

THAMES WATER MAINS REPLACEMENT WORKS

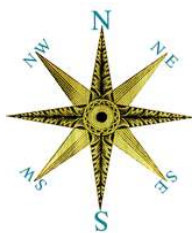
DETAILED REPORT ON SEETHING LANE, CITY OF LONDON,
EC3

INTERIM REPORT: CROUCH HILL 55 (JUNE 2009 – AUGUST
2010)



August 2010

COMPASS



ARCHAEOLOGY

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LONDON, EC3**

**INTERIM REPORT: CROUCH HILL 55 (JUNE 2009 –
AUGUST 2010)**

SEETHING LANE - NGR: TQ 33390 80864 (N) / 33392 80815 (S)

SITE CODE: TZJ 09

OTHER CH55 WORKS - SITE CODE: TMM06

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August 2010

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Abstract

In the course of the archaeological watching brief for the Thames Water Victorian Water Main Replacement Project (VMR) within the area 'Crouch Hill 55' a significant archaeological discovery was made in Seething Lane. Two sections of the Thames Water trench were investigated and recorded in May 2009. This uncovered a number of archaeological remains, which were subsequently recorded and left in situ (with the exception of a few brick samples and key finds).

The remains of a late 18th Century East India Company Warehouse wall was uncovered in both trenches recorded. The uncovered sections were one small part of what would have been a huge building.

Remains associated with a possible medieval extension of St Olave's Church graveyard were also uncovered, including part of a human inhumation and grave-fill.

Some archaeological deposits relating to other buildings or structures were also uncovered, including a wall which has three possible phases of construction and which may reflect the development of buildings over time - those associated with the 17th Century Navy Office, the East India Company warehouse, and a 19th Century public house.

The following report discusses these archaeological discoveries, in reference to their documentary and cartographic background, and the excavations themselves. The interim report for all archaeological monitoring works undertaken in Crouch Hill 55 from June 2009 to August 2010 is also included, although it should be noted that relatively few groundworks took place during the first half of 2010.

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I: DETAILED REPORT ON SEETHING LANE:

1. Introduction

- 1.1 This report forms an interim summary of the results of the archaeological monitoring work undertaken to date in Crouch Hill 55 in relation to the Thames Water mains replacement works. This report includes both a summary of all monitoring work undertaken in Crouch Hill 55 during the interim period of 8th June 2009 to 19th August 2010 (see section II), and a more focused discussion of the results of excavations in Seething Lane in May 2009, including discussion of the documentary and cartographic sources and the excavations themselves.
- 1.2 The investigation at Seething Lane covered two specific areas of one Thames Water trench. Seething Lane is located in the City of London, south-west of Fenchurch Street station and north-west of the Tower of London. Both trenches were located on the eastern side of Seething Lane, one north of Pepys Street and one south (figs. 1 and 2).
- 1.3 On-site work took place in May 2009 following the discovery of a brick wall during the digging of a long trench for the replacement of the water mains.
- 1.4 The archaeological work was undertaken on behalf of Thames Water Utilities and Clancy Docwra Ltd.

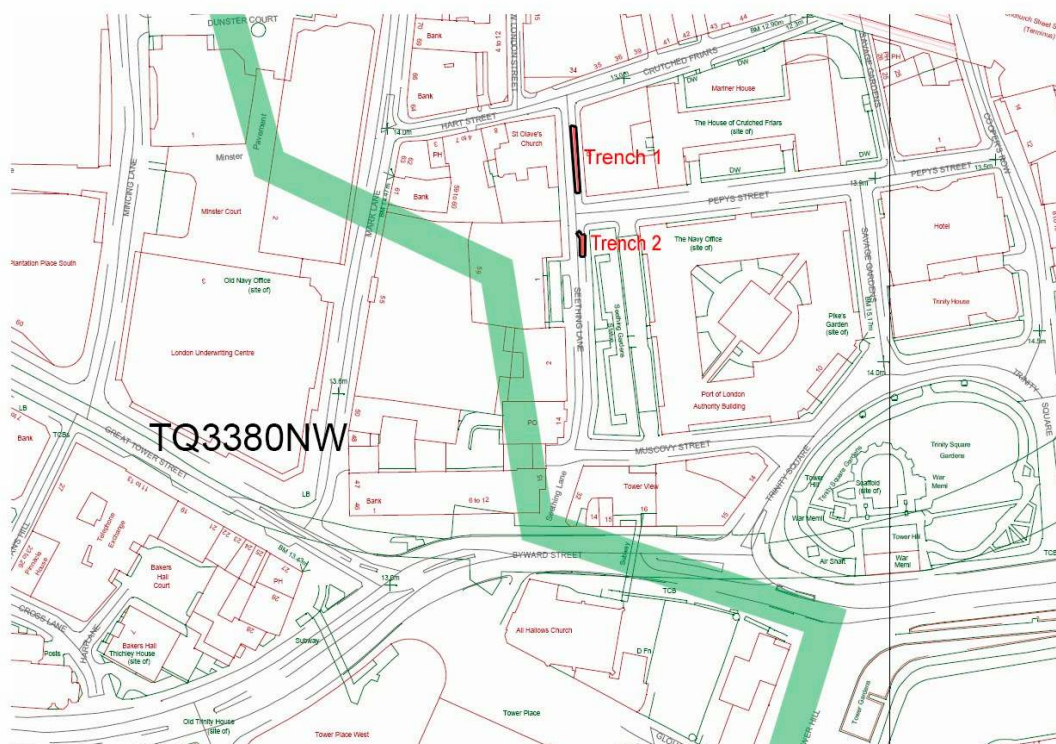


Fig. 1 Location of trenches in relation to an OS plan extract of the City of London

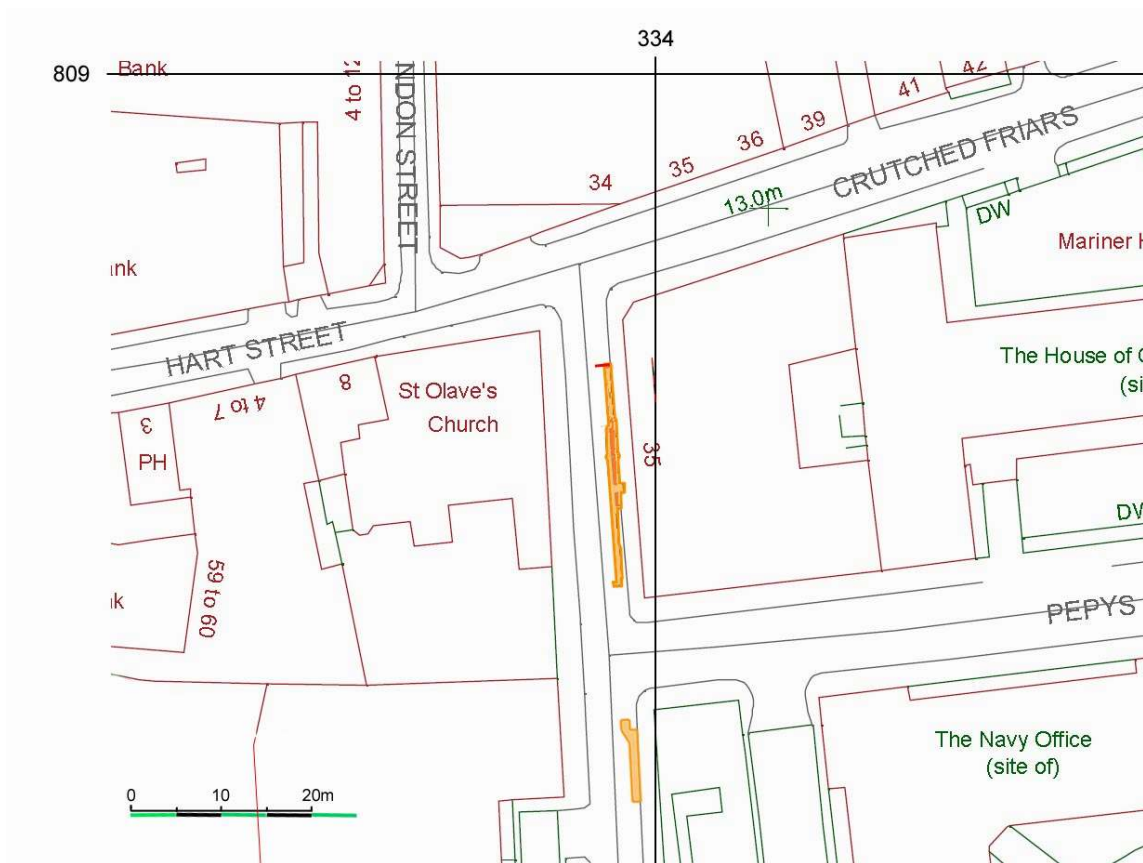


Fig. 2 Location of trenches in relation to a larger scale OS map.

2. Archaeological and Historical Background

- 2.1 Seething Lane (labelled 'Sydon Lane' in pre-1530s maps) appears to have been developed from at least the 13th Century. The first records of St Olave's Church, on the north-west corner of Seething Lane, appear in the 13th Century, where it is recorded as "St-Olave-towards-the-Tower". This was a stone-built church, and probably replaced an earlier wooden church on this site.
- 2.2 The present St Olave's church building dates to around 1450 and was one of the few medieval churches to survive the Great Fire in 1666. Although gutted by fire in 1941 by German bombing, much of the 15th century stonework survived and was conserved through the church's restoration in 1954 (Tucker 2006). St Olave's is mentioned by Charles Dickens in 'The Uncommercial Traveller' (1860) and features frequently in the diary of Samuel Pepys who described it as "our own church" (Wheatley 1893).

- 2.3 Many of the medieval properties on Seething Lane were owned by wealthy families, such as the Lords Grey of Codnor and Sir Richard Knollys, and by religious institutions such as Crutched Friars and St Mary Spital. Furthermore, a number of prominent historical characters lived in this street during the 16th and 17th Centuries. Sir Francis Walsingham, Elizabeth I's 'spymaster', lived here from 1580 until his death in 1590, with Samuel Pepys also living in Seething Lane from 1660 until his death in 1703 (Gater and Walter 1934).

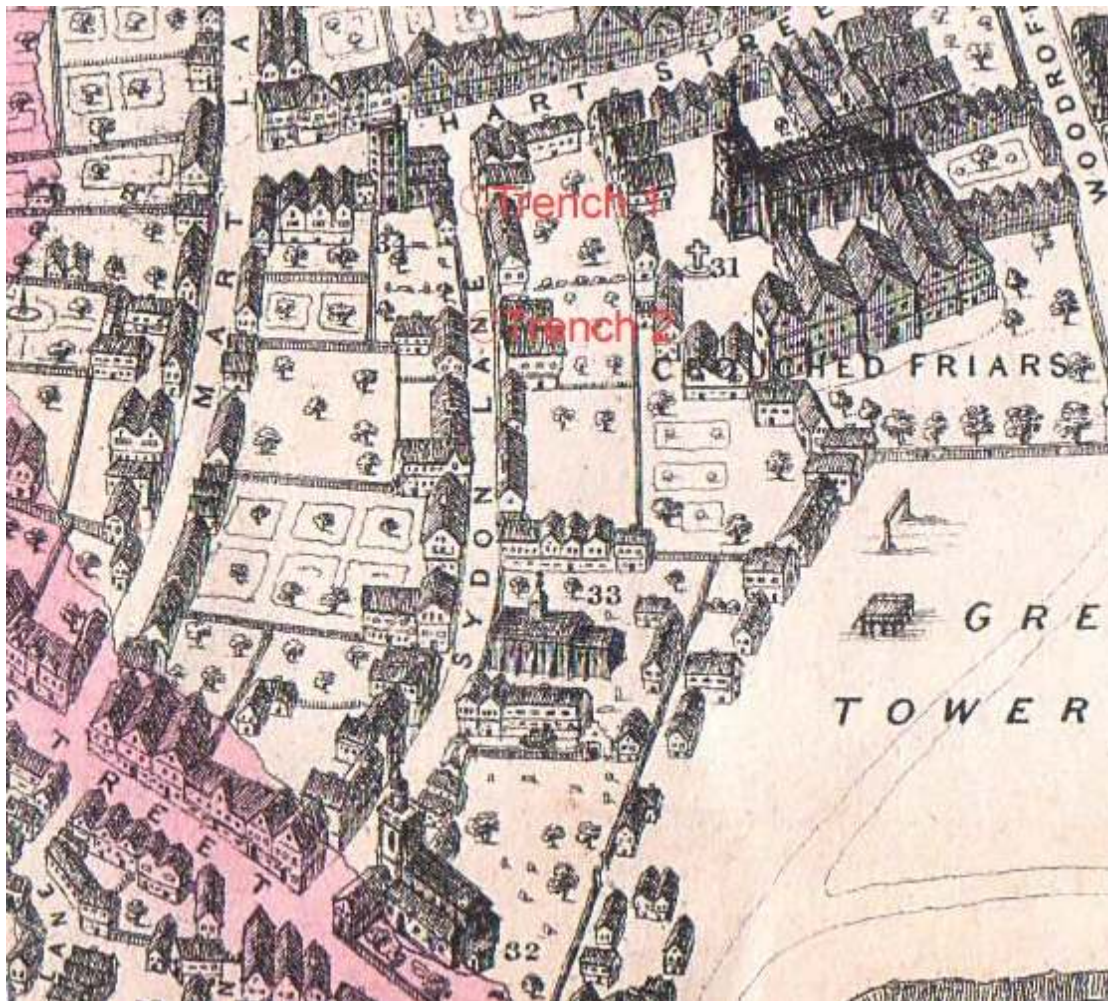


Fig. 3: Newton's map, 1855, compiled from pre-1530s sources (depicts medieval properties and St Olave's Church)

