was also visible in the south facing section, however there was no distinct cut and fill noticed for this feature (fig.9).



Figure 9: South facing section of Trench 2, showing made ground and services below the road surface. Facing N. Scale 1m.

7.3.4 As with Trench 1, no finds or features of archaeological interest were observed.

7.4 Trench 3

7.4.1 The third trench undertaken was located towards the northern end of Vanbrugh Park Road, on the eastern side of the carriageway opposite garages belonging to Nos. 9-12 Vanbrugh Park Road. The trench was located wholly within the carriageway, with the eastern edge abutting the existing kerb, measuring 2.5m in length, (east-west) by 1.5m in width and up to 0.9m in depth, (fig.10).



Figure 10: Trench 3, located within the existing carriageway. Facing W. Scale 1m.

7.4.2 The stratigraphy observed in this trench differed from Trenches 1 and 2 as much of the ground was heavily truncated by a large service cut. The eastern 1.6m of the trench was comprised wholly of loose shingle, only partially excavated, sloping down westwards to overlay a large N-S service.

7.4.3 Made ground was visible in the east facing section, consisting of 200mm - 220mm of asphalt road surface (1) overlying a ground of tipped backfills (6). These deposits measured up to 740mm in thickness, and were characterised by a mixture of gravels, soil and sand, with some shingle, (fig.11).



Figure 11: Trench 3, east facing section. What is taken to be a series of modern backfills can be seen tipping downwards towards the scale bar, with some darker deposits of sand and soil seen towards the trench base. Facing W. Scale 1m.

- 7.4.4** Two deposits of very dark orange sand were seen in the corners of this section and may represent natural geology, truncated by made ground in the centre, however, as only a small portion was exposed this is inconclusive.
- 7.4.5** The majority of the remainder of the trench was disturbed by a large north-south service cut, [10], which had vertical sloping sides measuring approximately 1m in width, and was again filled by a homogenous deposit of MOT Type 1 (11) and sealed by a thick layer of asphalt (1).
- 7.4.6** No finds of archaeological interest were recovered, and with the exception of the variations in the made ground, no further features were recorded.

7.5 Trench 4

7.5.1 The final trench to be recorded during the watching brief was located at the northern end of Vanbrugh Park Road, within the eastern side of the carriageway, situated opposite the junction with Combe Avenue. The trench measured approximately 2.2m in length, (E-W), by 0.64m in width and 0.6m - 1.1m in depth, becoming deeper towards the west (fig.12).



Figure 12: Trench 4, showing N-S services below modern made ground. The remains of a ceramic drain can be seen towards the top right of the trench. Facing W. Scale 0.5m.

7.5.2 The stratigraphy recorded in this trench comprised 90mm of the asphalt road surface (1) overlying a coarse gravel abundant with red and yellow brick rubble (12), measuring up to 220 mm in thickness. This sealed 500mm - 850mm of gravelly backfill (5), as seen in Trenches 1-3. No orange natural sand was observed.

7.5.3 Again, the made ground was truncated by an N-S service cut [7], measuring 300mm in width and filled by MOT Type 1 (8). No cut was visible for the second set of services, seen towards the top of frame in fig.12 above.

7.5.4 The brick rubble appeared to be a backfill deposit, rather than a bedding layer for the road surface and most likely originated from bomb debris material from the Victorian villas damaged by a Second World War V1 flying bomb.

7.5.5 No finds or features of archaeological interest were observed.

8 CONCLUSION

The following section provides a summary of the work undertaken with reference to the original research questions set out above.

8.1 *Is there any evidence for prehistoric activity on the site?*

No evidence of prehistoric activity was encountered during the watching brief. The archaeological stratigraphy was very limited, and truncated and disturbed by a number of modern services, particularly in Trench 3.

8.2 *Is there any evidence for development &/or land-use in the medieval and early post-medieval periods?*

The stratigraphy recorded was limited to later post-medieval made ground, with what is taken to be natural sand appearing in Trench 1 and possibly Trench 3.

8.3 *What is the evidence for post-medieval activity on the site? What form does the evidence for such activity take?*

Post-medieval archaeological activity was limited to a small deposit of brick rubble observed in the north and south facing sections of Trench 4. This 220mm thick layer is taken to be backfilled bomb damage debris, laid down beneath the road surface. The layer appears to be a levelling or dumping layer, rather than a deliberate bedding surface for the asphalt above.

8.4 *At what levels do archaeological and/or geological deposits survive across the site?*

Made ground was encountered below the asphalt road surface at a depth of approximately 200mm (41.6mOD – 41.4mOD). An orange sand, taken to be natural, was encountered in Trench 1 at a depth of c0.75m (c40.85mOD).

