

**LAND AT COURTLANDS FARM,
PARK ROAD, BANSTEAD, SM7 3EF**

**HISTORIC BUILDINGS RECORD
(HISTORIC ENGLAND LEVEL 4)**

(NGR 526799 159153)



**Prepared for
HA & DB Kitchin Ltd**

**LAND AT COURTLANDS FARM, PARK ROAD,
BANSTEAD, SURREY, SM7 3EF**



**Historic Buildings Record
(Historic England Level 4)**

NGR: 526799 159153

Planning Ref: 16/01013/F

**ASE Project No: 171102
Site Code: CFB17**

**ASE Report No: 2017537
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Prepared by:	Hannah Samuels	Archaeologist	
Reviewed and approved by:	Amy Williamson	Project Manager	
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**Archaeology South-East
Units 1 & 2
2 Chapel Place
Portslade
East Sussex
BN41 1DR**

SUMMARY

Between December 2017 and July 2018 Archaeology South-East (a division of the Centre for Applied Archaeology, UCL) carried out a historic buildings record of Courtlands Farm, Park Road, Banstead, Surrey, SM7 3EF. The work was commissioned by HA & DB Kitchin Ltd and requested by Reigate and Banstead Borough Council, to be addressed as a condition placed on planning consent relating to the removal of hardstanding and buildings and the erection of nine detached houses and retained 'bunker' with associated access, parking and landscaping (planning ref. 16/01013/F).

The site identified as Equipment Ammunition Depot Magazine (EAM) No. 116 was built as part of the countrywide anti-aircraft defence preparation works established in the lead-up to the Second World War. The ammunition depot at Banstead formed one of three depots built between 1938 and 1939 to store and supply ammunition for the anti-aircraft batteries surrounding and defending London during war time, including the Battle of Britain, which was the first major Nazi defeat and a crucial turning point in the conflict, and the Blitz. The Banstead site supplied the South West London sector from Dulwich to Raynes Park and included the South East region for a short period in 1940. The site officially closed as an active storage / distribution centre in 1953 and was auctioned-off in 1966. After, it was utilised for agricultural, storage and workshop use. The site is now redundant of use.

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1.0 INTRODUCTION

1.1 Between December 2017 and July 2018 Archaeology South-East (a division of the Centre for Applied Archaeology, UCL) carried out a historic buildings record of Courtlands Farm, Park Road, Banstead, Surrey, SM7 3EF (Figures 1 and 2; NGR 526799 159153).

1.2 The work was commissioned by HA & DB Kitchin Ltd and requested by Reigate and Banstead Borough Council, to be addressed as a condition placed on planning consent relating to the removal of hardstanding and buildings and the erection of nine detached houses and retained 'bunker' with associated access, parking and landscaping (planning ref. 16/01013/F). Condition 15 of the planning consent states that:

Before demolition commences the site shall be fully archaeologically record(ed) including building archaeology in a scheme submitted to and approved in writing by the LPA in consultation with Surrey County Council and Historic England and the resulting record placed in archives agreed by the LPA.

Reason: To ensure the proper investigation, preservation and recording of the archaeological interest in the site with regard to Reigate and Banstead Borough Local Plan policy Pc8.

2.0 SCOPE & METHODOLOGY

2.1 The recording involved the survey of all the buildings on the site that are to be affected by the proposed work (Figure 2). They were recorded at Level 4 as defined by Historic England (2016), in accordance with the consultation response issued by Surrey County Council's archaeological advisor, Nick Truckle. The work is set out in a written scheme of investigation (Archaeology South-East 2017).

2.2 The initial record was made by Hannah Samuels and Susan Chandler on 11th and 12th December 2017. The site was visited again on 13th July 2018 by Hannah Samuels and Vas Tsamis, in order to record previously inaccessible structures and to augment an existing topographical survey of the site. Subject to accessibility and survival, a digital photographic record was made of the exterior and interior of the buildings to give an overall impression of their size and shape, and of the appearance of principal rooms and circulation areas. Detailed photographs of surviving architectural details, fixtures and fittings were also photographed. An index of the digital photography, together with location plans are included as an appendix to this report.

2.3 The written description and photographic record is supplemented by a drawn record of the site, comprising a location plan, as-existing floor plans and elevations of all buildings and selected section drawings. These were produced as part of the site survey and have been annotated to show all major components.

2.4 The site has been subject to a previous historic building assessment (ASE 2016), from which much of the historic background information has been derived. Additional information was sought from The Imperial War Museum Photography Archive, Historic England Archives (Swindon), the National Archives (Kew) and the Environment Agency's LIDAR data archive. Together with other published sources – principally a booklet compiled by the Banstead History Research Group through

communication with the site's former foreman, Mr Trusler – this has been used to inform the analysis of the site.

3.0 SITE LOCATION

3.1 The site is located to the south-east of Banstead in Surrey, within the Metropolitan Green Belt. Access to the site is from the east side of Park Road via a private driveway that is shared by Courtlands Farm and a number of residential properties within the immediate vicinity. The site contains eight rectangular concrete structures that formerly comprised an MoD Anti-Aircraft Ammunition Depot dating to 1938, and a number of disused modern farm buildings. In addition to these there are four concrete blast barrier traverses, known as 'bunds'. The site covers an area of 1.82ha, set within a larger compound measuring 18.21ha and is bounded in part by vegetation and banked earth. The site is currently redundant of use, but most recently served as a car storage facility. Within the outer compound are an air-raid shelter and two sentry air-raid posts.

4.0 STATUTORY DESIGNATIONS

4.1 None of the structures on the site are statutorily listed. By request the site was assessed by English Heritage (now Historic England) for statutory listing in February 2004. It was concluded that the site was not of sufficient special architectural or historic interest to merit listing at the time, despite being of national importance. It was considered that the buildings on the site do not hold group value with any listed buildings, nor do they have group value with any related wartime structures or any structures which they were designated to serve (English Heritage 2004).

Other designations

4.2 The site, known as the former Banstead Anti-Aircraft Ammunition Depot, is locally listed (designated by Surrey County Council) as a site of archaeological importance (Monument ID 1437822), with the surrounding land designated as a site of high archaeological potential.

4.3 The site lies within the Metropolitan Green Belt.

5.0 HISTORIC BACKGROUND AND CARTOGRAPHIC EVIDENCE

Historic Background

5.1 The Equipment Ammunition Depot Magazine No. 116 at Banstead forms part of the countrywide anti-aircraft defence preparation works established in the lead-up to the Second World War. As part of these preparations, sites for the storage of ammunition were necessary for supplying a large number of mobile gun sites (English Heritage 2001, 127). By the declaration of war the national requirement for these mobilisation stores totalled 108 buildings (English Heritage 2001, 127). The largest of these store centres comprised the Command Ammunition Depots (CADs), such as that at Bramley, near Basingstoke. The support infrastructure for these included nine Intermediate Ammunition Depots (IADs) and 34 Equipment Ammunition Magazines (EAMs). Operating from the IADs were teams of soldiers of the Royal Army Ordnance Corps, trained as Ammunition Examiners, who would

check ammunition batches for deterioration throughout the depot system. Inspection typically comprised a percentage of 5%. If faults were identified, all batches from the factory were subject to examination (BHRG 2006, 8). The Banstead site formed one of the EAMs, which formed the smallest and most numerous type of depot within the wider system (BHRG 2006, 8). When operational, the ammunition was distributed to the smaller units via the CADs and transported via rail or trucks run by the Royal Army Service Corps.

- 5.2 The site was one of three depots built between 1938 and 1939 to store and supply ammunition for the anti-aircraft batteries surrounding and defending London during war time (BHRG 2006, 4). A depot at Mill Hill supplied North London, one at Sevenoaks supplied East London and the Thames Estuary, and the one at Banstead supplied the South West London sector from Dulwich to Raynes Park (English Heritage 2004), including sites at Mitcham Junction and Primrose Hill (BHRG 2006, 9). During a brief period in 1940, the Banstead depot held the additional task of supplying the South East in addition to its usual deployment.
- 5.3 On a local level, the purchase of the farmland attached to Banstead Place in 1937 by the War Department, with the intention to construct the depot, formed part of a wider scheme of defensive works established in Banstead in the run up to the Second World War. By September 1939 the Banstead Urban District Council set up an Air Raid Precaution Committee as part of a countrywide acceleration in civil defensive measures (BHRG 2006, 1). Trenches were completed in the Lady Neville Recreation Ground and by 26th August 1939 council telephone lines were monitored continuously in anticipation of government instruction. In addition, the masking of traffic lights was undertaken and warden posts were established (BHRG 2006, 2).
- 5.4 The site was constructed under the responsibility of the Royal Army Ordnance Corps, with its principal function to store shells for 3.7" and 4.5" aircraft guns and rockets, with a maximum capacity of 960 tons of ammunition. The depot was fully stocked by mid-August 1939 and its first issues were made in September 1939, four days before the declaration of war (English Heritage 2004). The foreman in charge of the site from March 1939 to August 1955 was a civilian employee, Mr Trusler, who had previously been employed at the Bramley CAD. Mr Trusler was supported by local employees totalling eight civilian staff and four night watchmen, all of whom arrived on site in March 1939. After the declaration of war, the site was defended first by a detachment from the Honourable Artillery Company, and subsequently by detachments of the East Surrey Regiment and the Royal Pioneer Corps (English Heritage 2004).
- 5.5 The Honourable Artillery Company were accommodated in tents during their early occupation, but as operations increased these were replaced by long wooden huts with sanitary and cooking facilities, forming a more established site. The Pioneer Corps became responsible for both the guarding of the depot as well as the site's daily operation (BHRG 2006, 10). By the end of the war the site was occupied by the No. 28 Group and No. 80 (Indian) Company, then based in Wimbledon, who were both withdrawn in August 1945, after which no troops were engaged at the depot (BHRG 2006, 10). The depot officially closed as an active storage / distribution centre in 1953 (English Heritage 2004) but the wooden huts were utilised as temporary accommodation for returning servicemen until 1955 due to a shortage of housing.

Cartographic Evidence

- 5.6 The County Series map of Surrey of 1868 (not reproduced) depicts the site in an area of undeveloped land within an established field system. The surrounding field boundaries and Park Road, situated to the west of the site were in place by this date. The main area of development within the immediate vicinity of the site was located to the north-west and comprised Banstead Place, Mint Farm and Cottages, each consisting of a large complex of structures. The remaining surrounding land was relatively undeveloped, with Scratch Wood occupying land to the east of the site. The site and surrounding area are shown unaltered on the Ordnance Survey of 1898 (not reproduced).
- 5.7 The Ordnance Survey map of 1913 (Figure 3) shows the site much the same. To the west of the site two structures had been erected alongside Park Road, each set within individually assigned land parcels. Development within Banstead, located to the north-west had continued to expand to the north and west but no substantial changes are evident. The area a short distance to the south of the site had undergone extensive development with the construction of numerous residential and agricultural building complexes.
- 5.8 The Ordnance Survey map of 1935 (Figure 4) shows development down the east side of Park Road, predominantly of a residential nature, although towards the south is a farmstead described as 'Courtlands'. To the south, residential development had continued to expand northwards to the west of Park Road. The site remained undeveloped.
- 5.9 The Ordnance Survey map completed by 1938 (Figure 5) was produced at a smaller scale than the previous map, but is nevertheless useful in showing the construction of a trackway extending to the site from Park Road, this was to become the sole access track to the site upon its completion. The track passes through a previously occupied plot, indicating demolition of the former building complex was undertaken to accommodate it. The ammunition depot itself is not yet shown despite its construction date of August 1938. The absence of the site from this map is most likely due to the date at which the mapping was revised and the speed of the depot's construction.
- 5.10 Aerial photography of the site dating to 1944 (raf_106G_1a_30_rp_3054), 1945 (Google Earth) and 1947 (raf_cpe_uk_1982_rp_3142) (Figures 6 - 9), provides a record the site layout during its war-time operation, through to its post-war occupation. Each photograph is relatively consistent, with no visible changes during the period covered. The site comprises a clearly defined octagonal footprint, set on a north-west – south-east alignment and accessed from the west via the access track from Park Road. Structures to the south-west of the access track comprised the residential bungalow 'The Bungalow' occupied by the site's foreman, Mr Trusler, in addition to the camp occupied by the Honourable Artillery Company during the war. The camp comprised four long rectangular huts, aligned roughly south-west – north-east, which are recorded in documentary sources to have been constructed of timber (BHRG 2006; Figure 7). A further structure, set to the south and on a slightly different alignment to the living quarter huts, is likely to have provided sanitary facilities. Two larger rectangular structures, aligned north-west – south-east, to either side of the access track, housed cooking facilities / mess areas (south) and an administration / office building (north). The access track to the east of the store compound is gated, flanked on either side by a small structure, each of which comprised a metal sentry box. To the east of the gate was a guard's lodge, which is

documented to have been accompanied by a switch gear shed. The inner core of the site, upon which the ammunition stores are located is sub-rectangular in shape and separated from the outer site boundary by a corridor of undeveloped land, framed by earth embankments, designed to mitigate bomb and blast damage to the site and wider environs. Two rectangular structures are situated within these land corridors to the south-east and south-west. Their position outside of the principal storage depot, indicates they were the non-explosive stores and the Home Guard's gun and equipment hut, as recorded in documentary sources (BHRG 2006). To the east of the gated entrance the track is shown to split into three, providing access to three rows of ammunition stores (eight in total), each surrounded on all sides by earthworks. The earthwork mounds surrounding each store are open at their north-west and south-east ends, in the location of the entrances and associated loading bays. A series of five concrete/earth bunds are located between the end-faces of the stores.

- 5.11 The layout described above is depicted best on the Ordnance Survey map of 1958-59 (Figure 10). This map labels the site as 'depot' and shows the layout of the inner storage compound to have remained relatively consistent with that observed in 1944. The ancillary structures shown to the south-east and south-west site boundaries remained *in situ* at this date. Changes to the site are predominantly concentrated along the access track, with the bungalow now occupying an 'L'-shaped footprint, suggesting it had been enlarged by this date. The former camp is no longer shown, except for a single rectangular open-roofed structure titled 'ruin', indicating that the camp was no longer in use for temporary housing as it had been immediately post-war (BHRG 2006). The surrounding land to the south of the site had undergone further residential development.
- 5.12 The Ordnance Survey map of 1962 (not reproduced) is of an insufficient scale to enable detailed analysis of the site. The site appears to be depicted broadly unchanged from 1959, with the exception that the rectangular structure to the site's south-western side is not shown. By June 1966 the War Department put the site up for public auction, and in August its use for agricultural purposes was approved; the 38 acres of land were purchased by Mr. and Mrs. Sparrowhawk on 16th February 1967. The Ordnance Survey map of 1973 (Figure 11) shows the bungalow and site labelled as 'Courtlands Farm' and 'Piggery', indicating the site's change in use. This map shows both outlying ancillary structures along the south-east and south-west site boundaries to have been still *in situ*. By 1990 (map not reproduced), the stores remained unaltered, with the exception of the construction of two modern agricultural stores along the north-west boundary (Buildings 2 and 3). The rectangular structure to the south-east of the site and the stores' surrounding earthworks continued to be shown; both these features no longer remain today.
- 5.13 Modern day aerial photography of the site indicates a degree of modification to the site. Changes include the removal of the majority of the earthworks formerly surrounding the ammunition store structures, in addition to the removal of the single bund formerly separating the two central stores (Stores 8 and 9). The majority of the stores have lost their associated entrance infrastructure and loading bay arrangements with only fragmentary remains visible. The rectangular structure previously shown to the site's south-eastern edge is no longer *in situ*, replaced by an area of concrete hardstanding for car parking. A series of agricultural structures (Buildings 7, 10b-e, 13 and 15) have also been constructed around the site in the intervening years since 1990. Courtlands Farm is depicted immediately to the west of the site entrance, with Alvis House occupying the bungalow plot. In October 2003

a planning application was made for the land's redevelopment for a residential housing estate to include 76 houses, which was rejected.

6.0 DESCRIPTION OF THE BUILDINGS

Overview

- 6.1 The site occupies an octagonal, north-west – south-east aligned compound accessed by a concrete track from Park Road. The track enters the depot at the southern end of the north-western boundary and branches out into a loop road with three arms. The concrete surfaces between and around the stores were surfaced with macadam (BHRG 2006, 6), but this has been replaced with modern tarmac.
- 6.2 The outer enclosure of the depot was originally surrounded by a high steel-mesh fence, comprising L-section iron uprights surmounted by arched brackets fitted with barbed wire. Locked gates are recorded at intervals within the fencing, although no evidence of these survive (English Heritage 2004). Sections of the fencing survive along the north-eastern boundary of the site (Figure 33; Plates 1, 2 and 3). More recent painted steel-grate fence panels (Plate 4), now enclose the site. Two metal sentry boxes formerly located next to the entrance no longer remain. Just inside the main gate was a guard's lodge, which was later incorporated into a larger brick-built domestic residence; the switch gear shed beside the guard's lodge was also rebuilt in brick (English Heritage 2004). In addition to these structures there were a number of other buildings: wooden accommodation huts, mess rooms, offices, non-explosive stores and the Home Guard's gun and equipment hut, as seen on the war-time aerial photography, all of which no longer survive. The foreman's bungalow was incorporated into a larger brick dwelling after the war (Alvis House).
- 6.3 The depot consists of eight rectangular 'ADGB Bin Type Storehouses', known as magazines (identified as Structures 4-6, 8, 9, 10a, 12 and 14, on Figure 2). Each store is set approximately 30m apart and separated at the end from the next by a high bund measuring c.23.5m (length) by c.8.5m (width). These anti-blast traverses consist of low walls of concrete, measuring 1.1m in height, topped by earthen mounds. The bund separating Stores 8 and 9 has been removed, however aerial photography dating between 1945 and 1947, identifies its form and size as consistent with those remaining. Each store is set within an outer blast barrier wall which would have originally been encased within an earthen bank on each side; the majority of the earthen banks have been removed to provide additional space for when the site was in use as a car storage facility. Stores 4, 5 and 6 each retain their earthen banks along their north-east elevations. Stores 8, 9 and 10a have lost all associated banks.
- 6.4 Outside the main site area are two metal sentry air-raid posts and a brick air-raid shelter. The sentry posts are located along the north-east and south-east boundaries and the air-raid shelter is located centrally along the south-western boundary. The north-eastern sentry post is located on the outskirts of small wooded copse within the surrounding fields, whereas the south-eastern post is situated at the base of an earthen bank which now bounds the central depot area along this side.
- 6.5 A number of modern stores (Structures 11, 13 and 15; Figure 2) have been constructed around the site, together with a concentration of larger agricultural sheds built within the site's north-western end (Structures 2, 3, 7 and 10b-10e).

- 6.6 Each store was built to a standardised design, and as such, each follows an identical arrangement. Since their construction, a number have been subject to varying degrees of alteration in order to accommodate temporary new uses. Due to the repetitive nature of their construction type, a generic store description has been produced, with only variations noted for each adapted structure. All accompanying plates within the principal store description are taken from Store 4, but are representative of the other structures.