- 2.4 In 1656 the Old Navy Office was constructed. This Office occupied a large square site in the corner between Seething Lane and Crutched Friars. It consisted of a free-standing central building surrounded on four sides by smaller buildings, including the house of Samuel Pepys on its west side. It functioned as a centre of naval administration until 1787 (surviving the Great Fire), when this building was destroyed and the office moved to Somerset House. (Gater and Godfrey 1934)

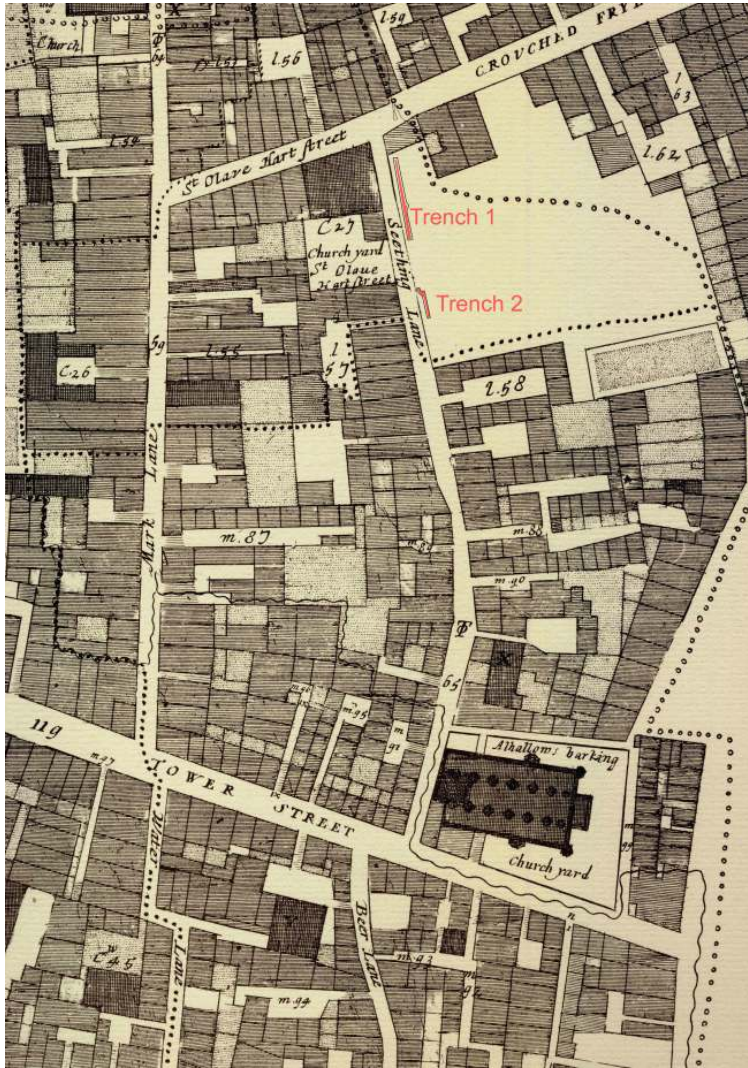


Fig. 4: Ogilby and Morgan's map, 1676 (depicts space for the construction of the Navy Office)

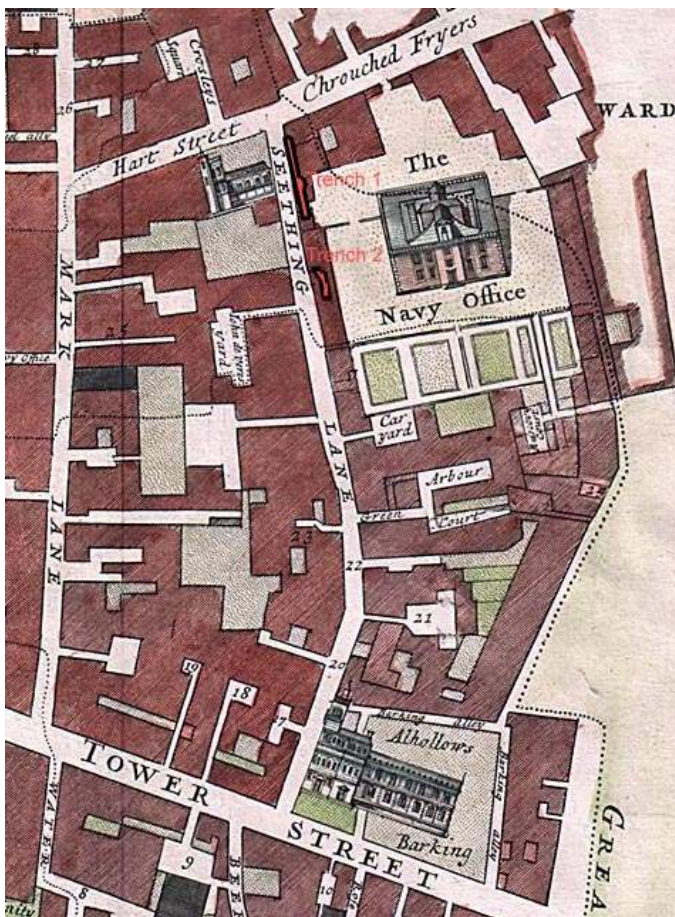


Fig. 5: Stow's map, 1720s (depicts the Navy Office)

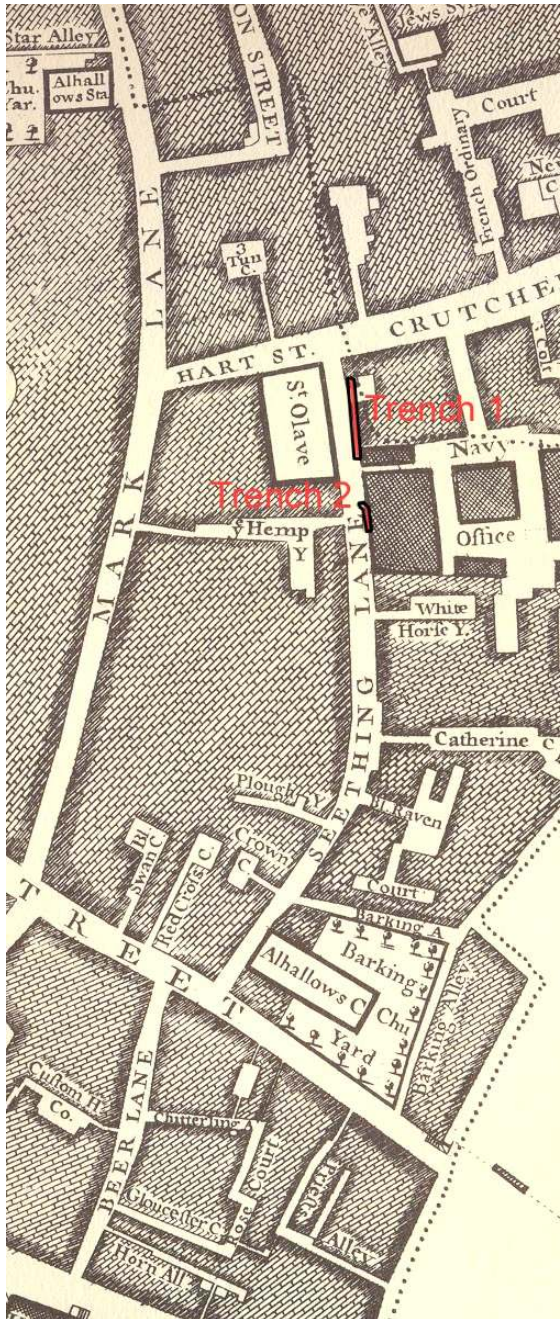


Fig. 6: Rocque's map, 1775 (depicts the Navy Office)

- 2.5** The Crutched Friars East India Company Warehouse was then built between 1787-1797 on the site of the Old Navy Office. This was part of a network of East India Company warehouses, along with those at Leadenhall Street, Fenchurch Street and the Stillyard, which lay between the docks and the East India Company headquarters at East India House, largely built in response to the expansion in the East India Company's trade in the late 18th century (Noorthouck 1773).
- 2.6** The construction of the warehouse took place in two main phases. In 1787 the warehouse was built on the freehold site of the Navy Office, with it being extended in 1796 and 1797 on the leasehold site of the Navy Office. This was through the East India Company obtaining a number of leases for this site, such as that in 1798 from the Carpenters Company, and that from the Trustees of the Baynhams.

2.7 This warehouse consisted of six stories of storerooms surrounding a central courtyard. According to Dickens's Dictionary of the Thames (1881), the warehouse was used primarily for storing tea with the upper floors reserved for Indian teas and the lower ones for Chinese teas. There were also extensive cellars used for storing luxury goods, especially shell and mother-of-pearl, but also hides, leather, skins and a range of other goods. The warehouse also incorporated two elegant showrooms.