9 SOURCES

9.1 Digital Sources

Bombsight Online (2017). <http://bombsight.org/#15/51.5050/-0.0900>

Greater London Historic Environment Record. glher@HistoricEngland.org.uk

London Archaeological Archive & Resource Centre (LAARC) database. <http://archive.museumoflondon.org.uk/laarc/caralogue/>

9.2 Bibliography

Chartered Institute for Archaeologists. (2014a). *Standard and guidance for the collection, documentation, conservation and research of archaeological materials.*

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Communities and Local Government. (2012). *National Planning Policy Framework.*

Hibbert, B. & Weinreb, C. (1983). *The London Encyclopaedia.*

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Royal Greenwich Borough Council, (2014a), *Royal Greenwich Local Plan: Core Strategy with Detailed Policies.*

Royal Borough of Greenwich, (2014b), *Maritime Greenwich World Heritage Management Plan: Third Review.*

9.3 Cartographic Sources

Ordnance Survey, *Essex Sheet LXXXI*, (surveyed 1862-68, published 1870)

London County Council (1939-45). *Bomb Damage Maps.* In Ward, L. (2015)

APPENDIX I: LIST OF CONTEXTS

Number	Description
(1)	Existing road surface
(2)	Existing tarmac / paving pavement surface
(3)	Light to mid-brown sandy soil
(4)	Dark orange sand - natural
(5)	Gravel abundant layer
(6)	Group: made ground layers
[7]	Cut for N-S service seen in Trenches 1 - 4
(8)	MOT Type 1 fill of service cut [7]
(9)	Shingle recorded in Trench 3
[10]	Cut for main recorded in Trench 4
(11)	MOT Type 1 fill of cut [10]
(12)	Rubble abundant deposit seen in Trench 4

APPENDIX II: OASIS RECORDING FORM

OASIS ID: compassa1-296565

Project details

Project name	Mains Replacement works at Vanbrugh Park Road: An Archaeological Watching Brief
Short description of the project	Small watching brief conducted on the 15th September 2017 at Vanbrugh Park Road, London Borough of Greenwich SE3 7NJ. Work was commissioned by Thames Water Utilities due to the site being located within the buffer zone of the UNESCO World Heritage Site 'Maritime Greenwich' and associated Area of High Archaeological Potential. The investigation comprised the monitoring of 4 trenches located within the northern part of the road. The trenches were aligned east-west, spanning the carriageway and pavement, measuring between 2.2 and 5.6m in length x 0.64-1.5m in width x a maximum of 1.1m deep. In general, the stratigraphy comprised made ground sealed by the existing road surface, truncated along the eastern side by a modern service cut. Natural sand was encountered in Trench 1 at c40.85mOD. No finds or features of archaeological interest were recorded.
Project dates	Start: 15-09-2017 End: 15-09-2017
Previous/future work	No / Not known
Any associated project reference codes	VPR17 - Sitecode
Type of project	Recording project
Site status	Conservation Area
Site status	World Heritage Site
Site status	Local Authority Designated Archaeological Area
Current Land use	Transport and Utilities 1 - Highways and road transport
Monument type	NONE None
Significant Finds	NONE None
Investigation type	"Watching Brief"
Prompt	National Planning Policy Framework - NPPF
Prompt	UNESCO World Heritage Site 'Maritime Greenwich' buffer zone

Project location

Country	England
Site location	GREATER LONDON GREENWICH GREENWICH Vanbrugh Park Road
Postcode	SE3 7NJ
Study area	20 Square metres
Site coordinates	TQ 539846 177350 50.937896291232 0.191860707693 50 56 16 N 000 11 30 E Line
Site coordinates	TQ 539842 177409 50.937949420225 0.191857512285 50 56 16 N 000 11 30 E Line
Height OD / Depth	Min: 40.75m Max: 40.85m

Project creators

Name of Organisation	Compass Archaeology
Project brief originator	Thames Water Utilities Ltd
Project design originator	Compass Archaeology
Project director/manager	Geoff Potter
Project supervisor	Heidi Archer
Type of sponsor/funding body	Thames Water Utilities
Name of sponsor/funding body	Thames Water Utilities

Project archives

Physical Archive Exists?	No
Digital Archive recipient	Museum of London Archaeological Archive
Digital Contents	"none"
Digital Media available	"Images raster / digital photography"
Paper Archive recipient	Museum of London Archaeological Archive
Paper Contents	"none"

Paper Media available "Context sheet", "Map", "Photograph", "Report", "Unpublished Text"

Project bibliography 1

Publication type Grey literature (unpublished document/manuscript)

Title Thames Water Rehabilitation Works, Vanbrugh Park Road, Greenwich SE3 7NJ: An Archaeological Watching Brief.

Author(s)/Editor(s) Archer, H.

Date 2017

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Description Summary report of the watching brief, including development background, historic and archaeological background, fieldwork methodology and fieldwork observations made, conclusions and appended context list and OASIS form. Accompanied by relevant site plans, historic maps and illustrative photographs.