Ammunition Store – Generic Description (Figures 14, 27 and 28)

- 6.7 Each store comprises a rectangular structure (Plate 5), aligned north-west – south-east and is constructed from shuttered concrete (Plate 6), reinforced with round mild-steel bars. The inner structure, measures 20.6m by 10m, has a shallow pitched roof and a pair of entrances at either end of its north-west and south-east elevations. It is encased on all sides by an external blast barrier wall (Plate 7), also constructed in shuttered concrete, measuring a thickness of c.0.3m (Plate 8). The south-west and north-east blast wall elevations are supported on their external face by five equally-spaced buttresses (Plate 9). The north-west and south-east elevations bear a single buttress, placed centrally (Plates 10 and 11). The external face of each elevation is additionally retained within an earth blast barrier embankment. The purpose of both the blast barrier walls and embankments was to provide shelter in the event of site bombing and to encourage any internal explosions upwards to avoid further damage to adjacent stores. The earth embankments would additionally have aided the camouflaging of the site from an aerial perspective. The blast barrier wall is pierced at its four corners to provide access to the inner store structure. Each entrance (Plate 12) comprises a flat-roofed passage with ramped side elevations. Each passage measures a width of 1.41m and is guarded at its centre by a gate. An integral locking bar (Plate 13), inset adjacent to the gate, indicates the gates are an original feature. Each entrance is served by a bull-headed ‘Revo’ light fitting (Plate 14). The south-west entrance is painted with an emergency fire instruction: ‘In case of fire, fire buckets can be filled from hydrant’.
- 6.8 Located adjacent to each blast wall entrance are remnants of the structure’s former loading bays (Plates 15 and 16). These comprise a series of low level concrete supports, measuring 0.69m (length) by c.0.97m (height) by 0.16m (width). These supports are placed at equal intervals between the gated entrance and an end platform. The end platforms (Plate 17) are rectangular in form, set level with the supports and measure 3.37m (length) by 1.7m (width). Each entrance has an adjacent loading platform; those to the north-east side are set at a distance of 6.8m from the external blast barrier wall, whereas those serving the south-west side of the store, adjoin the projecting entrance. Fixed between each support and platform, would have been metal rollers and a belt for a conveyor system, used to transport the ammunition in and out of the inner store structure. Although these conveyor systems have been removed, elements of their metal ‘I’ and ‘U’-section, flat plate fastenings are retained on both the platforms and on a number of supports. Examples of steel girders, some comprising a shallow ‘U’-shape with gritted belt liner, are situated throughout the site and are likely to have formed part of this system. Each loading platform retains elements of an electric lamp, used to light the external loading areas. Each retained lamp comprises a slender post, goose-neck support with single domed light, topped by a scrolled finial and floral detailing. When operational, ammunition would have been unloaded/loaded near the site entrance, before being carried or wheeled in containers driven by a diesel engine, to the individual storehouses. Upon reaching a store, the ammunition crates were unloaded onto the platforms and conveyed into, and along, the passage within the

store by a chain of manually operated rollers. Here they would be unloaded into the storage 'bins' (BHRG 2006, 7).

- 6.9 The inner store structure and blast barrier walls are separated by an unroofed walkway (Plates 18 and 19), measuring c.1m in width, which continues unbroken around the perimeter. The walkway is served by a concrete footpath with inset drainage channels. Adjacent to each blast barrier entrance, the footpath is ramped with an internal drainage channel.
- 6.10 The store walls, measuring 0.24m thick, are constructed in solid shuttered concrete, and are pierced by ventilation shafts and grilles in order to discourage internal high moisture conditions. The north-east and south-west elevations presently contain two window openings at either end (Plate 20). Each contains a paired, six-light, steel-framed window; the top three panes of which comprise a single bottom-hung opening section. The windows themselves are set within a run of six blocked openings (Plates 21 and 22), which continue the length of both elevations. Each aperture measures 3m in length and c.1m in height, and is blocked with aerated red-brick dust coloured concrete blocks. The exact nature of these blocked sections is unknown, however they appear to form original window openings set 2m above the internal floor height. Presently the store interiors are predominantly served by electric lighting, with the current windows providing only limited natural light. At the start of the war, it was presumed that the majority of German air assaults would be carried out by day. However, as the war developed, advancements in aviation and navigation technology allowed for night-time bombing raids, so that by the autumn of 1940, it was recognised that the majority of German air strikes were being undertaken at night (Historic England 2017). This circumstantial information would support the suggestion of a war-time transition from the use of light wells, to electric lighting, within the ammunition stores on the site. This explanation is also consistent with data collected about the site's ammunition replenishment, which although was initially assumed would be a day-time operation, in practice seems to have generally been undertaken at night (WO 166/5331 & WO 166/5338). This transition from day-time to night-time bombing would have rendered such substantial light wells redundant, perhaps explaining their blocking. Drainage on these elevations comprise a combination of cast iron downpipes which run through the overhanging eaves of the felted shallow pitched roof (Plate 23), and modern uPVC replacements.
- 6.11 A single wall-mounted electric lamp (Plate 24) is situated centrally on both the north-west and south-east elevations, the electrics of which are carried through the concrete wall of the inner store. Each lamp is elevated c.1m above the height of the roof pitch, presumably to provide additional lighting to the loading areas. A number of pipes extend between the inner store and blast wall, presumably providing cover for the electrics serving the loading platform lamps.
- 6.12 A steel double-width door, measuring 1.26m, is located at all four corners of the inner store structure (north-west and south-east elevations). The rear doors accommodate a low-level wall, comprising a base for the former conveyor trackway (Plates 25-27; Section 6.7). The door comprises two-leaves, one full and one half-height, strengthened with riveted crossed-braced straps and substantial hinges. Two padlock hooks flank each entrance (Plate 28), in order to fix the doors in place when open or closed. A number of doors are marked with layers of stencilled signage reading 'VENT CLASS A' (Plate 29). It was not possible to discern with certainty what subject matter the classification relates to, however, the consultation of war diaries from Equipment Ammunition Depot No. 115 (WO 166/5319), suggests these sites handled a variety of ammunition types, that would need individual identification

and storage. It is recorded that the Banstead site could hold a maximum capacity of 960 tons of ammunition, comprising shells for 3.7" and 4.5" aircraft guns and rockets (BHRG 2006), suggesting that 'Class A' may relate to the type of ammunition being stored in that particular store and/or the storage conditions required for each type. The external door faces are each painted green. Each entrance is set within a protruding concrete door-case, with overhanging concrete lintel.

- 6.13 Internally each store is split longitudinally into two rows of nine walled bays, or 'bins' (Plate 30) separated by a central wall consisting of two skins with soil infill, overall measuring 0.9m thick. All walls are treated with a pale white wash. Each of the nine bins are separated by 0.3m thick walls that stop short of the ceiling by c.0.8m (Plates 31 and 32). The open frame at roof level would have aided in the distribution of pressure if an internal blast were to have occurred. The roof is constructed in shuttered concrete and measures a depth of 0.1m. This would be substantial enough to withstand bombing, but also thinner than the structure's containment walls, to direct any internal blasts upwards – a typical method of blast containment. The roof is punctured by three air vents, situated above the central soil-filled void. A series of reinforced concrete cross-beams (Plate 33), set in-line with the bin walls, support the roof underside.
- 6.14 Each bin measures a length of 2.60m and a width of 1.97m and is individually identified by stencilled white numbers set against a black panel (Plate 34). These numbers are situated adjacent to each bin and on each bin containment wall. An earlier phase of stencilling can be seen below each and on the end of each bin containment wall. The bins are numbered 1 – 9 within the north-eastern half and 10 – 19 in the south-western half. All numbering starts from the north-west end of each store and terminates to the south-east, indicating a one-way system operated throughout each store.
- 6.15 Each bin has either four redundant timber-framed floor sockets (Plates 35 and 36), or no sockets but a black painted line dividing the bin from the adjacent passage. It appears that the floor sockets, which extend to a depth of c.0.3m, originally formed mortices for a crate shelving unit or further wall divisions for ammunition storage. The differences between the type of storage systems, are probably reflective of the different types of ammunition being stored on the site. The end of each bin partition wall has a low level, shallow protecting plinth (Plate 37), which presumably either aided in supporting the bin racking, or formed part of the conveyor system, perhaps supporting drop-down hatches.
- 6.16 Each bin is lit by a single bulb (Plate 38) with simple steel back plate and is connected to a mains fuse box, mounted within Bins 1 and 10. The fuse boxes comprise a combination of automatic and 50p coin operated types (Plate 39), both issued by the 'Electric Meter Servicing Co. Ltd. Leatherhead, Surrey'.
- 6.17 The bins are accessed via a passage, which runs the length of each row. Fragmentary evidence survives of eight former concrete supports (Plate 40) that formerly served the internal conveyor system – the supports are as those described externally. A number of passages contained a wall-mounted, timber information board at their north-west ends (south-west halves only). Although of a degraded nature, a number are clearly marked with 'FIRE ORDERS' and 'REGULATIONS' and contain printed information about types of ammunition and fire precaution and alarm procedures; all set against a painted red backdrop. Wall hooks consistent with this location, are found within each store, indicating notice boards were formerly situated in all stores.

Individual Ammunition Store Descriptions

Store 4 (Figure 14)

- 6.18 Store 4 (Plate 41) is proposed for retention within the new development. The structure is as described above, with no notable internal alterations. The outer blast wall retains its earth blast barriers to the north-east and south-east but those formerly situated to the north-west and south-west, as seen on aerial photography, have been removed. The western loading platform serving the store's north-west elevation has been removed, with only fragmentary footings providing evidence of its former location. The loading platforms at the southern end retain elements of their original lamps, comprising a single post (south-west; Plate 42) and displaced light (south-east; Plate 43). The north-east (Plate 44) and south-west platforms retain flat-bar steel fixings (Plate 45) for the conveyor system. The structure has lost the majority of its conveyor system bases, although four out of the six which remain, situated externally along the eastern side, survive as the most complete example of the former conveyor system on the entire site; with all four bases and end platform surviving, the majority of which preserve original metal brackets (Plate 46).
- 6.19 The north-west entrance retains its 'Vent Class A' stencilling, in addition to a small painted 'A' on the western door jamb (Plate 47). The nature of this additional signage is uncertain, although it may reflect an alphabetical signage system to indicate the direction of travel through the western half of the structure from north-west to south-east. The south-eastern entrance has been latterly framed to the east and west by low-level concrete block walls (Plate 48), which block the inner walkway in this location – the reason for these is not clear but they were presumably added as a much later alteration due to the nature of the blocks. None of the entrances retain any fire hydrant signage as described previously. The structure's north-east and south-west inner walls contain a continuous sill along their entire lengths (Plate 49), presumably serving the blocked openings in these locations, which are consistent with this height. Curiously the openings within this store, are set significantly lower than those elsewhere, at a height of c.1m above the inner ground level. The existing window (Plate 50) lighting the northern end of the eastern half of the structure, has lost its upper opening casements, these have however been stored internally within Bin 1 (Plate 51).
- 6.20 Internally, each bin contains four redundant floor sockets and each retains its numerical signage (Plate 52). Many of the bin numbers show signs of successive phases of repainting and the signage for Bin 9 (Plate 53) has been latterly covered by white paintwork, suggesting the structure was repainted in-part internally during its subsequent use. The coin-operated fuse box located within Bin 1 partially covers the signage of this storage bay, indicating that it was mounted to the wall at a later date – supporting the theory that the transition from day to night working conditions, necessitated additional electric lighting (see Section 6.9). Other notable features include the absence of the timber signage board within the northern end of the western half of the structure (as seen elsewhere). Although no fabric remains of the board itself, its former location can be marked by a series of metal brackets in this location.

Store 5 (Figure 15)

- 6.21 The north-east and north-west sides of Store 5 remain encased within their earth blast barriers, while those to the south-west and south-east have been removed (Plates 54 and 55); the bund separating Stores 4 and 5 remains in place (Plate 56).

The three roof vents are covered with sloped steel hoods (Plate 57), presumably to limit internal moisture levels. The structure retains all of its loading platforms except that to the south-west, and the lamp fitting serving the north-east loading platform also endures (Plate 58). The conveyor system bases have all been lost, with the exception of a single example, situated in the north-west doorway (Plate 59), which is complete with metal brackets. None of the entrances retain any signage, although the north-west door is labelled with a painted 'R' (Plate 60) and the south-east door with an 'H' (Plate 61). Both letters appear to have been painted free-hand rather than stencilled like all other original lettering found elsewhere and presumably therefore relate to one of the post-war uses of the structure. In line with the generic description, this store's blocked openings to the north-east and south-west, are set c.2m above the internal floor level (Plate 62) and are not accompanied by a cill (Plate 63).

- 6.22 Internally (Plates 64 and 65), both halves have been subject to limited later alterations, principally comprising the addition of later timber stud framework to the roofs of a number of the bays. Examples of this modern framing are best observed in Bins 16 and 10 (western half; Plate 66). Two curved track brackets (Plate 67) frame Bin 16, their nature is unknown, however their singularity, suggests they form part of the later modifications. The bay is additionally flanked by steel rods, which pass between the containment walls and passage wall, again these appear to be later features. A number of bays within the western half of the structure have had their electric lighting replaced with later hanging bulbs and modern strip lighting. The containment walls of Bin 4 are soot-stained from a former wall-mounted timber frame (Plate 68) which has been destroyed by fire. Each bin contains four redundant floor sockets as described above. The end face of each bay containment wall is marked with a numerical stencil (Plate 69), which is smaller than those marking the inside of each, and are presumably of earlier origin, due to the fact that many are on paint layers beneath the larger signage. At odds to the white paintwork found elsewhere, the southern end of the eastern half of the structure has a turquoise painted finish (Plate 70). Situated within the northern end of the western passage are the fragmentary remains of a timber notice board (Plate 71), the brackets of which remain *in situ* within the wall (Plate 72).

Store 6 (Figure 16)

- 6.23 Store 6 (Plate 73) retains its earth blast barriers along its north-east and north-west elevations only (Plate 74). The loading platforms formerly serving the north-western end of the store have been removed (Plates 75 and 76), as with the majority of the conveyor system bases (Plate 77). The remaining south-eastern loading platform (Plate 78) retains its post for the former lighting. The bases that do remain number five in total and are situated within the north-western loading bays. Both bases, complete with metal fixings, remain within the north-western doors (Plate 79) and covered entrances, providing a good example of how each entrance was originally constructed.
- 6.24 None of the covered entrances retain any stencilled signage, although the doors providing access to the western half of the structure are marked with 'A' (north-west) and 'K' (south-east), in the same manner as Store 4. Two blocked windows are clearly visible within the north-east elevation of the inner wall (Plates 80 and 81). Both measure 0.97m (length) by 0.86m (height) and previously had lintels measuring 0.24m in height. These represent later 20th century additions. The downpipes along the same elevation are additionally served by channelled concrete blocks at ground level, presumably used to direct rainwater away from the

foundations on this side. The drainage gullies situated around the inner walkway perimeter (as above) are served by cast iron drainage pipes (Plate 82), which are visible to the south-eastern end due to degraded hardstanding.

- 6.25 Internally (Plates 83 and 84), the structure differs little from the generic description, with only minor later alterations, including the loss of the conveyor bases and the original lighting in each bay. Each bay (Plate 85) contains four redundant floor sockets and retains a mixture of both large (Plate 86) and small (Plate 87) numerical stencil signage. The most notable feature within this store is the retention of its red timber notice board within the north-western end of its western half (adjacent to Bin 10). The board has remained mounted to the wall and is marked 'FIRE ORDERS' (Plates 88 and 89), although fragmentary paper signage appears to have been mounted to the board historically; no words were discernible.

Store 8 (Figure 18)

- 6.26 Each of the former earth blast barriers (Plate 90) surrounding Store 8 (Plate 91) has been removed, as have the loading platforms serving the western half of the structure (Plates 92 and 93). Five of the conveyor system bases remain within the southern loading bay but all others have been removed. All of the surviving bases (Plates 94 and 95) and loading platforms (Plate 96) retain metal brackets used to secure the conveyor belt. Only partial fabric of a lamp post remains on the southern loading platform. The south-eastern doorway serving the eastern half of the structure is marked with 'Vent Class A' stencilling, as per the generic description. The covered entrance on the same side but serving the western half, contains stencilled signage comprising white stencilled letters, set against a black background (Plate 97). Although slightly degraded, the sign located on the west wall to the south of the gate, reads, 'IN CASE OF A FIRE BUCKETS TO BE FILLED FROM HYDRANT'. To the west of the structure is a displaced steel girder (Plate 98); its nature is not known for certain, but it is plausible that it formed part of the now-removed conveyor system.

- 6.27 Both halves of the interior have been negatively affected by fire damage (Plates 99-102). Damage caused to the inner wall has clearly revealed the brick-dust coloured concrete blocks (Plate 103) used to fill the high openings within each passage. The bays within this structure contain no floor sockets. Instead, each bay is visually separated from the adjacent passage by a painted black line (Plate 104). This may perhaps signify that this store was used to store a different type of ammunition, as suggested previously. The timber notice board within this structure has suffered significantly from fire damage (Plate 105), with only half remaining and displaced from its original position (adjacent Bin 10). The board titled 'FIRE O[R]DERS' was located within Bin 10 and retains no discernible writing. The northern end of the eastern passage wall contained additional signage (Plate 106), to what would have been the northern side of the original notice board location. The signage, set against a black backdrop, contained white and green stencilled lettering, however its degraded nature made it illegible. The remainder of the interior was unchanged from the generic description (Plate 107).

Store 9 (Figure 19)

- 6.28 All earth blast barriers have been removed around Store 9 (Plates 108 and 109). As with Store 5, the roof vents over this structure are served by steel-hoods (Plate 110). The loading platforms (Plates 111 and 112) formerly serving the western half of the store have been removed. The south-eastern loading platform retains a very good

example of one of the lamp posts which formerly lit the loading bay operations (Plates 113 and 114), in addition to the metal fastening mentioned previously. On the same platform is a slender timber ladder, covered with a red paint covering. It is not known for certain if this ladder is an original fixture, although the shade of red matches that of the surviving internal notice boards, suggesting it forms part of the site's general war-time equipment. Adjacent to the platform is a steel 'U'-shaped girder (Plate 115) with two redundant circular sockets at each end. It is likely that this girder formed part of the now dismantled conveyor system, however its exact nature is uncertain. The northern loading platform contains what appears to form part of the former conveyor system (Plate 116). It comprises a shallow 'U'-section steel girder, lined with gritted felt, measuring c.3m (length) by c.0.7m (width) and containing two small fixing holes at its southern end and along each lip, presumably used to fasten the belt sections together. Each of the conveyor system bases remain at the northern end, including those within the north-eastern entrances and doorways (Plate 117). All others elsewhere have been removed. The western covered entrance (Plate 118) in the south-eastern elevation contains a good example of the fire hydrant signage (Plate 119) described within the principal description. No other signage was noted on any other entrance or inner wall (Plate 120).

- 6.29 The store interior (Plates 121-124) is largely unchanged and provides particularly clear examples of the larger numerical bay signage (Plate 125), the markings being especially vivid against white-washed walls within the eastern half of the structure. The bays within this structure contain no floor sockets (Plate 126), but instead have painted black lines (Plate 127), visually separating the bins and internal passage. The most notable feature within this store is the retained timber notice board (Plate 128) adjacent to Bin 10. The board now rests on the floor, having separated from its wall mounting which remains *in situ*. Despite fire damage caused to its southern end, the board forms an valuable insight into the information given on these boards, with fragmentary evidence in the form of historic paper notices visible behind the red paint covering. The board, headed 'REGULATIONS' (Plate 129) contains information on the general storage and use of oil (lamps?) and stoves (Plate 130), in addition to fire precaution matters and actions to be taken in the event of a fire – including hailing 'FIRE! FIRE! FIRE' and sounding the fire alarm (Plate 131). Tantalising fragments of instructions are provided for the storage procedures for the different types of ammunition (Plate 132), including the use of an ordered tiered storage system and the use of wooden wedges and battens to protect ammunition on the bottom tier and to prevent 'projectiles ... from being slewed over the [?place / floor]'. Further information is given for the correct temperatures different types of ammunition should be stored at and for what time duration, stating that 'a) Cordite, Mk., I, R.D.B.' and 'b) Cordite, M.D., M.C. ... R.D.N. ... to remain for more than a month'. It also provided fire information regarding 'Ammunition containing Phosphorus', which states that 'if they ignite ... It (the phosphorus) is easily extinguished ...'.

Store 10 (Figure 20)

- 6.30 Store 10 (Plate 133) has been encased on all but its south-eastern side by later structures (Buildings 10b – 10e; Plate 134); as a result, all earth blast barriers have been removed, except for the bund separating it from Store 12 (Plate 135). The construction of Building 10c / 10d has resulted in significant modification to the store's northern loading bay, including the removal of the western loading platform, the partial demolition of its western covered entrance and the removal of all of the external conveyor system bases. The southern loading bay has similarly been

subject to alteration, with the removal of the western platform, along with all but one conveyor system base. All bases have been demolished internally, except for one within the northern doorway serving the eastern half of the structure. No evidence of former light fixtures or metal brackets were noted on the surviving platforms or bases. Nor was any evidence found of former signage on any of the covered entrances. The northern door serving the eastern half of the structure did retain its 'Vent Class A' signage (Plate 136), as described above.

- 6.31 Internally, the structure differs little from the generic description (Plates 137 and 138). Alterations include the removal of the conveyor system bases, as elsewhere, and the removal of its timber notice board. Bay division signage within the western half has suffered from damp conditions and is such only partially legible; signage within the eastern half is comparatively clear as it is set against a bright white-washed wall (Plate 139). None of the bays contain floor sockets (Plate 140), although the black lines dividing the bins from the passages are no longer visible. A number of the bays have retained their original light fixtures (Plate 141). No other notable features were observed internally.