Fig. 7: 1805 elevation of part of warehouse looking onto Crutched Friars



Fig. 8: The East India Crutched Friars warehouse by Valentine Davis 1806. Guildhall Library Pr. 189/CRU, Catalogue no. q3698840

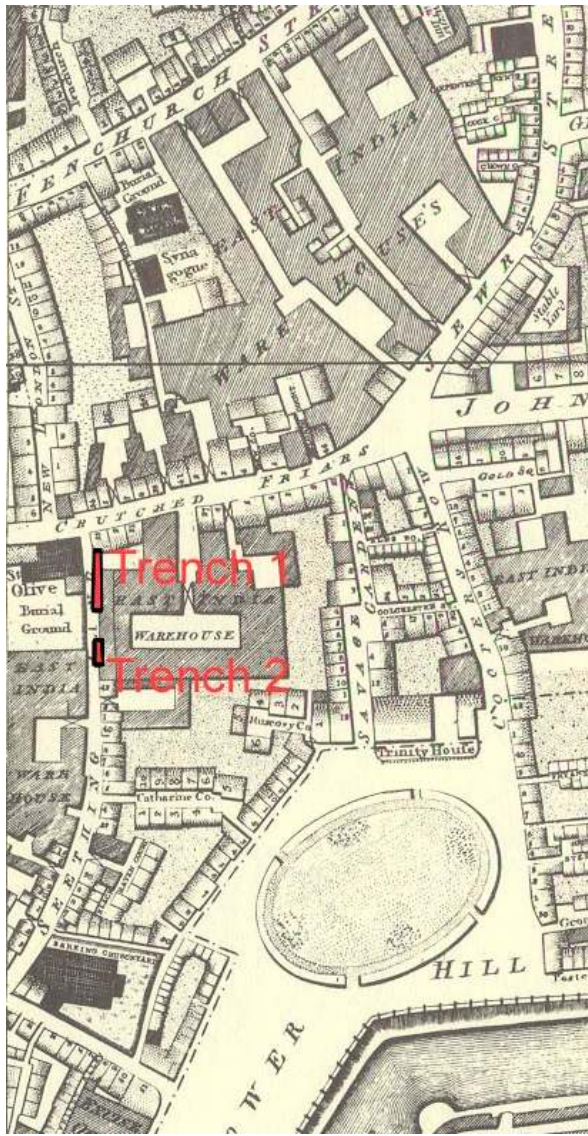


Fig. 9: Horwood's map, 1799 (depicts the East India Company warehouse)

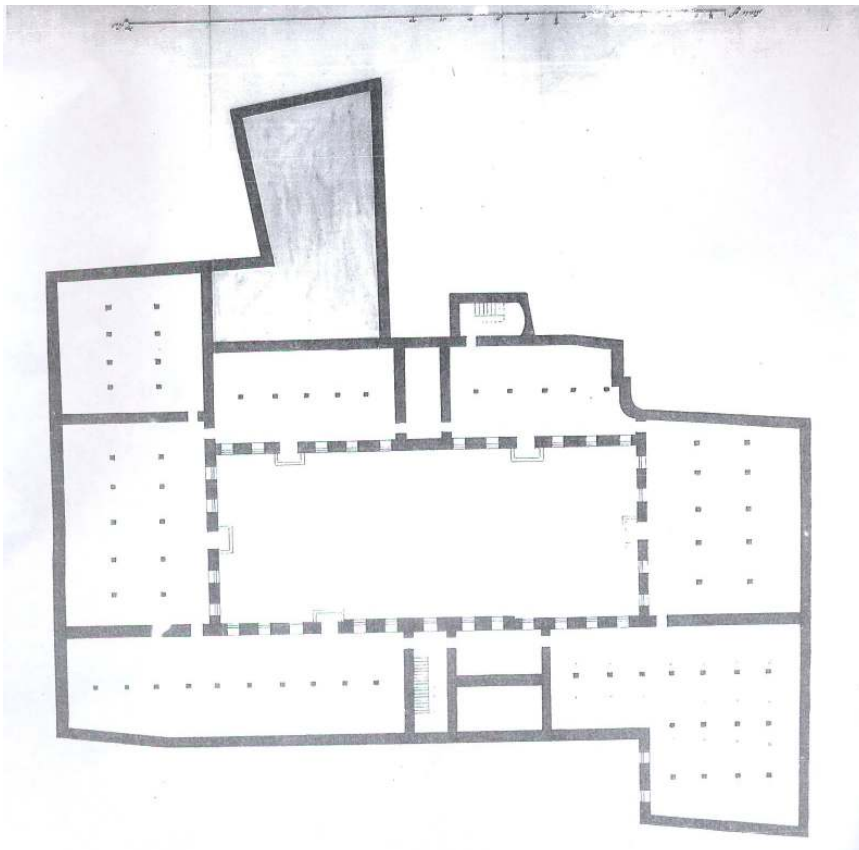


Fig. 10: Plan of basement of warehouse, 1805 (re-oriented North)

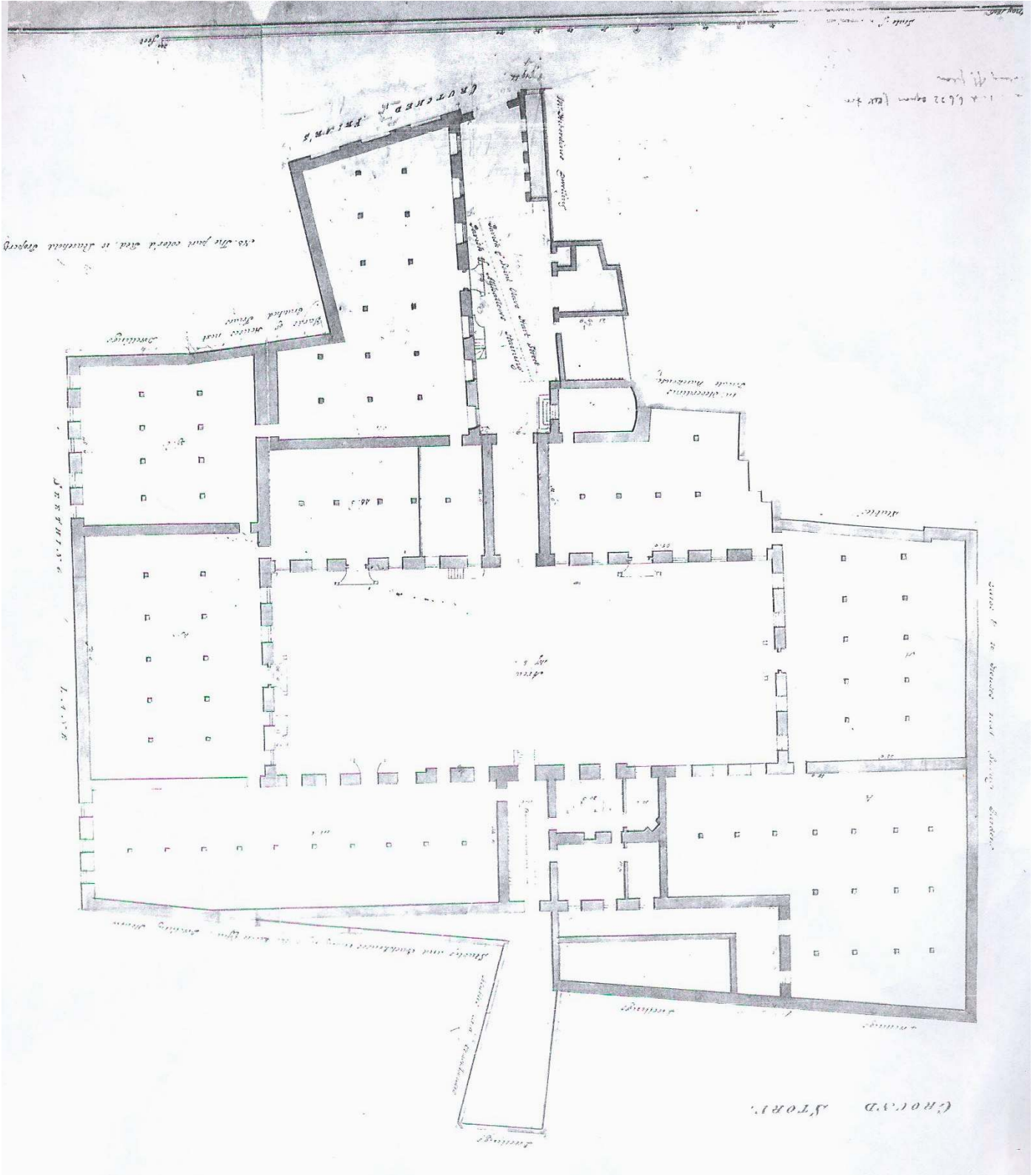


Fig. 11: Plan of ground floor of warehouse, 1805 (re-orientated North)

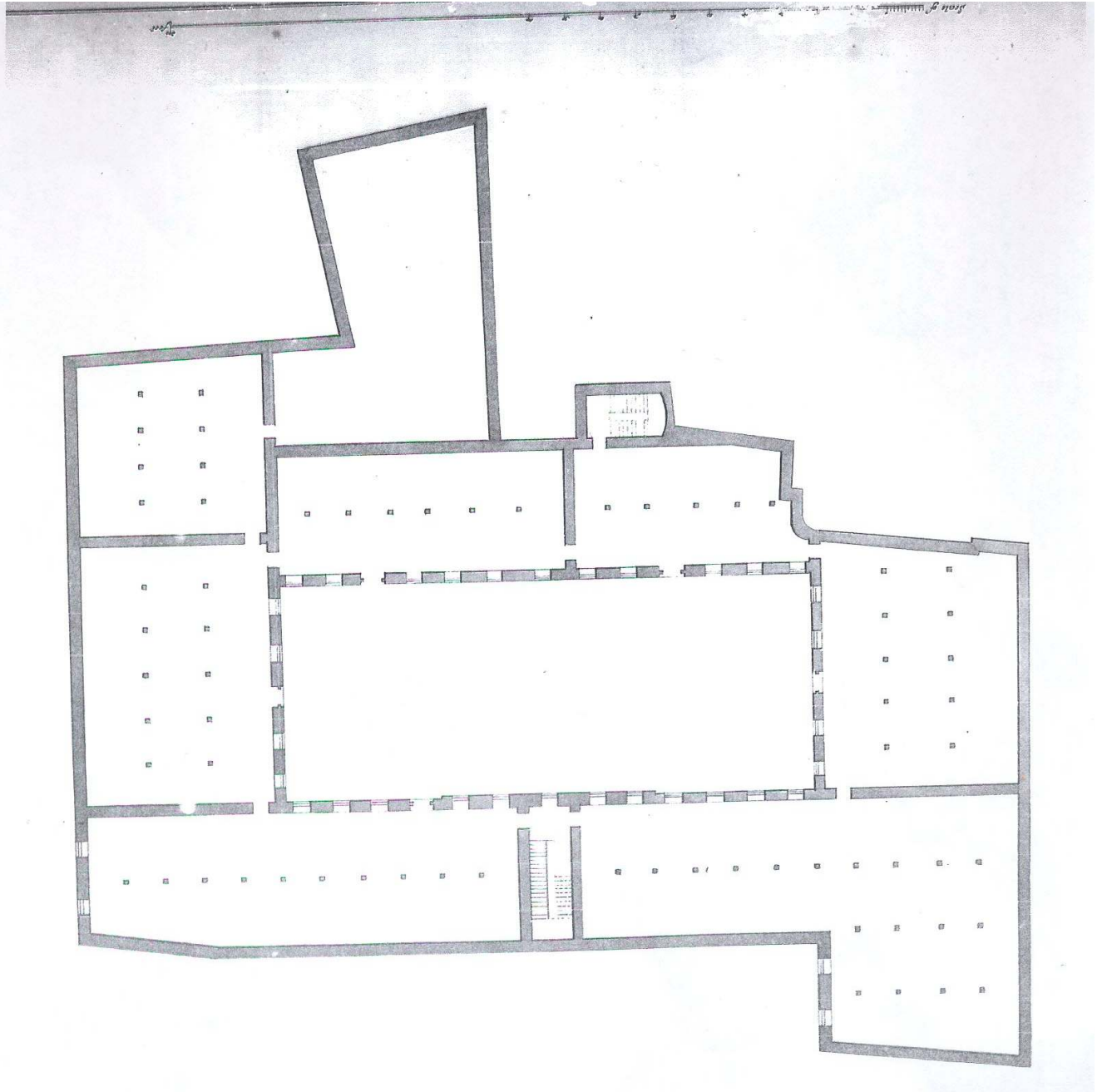


Fig. 12: Plan of first floor of warehouse, 1805 (re-orientated North)

- 2.8** In 1835 the East India Company sold the property to the East India Dock Company, who then sold it to the London and India Docks Company. The greater part of Crutched Friars Warehouse then came into the possession of the Port Authority under the 1908 Act as the successors of the London and India Dock Company.
- 2.9** The warehouses themselves were demolished *c.*1910-12 prior to the construction of the Port of London Authority building.