Store 12 (Figure 22)

- 6.32 Store 12 (Plates 142 and 143) retains just one of its original earth blast barriers, situated along its north-west elevation (Plate 144). The loading platforms serving the western half of the structure have also been removed. Those serving the eastern half remain and the northern platform retains its original lamp post minus its lamp (Plate 145). To the north-east of the platform is an additional post which forms part of a wider system of timber electricity posts which carry cables between each store. All but one conveyor system base, situated to the eastern side of the northern loading bay, have been demolished.
- 6.33 The southern covered entrance serving the western half of the store contains a good example of a 'Revo' light fitting (Plate 146) and fire hydrant signage (Plate 147), as described within the principal description. Situated at this entrance is a displaced steel girder, as elsewhere, which likely comprises part of the dismantled conveyor system. The western doorway accessed from the southern loading bay, contains 'Vent Class A' stencilling on its external face, as does the western doorway accessible from the northern loading bay (Plate 148). The northern doorway signage is particularly clear and shows signs of different phases of markings as elsewhere (Plate 149). All doors are fitted with rotating steel hook fastenings (Plate 150), which appear to have been added at a later date, given the absence of similar fixings on any of the other stores.
- 6.34 With the exception of the southern end of the western passage, the interior was not inspected as it was deemed unsafe. The window set within the southern end of the inner wall's north-west elevation, has been removed (Plate 151). This area has been subject to a later phase of opening-up, which has subsequently been filled by a two-skin concrete block arrangement, the jambs of which are formed of solid shuttered concrete. From the door openings it was observed that the interior (Plate 152) retains little in the way of original fixtures and fittings, except for limited evidence of numerical signage identifying a number of the retained bays. It appears the structure was been converted to agricultural use (livestock pens) during the latter half of the 20th century, evidence of which can be seen throughout the building. Alterations include the construction of half-height concrete block walls to each bay (absent of floor sockets), separating them from the passage, in addition to the insertion of modern timber framed ceilings above the open-roof arrangement within the bays

and passage (Plate 153), presumably added for insulation purposes. Modern electric strip lighting has been added to the passage, to compensate for the blocking of the original windows.

Store 14 (Figure 23)

- 6.35 Store 14 (Plate 154) is contained on its north-western side by one remaining earth blast barrier (Plate 155). The loading platforms serving the eastern half of the structure remain *in situ*, whereas those to the west have been demolished, with only fragmentary footings remaining. The southern loading platform (Plate 156) retains elements of its lighting, although the lamp itself no longer survives. The western loading platform to the north of the structure appears to have been demolished to accommodate the construction of Building 13, which post-dates the 1990 mapping. The northern loading bay has undergone various alteration since construction, principally due to the structure's use as a piggery during the latter half of the 20th century by Albert Sparrowhawk (BHRG, 2006). Alterations include the partial closing of the eastern entrance with a low level concrete block wall, and the construction of a concrete wall adjacent to the western entrance. It is likely this wall formed part of an enclosure between the store and adjacent earth blast barrier (Plate 157) (now Building 13). The blocking of the eastern entrance is likely to have been part of a managed circulation system, ensuring livestock could not access this area to the east of the earth blast barrier. To the immediate north-east of the northern loading bay (Plate 158), are the former containment walls for the removed earth blast barrier which originally separated Stores 8 and 9. All but three of the conveyor system bases have been removed.
- 6.36 The southern entrance serving the eastern half of the structure is marked with the 'Vent Class A' signage (Plate 159), as previously described. The southern doorway serving the western side of the store, is marked with a hand-painted 'B'. Fire hydrant signage has been retained on their western entrance serving the southern loading bay (Plate 160).
- 6.37 The interior of the store (Plates 161 and 162) has been modified to accommodate its use as a piggery from the 1960s onwards. Each bay (Plate 163) has been fitted with a tubular steel grate feeding rack in its far northern corners and a trough to the southern side, adjacent to the passage. Each trough (Plate 164) comprises a cast concrete surround, filled with a glazed terracotta half-section pipe. Securing each bay / pen were steel bar gates, one of which remains within the southern end of the western passage. The gates were secured to a series of L-brackets (Plate 165), which have been secured to the end of each bay containment wall. A number of bays / pens retain ephemeral timber framework and plastic sheeting below the roof openings, which appears to have formed a rudimentary ceiling, presumably added for insulation purposes. Bay signage within each half is significantly degraded and little of the original features survive. An exception to this is the retention of a number of original back-plate light fittings (Plate 166) within the eastern bays. Mid-20th century electric lighting has been strung along the eastern passage wall, lighting each bay following the insertion of the modern ceilings.
- 6.38 A notable feature was observed to the south-east of Store 14, comprising a steel U-shaped girder with rolled edge detailing (Plate 167), set within a cast concrete surround at ground level. The feature, aligned south-west – north-east, extends between Stores 14 and 9 and separates the structures from the area of concrete hardstanding within the south-eastern site boundary. The nature of the feature is unclear, perhaps comprising part of a displaced conveyor system or forming part of

a drainage channel. Its presence cannot be located for certain on the historic photographs dating to 1944, however its position does align to a clearly defined boundary visible by this date. It is possible it was associated with the former Home Guard's equipment store which was previously located to the immediate south.

Perimeter Structures

Sentry Air-Raid Posts (Figures 29 & 30)

Overview

- 6.39 The sentry air-raid posts located around the site perimeter (Figure 2) form part of the original wartime site design. Due to their size (and possibly former camouflage) they are undetectable on historical aerial photography. The structures form a variant of a more standard type of 'Consul' shelter, which appear to have been set to a standardised conical design, and were used for civil defence personnel (i.e. police, air raid precaution and fire-watchers) to shelter in during air raids. Examples of this more common conical design can be found on display at the Amberley Working Museum, Amberley, West Sussex and at Eden Camp, Malton, North Yorkshire (Figure 29). Such shelters were advertised as portable and held in place with footings with large holding-down bolts. The shelters at Courtlands, however, require a concrete footing to be formed into which the steel shell is inserted, and therefore are not readily relocated. Another variation to the standard design, is that they do not taper into a cone shape, which allowed for a larger internal area. These variations in their design may be a result of the relatively early construction date of the Banstead Depot, in 1938 – perhaps these shelters are an early version of the design, which was later modified, or perhaps they were just produced by different manufacturers (note the tepee style shelter found on Walney Island, Burrow-in-Furness, constructed by a firm called Vickers-Armstrong; Figure 29).
- 6.40 At Courtlands, these structures are likely to have provided shelter in the event of an air-raid for the sentries that would have patrolled the metal perimeter fence. Both of the sentry posts are of an identical design and as such a single description has been given below. The main difference between the two structures is the survival of the door (now detached) of the north-eastern structure (Plate 168), whereas that belonging to the south-eastern structure (Plate 169) has been removed elsewhere.

Exterior

- 6.41 Each sentry post comprises a circular-plan steel structure, with shallow domed roof and single round-arched doorway. The walls are constructed from steel sheets, each measuring a thickness of 15mm, that are welded at their joins (Plate 170). The roof comprises a single domed unit, which is similarly welded in place to the top of the wall sheets. Where the roof and walls join, there are four integral air vent slits (Plate 171). Each measures 170mm in length and 10mm in height, and has a small chamfer to its outer edges. The roof apex (Plate 172) is set at a height of 1.98m and is externally topped by a steel loop. This is thought to be a 'lifting eye' for transporting and positioning the structure, but could have served the additional function as a fixing for camouflage netting to the exterior, disguising it from the air. The base of the walls rest within a circular steel hoop (Plate 173) which measures a diameter of 1.38m, rises 200mm in height and is a thickness of 12mm. This frame is mounted on a concrete base, that is ramped up on its external face by a mortar fillet (Plate 174), extending 150mm from the structure. Each doorway faces strategically away from the depot, presumably to avoid the force of any explosion from within the site.

The retained door measures a height of 1.5m and is relatively narrow at 0.5m wide. The door (Plate 175) is constructed of steel which conforms to the curve of the structure and is of the same thickness as the sheets used for the walls. The door contains a single manual handle, also constructed of steel, on both the interior and exterior faces. It was originally hung with two substantial aluminium hinges (Plate 176), each fixed with two large bolts.

Interior

- 6.42 The interior (Plates 177 and 178) of each sentry post measures a diameter of 1.35m, with a maximum height of 1.95m. Originally, the structure would have accommodated two opposing benches, suitable for two people in total. These have since been removed, however, the steel fittings that would have supported bench brackets remain *in situ* (Plate 179). These comprise four flat-plate steel fittings to each former bench, each containing a single hole, presumably for bolt fixings and each is welded to the interior wall face. Examples of these benches are typically found in sentry air-raid posts of a conical shape, for example those on display at Amberley Working Museum and Eden Camp, North Yorkshire (Figure 29). In addition to the benches are five steel hooks (Plate 180); two above each bench and one set centrally opposite the doorway. Each is located to the top of each wall plate and is welded in place. These are likely to be for equipment/coat storage. The only other notable internal feature is a welded steel flat-plate fillet (Plate 181), which lines the internal face of the semi-circular door arch. This would have prevented the door from swinging into the shelter during the resultant force of an air-raid, either due to direct blast impact or air turbulence.
- 6.43 Miscellaneous objects found within the north-eastern structure included a small amount of heavily corroded steel wire (Plate 182), the nature of which would not be ascertained. The south-eastern structure contained four fragmentary pieces of modern cinder block, one fragment of glazed ceramic (unmarked) pot, the remains of a 20th century brick, five partial painted timber shards and three aluminium sheets (Plate 183). The aluminium sheets (Plate 184) are common to those associated with water tanks and as with all other items, are thought to be unrelated to the sentry box. Two curved steel brackets (Plate 185) were also found within the south-eastern structure, and are identical to those forming the original fencing along the north-eastern site boundary. These are therefore thought to be displaced fence brackets from the south-eastern site boundary, which no longer remains *in situ*.

Brick Air-Raid Shelter (Figure 31)

Overview

- 6.44 The air-raid shelter (Plate 186) is located centrally along the south-western site boundary (Figure 2), within the confines of the modern perimeter fence. The structure is clearly visible on aerial photography dating to 1944 (Figures 6 and 7) and forms part of the original site design. Its location and size would indicate it was intended as a shelter for those situated within lodgings along the south-western side of the site, including those within the wooden huts and the foreman's bungalow.

Exterior

- 6.45 The structure is rectangular, measures 5.05m (length) by 3.75m (width) and is set on a north-east – south-west alignment. The external walls are constructed of English-bonded orange-brown brick, set within a hard cement mortar. The roof

(Plate 187) has a domed pitch and is constructed of a substantial concrete slab. This contains coarse pebbles and rocks of various sizes set within the cement matrix. Each elevation is plain, except for a number of air bricks (Plates 188 and 189) and a ventilation grille set within the north-east and south-west sides. Those within the north-east elevation are a mix of ceramic and cast iron types.

- 6.46 The entrance (Plate 190) is situated at the north-east end of the structure and is arranged as a typical 'L'-plan baffle to protect against bomb blasts. It projects centrally from the elevation (Plate 191) and is of the same brick construction, the walls measuring 340mm thick. The roof of the entrance is a flat shuttered concrete slab, measuring a thickness of 150mm and set at a height of 2.10m. It is usual for air-raid shelters to have a secondary means of escape, to be used in the event of a blockage of the primary entrance. This can present itself as an area of intentional loosely mortared brickwork, which can be struck internally, for use in an emergency. This structure did not display any evidence of this safety feature.

Interior

- 6.47 The interior of the entranceway (Plate 192) comprises bare brick walls and a shuttered concrete ceiling, rising to a height of 1.95m. The internal doorway (Plate 193), set slightly off-centre within the north-east elevation, contains no door (or evidence of), measures a width of 0.83m and has a concrete lintel. The shelter interior (Plate 194) is domed, with a maximum height of 2.12m. The south-west and north-east walls are constructed of bare brick, with occasional air bricks (see above) (Plate 195) and are coloured with a yellow hue, either displaying a historic paint cover or occurring through discolouration from water ingress. The north-west and south-east walls (Plate 196) are constructed using an outer brick shell (34mm thick), overlain with cement infill with coarse aggregate inclusions (110mm thick). An inner layer of curved corrugated metal sheets (10mm thick) line both walls in addition to the roof, producing the characteristic domed interior. Originally, the metal sheet layer would have covered the entirety of both side walls and roof area, however, due to heavy corrosion, the base of each sheet no longer remains intact. The floor (Plate 197) is of concrete and measures 4.37m in length and 2.87m in width. No evidence of fixtures or fittings were noted.

Late 20th Century Buildings

Building 2 (Figure 12)

- 6.48 Building 2 (Plate 198) comprises a modern agricultural shed, constructed on concrete foundations inscribed with a date of 1979 (Plate 199), laid by Albert Sparrowhawk and shown on the 1990 mapping. The structure, situated at the western end of the compound's northern boundary, occupies a rectangular footprint. The building is aligned essentially east – west, with its principal elevation facing south towards the principal driveway entering the site. It is constructed of a breezeblock plinth, clad with panels of corrugated asbestos at its upper level. The roof has a shallow pitch and is covered with the same asbestos-cement sheeting. A rudimentary narrow lean-to, constructed of timber boarding, adjoins the southern end of the west elevation. This structure appears to have originally had a central doorway, which has since been blocked. A single doorway is set at the southern end of the west elevation (now boarded), and is served by a steel bar gate, indicating its use for cattle. The principal entrance is set centrally within the south elevation and comprises a substantial, full-height opening, fixed with a corrugated steel sheet door, set within sliding runners made of steel girders. The interior (Plate 200)

comprises an open-plan, three-bay, cattle shed, the eastern bay of which is split into two storeys with a mezzanine level used for storage. The lean-to comprises a hay store and has no interior access. The floor comprises a continuous concrete hardstanding. A steel grate drainage channel is located adjacent to the western doorway. The interior is lit with modern strip lighting and a number of PVC panels within the roof cladding. The external cladding is supported internally by a simple steel frame, comprising slender I-section girders. The roof is formed by three simple steel trusses and a number of tension rods added for additional support. The mezzanine is constructed using two U-section steel uprights and I-section girders, clad with softwood timber cross-beams and boarding. A continuous steel trough and steel hay-rack are located along the structure's eastern wall.

- 6.49 Two notable features are situated adjacent the structure's east elevation. The first appears to comprise part of the conveyor system (Plate 201 and Plate 202) which was situated throughout the site. The feature forms a shallow U-section plate, comprising felted timber board as a base, framed by steel plate sides and end L-brackets; the brackets themselves are punctured with fitting holes, presumably to fix the feature to an associated base. The nature of the second feature is not known, although the manner of its construction materials would suggest it is contemporary with the other war-time features on the site. Its location does not appear to be original and instead has been relocated here from elsewhere on the site. It comprises a square, concrete block (Plate 203), not dissimilar to the conveyor system bases found in each loading bay. It measures a depth of c.150m and has steel plate fixings attached to its upper face. The width between the metal fixings are consistent with those of the conveyor belt section found on the loading platform of Store 9, and potentially has an associated use. The centre of its upper face contains a central steel fitting which is handle-shaped, suggesting a potential hatch use, although this is unlikely due to its thickness.

Building 3 (Figure 13)

- 6.50 Building 3 (Plate 204) comprises a late 20th century agricultural shed (pre-1990 mapping), laid on concrete hardstanding dated to 1979 and was constructed by Albert Sparrowhawk. The building bounds the northern site boundary and occupies a south-west – north-east alignment, with its principal elevation facing south-east. The building was constructed as a mixture of workshops and stores. It is constructed using a slender steel frame, set on a concrete plinth, the exterior of which is clad with a combination of steel and corrugated asbestos-cement sheeting. The roof slopes down in a north-westerly direction towards the rear of the structure and is similarly clad in both types of sheet panelling, the underside being supported by I-section steel girders and U-section uprights. The building is divided internally into five bays, all of which are accessed via an entrance set within the south-east elevation. Doors comprise substantial full-height steel sheets, set within individual sliding steel girder frameworks; two of which also include single-width personnel doors (now boarded). The internal five bays are divided with ephemeral timber stud partitions, the walls dividing the western four bays include centrally-placed single-width doorways. A series of timber telegraph poles are situated across the central alignment of the structure (Plate 205), in addition to framing each wall of Bay 4 where they provide additional support to the roof truss arrangement. The walls are clad internally with timber boarding, primarily for insulation purposes and to provide a suitable base for shelving. Workshop equipment and machinery remains within Bay 1 (Plate 206).

Building 7 (Figure 17)

- 6.51 Building 7 (Plate 207) was constructed at some point between 1990 and 2003 (Google Earth) as a store associated with the agricultural use of the site. The structure is situated to the north-west of Store 8, on land previously occupied by earth embankments. The building is rectangular and occupies a north-west – south-east alignment, with the principal elevation facing south-west. The structure is set on a concrete base, elements of the north-western end of which match the construction detailing of the bund containment walls and loading bay platforms, suggesting the building utilised these elements as foundation materials. The walls are constructed in breezeblocks, with the external north-east elevation clad with corrugated asbestos-cement sheeting. The structure is open-fronted to the north-west and its interior accessed via nine doorways set along the south-west elevation. The roof is steeply sloped to the north-east and clad with corrugated asbestos-cement sheets. The roof over the open-fronted north-western end is supported by a pair of telegraph poles, used as central posts. The doorways set along the south-west elevation are served by a covered corridor, roofed with a continuation of the sheeting, supported on breezeblock plinths. The interior (Plate 208) is divided into eight bays, divided by breezeblock partition walls and lit from the north-east by narrow, high-set timber-framed window casements. All bays are accessed externally, except for the two north-western end bays, which are additionally connected by an internal doorway. All internal areas serve as stores, with restricted headroom to their north-eastern ends, exacerbated by the addition of polystyrene cladding to the roof underside for insulation purposes.

Building 10b (Figure 20)

- 6.52 Building 10b (Plate 209) comprises a rectangular, four-bay lean-to, constructed against the south-west elevation of Store 10. The building is constructed in breezeblocks, with a corrugated asbestos-cement sheet roof. The building is accessed from four substantial openings set along the south-west elevation. Each door is set within a sliding frame constructed of I-section steel girders. The internal bays are divided by breezeblock walls (Plate 210), each additionally supported by a pair of plinths. The rear wall of each bay comprises the outer blast wall of Store 10, which retains each of its buttresses. Each of the bays retain machinery and storage equipment, indicating that these areas were used as stores and workshops. The north-west and south-east elevations are plain.
- 6.53 A notable feature to the south-west of the structure is a single loading platform (Plate 211) which is visible on the aerial photography taken in 1944 and presumably served the principal site entrance to the north-west. The photography also identifies a second platform situated to the south-west of Store 12, along the same boundary, although no evidence of this now remains.

Building 10c / 10d (Figure 20)

- 6.54 The buildings identified as 10c and 10d (Plate 212) comprise a single structure with an internal division. The structure has been constructed to the immediate north-west of Store 10, incorporating the remaining elements of the store's northern loading bay into the interior of 10d. The structure is rather rudimentary in nature, constructed of corrugated asbestos-cement sheeting, hung over a simple I-section steel girder frame, all set above a breezeblock plinth. The rectangular structure is aligned south-west – north-east and has substantial doorways set within both ends. Each doorway comprises corrugated iron panels, set within a steel girder sliding frame. The roof is

pitched along the same alignment and covered with the same sheeting as the walls. Building 10c is accessed from the south-west and comprises a single rectangular store. The south-east elevation of 10c is additionally supported by four external breezeblock buttresses. The rear north-west wall, separating the interior and 10c and 10d, is made of ply-board covered timber studs. The interior of 10d was accessed from the north-east, although this entrance is now blocked. The interior appears to have served as a barn and storage facility. The interior (Plate 213) occupies an L-shape and retains the eastern loading platform of Store 10's northern loading bay. The western platform has been demolished, presumably to accommodate the construction of this building.

Building 10e (Figures 20 and 24)

- 6.55 Building 10e (Plate 214) comprises a large, rectangular, lean-to, constructed against the north-east elevation of Store 10. The building follows the same alignment as the store, with its principal elevation facing north-west. Its north-west and south-east elevations comprise low breezeblock plinths, with regularly-spaced piers supporting corrugated asbestos-cement sheet cladding. The north-east elevation is built in stretcher-bonded orange brickwork (Plate 215), suggesting that the structure may have originally been open-fronted. Two timber-framed windows with timber lintels are set to either end of the north-east elevation. The roof is pitched and covered in corrugated asbestos-cement sheeting. The building interior (Plate 216) is accessed from two openings set within the north-west elevation, comprising a substantial sliding doorway (as elsewhere) with adjacent ramp and a single timber-boarded personal doorway set to the east. A single external doorway is additionally set centrally within the south-east elevation, providing quick access to Building 11. The interior comprises a single open-plan barn, with continuous cement screed floor. The south-west wall comprises the outer blast wall of Store 10 (Plate 217), complete with retained buttresses. The roof has been ceiled with aluminium composite panels, secured to a timber frame.

Building 11 (Figure 21)

- 6.56 Building 11 (Plate 218) was constructed at some point between 1973 and 2003 (Google Earth), and therefore forms part of the site's later agricultural use; it was most recently in use as a fuel store. The structure occupies a rectangular footprint and is orientated north-west – south-east, with its principal elevation facing north-east. The structure backs onto the southern loading bay of Store 10, on its north-east side. It is constructed in concrete breezeblocks, and has a steeply sloping roof comprising softwood timbers, clad with corrugated iron sheeting. The principal elevation is filled at its centre by a wide full-height, ramped vehicle entrance, latterly blocked with corrugated iron sheeting and timber board. A single timber door fills the western end of the north-west elevation. The remaining elevations are plain. The interior comprises an open store. To the immediate south-east of the building is a small shed, constructed in the same manner, with a blocked single doorway set in its north-east elevation.

Building 13 (Figures 23 and 25)

- 6.57 Building 13 (Plate 219) post-dates the historic mapping dating to 1973 and comprises a rectangular structure, aligned south-west – north-east. The building has been constructed within the northern loading bay of Store 14 to the immediate south-west of the earth blast barrier separating Stores 14 and 12. The principal elevation faces south-west, with the north-east elevation adjoining the remaining loading

platform serving Store 14. The building is constructed in concrete blocks, laid in a haphazard manner, with sections built in modern breezeblock. The roof is sloped to the north-west, constructed in softwood timbers and clad with corrugated asbestos-cement sheeting. The south-east elevation is divided into four bays, divided with concrete block piers. The lower half of each bay is filled with modern breezeblock and clad to eaves level with asbestos sheeting. It appears as though the structure may have been constructed as open-front shelter shed, presumably related to the piggery use of Store 14, and latterly filled to form a store. The south-west elevation is dominated by a wide opening, secured with a steel bar gate at its lower level, again suggesting a former agricultural use. The upper half of the opening, originally open, has been blocked with metal sheeting. The remaining elevations are featureless. Internally the structure forms an open store.

Building 15 (Figures 14 and 26)

- 6.58 Building 15 (Plate 220) appears after 1973 and comprises a rudimentary rectangular shed, aligned south-west – north-east. The structure is situated to the immediate south-east of Store 4 and abuts the loading platforms in this location. The principal elevation faces south-west and is built in orange brickwork laid in stretcher bond, covered with a cement render. All other elevations are constructed in breezeblocks. The roof is sloped to the north-west and formed by sawn timber rafters, covered with asbestos cladding. The building is accessed via a double-width entrance to the south-west and is lit by two high-level, narrow windows (no glazing) set within the south-east elevation. The interior most recently served as a store but is now in a dilapidated condition.

7.0 DISCUSSION

- 7.1 The site identified as Equipment Ammunition Depot Magazine No. 116 was built as part of the countrywide anti-aircraft defence preparation works established in the lead-up to the Second World War. The ammunition depot at Banstead formed one of three depots built between 1938 and 1939 to store and supply ammunition for the anti-aircraft batteries surrounding and defending London during war time, including the Battle of Britain, which was the first major Nazi defeat and a crucial turning point in the conflict, and the Blitz. The Banstead site supplied the South West London sector from Dulwich to Raynes Park and included the South East region for a short period in 1940. The site officially closed as an active storage / distribution centre in 1953 and was auctioned-off in 1966. After, it was utilised for agricultural, storage and workshop use. The site no longer remains in use.
- 7.2 The eight stores which form the ammunition depot survive, although the majority of their associated infrastructure no longer remains *in situ*. The structures' external infrastructure, principally their loading bays, conveyor systems and earth embankments, have either been demolished or subject to alterations to adopt the site for agricultural and storage use. The loss of these elements has diminished the appreciation of the structures' original form. In addition, the wooden accommodation huts and the two rectangular structures shown on mapping, forming part of the mess areas and non-explosive stores, have been demolished.
- 7.3 Despite the loss of these elements, the character of each store is preserved internally through the survival of various original features such as doors, windows, 'bin' divisions, signage and light fittings. The preservation of numerous site features, including all but one of the anti-blast traverses (bund), signage, two pre-war lamp

posts and sections of steel mesh used to fence the site's north-eastern perimeter, combine to form a relatively good survival of the anti-aircraft equipment ammunition depot dating to the Second World War.

- 7.4 The configuration of the original access track from Park Road, the original perimeter fence posts, the location of the site entrance and the road through the site, are all preserved and add to the understanding of the site's function as a wartime storage and distribution centre. The site has retained its secluded rural setting, which is a characteristic trait of this type of military facility.
- 7.5 The two two-man air-raid posts and larger air-raid shelter located outside the main site area, remain relatively intact and provide good examples of the site's safety and security measures. The sentry posts are a less common variant of a Consul shelter, produced for civil defence personnel to shelter in during air raids. In this instance, they would have provided shelter to sentries whose job it was to patrol the metal fence perimeter. The brick air-raid shelter along the south-western boundary, is likely to have been intended for use by those located within the vicinity of the site lodgings, including the wooden huts and foreman's bungalow.
- 7.6 The site structures additionally form an example of the early use of reinforced concrete for defensive military structures. Reinforced concrete was a relatively new building material during the early/mid-20th century and the lack of material knowledge may, in-part, have contributed to the current poor condition of the structures.
- 7.7 Store 4 has been identified for preservation as part of the most recent plans for the site. This represents the best example due to its condition and survival, both internally and within its southern loading bay, with retained conveyor supports and earth embankments towards its eastern side. Despite the loss of internal divisions between the 'bins', the passage and associated conveyor supports, the retention of the store's associated loading and distribution configuration, served by original door latches, bull light fixtures and gated entrances, combine to help illustrate the site's operational narrative. Salvaging individual elements from elsewhere on the site, such as pre-war lamp posts / light fittings, timber notice boards, fragmentary remains of conveyor tracks and original door / entrance signage, as well as the reestablishment of the structure's lost earth embankments, would all aid in increasing the individual store's readability and its value in representing the wider depot following any redevelopment.

8.0 DEPOSITION OF THE ARCHIVE

- 8.1 The project archive will be deposited within a suitable local museum, under the site code CFB17. The archive will comprise all survey material collected and produced in undertaking the project. A copy of the report will also be supplied to Surrey County Council HER (in PDF digital format), in order for it to be added to the County's Historic Environment Record.

9.0 ACKNOWLEDGEMENTS

Archaeology South-East would like to thank HA & DB Kitchin Ltd. for commissioning this report. In addition ASE would like to extend thanks to all those who helped with archival enquiries at Historic England (Swindon) and The National Archives (Kew), and to the members of the Banstead History Research Group for their help in providing relevant resources.

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WO 166/5319	115 Corps A.D. RAOC (War Diaries)	August – December 1941
WO 166/5354	Ordnance Depot, Chief Ordnance Officer, Ashford (Tonbridge) (War Diaries)	April – December 1941
WO 166/5360	Central Ordnance Depot RAOC (Greenford) (War Diaries)	May – December 1941
Historic England Archive Ref.	Description	Date
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Raf_cpe_uk_1982_rp_3141	Aerial photo of the site	11 th April 1947
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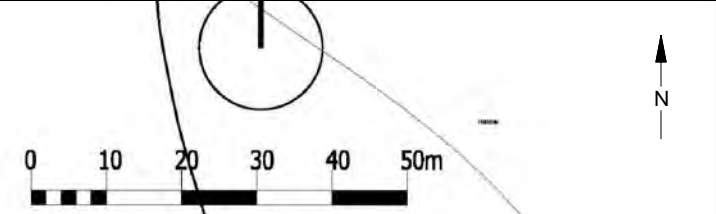
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Contains Ordnance Survey data
Crown copyright and database right 2014

© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 1
Project Ref: 171102	Feb 2018	Site location	
Report Ref: 2017537	Drawn by: HS		



Note: for existing sections see drawing J002196/ PL 26

KEY

- Buildings to be retained
- Buildings to be demolished
- Hardstandings

Ref	Description
1	Stable (off site)
2	Barn
3	Barn
4	Bunker (to remain)
5	Bunker
6	Bunker
7	Shed
8	Bunker
9	Bunker
10a	Bunker
10b	Lean-to shed
10c	Barn
10d	Lean-to shed
10e	Barn
11	Fuel store
12	Bunker
13	Store
14	Bunker
15	Store

Rev	Date	Description
A	27.5.16	Existing section refs added

WS PLANNING & ARCHITECTURE
 Europe House, Banchoff's Road, Reigate Surrey, RH2 7RP
 T. 01737 225711 F. 01737 226311 www.wspla.co.uk

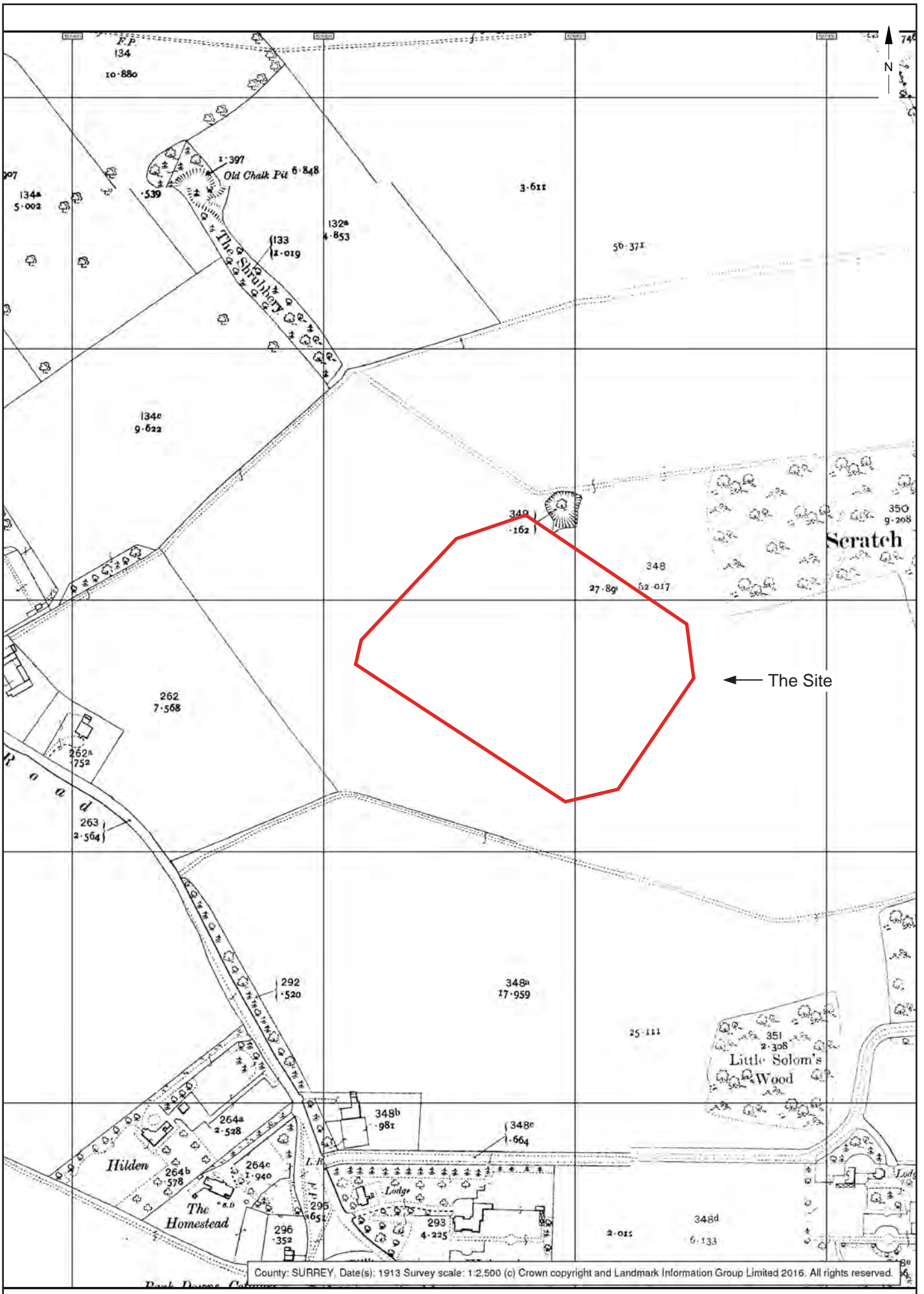
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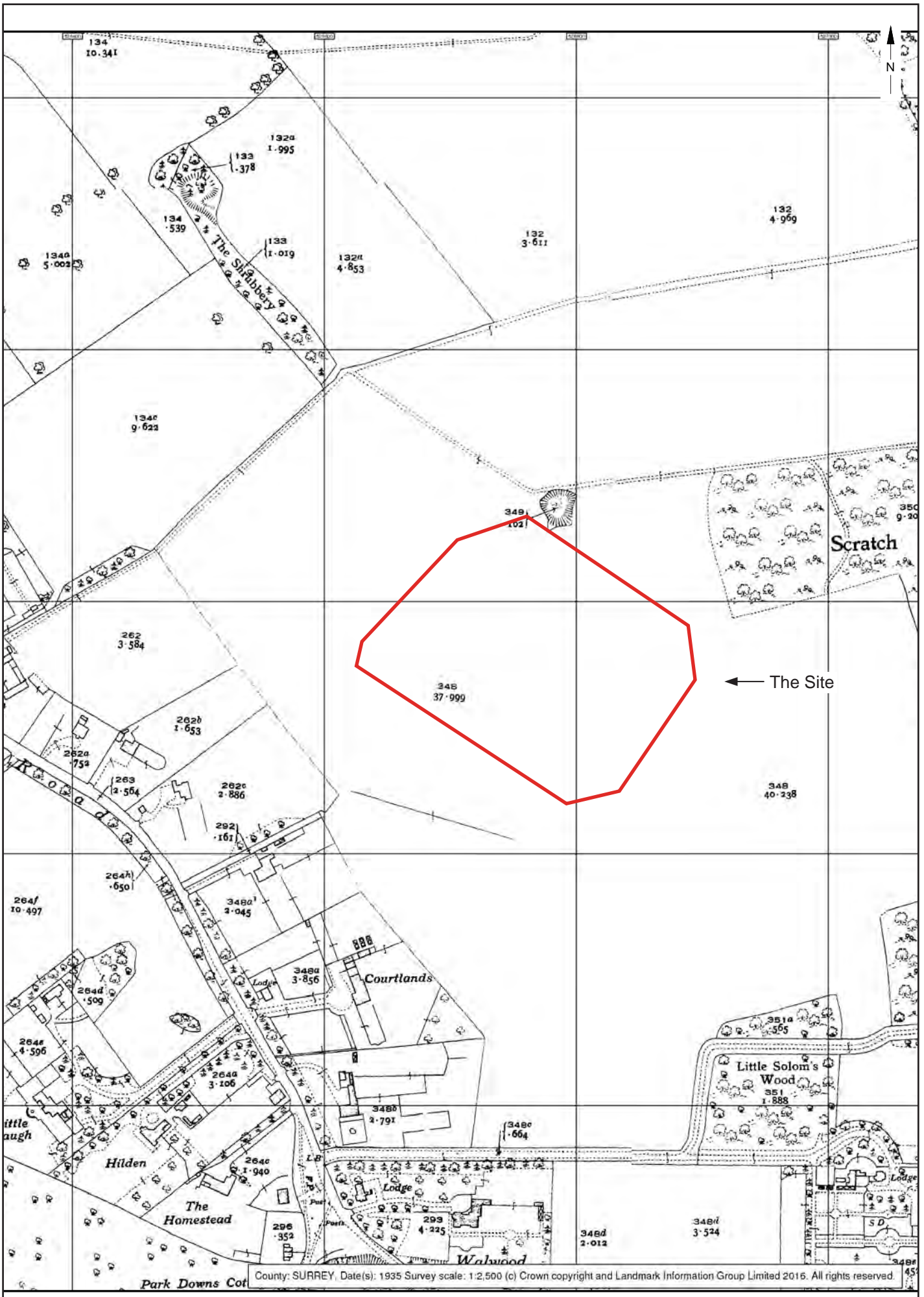
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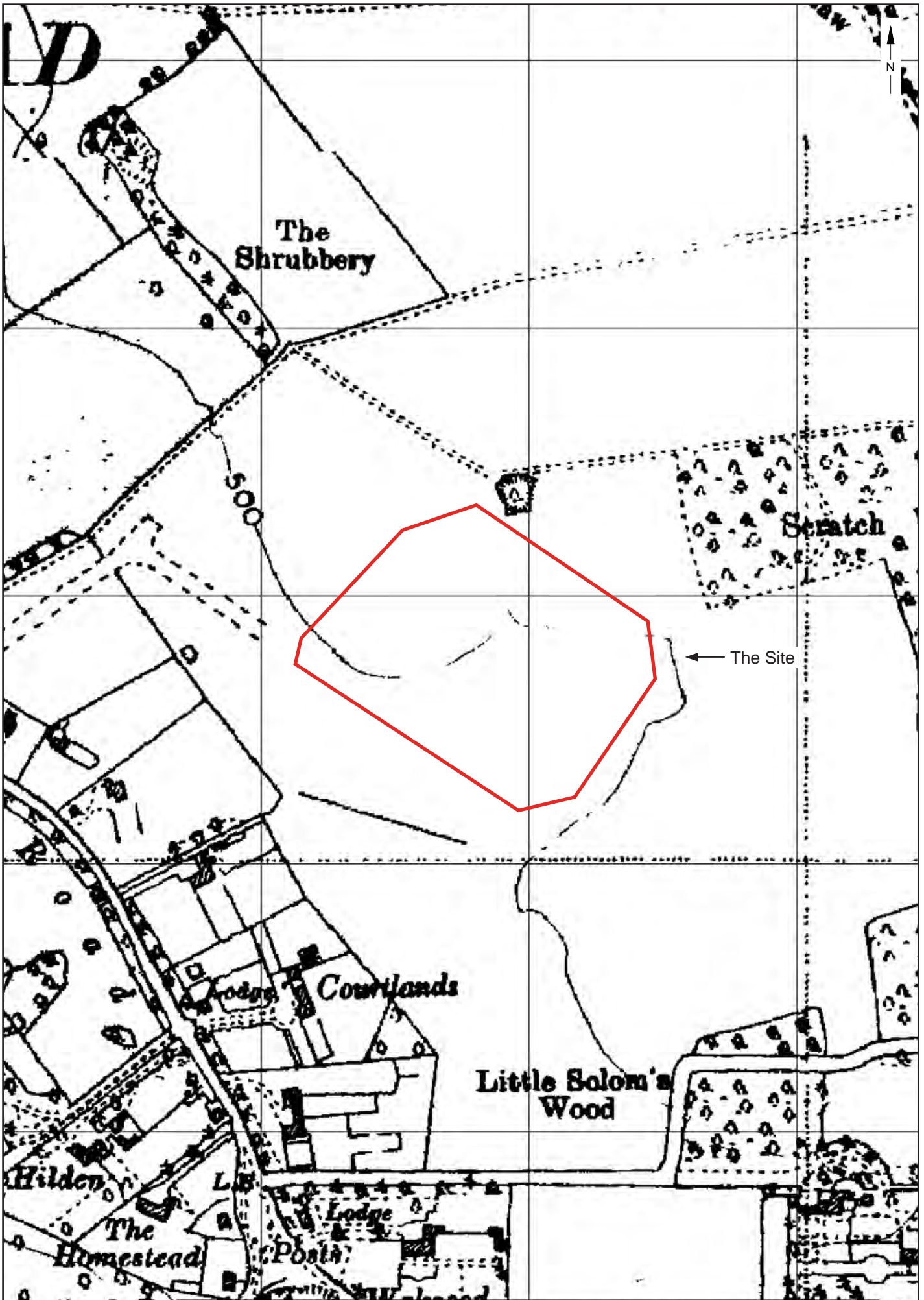
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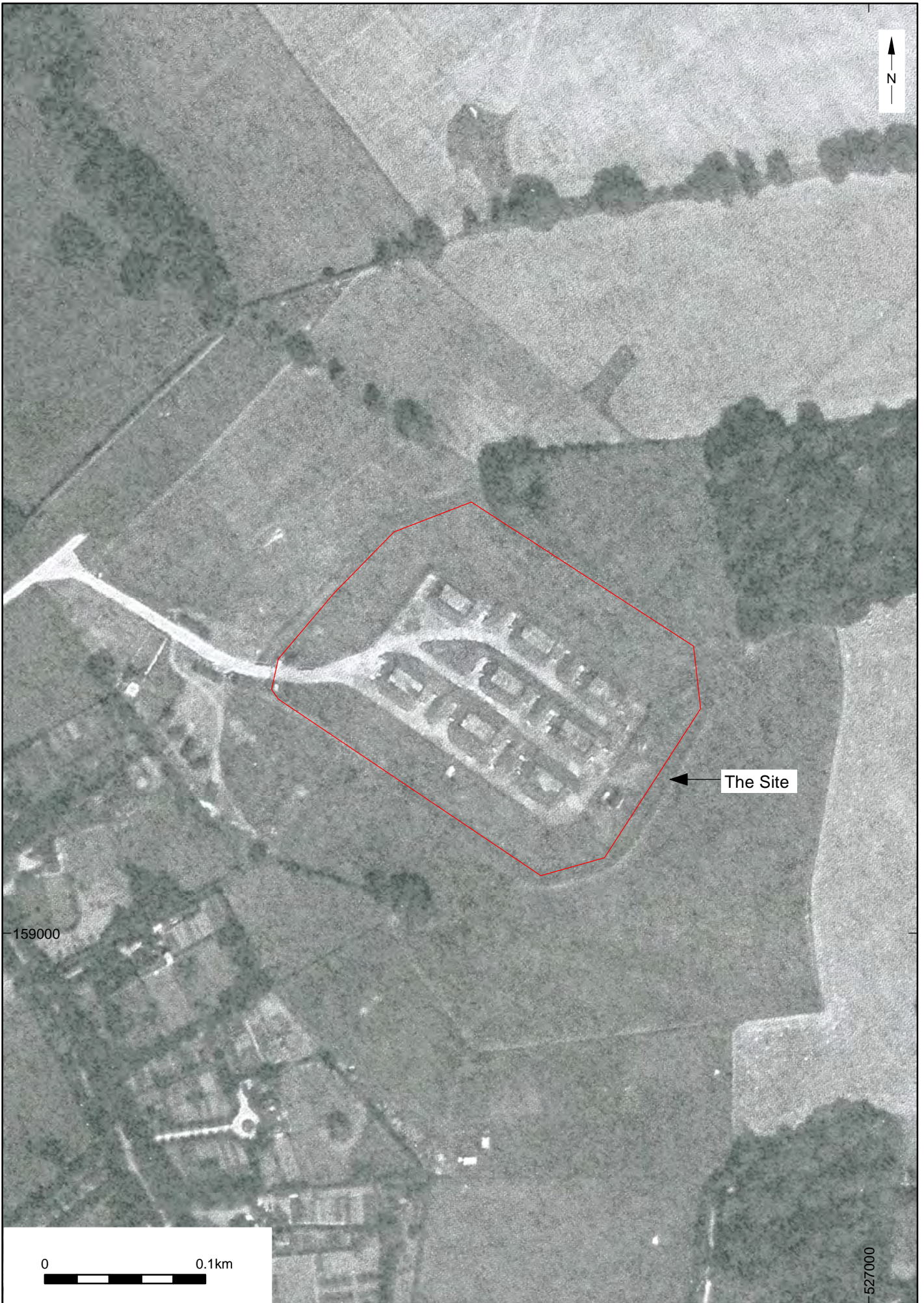
© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 3
Project Ref: 171102	January 2018		
Report Ref: 2017537	Drawn by: HS	Ordnance Survey map of 1913 (1:2500, 2nd rev.)	



© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 4
Project Ref: 171102	January 2018	Ordnance Survey map of 1935 (1:2500, 3rd rev.)	
Report Ref: 2017537	Drawn by: HS		



© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 5
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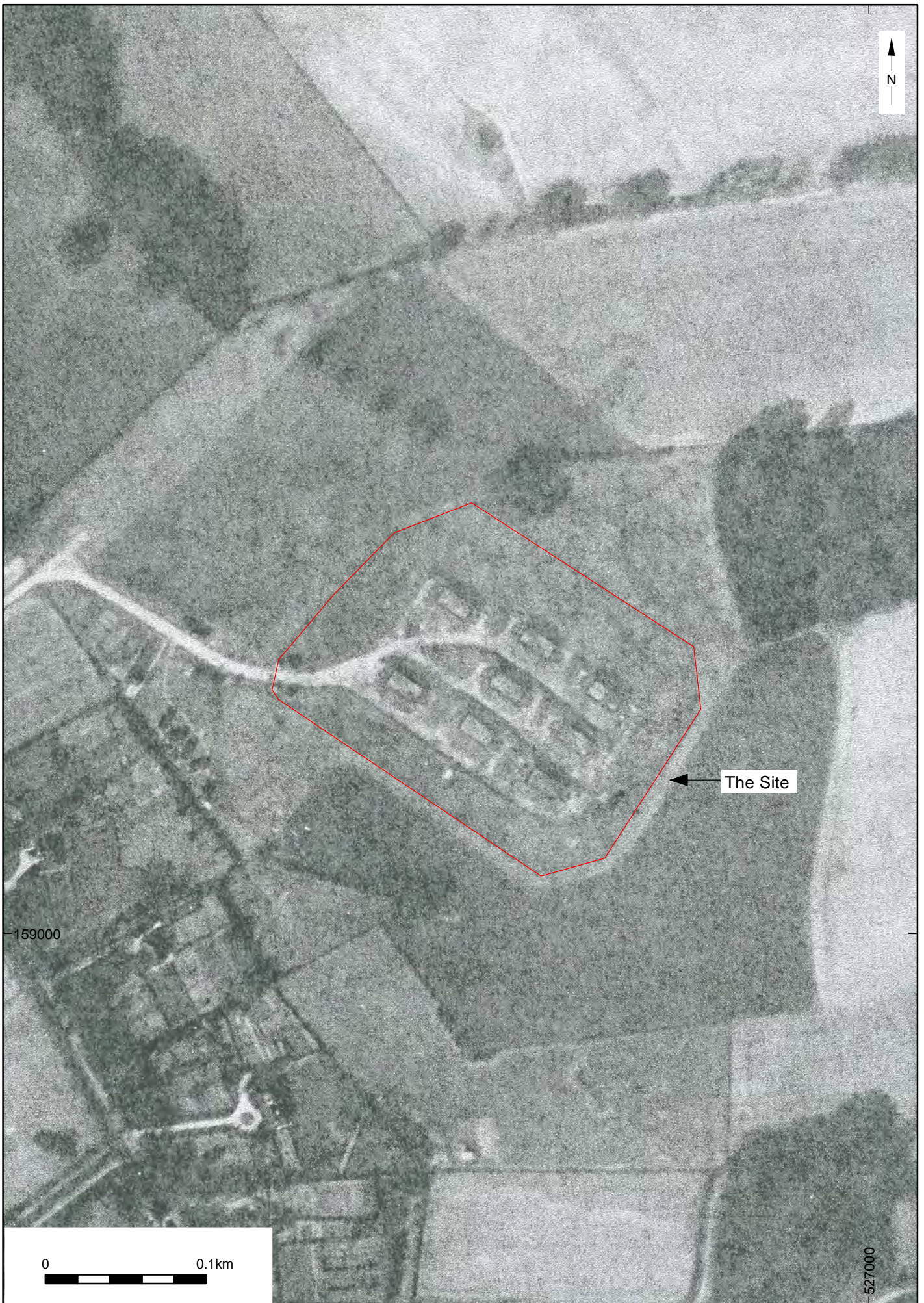
© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 6
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Report Ref: 2017537	Drawn by: JLR	(RAF 106G 1G 30 RP 3054)	



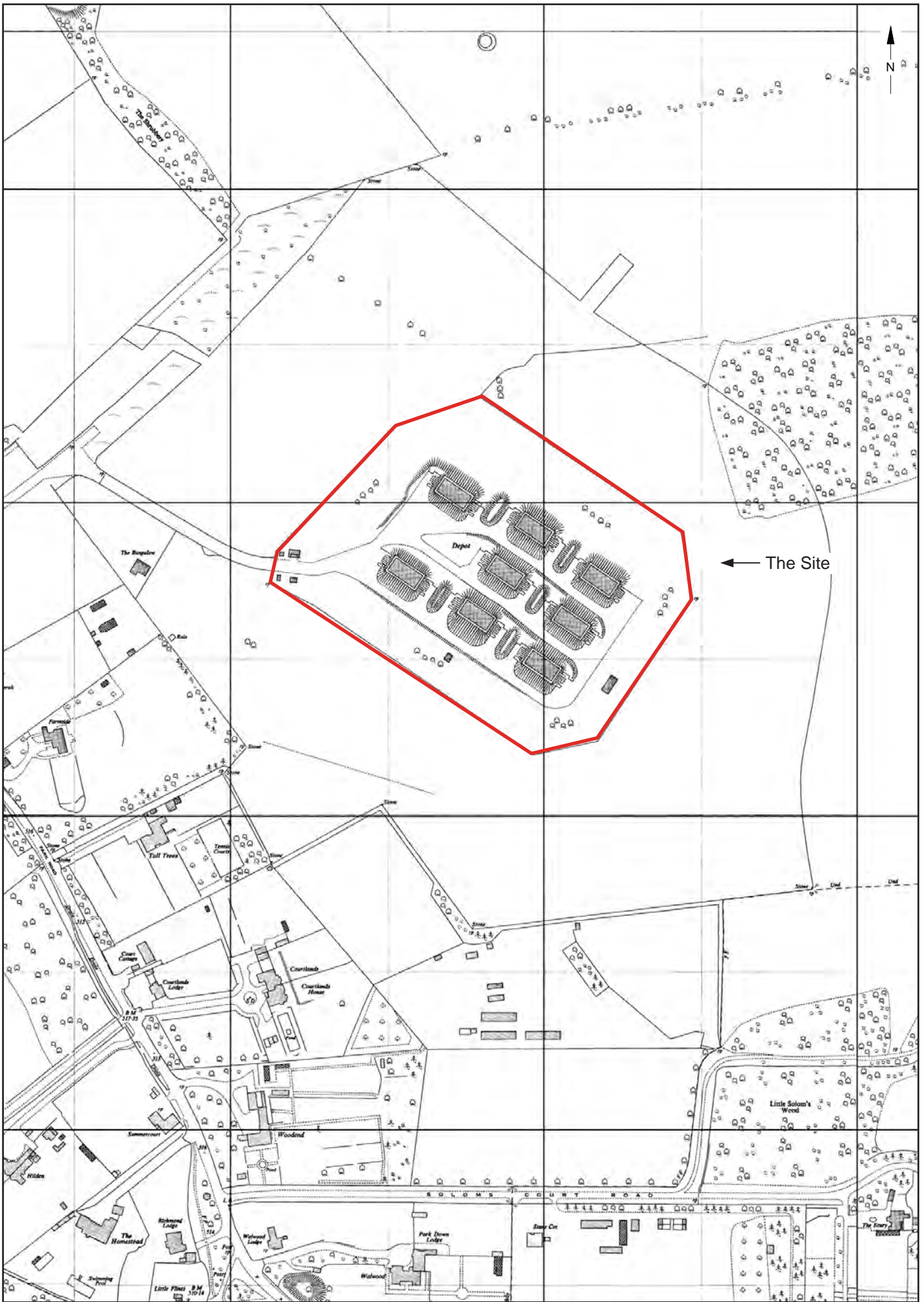
© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 7
Project Ref: 171102	January 2018	1947 aerial photograph overlaid with block site plan	
Report Ref: 2017537	Drawn by: JLR	(RAF 106G 1G 30 RP 3054)	



© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 8
Project Ref: 171102	January 2018	Aerial photograph of the site, 1945 (Google Maps)	
Report Ref: 2017537	Drawn by: HS		



© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 9
Project Ref: 171102	January 2018	Aerial photography of the site, 11th April 1947 (raf_cpe_uk_1982_rp_3142)	
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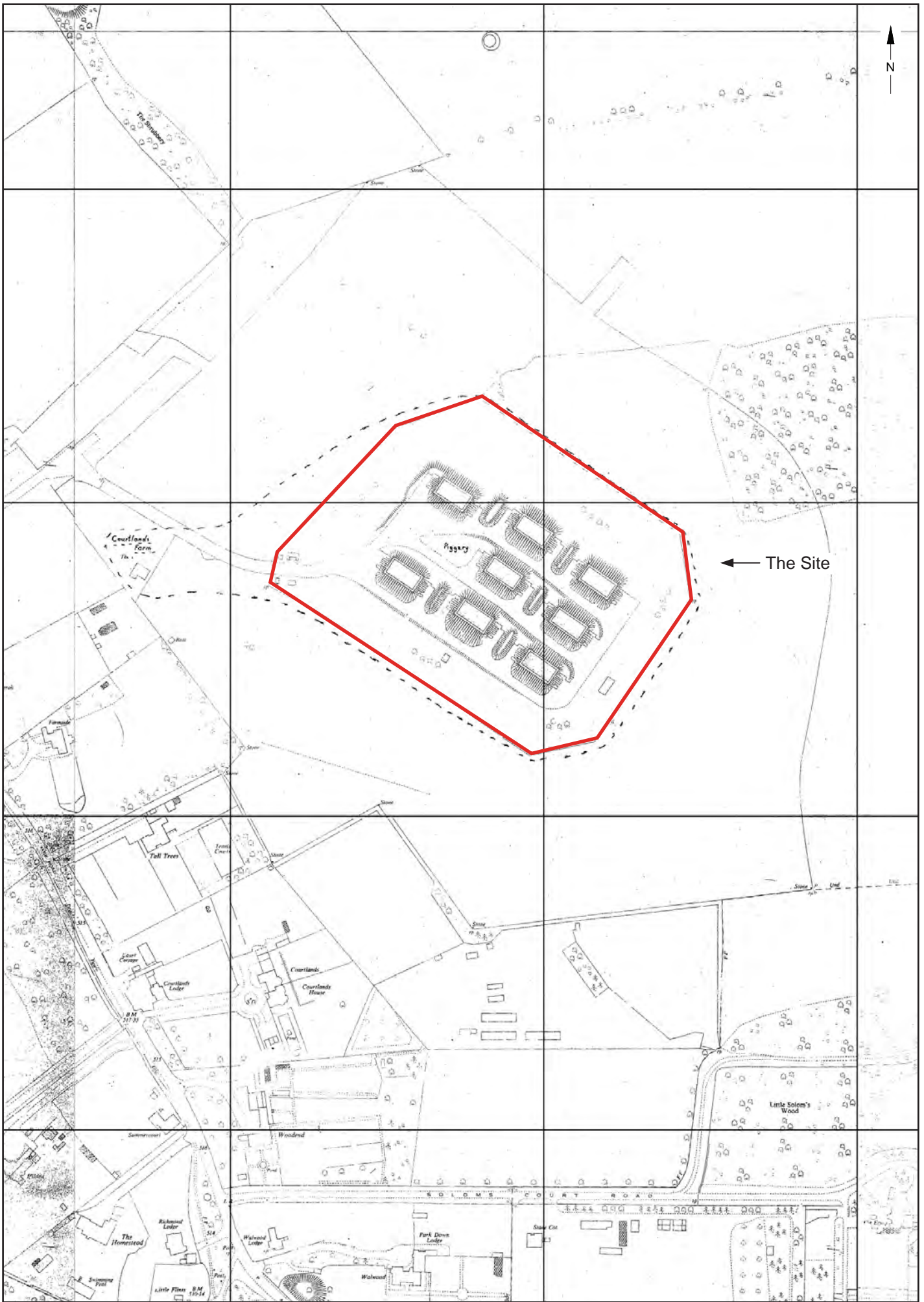
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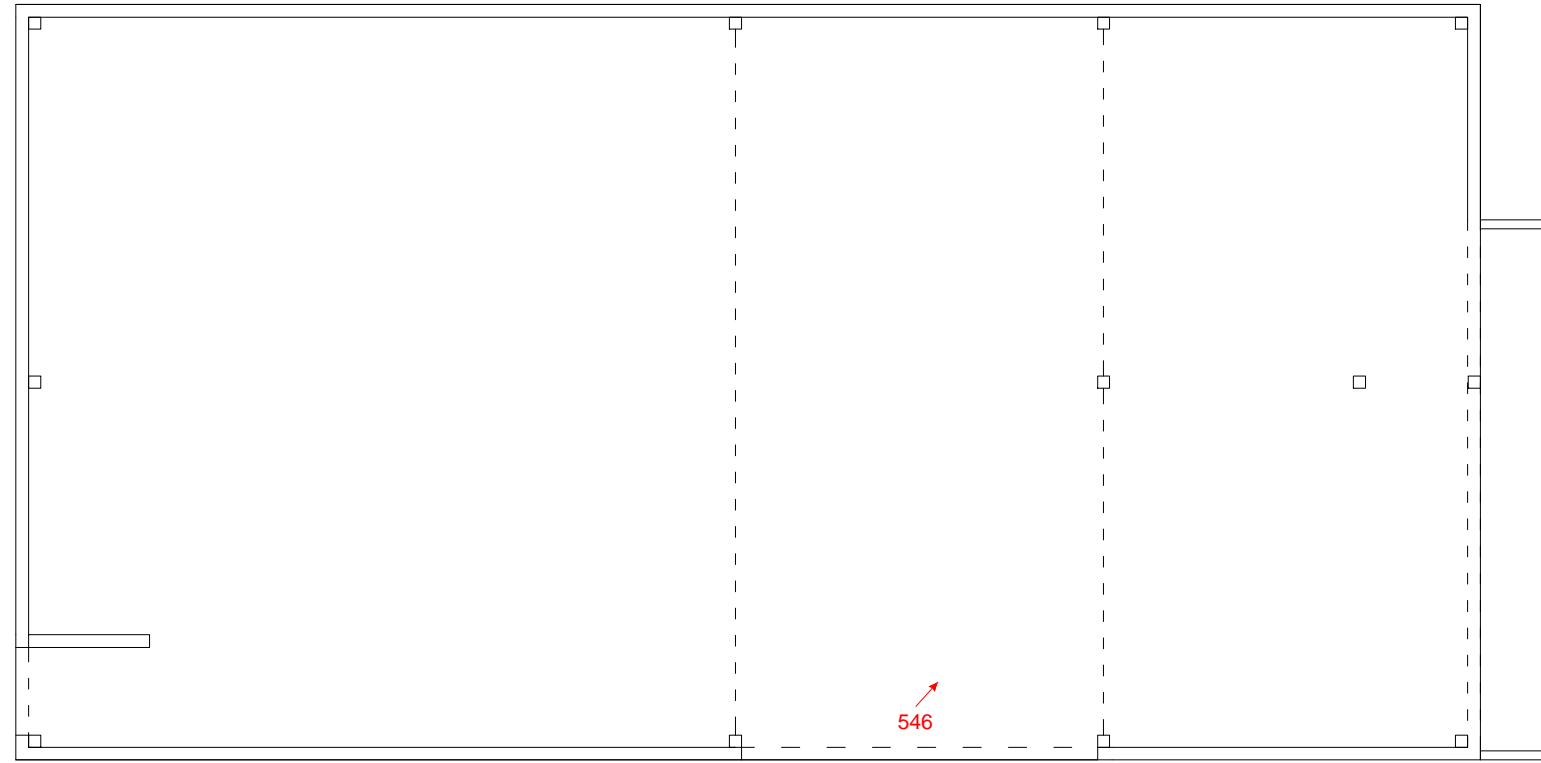
January 2018
Drawn by: HS