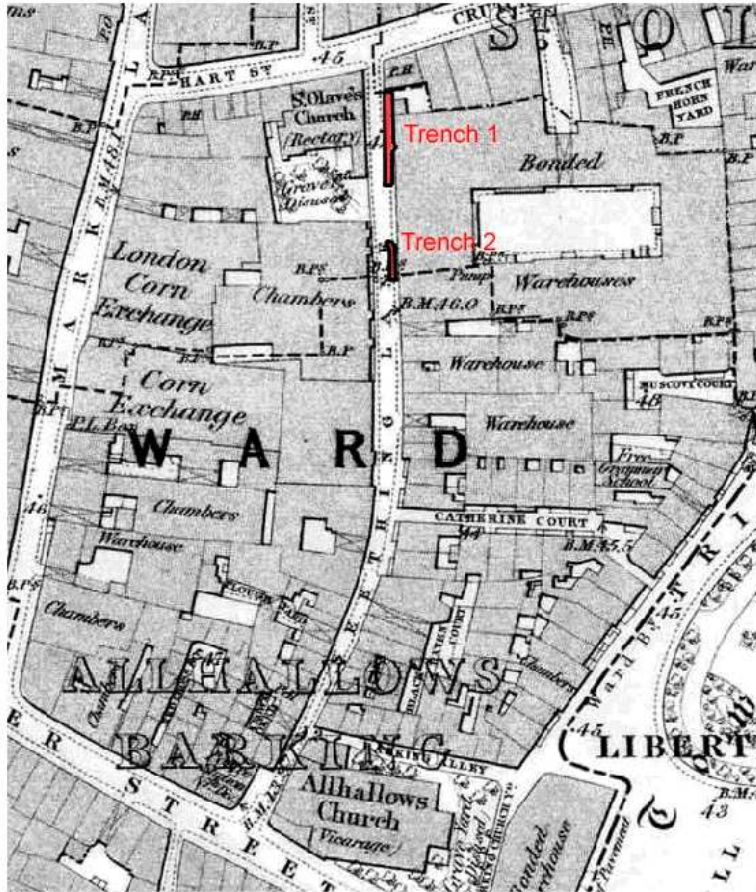


Fig. 13: 1872 OS 25-inch: mile Map (depicts the 'Bonded Warehouse')

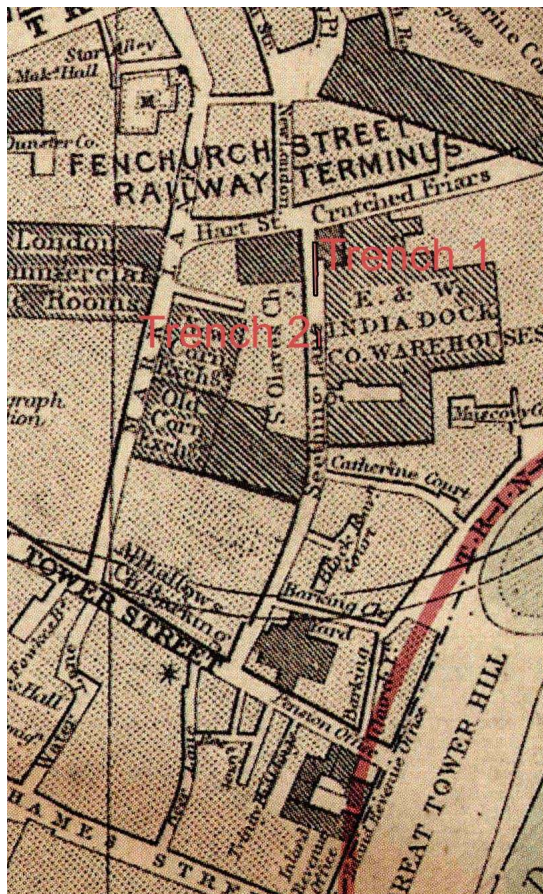


Fig. 14: Bacon's map, 1893 (depicts the East and West India Dock Company Warehouse)

3. The Archaeological Programme

The field and post-excavation work was carried out in accordance with English Heritage guidelines (in particular, *Standards and Practices in Archaeological Fieldwork, Guidance Paper 3*).

Works also conformed to the standards of the Institute for Archaeologists ('IfA Standard and Guidance for Archaeological Watching Briefs'). Overall management of the project was undertaken by a full Member of the Institute. The recording system followed the procedures set out in the Museum of London recording manual.

3.1 Fieldwork

3.1.1 Methodology

Exposed deposits were recorded and cleaned in stratigraphic sequence. The principal deposits and features were described on *pro-forma* sheets by written and measured description, and related to the general site plan and to the OS grid. The fieldwork record was supplemented as appropriate by digital photography.

3.2 Post-Excavation

3.2.1 The fieldwork was followed by off-site assessment of finds and samples, and by compilation of this report.

A short summary of the project is appended using the OASIS Data Collection Form, and in paragraph form suitable for publication within the 'excavation round-up' of the *London Archaeologist*.

4. The Archaeological Investigation

The results of the investigation are described below in relation to the two sections of the trench investigated (sections 4.1 & 4.2).

4.1 Trench 1

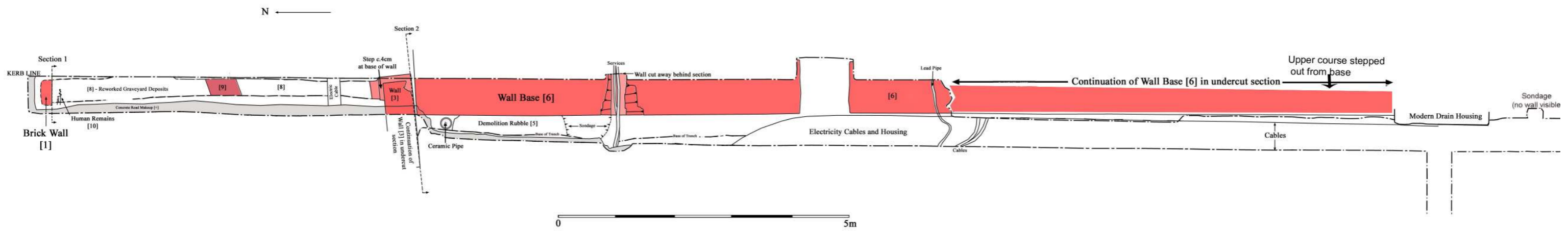


Fig. 15: Plan of trench 1

Trench 1 was located on the eastern side of Seething Lane, towards the northern end (immediately north of Pepys Street) and in front of No. 35 Seething Lane. It was approximately 25m long (north-south) by 0.55m to 1.15m wide and was excavated to an average depth of 1.2m. The present surface was made up of a tarmac road-surface over concrete base, c.0.3m thick.

The archaeological contexts encountered in this trench are described in the table below:

Context	Description	Interpretation
1	Southern face of a brick wall, running east-west at the northern end of trench 1. 0.95m in height, 0.56m in width. Mainly bright red brick, with some purple and black fabric in the upper courses. Lower part of the wall consisted of 3033 fabric (mid-late 17 th Century) set within a yellowy-grey sandy mortar. Mix of 3032 and 3033 brick-fabric in middle section of wall, similar to bricks in context 6 and possibly contemporary with this. Upper part of wall consisted of 3035 brick-fabric set within a Portland cement mortar (late 18 th Century – 1900). (see brick report – appendix IV)	The different types of bricks and mortar may relate to three different stages in the development of buildings on the site – those associated with the Navy Office; the East India Company Warehouse; and the later public house.
2	Cut for wall (context 1).	Shows that context 1 was trench-built. Cuts through the possible graveyard deposits (context no. 8) at its base (therefore later in date).
3	Brick-wall running east-west at a point c.6m south of the northern end of the trench. Reused earlier bricks (3032 and 3033). Brick-fabric intermediate between 3034 and 3035 set within a white sandy lime mortar (late 18 th Century – 1900). Set alongside context 6 (brick wall of the East India Company Warehouse) (see brick report – appendix IV)	The two types of bricks suggest that the wall may have been built in two phases, with the upper section possibly having been rebuilt at a later date. This was evidently one of the walls associated with the rectangular building at the back of the public house (depicted on the 1872 OS map, fig. 13)
4	Cut for wall (context 3).	Shows that context 3 was trench-built. Abuts context 6 and its' construction identifies that it was later in date than the warehouse. Cuts through possible graveyard deposits to the north (context 8) (& therefore later in date).

5	<p>Continuous line of demolition rubble material to the west of context 6. 1.9m deep (although not bottomed). Brick rubble, mortar, occasional fragments of chalk, and occasional glass and pottery fragments. The brick rubble was the same fabric as that in context 6. Included a tin-glazed floor-tile with monochrome floral decoration and 'tulip' corner motifs (produced c.1618-1663, probably at either Pickleherring (Southwark) or Platform Wharf (Rotherhithe) (see tile report, appendix III). Two stem fragments of a clay pipe were also uncovered. (see clay pipe report, appendix II)</p>	<p>Construction backfill associated with construction of East India Company warehouse, against the external face of the western wall of the warehouse (context 6).</p>
6	<p>Brick wall base. Irregular bond, with fragmentary bricks and brick-rubble. These dated from differing periods, as were a mixture of earlier reused bricks (e.g. 3047) and bricks dating to the construction of the warehouse (e.g. 3032) (see brick report, appendix IV).</p>	<p>Part of the western external wall of the East India Company Warehouse (location fits that on historical maps, such as Horwood's 1799 map (fig. 9)).</p>
7	<p>Theoretical cut for context 6. Not visible.</p>	
8	<p>Compact, dark brown, reworked fill deposit. Extended for 6m from the north end of the trench to wall (context 3), spanned the width of the trench (0.45m), maximum depth of 0.9m. Consisted mainly of reworked soil (c.50% silt, 20% clay, 10% sand, and 20% inclusions of tile, brick, bone, mortar, chalk, ash and pebbles). Pottery finds included broken sherds of tin glazed urban ware (c.1500-1700) (see fig. 24), fragments of post-medieval redware, white saltglazed stoneware, and 13th and 14th Century medieval pottery fragments. (see pottery report, appendix II) A complete bowl of clay tobacco pipe (late 18th Century), two stem fragments, and the base of a bowl were also uncovered. (see clay pipe report, appendix II)</p>	<p>Possibly a graveyard fill deposit, as grave-cut (context 12) was cut through it, but reworked in post-medieval period (hence later finds).</p>

9	Small brick feature crossing the trench at approximately 3m south from the northern end of the trench. 0.54m N-S, 0.25m E-W (although could be longer as latter figure was the width of the trench), and one brick deep. Loosely compacted selection of bright red broken bricks, closely lain side by side. Some domestic debris, such as post-medieval redware (19 th Century) and glass was uncovered. (see brick report, appendix IV, and pottery report, appendix I)	Possibly the counterpart to the wall further south (context 3), therefore possibly the northern wall of the small building shown on the 1872 OS Map (fig. 13), although it could be part of an earlier structure.
10	Small section of an articulated, supine, extended adult inhumation. Only the left lower tibia, broken fibula, upper tarsal, and two smaller toe bones (possibly metatarsals) were visible. The remains were preserved <i>in situ</i> .	Apparently a burial in the churchyard of St Olave's Church, assumed to have originally extended across the line of the present road.
11	Grave-fill associated with the inhumation (context 10). Not archaeologically distinct from the graveyard fill deposit (context 8). No finds or samples uncovered.	Although not archaeologically distinct, the fact that the skeleton was articulated means that there must have been some form of grave-fill.
12	Theoretical cut for inhumation (context 10). Not visible.	Probable that the northern side of this grave-cut may have been truncated by the construction of the wall (context 1) and its cut (context 2).

4.1.1 The major brick wall base encountered in the middle section of this trench (context 6) is evidently part of the western wall of the East India Company Warehouse erected in the late 18th Century. The location of this wall fits with that depicted on historical maps, such as Horwood's 1799 map (fig. 9) concerning the warehouse's location.

The section of wall-base uncovered here would have been below ground level, but would have been associated with the basements that are known to have existed in the warehouse (see 1805 plan, fig. 10).

The dating of some of the bricks in this wall-base to the late 18th/early 19th Century would fit the recorded date of construction of the warehouse, alongside the reuse of earlier bricks (in most cases probably from the demolition of the earlier Navy Office on this site).

This wall-base continues to the south in the undercut of the section, until it is truncated by modern drain housing. It is likely that this continues further

south, at least at a lower level (according to depictions of the warehouse on historical maps), despite the sondage dug into the eastern section which revealed no traces of this wall. However, the wall in trench 2 (see below) is almost certainly a continuation of this structure. In contrast, the northern corner of the wall-base uncovered in trench 1 must mark the true extent of this wall and eastward return, as reflected on historical maps.



Fig. 16: View along context 6 (brick wall), looking north



Fig. 17: View along the continuation of the wall-base in the undercut section (context 6), looking north-east.