Ordnance Survey map of 1958-59 (1:1250)

Fig. 10



© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 11
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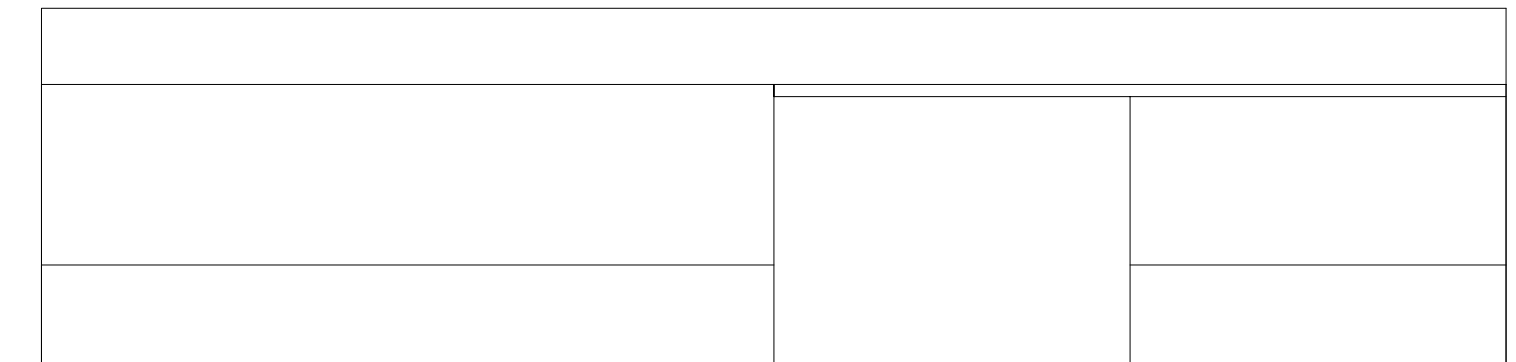
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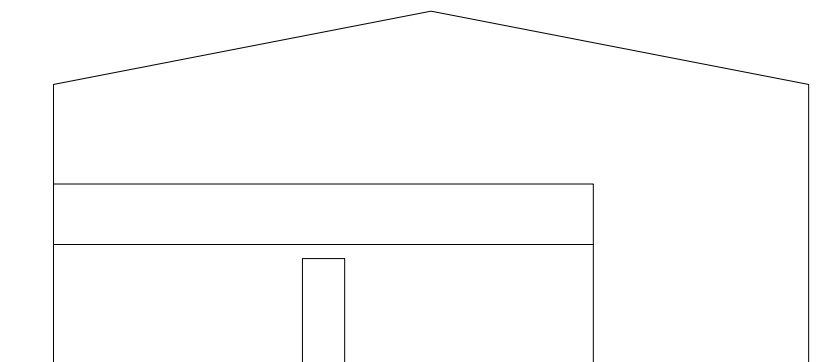
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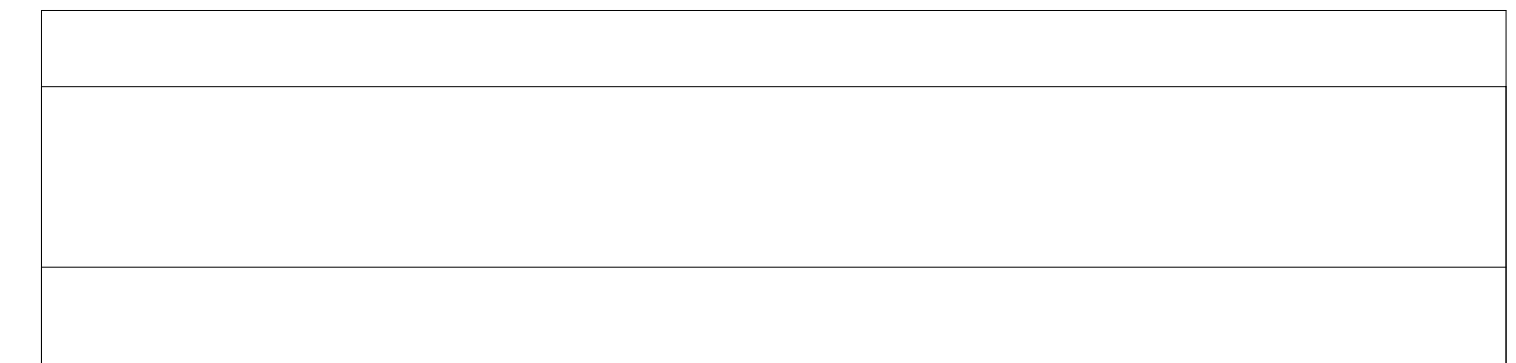
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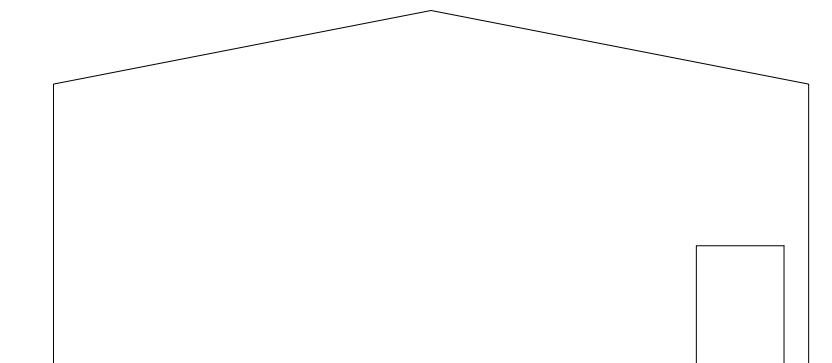
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South-east elevation



Building 2
North-east elevation

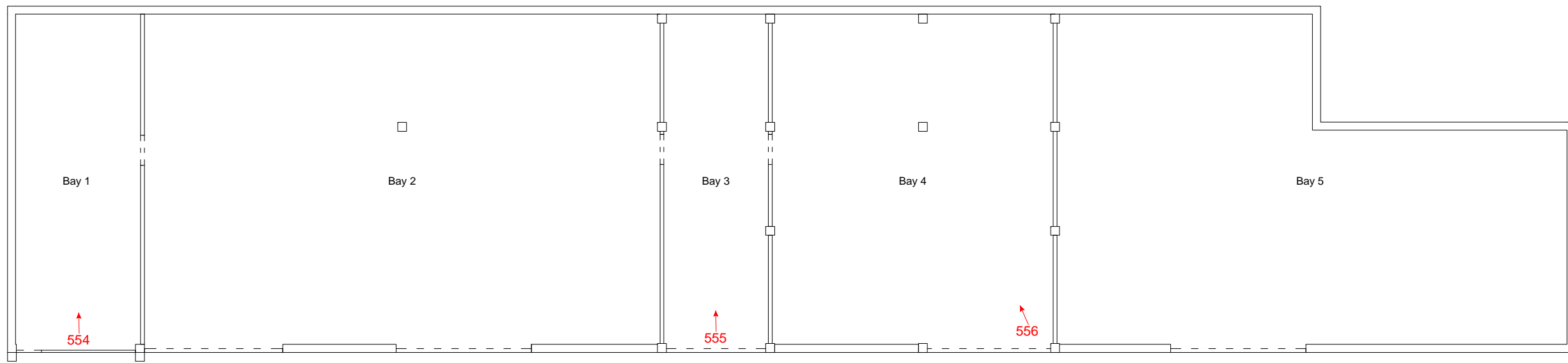


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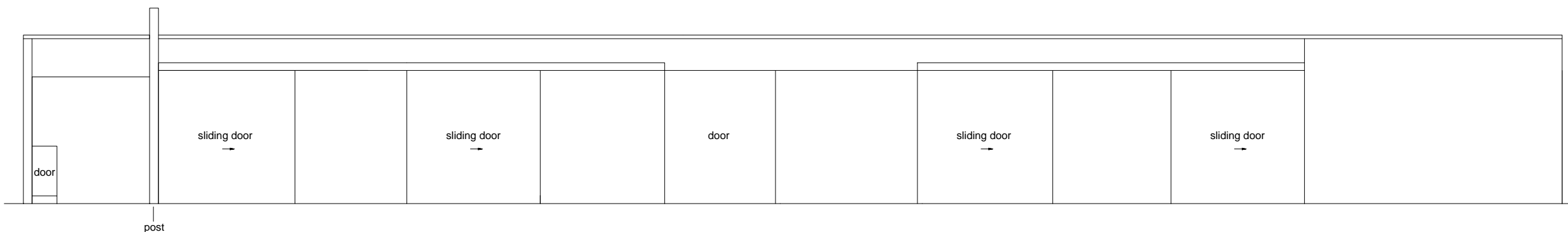


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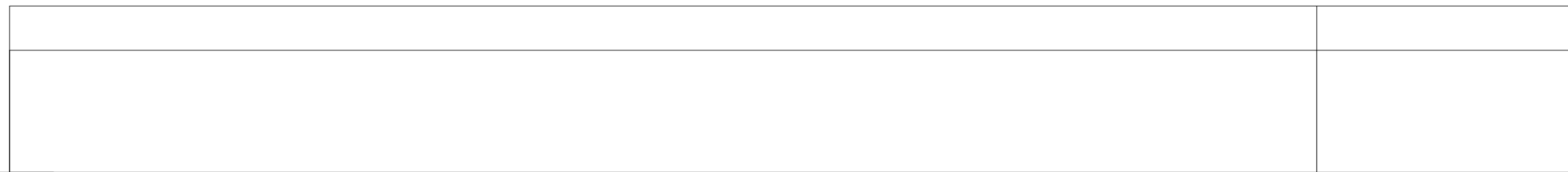
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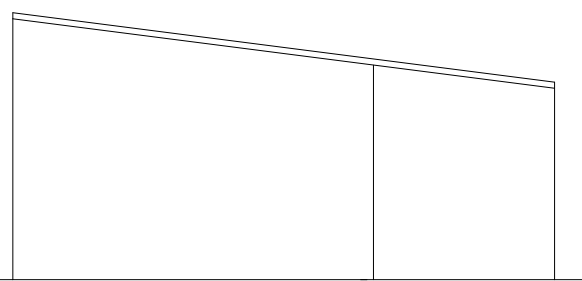
548&549 Building 3 Existing floor plan



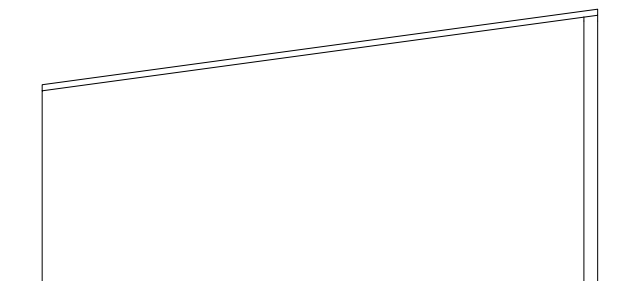
Building 3 South elevation



Building 3 North elevation

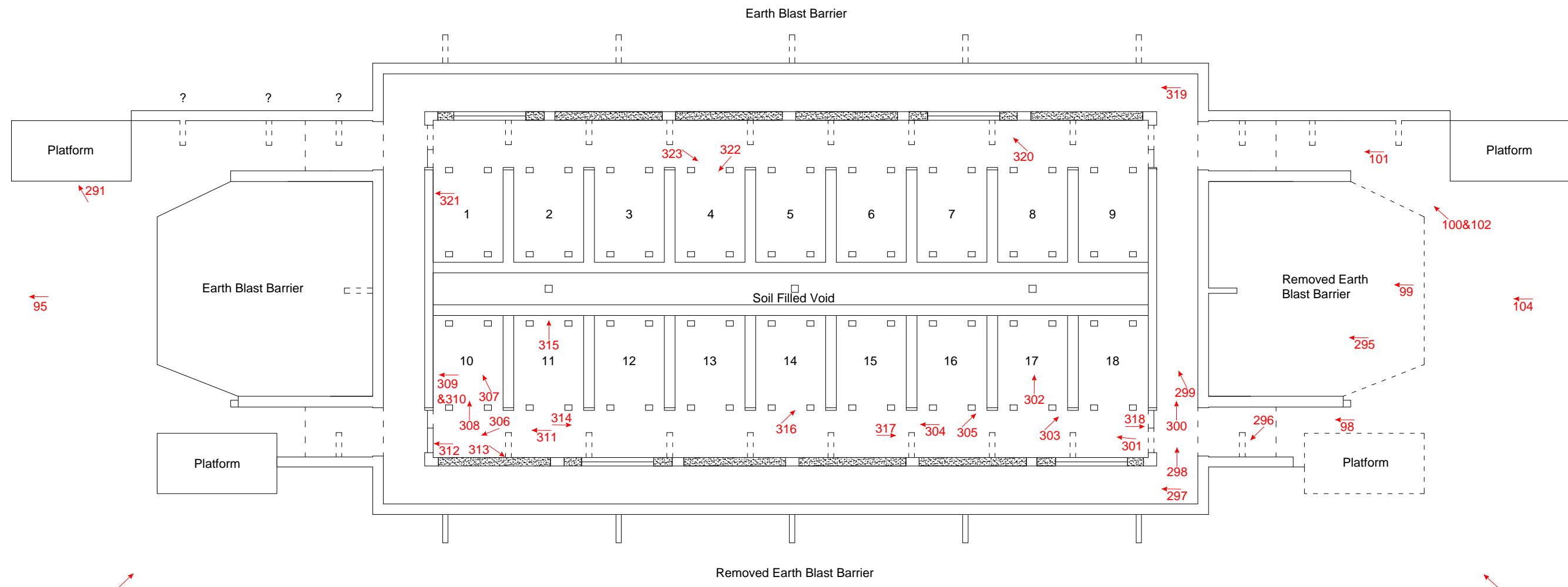


Building 3 East elevation



Building 3 West elevation





Store 5
Existing floor plan

290 292 96

293

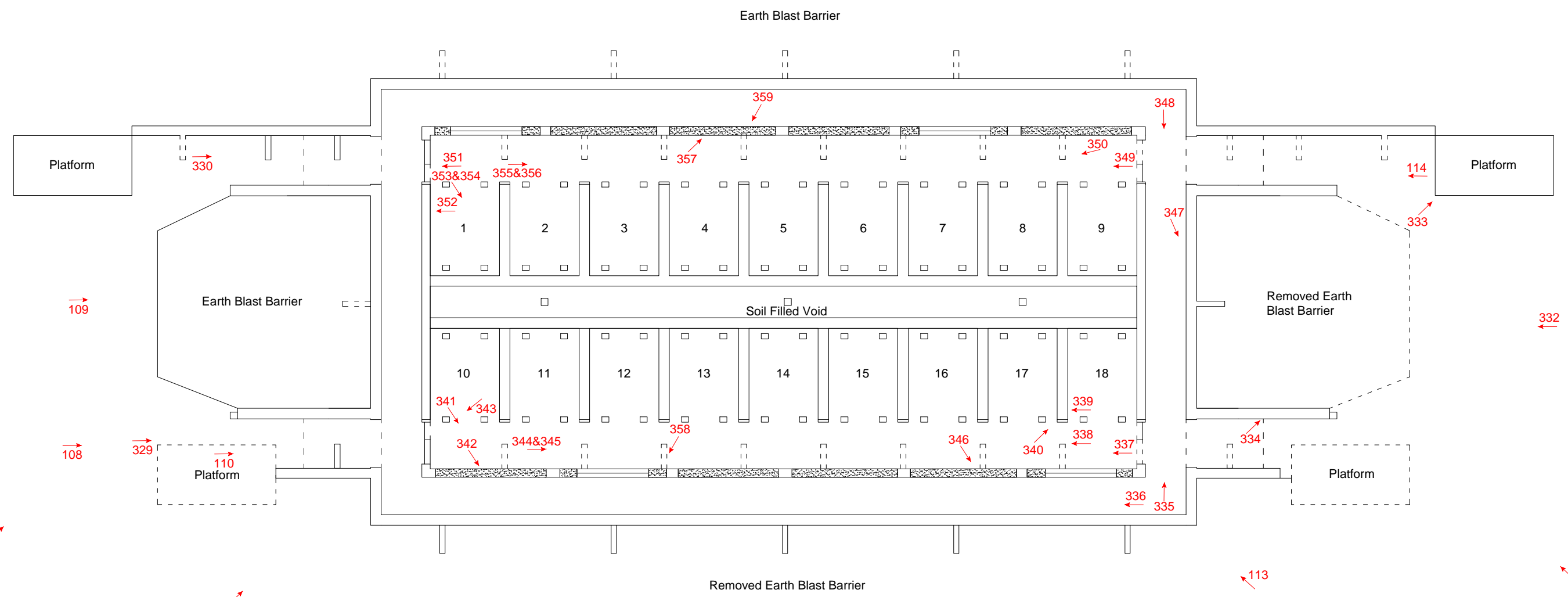
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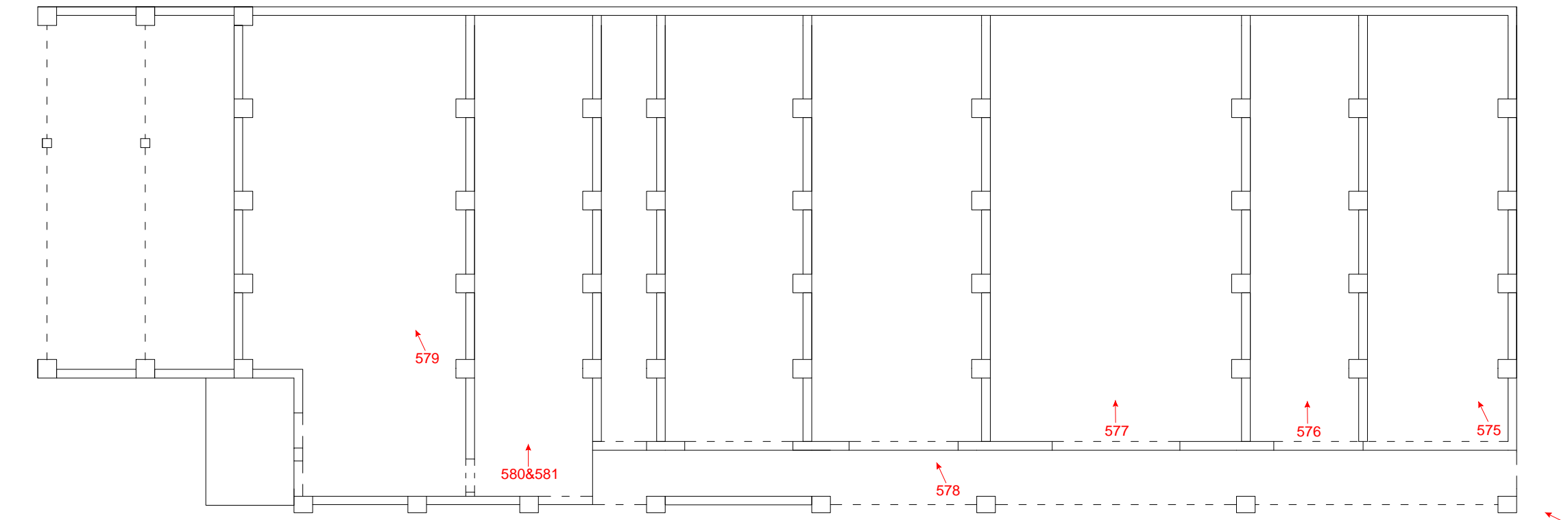
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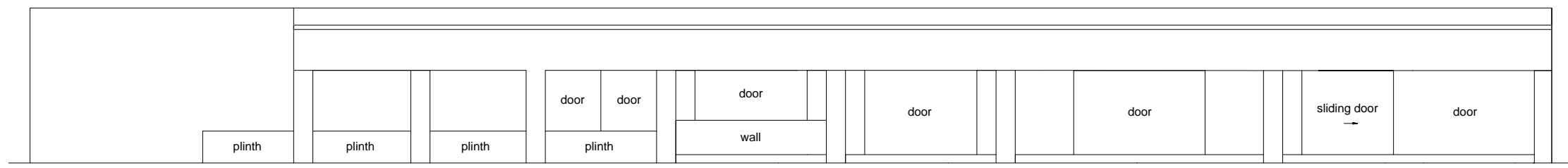


Store 6
Existing floor plan

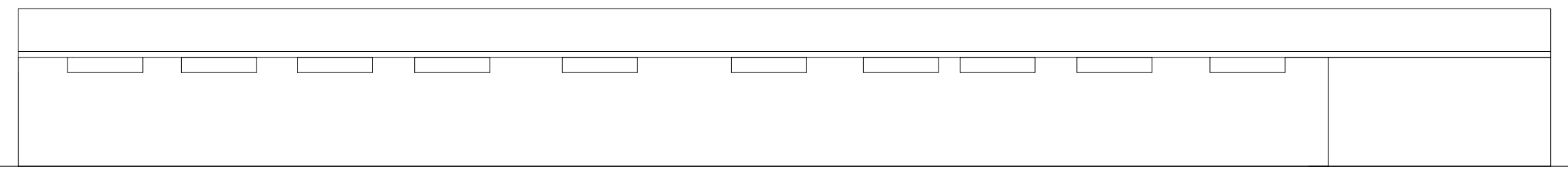




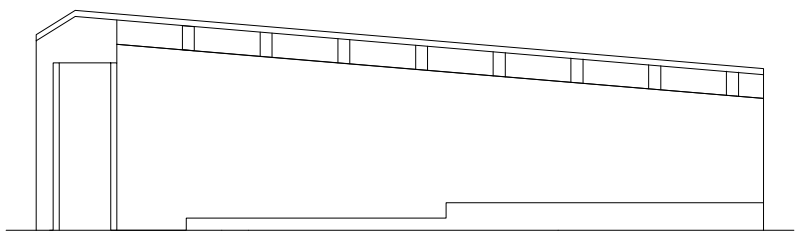
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Existing floor plan → 568



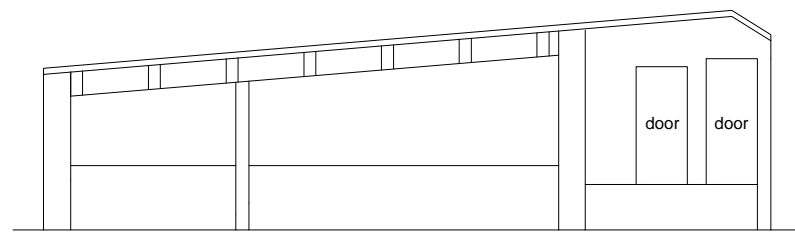
Building 7
South-west elevation



Building 7
North-east elevation

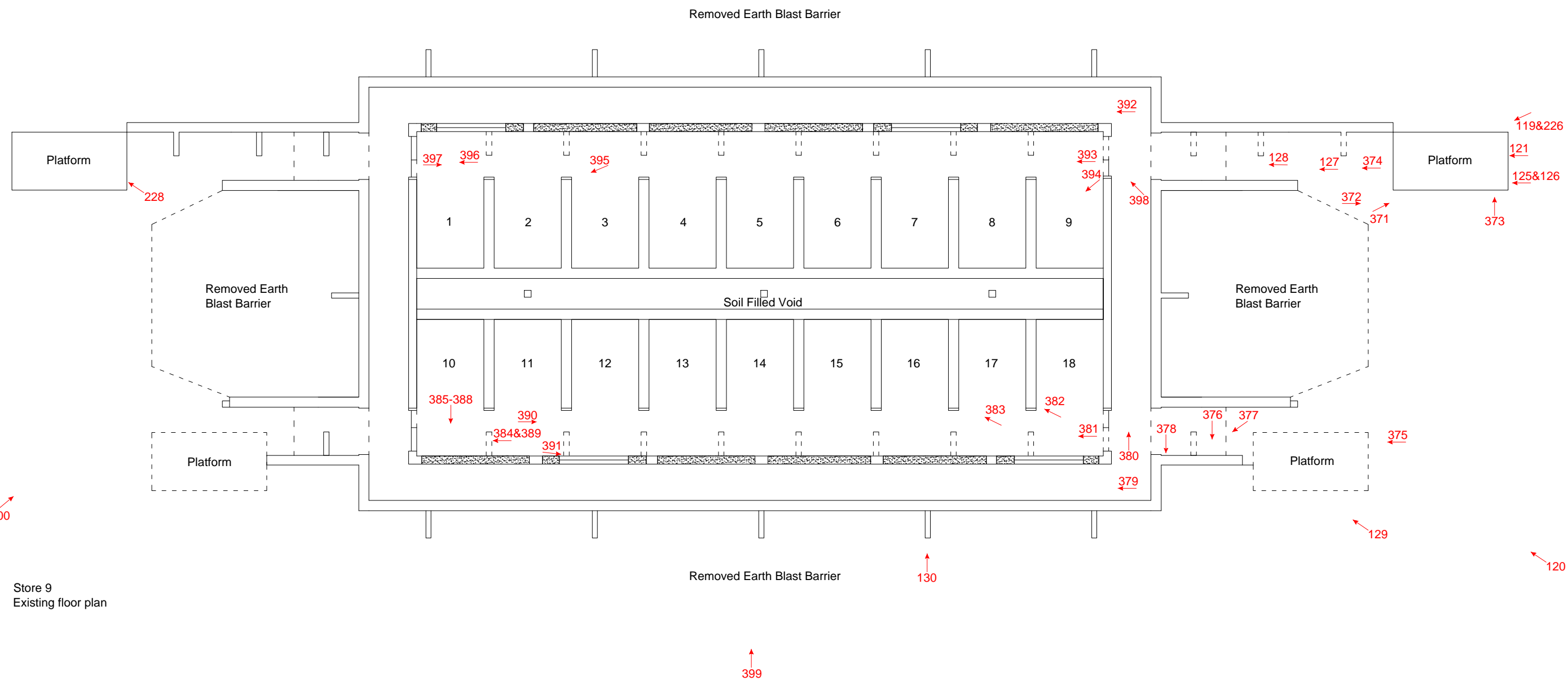


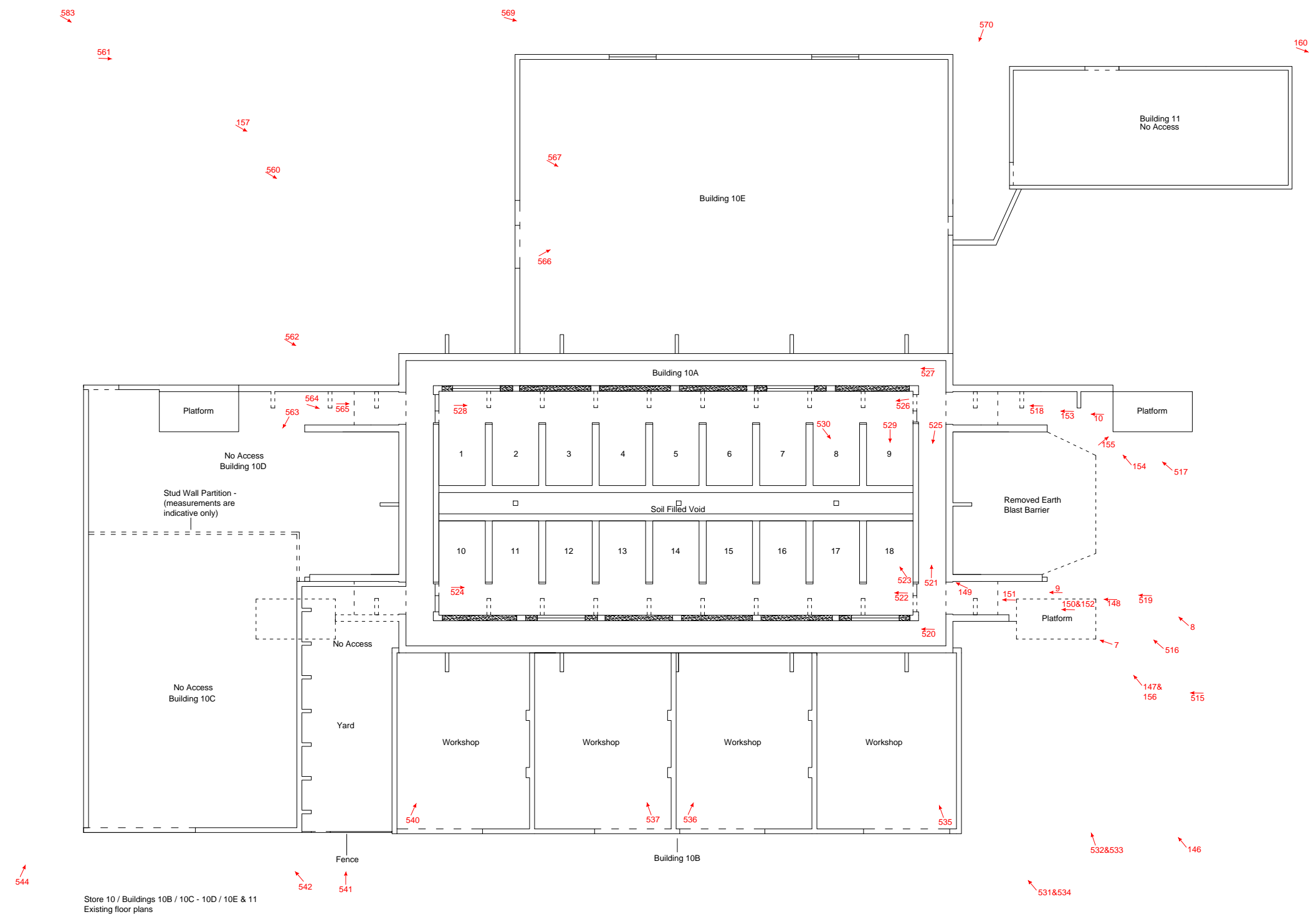
Building 7
South-east elevation



Building 7
North-west elevation

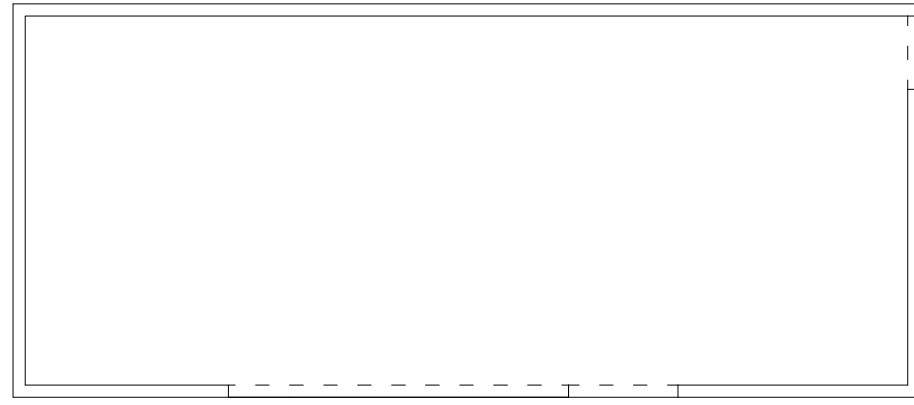






Store 10 / Buildings 10B / 10C - 10D / 10E & 11
Existing floor plans



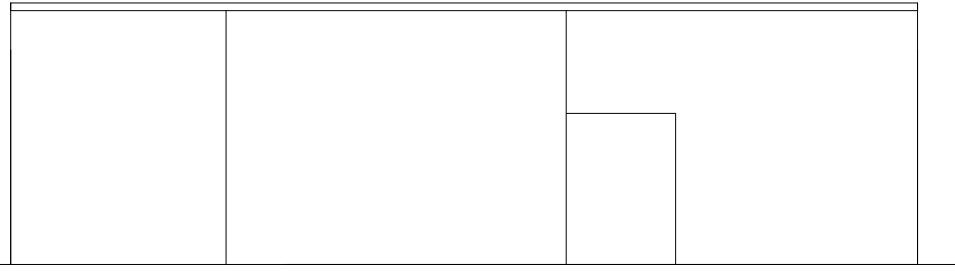


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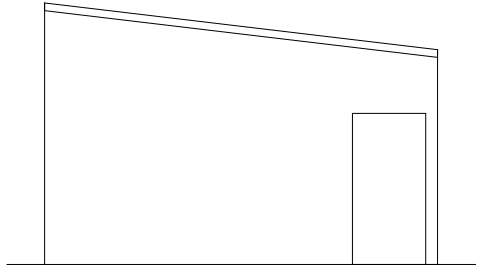
Building 11
Existing floor plan

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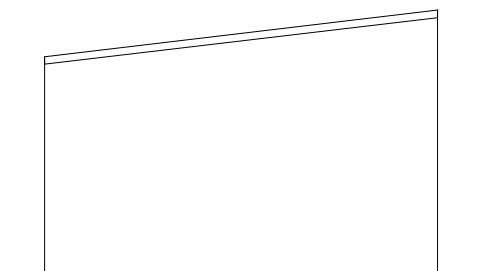
Building 11
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Building 11
North-west elevation

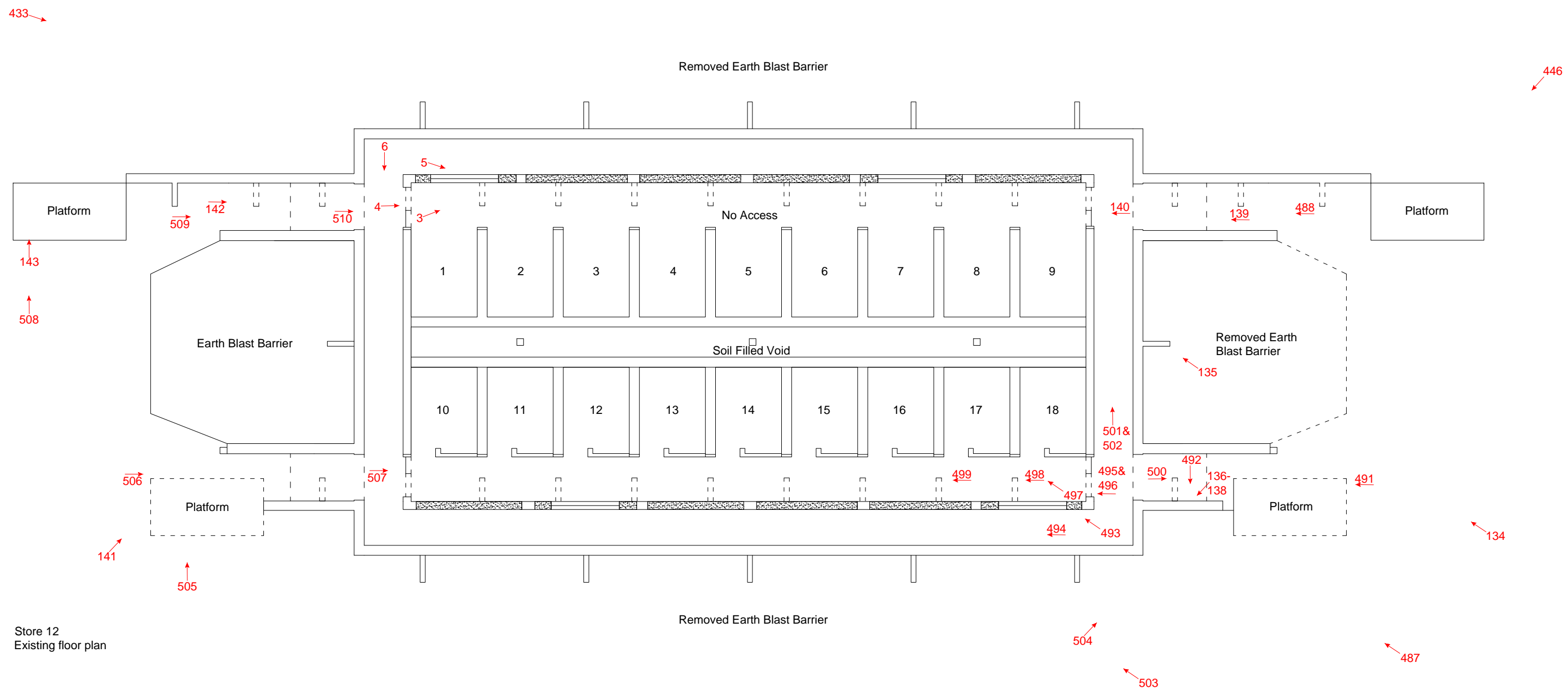


Building 11
South-west elevation



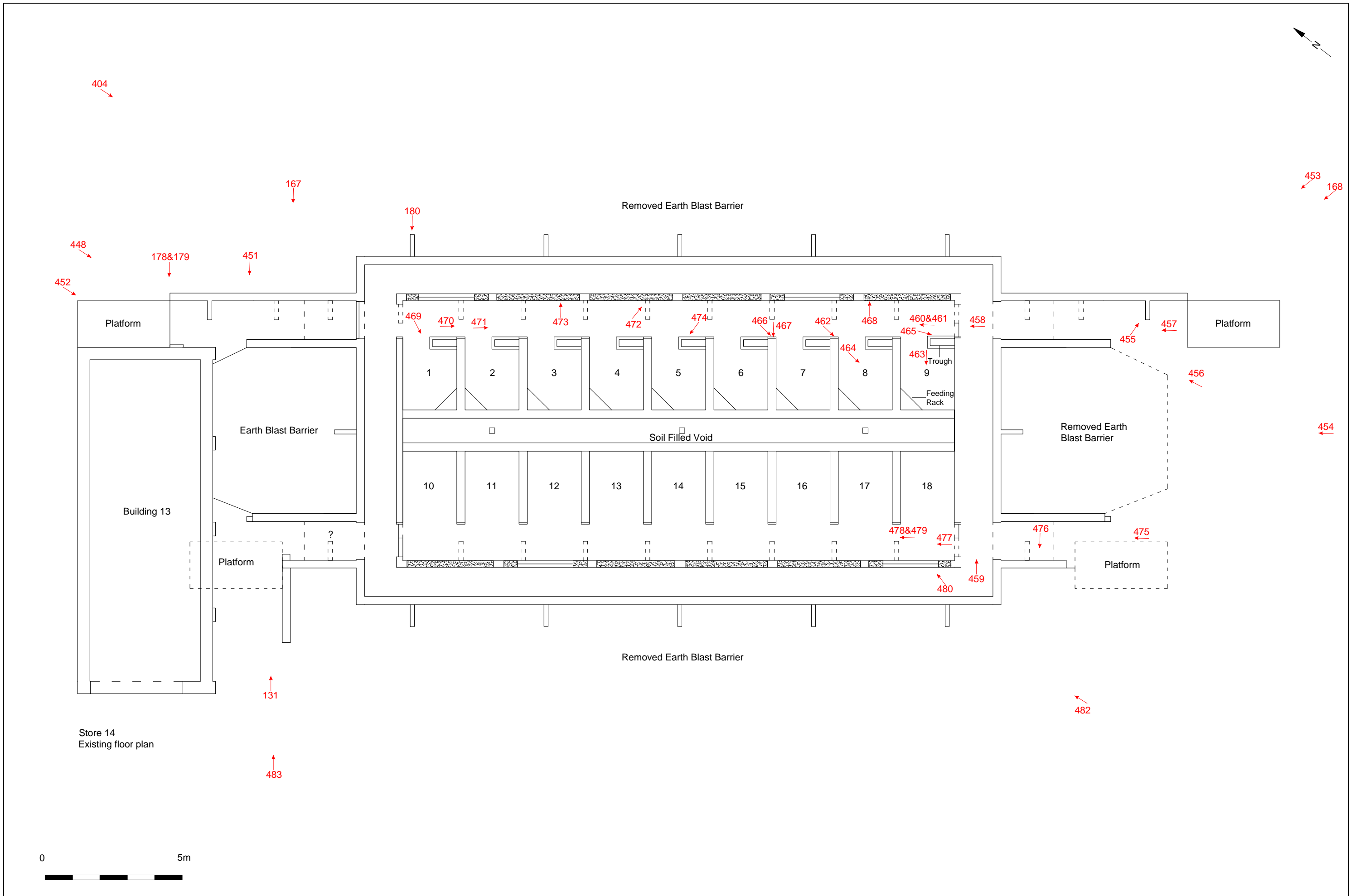
Building 11
South-east elevation





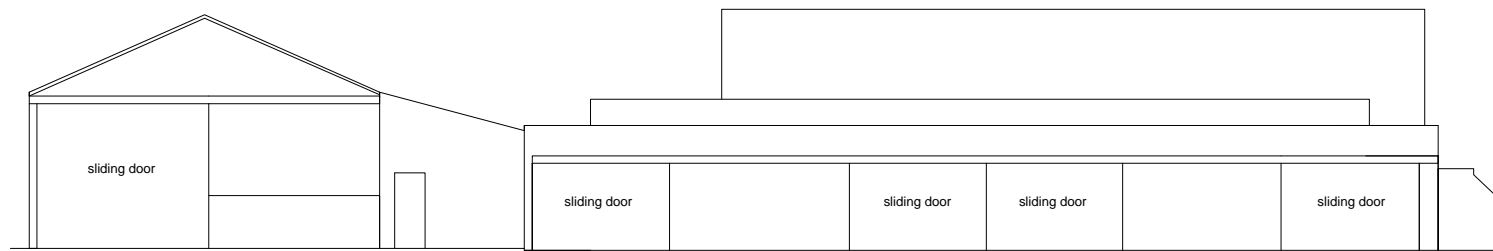
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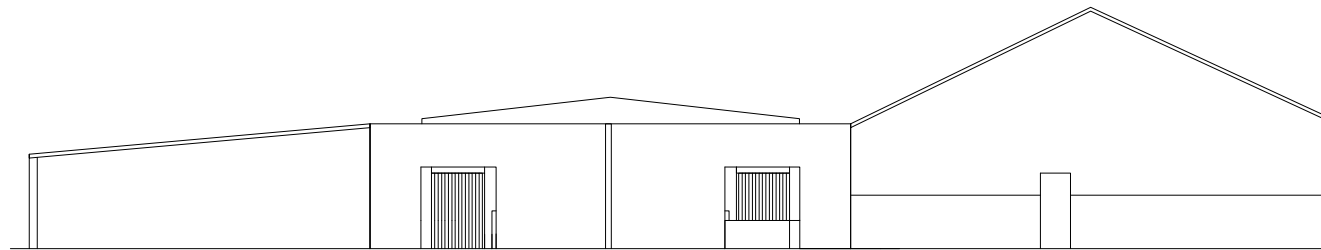