Fig. 18: Close-up view of context 6 (wall)

- 4.1.2** The three apparent stages in the construction of context 1 (the wall uncovered at the northern end of trench 1) may reflect three different stages in the development of buildings on this site, which are reflected in historical maps (figs. 5, 9, and 13).

The earliest bricks and mortar (fabric type 3033 with yellow lime mortar) may relate to the construction of buildings associated with the Navy Office (as depicted on Stow's 1720s map, fig. 5), since they date to the mid-late 17th Century (around the date of the construction of the Navy Office).

The mix of bricks (fabric types 3032 and 3033) in the middle section of the wall are similar to those uncovered in context 6, and may therefore be part of buildings constructed in association with the construction of the East India Company Warehouse (as depicted on Horwood's 1799 map, fig. 9). Although this was found at a point too far north to be directly part of the East India Company warehouse, the adjacent buildings may have been rebuilt around the date of the construction of the warehouse.

Finally, the brick-fabric 3035 and Portland cement mortar may date to the second half of the 19th Century, and therefore relate to the construction of the public house (as depicted on the 1872 OS Map, fig. 13).

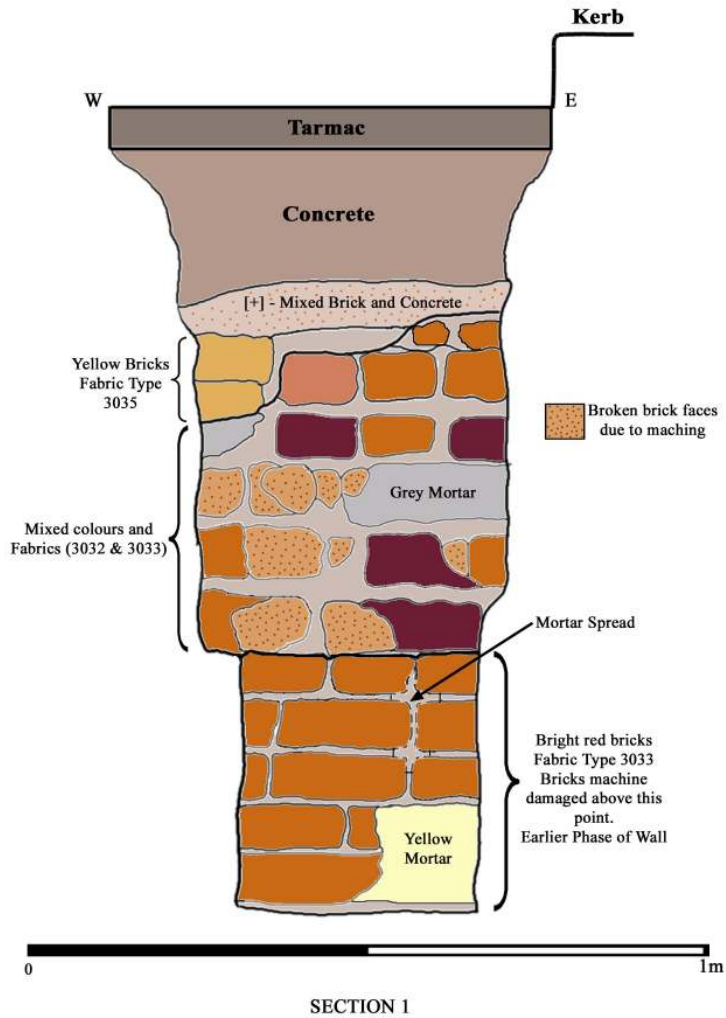


Fig. 19: Section of context 1 (brick wall)



Fig. 20: View of context 1 (brick wall), looking north.

4.1.3 The east-west brick wall (context 3) which abuts context 6 (the East India Company Warehouse wall) may relate to the small free-standing building standing at the north-western corner of the East India Company Warehouse depicted on the 1872 Ordnance Survey Map (fig. 13), but not depicted on Horwood's 1799 Map (fig. 9).

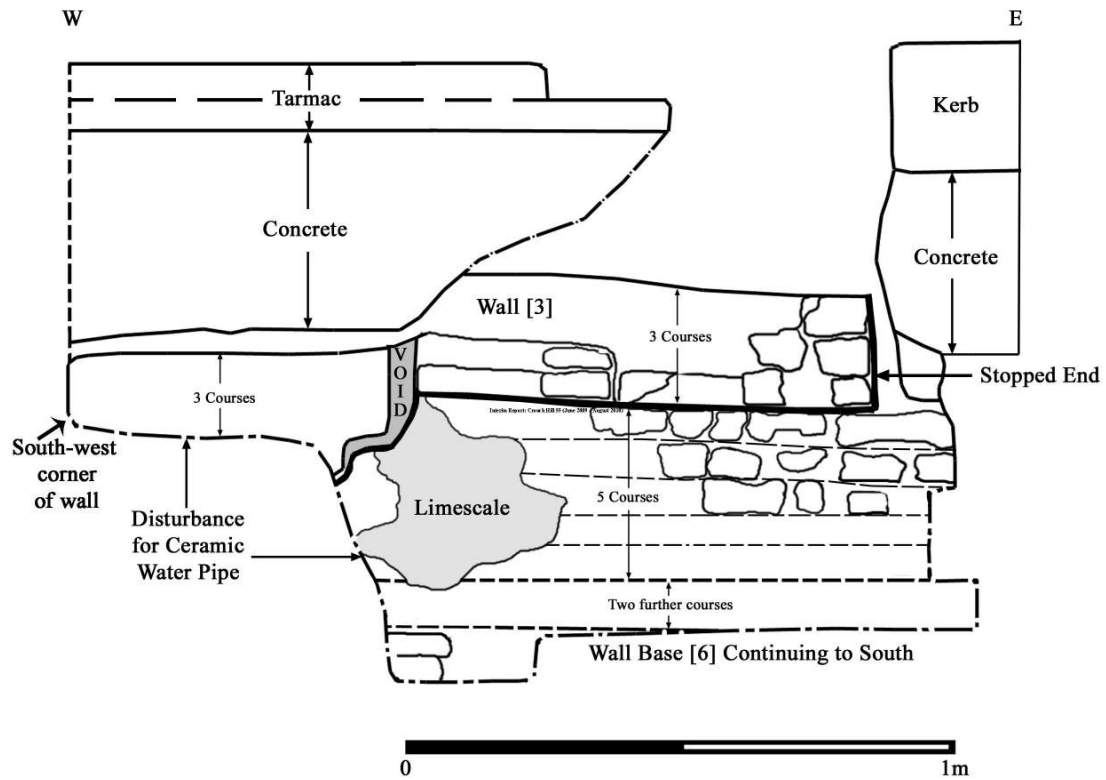


Fig. 21: Section of context 3 (wall)



Fig. 22: View of context 3 (E-W wall), with major wall (context 6) in foreground

4.1.4 The northern part of this trench apparently consisted of a disturbed or reworked graveyard fill (context 8), with part of an inhumation within it (left *in situ*). This may represent part of a more extensive graveyard around St Olave's Church, extending across the line of what is now Seething Lane. If so, the graveyard (and burial) would be of medieval date, as indicated by mid 12th – 13th Century grey-ware pottery (see pottery report, appendix I). However, a road is shown in this area at least as early as the 'Agas' map of 1562, such that any extension of the graveyard, if it existed, had been destroyed by this date.



Fig. 23: View of context 10 (remains of adult inhumation), with later wall (context 1) at base of frame.



Fig. 24: Reconstructed fragments of English tin-glazed ware recovered from context 8 (reworked possible graveyard soil)

4.2 Trench 2

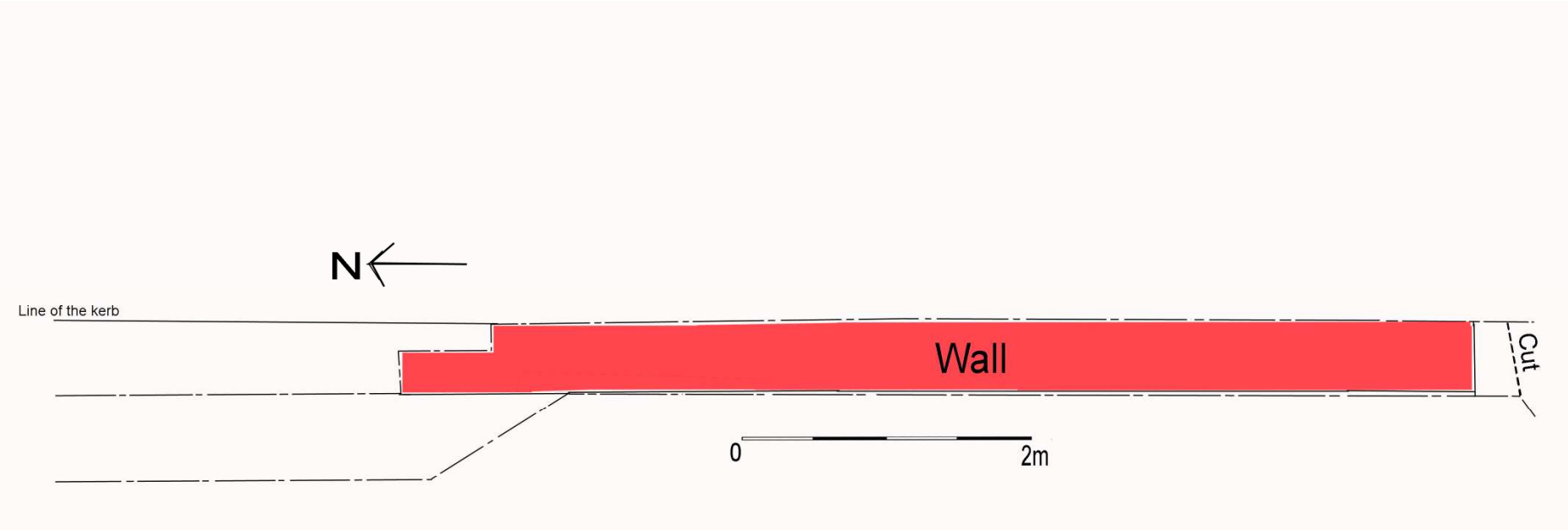


Fig. 25: Plan of trench 2

4.2.1 This trench was located on the eastern side of Seething Lane, immediately to the south of Pepys Street and some 15m to the south of trench 1. No significant remains were observed in the intervening area, across the Pepys Street junction. Trench 2 was approximately 11m long (north-south) by 0.5m wide and was excavated to an average depth of 1.2m. The present surface was made up of a tarmac road-surface over concrete base, *c.*0.3m thick.

The archaeological contexts encountered in this trench are described in the table below:

Context	Description	Interpretation
13	Brick wall, <i>c.</i> 7m in length (recorded length). Continued into unexcavated section to the north of the trench, and was cut away by modern services to the south. The west face was just inside the western edge of the trench but overall width unknown (eastern side of wall was not exposed). However, must be more than 0.6m (width of trench was 0.5m and removal of brick samples from the eastern section of wall show that it extended at least another 0.1m). Total depth unknown, but localised excavation near the northern end of the wall showed the brickwork continuing at least 1m below present road surface. Reused earlier bricks (3033) and 17 th - 19 th Century bricks (3032) – see brick report (appendix V).	Probably part of the external western wall (beneath ground-level) of the East India Company's Warehouse, and a continuation of the wall found in trench 1 (context 6).

4.2.2 The location of this wall on historical maps of the East India Company Warehouse on Seething Lane, such as Horwood's 1799 map (fig. 9), indicates that this wall was part of the external western wall of the East India Company Warehouse (which ran along the eastern edge of Seething Lane) and was a continuation of the same wall uncovered in trench 1 (context 6).

The uncovered sections of this wall would have been beneath ground level (possibly relating to the basements known to have existed in the warehouse). The uncovered sections were part of the core of the wall and western face as the eastern face was not uncovered. The upper section of the wall was heavily truncated to the west by previous services.

The wall consists of both reused earlier, possibly Tudor, bricks and bricks dating to around the period of the construction of the warehouse.

It would appear from contemporary maps that this wall continued by at least another 10 metres south (approximately). This was cut away by modern services, although the new trench was then stepped out by about 2m to the west and so well away from any further potential evidence for the wall.



Fig. 26: View along context 13 (brick wall in trench 2), looking south



Fig. 27: View of northern end of context 13 (brick wall in trench 2) – original face just to the left of scale.

- 4.3** A later site visit in June 2009, in relation to the watching brief for Mains Replacement Works in this area, extended the northern end trench 1 for approximately 8m into Crutched Friars. The trench measured an average of 0.55m in width and was excavated to a depth of *c* 1m below the existing road surface. A small section of brickwork was uncovered at the base of this trench, truncated by a watermain valve to the south but likely to have been the continuation/northern face of the wall [1] recorded in the May 2009 investigation. This brickwork measured 0.72m in width (N-S), at least 0.6m high and 0.55m+ across base of trench.
- 4.4** A further watching brief in December 2009 was undertaken in a similar area, just north of the May 2009 investigation. This was of a 3.5m trench running north-south along the eastern edge of Seething Lane up to the junction with Crutched Friars. This trench was only 600mm deep, such that any archaeology (relating to the East India Company or the graveyard of St Olave's Church) was preserved *in situ*.

5. Summary and Conclusions

- 5.1** The watching brief revealed a number of significant archaeological deposits, all of which were appropriately recorded and left *in situ*.

A substantial north-south wall-base was uncovered in trenches 1 and 2. This was evidently part of the western wall of the East India Company Warehouse on Seething Lane, constructed between 1787 and 1797. This warehouse was a huge building, consisting of six stories and covering an extensive area of the whole corner-block between Seething Lane and Crutched Friars (see fig. 9). The uncovered sections in this investigation are only a small part of this structure. The walls uncovered in this investigation were constructed of both reused and 'contemporary' bricks.

Some evidence relating to the possible original extent of St Olave's Church graveyard was also uncovered in trench 1. This consisted of part of an inhumation, and a probable grave-fill. Much of this was truncated at a later date by buildings associated with the East India Company warehouse and adjacent structures.

Further walls were uncovered in trench 1 which were related to other buildings in this area. For example, the three possible construction phases of context 1 (the brick wall at the northern end of trench 1) may reflect the development of buildings associated with the 17th Century Navy Office, the East India Company, and the later public house. Furthermore, context 3 (the east-west brick wall abutting context 6) is probably part of the free-standing building at the north-west corner of the warehouse, constructed in the early-mid 19th Century.

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Appendix I – Pot Report

Pottery from Seething Lane, London (Site TZJ09)

Paul Blinkhorn

The pottery assemblage comprised 32 sherds with a total weight of 577g. It comprised a mixture of Roman, medieval and later wares. The fabric codes utilized are those of the Museum of London post-Roman type-series (Vince 1985), as follows:

CHINA:	'Ironstone' china, 1800-1900. 1 sherd, 9g.
MPUR:	Midlands purple ware, 1400-1500. 1 sherd, 7g.
PMR:	Post-medieval redware, 1580 – 1900. 9 sherds, 256g.
SHER:	S. Herts./Limpsfield grey wares, 1140-1300. 4 sherd, 40g.
SWSG:	Staffordshire white salt-glazed stoneware, 1720-1780. 5 sherds, 13g.
TGW:	English tin-glazed ware, 1600-1800. 9 sherds, 218g.

In addition, three sherds (34g) of residual Romano-British material (RB) were present. The pottery occurrence by number and weight of sherds per context by fabric type is shown in Table 1. Each date should be regarded as a *terminus post quem*. The range of pottery types is typical of London and its hinterland. They suggest that there was activity at the site in the medieval period, probably in the second half of the 12th century, and then again perhaps at end of the period, with the site occupied from that point onwards. All the medieval wares are from unglazed vessels, probably jars, with the post-medieval material comprising a mixture of utilitarian- and tablewares. Six of the eight sherds of TGW from context [8] were all from the same vessel, a squat jar with horizontal painted pale blue stripes. It is reconstructable to a full profile, 94mm high by base diameter of 113mm (see Fig 24). The sherds of SWSG are from tea-bowls and tankards, which are typical products of the tradition.

Table 1: Pottery occurrence by number and weight (in g) of sherds per context by fabric type

Cntxt	RB		SHER		MPUR		PMR		TGW		SWSG		CHINA		Date
	No	Wt	No	Wt	No	Wt	No	Wt	No	Wt	No	Wt	No	Wt	
5	2	31	1	13					1	1					17thC
8	1	3	3	27	1	7	3	98	8	217	5	13			E18thC
9							6	158					1	9	19thC
Total	3	34	4	40	1	7	9	256	9	218	5	13	1	9	

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Appendix II – Clay Pipe Report

Clay tobacco pipe

One bowl, one fragmentary base and four pieces of stem were recovered from contexts [5] and [8]. In the descriptions below bowls have been classified and dated according to the Chronology of London Bowl Types (Atkinson Oswald 1969) and the Simplified General Typology (Oswald 1975, 37-40), the prefixes AO and OS being used to denote each typology:

Context [5] – 2 stem fragments, 75mm x 8.5mm (length x diam) and 59mm x c 9mm

Context [8] – 2 stem fragments, 50mm x c 6.5mm and 34mm x c 12mm. A slight lip on the latter indicates that it was located immediately before the bowl.

“ “ – Bowl: projecting spur chipped away at front/base. Crudely initialled either side: on right hand side may be ‘C’ or ‘G’, otherwise quite plain. Comparable to AO Type 26 (c 1740-1800); also OS Type 23 (1760-1800).

“ “ – Bowl: only base with flat foot & fragment of wall above stem surviving. No maker’s mark or other decoration, & not identifiable to type.

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Appendix III – Museum of London Tile Reference

A single broken tile, measuring approximately 13cm by 10cm by 1cm, was recovered from context 5 (the line of demolition rubble material behind the major wall, context 6). It is a tin-glazed floor-tile with floral decoration and 'tulip' corner motifs. Although this is in monochrome the design is directly identifiable with an example listed under the Museum of London Accession number 6948 (see Museum of London online database – basic details reproduced below).

MoL Accession number: 6948

Production place: Pickleherring (Southwark) or Platform Wharf (Rotherhithe), London

Production date: 1618-1663

Summary: Tin-glazed floor tile with polychrome floral decoration. 'Tulip' corner motifs.



Fig. 28: Tile from context 5

Appendix IV – Brick Report (compiled in consultation with John Brown)

Trench 1

Context 1

- Examples of 3033 brick-fabric in lower part of wall. Set within a yellowy-grey sandy mortar with few visible inclusions. Irregular stretcher bond. Mid-late 17th Century.
- Examples of a mixture of 3032 and 3033 brick-fabric in middle section of wall. Similar to bricks in context 6 (which are dated c.1730-1830)
- Examples of 3035 brick-fabric in upper part of wall. Set within a Portland cement mortar (mid 19th Century – 1900).
- These three types of bricks may relate to the three stages in the development of the buildings on this site. The 3033 bricks may relate to the buildings associated with the Navy Office (see Rocque's map, fig. 6), the mixture of 3032 and 3033 bricks may relate to buildings associated with the East India Company Warehouse (see Horwood's map, fig. 9), and the 3035 bricks may relate to the late construction of a pub on this site (see the 1872 OS map, fig. 13).

Context 3

- Examples of 3032 and 3033 brick-fabrics. Reused earlier bricks.
- Examples of brick-fabric intermediate between 3034 and 3035. Set within an off-white sandy lime mortar with frequent lime inclusions (up to 12mm) and occasional charcoal inclusions (up to 0.5mm). Late 18th – 1900.
- These two types of bricks suggest that the wall may have been built in two phases, with the upper section of the wall possibly being rebuilt at a later date.

Context 6

- Example of 3047 brick-fabric. Probably a 3033 variant. Dimensions: 202mm X 88mm X 57mm. Unfrogged, with sharp arrises. Lots of iron oxide in black flecks. Possibly used in gauged brickwork, and has fine lime putty mortar on the header-end. Reused brick, reused in the wall core.
- 3 samples of 3032 brick-fabric. Dimensions: 215-235mm X 100-106mm X 60-66mm. Unfrogged with sharp arrises. Set within a grey lime and sand mortar with lime, brick-dust and coal inclusions. The size and type of brick suggests a date of 1730-1830 (as they have a depth of over 60mm, so must be after c.1730). The type of mortar also suggests a date of mid-late 18th Century.
- Example of 3034 brick-fabric. Dimensions: 218mm X 98mm X 63mm. Strike marks on top face. Similar to that in context 3. Late 18th-19th Century.

- The wall used an irregular bond, with fragmentary bricks and brick-rubble. These dated from differing periods, as were a mixture of earlier reused bricks (e.g. 3047) and bricks dating to the construction of the warehouse (e.g. 3032).

Context 9

- Example of 3033 brick-fabric. Dimensions: 2328mm X 108mm X 57mm. Reused brick (can tell from the mortar), possibly in its third use (very degraded). 1450-1700.

Trench 2

Context 13

- Example of 3033 brick-fabric. Dimensions: 225mm X 107mm X 60mm. 1450-1700 (possibly pre-1600, as has characteristic Tudor qualities). Possibly reused from Walsingham's house.
- 2 samples of 3032 brick-fabric. Dimensions: 218mm X 106mm X 62mm; 202mm X 102mm X 65mm. One dated c.1630-1700. One dated c.1730-1900 (as depth is over 60mm).
- This wall is similar to that in context 6, consisting of reused earlier bricks (3033) and bricks dating to around the period of the construction of the East India Company Warehouse (3032).

Appendix V – OASIS Form

OASIS ID: compass1-81633

Project details

Project name	Seething Lane
Short description of the project	Following a watching brief in relation to the Thames Water Victorian Mains Replacement Programme and the discovery of brick walls, a field investigation of two sections of a trench in the north-eastern part of the road was undertaken. This revealed the remains of parts of major brick walls of the East India Company warehouse constructed in Seething Lane in the late 18th Century. Remains of other buildings were also uncovered: these included a 19 th Century public house and a possible ancillary building to the 17 th Century Navy Office. Part of an inhumation was also uncovered, suggesting that the medieval St Olave's Church graveyard once extended across the present road.
Project dates	Start: 12-05-2009 End: 15-05-2009
Previous/future work	Yes / No
Type of project	Field evaluation
Current Land use	Transport and Utilities 1 - Highways and road transport
Monument type	WALL Post Medieval
Monument type	EXTENDED INHUMATION Medieval
Significant Finds	POT Medieval
Significant Finds	POT Post Medieval
Significant Finds	HUMAN BONE Medieval
Significant Finds	CLAY PIPE Post Medieval
Significant Finds	FLOOR TILE Post Medieval
Significant Finds	BRICK Post Medieval
Methods & techniques	'Annotated Sketch','Documentary Search','Photographic Survey','Targeted Trenches','Visual Inspection'
Development type	Pipelines/cables (e.g. gas, electric, telephone, TV cable, water, sewage, drainage etc.)
Prompt	Water Act 1989 and subsequent code of practice
Position in the planning process	Not known / Not recorded

Project location

Country	England
Site location	GREATER LONDON CITY OF LONDON CITY OF LONDON Seething Lane
Postcode	EC3
Study area	35.00 Square metres
Site coordinates	TQ 3339 8084 51.5102231296 -0.077609858825 51 30 36 N 000 04 39 W Point

Project creators

Name of Organisation	Compass Archaeology
Project brief originator	Development Manager-Historic Environment, City of London
Project design originator	Compass Archaeology
Project director/manager	Compass Archaeology
Project supervisor	Geoff Potter
Type of sponsor/funding body	Water utility/company
Name of sponsor/funding body	Thames Water Utilities and Clancy Docwra Ltd

Project archives

Physical Archive recipient	Museum of London archaeological archive
Physical Contents	'Ceramics'
Digital Archive recipient	Museum of London archive
Digital Contents	'other'
Digital Media available	'Images raster / digital photography','Text'
Paper Archive recipient	Museum of London Archive
Paper Contents	'other'
Paper Media available	'Context sheet','Map','Plan','Report','Section','Unpublished Text'

**Project
bibliography 1**

Publication type Grey literature (unpublished document/manuscript)

Title Thames Water Mains Replacement Works: Detailed Report on Seething Lane EC3; Interim Report on Crouch Hill 55 (June 2009 - August 2010)

Author(s)/Editor(s) Jeffery, E

Date 2010

Issuer or publisher Compass Archaeology

Place of issue or publication 5-7 Southwark Street, SE1 1RQ

Description Report covering both the field investigation of Seething Lane, and the watching brief undertaken on Crouch Hill 55 from June 09 - August 10 in relation to the Thames Water Mains replacement works programme. Includes detailed discussion of history of Seething Lane site (use of documentary, cartographic and pictorial sources) and discussion of field investigation (discussion of trenches, finds and interpretations - with relevant plans, sections and photographs). Interim report includes a brief discussion of each trench monitored.

Entered by Emma Jeffery (emma.jeffery@ukonline.co.uk)

Entered on 26 August 2010

Appendix VI: London Archaeologist Summary

Site Address: Seething Lane, EC3

Project type: Watching brief and Field Investigation

Dates of Fieldwork: May 2009

Site Code: TZJ 09

Supervisor: Geoff Potter

NGR: TQ 33390 80864 (N) / 33392 80815 (S)

Funding Body: Clancy Docwra Ltd.

An investigation of two sections of a trench in the north-eastern part of Seething Lane was undertaken in May 2009 after the discovery of brick walls during a watching brief undertaken in relation to the Thames Water Victorian Mains Replacement Programme.

This uncovered two parts of the western wall of the major East India Company warehouse on Seething Lane, constructed in the late 18th Century. Walls relating to other buildings in this area were also uncovered: these included a 19th Century public house and a possible ancillary building to the 17th Century Navy Office. The discovery of part of an inhumation raises the possibility that this was once part of St Olave's Church graveyard, which extended across the present road before the 16th Century.

II: CITY INTERIM REPORT CROUCH HILL 55 (8TH JUNE 2009 – 19TH AUGUST 2010)

Archaeological monitoring has continued on all mains replacement works across Crouch Hill 55 in the City of London. Regular visits have been made to all sites where groundbreaking has occurred, and a standard written and photographic record taken where applicable. The vast majority of sites have exposed nothing of archaeological significance, usually consisting of road makeup layers, intrusive service backfill and made-ground deposits.

This interim report follows on from Interim 29, which covers all archaeological monitoring undertaken in the City of London from 9th March – 7th June 2009, although it should be noted that relatively few groundworks took place during the first half of 2010.

Seething Lane (excluding trenches described in Section 1 above)

Approximately 21m of trenching was observed on the east side of Seething Lane, towards its south end and running eastwards into Muscovy Street. The trench measured an average of 0.5m in width and was excavated to a depth of 0.75m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark-brown sandy made ground with some brick inclusions. No archaeological finds or features were observed.

Approximately 17.5m of trenching was observed running north along the western side of Seething Lane from its southern end, adjacent to No.12 Byward Street. The trench measured an average of 0.48m in width and was excavated to a depth of 0.62m below the existing road surface. Paving slabs overlying a sand and concrete bedding was exposed, overlying a yellowy sandy service backfill with gravel inclusions. No archaeological finds or features were observed.

Approximately 9m of trenching was observed in the small lane running parallel and to the east of Seething Lane, between Pepys Street and Muscovy Street and adjacent to the rear of the Port of London Authority building. The trench measured an average of 0.6m in width and was excavated to a depth of 0.88m below the existing road surface. Tarmac and existing hardcore was exposed overlying a dark brown silty made ground layer with moderate ceramic building material and gravel inclusions. No archaeological finds or features were observed.

Northumberland Alley

Approximately 21.5m of trenching was observed on the west side of Northumberland Alley, running south from Fenchurch Street. The trench measured an average of 0.6m in width and was excavated to a depth of 0.52m below the existing road surface. Paving slabs overlying a concrete bedding was exposed over a yellow sandy service backfill. No archaeological finds or features were observed.

Fenchurch Street

Approximately 1.3m of trenching was observed on the south side of Fenchurch Street, adjacent to St Katherine's Row. The trench measured an average of 0.55m in width and was excavated to a depth of 1.0m below the existing road surface. Tarmac and existing hardcore was exposed overlying a grey-brown sandy made-ground deposit with gravel and infrequent ceramic building inclusions (figure 29). No archaeological finds or features were observed.

To the east of this one pit on the south side of the street close to the junction with Lloyd's Avenue revealed modern services and backfill. No archaeological finds or features were observed.



Fig. 29: View of Fenchurch Street trench (made-ground deposit)

Approximately 14m of trenching was observed on the south side of Fenchurch Street, outside No.60. The trench measured an average of 0.5m in width and was excavated to a depth of 1m below the existing road surface. Tarmac and existing hardcore was exposed overlying a grey-brown rubble-based backfill with frequent brick and gravel inclusions. No archaeological finds or features were observed.

A further 17.5m of trenching was observed on the south side of the road, running from No.58 Fenchurch Street to the junction with Mark Lane. The trench measured from 0.8-2.2m in width and was excavated to a depth of 0.96m below the existing road surface. Tarmac and existing hardcore was exposed overlying a grey-brown sandy made-ground with moderate gravel, ceramic building material, and concrete rubble inclusions. No archaeological finds or features were observed.

Approximately 20m of trenching was observed on the south side of Fenchurch Street, outside No.40-45. The trench measured an average of 0.83m in width and was excavated to a depth of 1.18m below the existing road surface. Tarmac and existing hardcore was exposed overlying a grey-brown sandy made-ground with frequent gravel inclusions. No archaeological finds or features were observed.

Another 3.2m of trenching was observed on the south side of Fenchurch Street, on the west corner of the junction with Mincing Lane. The trench measured an average of 2.3m in width and was excavated to a depth of 1.03m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark-brown sandy-clay made-ground and pink MOT Type 1 crushed stone. No archaeological finds or features were observed.



Fig. 30: View of Fenchurch Street trench with MOT Type 1 crushed stone

Another 28m of trenching was observed on the south side of Fenchurch Street, running west from Mincing Lane and the previous trench. The trench measured an average of 0.6m in width and was excavated to a depth of 1.12m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark-brown sand and gravel made-ground with occasional ceramic building material inclusions. No archaeological finds or features were observed.

Approximately 8m of trenching was observed on the south side of Fenchurch Street, opposite No.135. The trench measured an average of 0.62m in width and was excavated to a depth of 1.3m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark-brown sandy-silt made-ground with moderate ceramic building material and gravel inclusions, cut by dark yellow clay backfill. No archaeological finds or features were observed.

Approximately 7.4m of trenching was observed on the north side of in Fenchurch Street, from No.143 to No.145. The trench measured an average of 0.52-2.2m in width and was excavated to a depth of 0.73m below the existing road surface. Tarmac and existing hardcore was exposed overlying grey sandy backfill deposit with frequent gravel inclusions and infrequent ceramic building material inclusions. No archaeological finds or features were observed.

The only notable feature in this area was observed on the south side of the street, in a trench running east-west across the junction with London Street. This measured approximately 11m in length and 0.75m in width at its narrowest, extending to

approximately 2.2m in the central 3m section, and from approximately 1m to 1.9m deep. The northern end of a rubble-filled and partly demolished Victorian brick cellar was uncovered, the upper 1.5m being exposed. To the south this consisted of a brick arch, spanning over 4m and some 380mm thick: the construction consisted of headers at the upper level, stretchers below, and rendering on the inner face. On the other side of the trench the northern end wall of the cellar was exposed, and to the west the remains of the side wall were also uncovered. The total dimensions of this cellar are unknown, but it would have been approximately 4.2m wide, and some 2.5m was exposed from north to south.



Fig. 31: View of cellar brick-arch, Fenchurch Street



Fig. 32: View of northern cellar wall, Fenchurch Street

Leadenhall Street

Approximately 24.5m of trenching was observed on the north side of Leadenhall Street, from No.100 Leadenhall Street to No.168. The trench measured an average of 0.89m in width and was excavated to a depth of 0.87m below the existing road surface. Tarmac and existing hardcore was exposed overlying mixed made-ground deposits, including MOT Type 1 crushed stone, yellow sandy service backfills, and dark-brown made ground with gravel and ceramic building material inclusions. No archaeological finds or features were observed.

A further 13m of trenching was observed on the north side of Leadenhall Street, running eastwards from Creechurch Lane to No.86 Leadenhall Street. The trench measured from 0.6-2.5m in width and was excavated to a depth of 0.65m below the existing road surface. Tarmac and existing hardcore was exposed overlying sandy grey-brown made ground with frequent gravel inclusions. No archaeological finds or features were observed.

Approximately 13m of trenching was observed on the north side of Leadenhall Street, from No.77 Leadenhall Street to No.80-84. The trench measured an average of 0.47m in width and was excavated to a depth of 0.8m below the existing road surface. Tarmac and existing hardcore was exposed overlying brown sand and gravel made-ground deposits with infrequent ceramic building material inclusions. No archaeological finds or features were observed.



Fig. 33: View of Leadenhall Street trench (modern road surface and made-ground deposits)

Approximately 13m of trenching was observed on the north side of Leadenhall Street, across the junction with St Mary Axe. The trench measured an average of 2.2m in width and was excavated to a depth of 0.48m below the existing road surface. Tarmac and existing hardcore was exposed overlying mixed service backfills, including yellow sandy service backfills and dark brown made ground with frequent gravel inclusions. No archaeological finds or features were observed.

Close to this two connecting pits were observed, also on the north side of the street and measuring approximately 3m by 4m. Tarmac and existing hardcore was exposed overlying mixed modern deposits. No archaeological finds or features were observed.

One 3m pit was observed opposite Billiter Street. It measured approximately 3m by 3m. Tarmac and existing hardcore was exposed overlying mixed modern deposits. No archaeological finds or features were observed.

Crosswall

Approximately 6-8m of trenching was observed on the north side of Crosswall, at the west end of the road. The trench measured from 2-3m in width and was excavated to a depth of 1.3m below the existing road surface. Paving slabs and sand was observed in the northern part of the trench (the pavement), with tarmac and existing hardcore in the southern part (the road). This overlay made-ground deposits with service backfills. A brick wall at least 8 courses deep, c.3.2m long, was uncovered. This consisted of purple stock brick, bright red brick, and pale white-yellow mortar.



Fig. 34: View of brick wall near the western end of Crosswall

A further 10.5m of trenching was observed on the north side of Crosswall, running east from the junction with Crutched Friars. The trench measured an average of 0.8m in width and was excavated to a depth of 0.7m below the existing road surface. Tarmac and existing hardcore was exposed overlying grey-brown silty-sand made-ground with frequent gravel inclusions. No archaeological finds or features were observed.

Approximately 25m of trenching was observed on the north side of Crosswall, running east from Vine Street to Minories. The trench measured an average of 0.55m in width and was excavated to a depth of 0.86m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark-brown silty made-ground with regular ceramic building material inclusions. No archaeological finds or features were observed.

Approximately 7m of trenching was observed on the north side of Crosswall, running west from the junction with Minories. The trench measured an average of 0.8m in width and was excavated to a depth of 1m below the existing road surface. Tarmac and existing hardcore was exposed overlying grey-brown sandy made-ground with frequent gravel inclusions. No archaeological finds or features were observed.

Jewry Street

Approximately 37m of trenching was observed on the west side of Jewry Street, running south from Carlisle Avenue. The trench measured an average of 0.5-0.7m in width and was excavated to a depth of 0.85-1.10m below the existing road surface. Tarmac and existing hardcore was exposed overlying modern made ground, consisting of red and yellow stock brick rubble and mixed loose soil, mortar, pebbles and sand. More recent services crossed this trench in a couple of places, and a probable brick-lined drain was uncovered crossing the trench approximately 17m from its northern end. No archaeological finds or features were observed.

Approximately 18m of trenching was observed on the west side of Jewry Street, running north from Carlisle Avenue. The trench measured an average of 0.6-0.9m in width and was excavated to a depth of 0.6m below the existing road surface. Tarmac and existing hardcore was exposed, overlying a coarse mix including concrete. No archaeological finds or features were observed.

A further 24.5m of trenching was observed in a similar location, on the west side of Jewry Street running from Crutched Friars to No.7 Jewry Street. The trench measured an average of 0.85m in width and was excavated to a depth of 0.92m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark yellow sandy clay with frequent ceramic building material, brick and gravel inclusions, and demolition rubble to the north. No archaeological finds or features were observed.

Four trenches were observed towards the northern end of Jewry Street, one immediately north of the India Street junction, and the other three over a distance of about 10m close to the junction with Aldgate. These lay just within the Scheduled Ancient Monument boundary, relating to the Roman wall (Scheduled Monument Number: LO26K). The trenches measured an average of 10m x 0.5m x 0.40-0.75m deep. Tarmac and existing hardcore was exposed overlying previous service backfills (previous pipes crossing the street and other services were noted). No archaeological finds or features were observed.



Fig. 35: View of one of the trenches at the northern end of Jewry Street (service backfills)

Crutched Friars

Approximately 7.4m of trenching was observed on the east side of Crutched Friars, across from the junction with Rangoon Street. The trench measured an average of 0.47m in width and was excavated to a depth of 0.97m below the existing road surface. Tarmac and existing hardcore was exposed overlying brown sandy clay backfill deposits with regular gravel inclusions. No archaeological finds or features were observed.

28m of trenching was observed on the north-west side of Crutched Friars, running south from Rangoon Street up to Northumberland Alley. The trench measured from 0.5-0.9m in width and was excavated to a depth of 0.96m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark-brown sandy made-ground with gravel inclusions. No archaeological finds or features were observed.

Beyond this approximately 8m of trenching was observed on the west side, opposite Crosswell. The trench measured from 0.5-1.7m in width and was excavated to a depth of 0.91m below the existing road surface. Tarmac and existing hardcore was exposed overlying yellow sandy service backfill. No archaeological finds or features were observed.

Shallow trenching across the northern part of Crutched Friars, between Rangoon Street and India Street, revealed modern rubble fill. No archaeological finds or features were observed.

Pepys Street

Approximately 12m of trenching was observed on the south side of Pepys Street, running west from Savage Gardens. The trench measured an average of 0.6m in width and was excavated to a depth of 0.97m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark-brown sandy made-ground with moderate ceramic building material, rubble and gravel inclusions. No archaeological finds or features were observed.

One shallow pit was also dug on the southern side of Pepys Street, measuring less than 750mm in depth. No archaeological finds or features were observed.

A trench on the northern side of Pepys Street, at the eastern end, uncovered modern services. No archaeological finds or features were observed.

Minories

Approximately 9.8m of trenching was observed on the west side of Minories, opposite Goodman's Yard. The trench measured an average of 0.84m in width and was excavated to a depth of 1.15m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark-brown made-ground with moderate inclusions of chalk flecks. No archaeological finds or features were observed.

Another 20m of trenching was observed on the west side of Minories, near the northern end. The trench measured from 0.6-0.95m in width and was excavated to a depth of 1.1-1.25m below the existing road surface. Tarmac and existing hardcore was exposed overlying mixed silty-sandy material with occasional ceramic building material inclusions, part of previous service trench backfills. No archaeological finds or features were observed.

On the eastern side of Minories approximately 21m of trenching was observed, running from the north side of Goodman's Yard west into Minories. The trench measured an average of 0.7m in width and was excavated to a depth of 0.95m below the existing road surface. Tarmac and existing hardcore was exposed overlying modern pink-grey MOT Type 1 crushed stone. No archaeological finds or features were observed.

A further 14m of trenching was observed on the east side of Minories outside River House. The trench measured an average of 0.6m in width and was excavated to a depth of 1m below the existing road surface. Tarmac and existing hardcore was exposed overlying brown-yellow sandy made-ground with frequent gravel inclusions and occasional ceramic building material inclusions. No archaeological finds or features were observed.

A small area of breaking was also observed on the eastern side of the road opposite India Street, but again no archaeological finds or features were seen.

Approximately 25m of trenching was observed on the eastern side of Minories, north of the junction with St Clare Street. The trench measured an average of 0.8m in width and was excavated to a depth of 1.25-1.45m below the existing road surface. Tarmac and existing hardcore was exposed overlying mixed layers of made-ground, including dark silty-sand and yellowy clayey sand with loosed mortar and brick-rubble (figure 36). Some bits of pottery and clay pipe, probably 18th Century, were uncovered.



*Fig. 36: View of Minories trench
(modern road base overlying possible
18th century made-ground)*

Portsoken Street

Approximately 22m of trenching was observed on the north side of Portsoken Street, running from Mansell Street westwards up to No.2 Portsoken Street. The trench measured an average of 0.6m in width and was excavated to a depth of 0.97m below the existing road surface. Tarmac and existing hardcore was exposed overlying dark sandy-clay deposit with moderate ceramic building material and gravel inclusions. No archaeological finds or features were observed.

Approximately 17.5m of trenching was observed on the north side of Portsoken Street, running east from the junction with Minories. The trench measured an average of 0.52m in width and was excavated to a depth of 0.97m below the existing road surface. Tarmac and existing hardcore was exposed overlying mid-brown silty made-ground with frequent ceramic building material and gravel inclusions. No archaeological finds or features were observed.

Goodman's Yard

Approximately 40m of trenching was observed on the north side of Goodman's Yard, running east from the junction with Minories. The trench measured an average of 0.9m in width and was excavated to a depth of 0.9-1.1m below the existing road surface. Tarmac and existing hardcore was exposed overlying made-ground with gravel inclusions (service backfill). No archaeological finds or features were observed.

An 'L'-shaped trench was observed on the east side of Goodman's Yard, adjacent to No.48-50 Minories. The trench measured an average of 6m in length, 3.5m in width and was excavated to a depth of 0.9m below the existing road surface. Tarmac and existing hardcore was exposed overlying brownish-yellow sandy made-ground with frequent gravel and ceramic building material inclusions. No archaeological finds or features were observed.

St Clare's Street

Approximately 25m of trenching was observed on the north side of St Clare's Street, running east from the junction with Minories to No.30-33. The trench measured an average of 0.48m in width and was excavated to a depth of 0.94m below the existing road surface. Paving bricks overlying sand and concrete bedding was uncovered, overlying brown-grey sandy made-ground with frequent gravel inclusions. No archaeological finds or features were observed.

Vine Street

Approximately 2.5m of trenching was observed on the east side of Vine Street, at the junction with Crosswall. The trench measured an average of 2m in width. Tarmac and existing hardcore was exposed, overlying service backfills. No archaeological finds or features were observed.

Another 17m of trenching was observed on the east side of Vine Street, running north from the junction with Crosswall. The trench measured an average of 0.6m in width and was excavated to a depth of 1m below the existing road surface. Tarmac and existing hardcore was exposed, overlying yellow sandy ballast mixed with grey-brown sandy made-ground and frequent gravel inclusions and moderate ceramic building material inclusions. No archaeological finds or features were observed.

Approximately 3.1m of trenching was observed on the west side of Vine Street, at the junction with Crosswall. The trench measured an average of 2m in width and was excavated to a depth of 1.2m below the existing road surface. Tarmac and existing hardcore was exposed, overlying brown-grey sandy made-ground with frequent gravel and occasional ceramic building material inclusions. No archaeological finds or features were observed.

A further 8m of trenching was observed on the west side of Vine Street, turning west into the parking lot outside No.35 Vine Street. The trench measured an average of

0.89m in width and was excavated to a depth of 0.89m below the existing road surface. Paving bricks overlying concrete hardcore was exposed, overlying dark-brown sandy made-ground with frequent gravel and ceramic building material inclusions. No archaeological finds or features were observed.

A square pit adjacent to the west kerb outside No.53 Vine Street was observed. It measured an average of 35m in length, 2m in width and was excavated to a depth of 1.11m below the existing road surface. Paving bricks overlying concrete hardcore was exposed, overlying mid-brown sandy made-ground with moderate ceramic building material and gravel inclusions. No archaeological finds or features were observed.

Hammett Street

Approximately 20m of trenching was observed on the north side of Hammett Street, running from the junction with Minories. The trench measured an average of 0.9m in width and was excavated to a depth of 1.05m below the existing road surface. Tarmac and existing hardcore was exposed, overlying made-ground deposits or backfill, consisting of sandy-silt with occasional ceramic building material inclusions and frequent stone inclusions. No archaeological finds or features were observed.

Savage Gardens

A small trench was observed running west from the east kerb of Savage Gardens, outside No.10 Pepys Street. Tarmac and existing hardcore was exposed, overlying light brown sandy made-ground with moderate ceramic building material inclusions and frequent gravel inclusions. No archaeological finds or features were observed.

A more recent trench on the east side of the road, running north-south from the junction with Crutched Friars for approximately 36m, was observed. This measured approximately 0.38-0.56m in width, and 0.8-0.9m in depth. Tarmac and existing hardcore was exposed, overlying a mixed backfill from the adjoining service trenches, which mainly consisted of a grey-brown sandy-silt with scattered pebble and ceramic building material inclusions. No archaeological finds or features were observed.

Hart Street

Approximately 2.2m of trenching was observed on the south side of Hart Street, outside No.3 Hart Street. The trench measured an average of 0.5m in width and was excavated to a depth of 0.54m below the existing road surface. Tarmac and existing hardcore was exposed, overlying mid-brown sandy made-ground with moderate gravel inclusions. No archaeological finds or features were observed.

A further 30m of trenching was observed in a similar location, on the south side of Hart Street, running westwards from the junction with Mark Street. The trench measured an average of 0.67-2.5m in width and was excavated to a depth of 0.85m below the existing road surface. Tarmac and existing hardcore was exposed,

overlying sand and gravel service backfill. No archaeological finds or features were observed.

An 'L'-shaped trench was observed on the south side of Hart Street, starting at No.3 Hart Street and running south into Mark Lane. The trench measured an average of 14m in length, 1m in width and was excavated to a depth of 0.5m below the existing road surface. Tarmac and existing hardcore was exposed. No archaeological finds or features were observed.

India Street

Approximately 7m of trenching was observed on the south side of India Street, running west from Minories. The trench measured an average of 0.6m in width and was excavated to a depth of 1m below the existing road surface. Tarmac and existing hardcore was exposed, overlying dark-brown sandy made-ground with occasional gravel and ceramic building material inclusions. No archaeological finds or features were observed.

A further 20m of trenching was observed on the south side of India Street, running from the junction with Vine Street up to Crutched Friars. The trench measured an average of 0.5m in width and was excavated to a depth of 0.78m below the existing road surface. Tarmac and existing hardcore was exposed, overlying yellow sand and gravel ballast. No archaeological finds or features were observed.



Fig. 37: View of India Street trench (disturbed by previous services and solid concrete base)

A trench was observed on the junction of India and Jewry Streets, running East-West. This area falls within the Scheduled Ancient Monument boundary relating to the Roman wall. To the west the trench had been heavily disturbed by previous services, and to the east a solid concrete base was uncovered at the base of the trench. It is not

entirely clear what this feature is, but it may have been part of a disused basement. No archaeological finds or features were observed.

Crescent

Several shallow open trenches were observed on the western and southern sides of Crescent. These only exposed rubble and modern services. No archaeological finds or features were observed.

Change Alley

Approximately 32m of trenching was observed on the east side of Change Alley, running from Lombard Street. The trench measured an average of 0.38m in width and was excavated to a depth of 0.46-0.63m below the existing road surface. Stone slabs over a sand base was exposed, overlying modern backfill, which overlay solid concrete slabs. This may have been part of a basement, which spanned the width of the alley, as the adjacent buildings appear to have been of the same construction and span the alley. No archaeological finds or features were observed.



Fig. 38: View of Change Alley trench (possibly basements?)

Muscovy Street:

Approximately 4.8m of trenching was observed on the north side of Muscovy Street, at its eastern end, running east-west and projecting slightly to the south. The trench measured an average of 0.45m in width and was excavated to a depth of 1m below the existing road surface. Tarmac and existing hardcore was exposed, overlying relatively modern made-ground, consisting of compact brick rubble and mortar, with lenses of dark gritty silty soil. No archaeological finds or features were observed.