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Existing floor plan

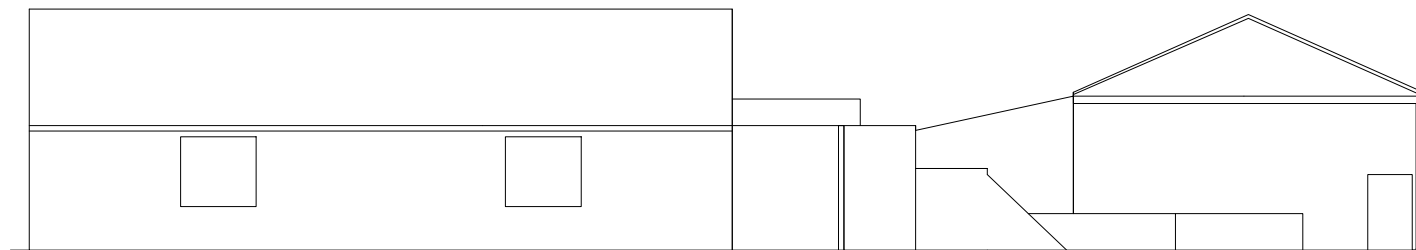




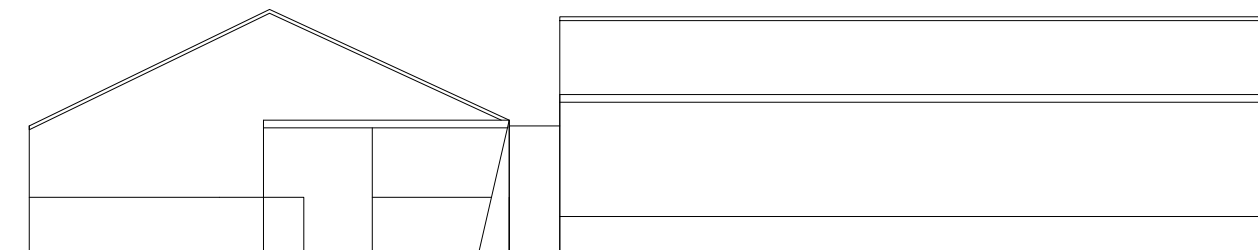
Store 10 / Building 10b & 10e
South-west elevations



Store 10 / Building 10b & 10e
South-east elevations

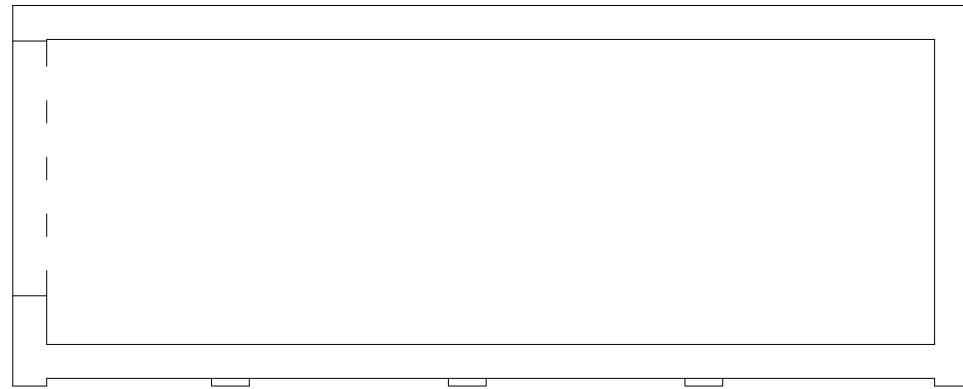


Store 10 / Building 10b & 10e
North-east elevations



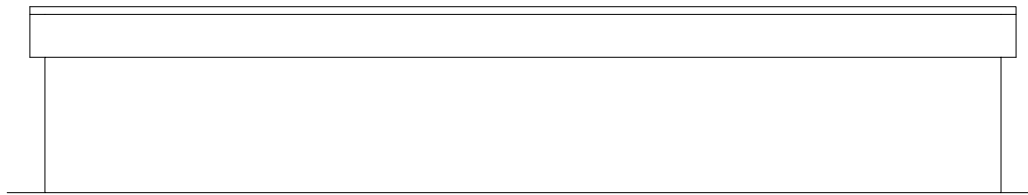
Store 10 / Building 10b & 10e
North-west elevations



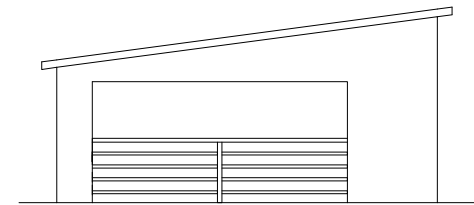


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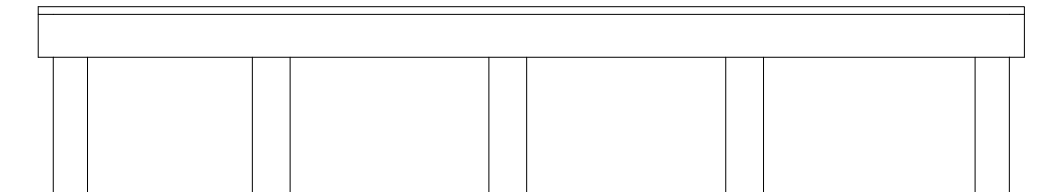
Building 13
Existing Floor Plan



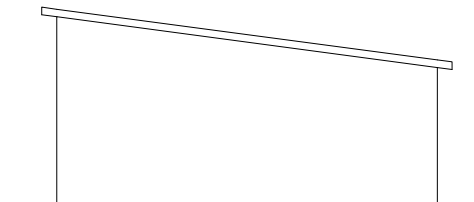
Building 13
North-west elevation



Building 13
South-west elevation



Building 13
South-east elevation



Building 13
North-east elevation



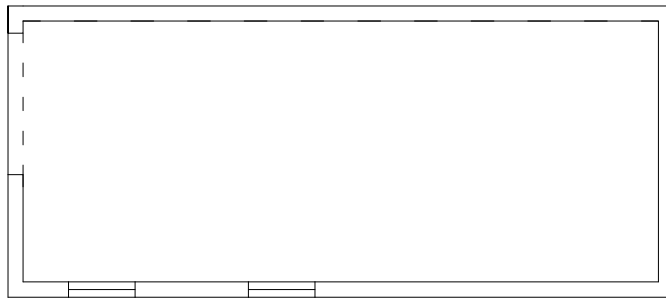
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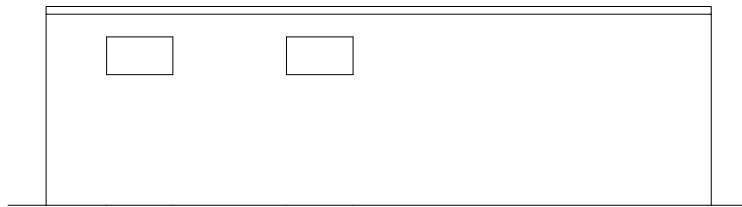
Land at Courtlands Farm, Park Road, Banstead, Surrey

Building 13 - Existing floor plan and elevations (1:100)

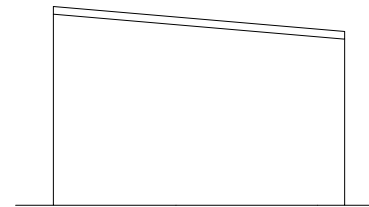
Fig. 25



Building 15
Existing floor plan



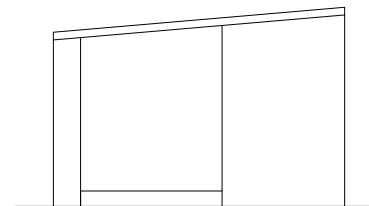
Building 15
South-east elevation



Building 15
North-east elevation

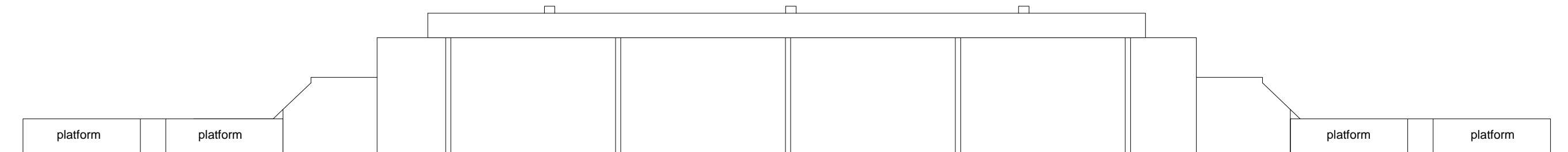


Building 15
North-west elevation

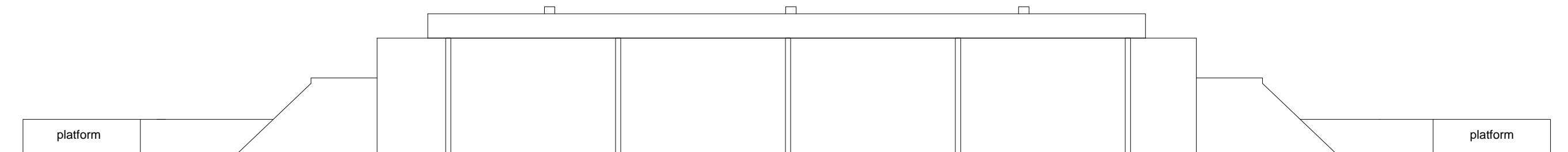


Building 15
South-west elevation

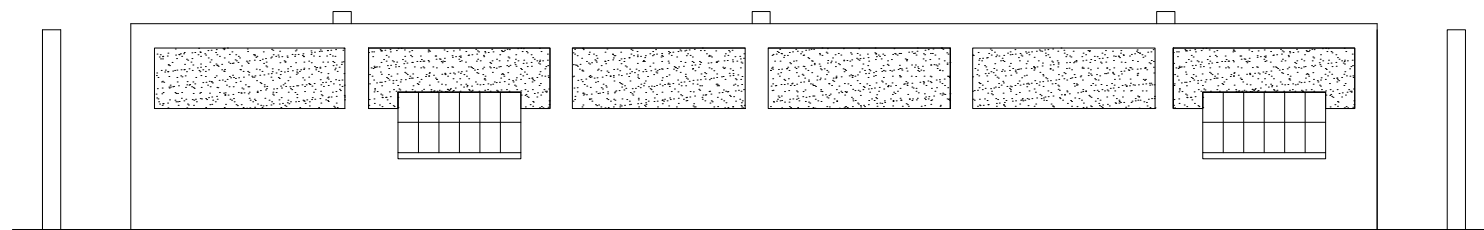




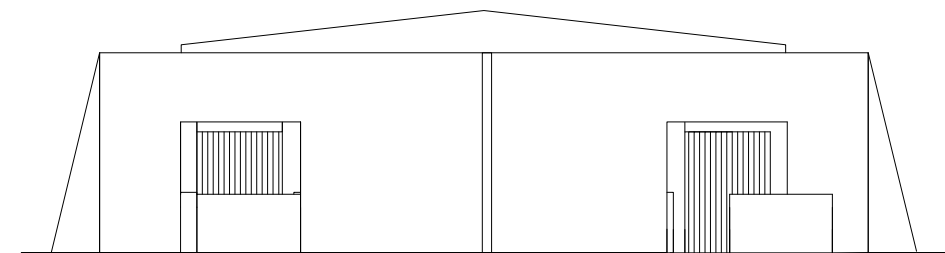
South-west elevation
(external)



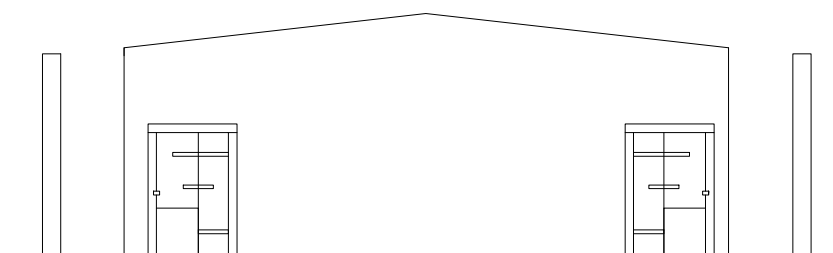
North-east elevation
(external)



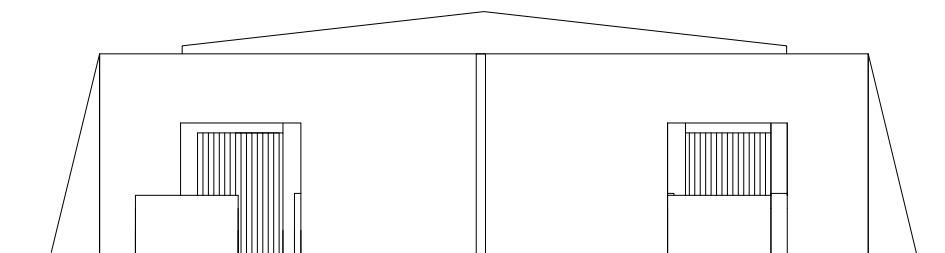
North-east / South-west elevations
(Internal structure)



North-west elevation
(external)

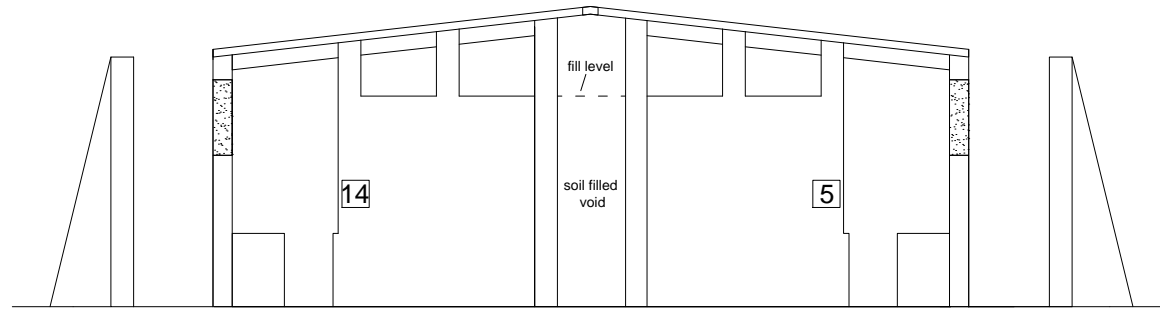


North-west / South-east elevations
(Internal structure)

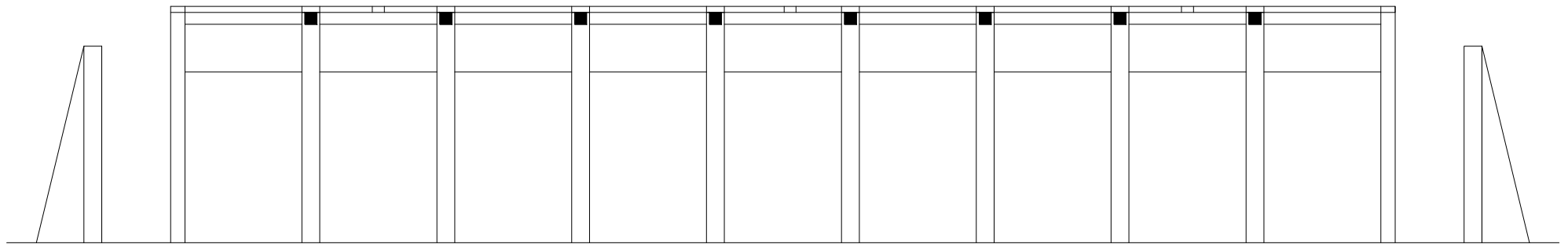


South-east elevation
(external)





Representative cross-section



Representative long-section



© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 28
Project Ref: 171102	January 2018	Representative store sections (1:100)	
Report Ref: 2017537	Drawn by: HS		



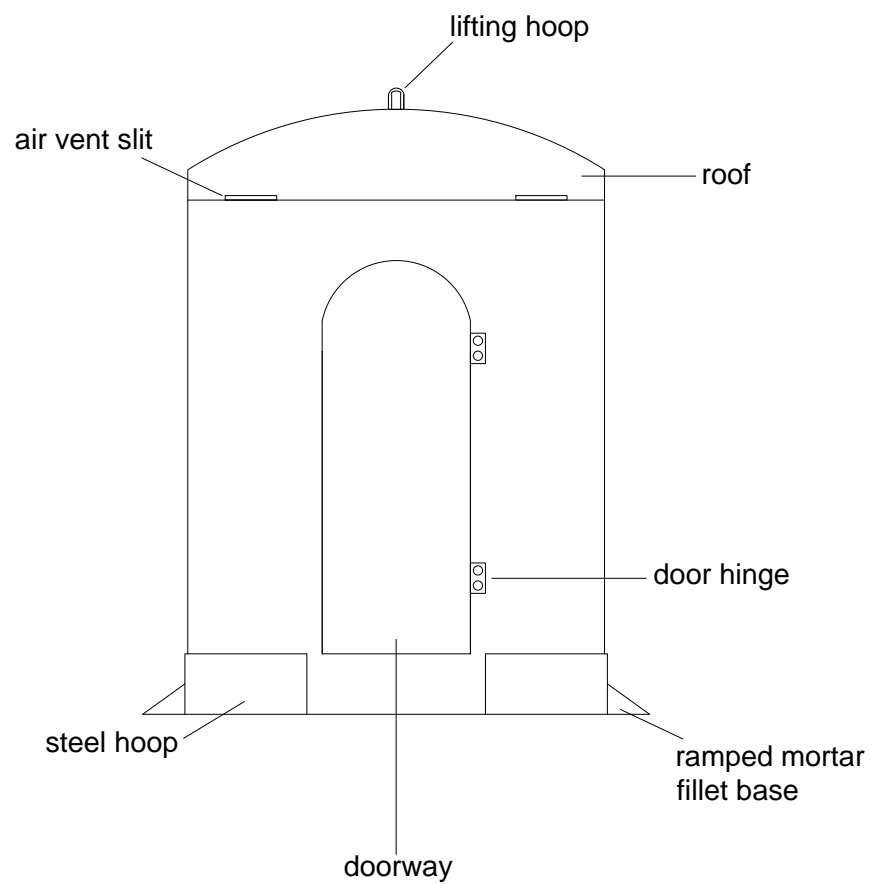
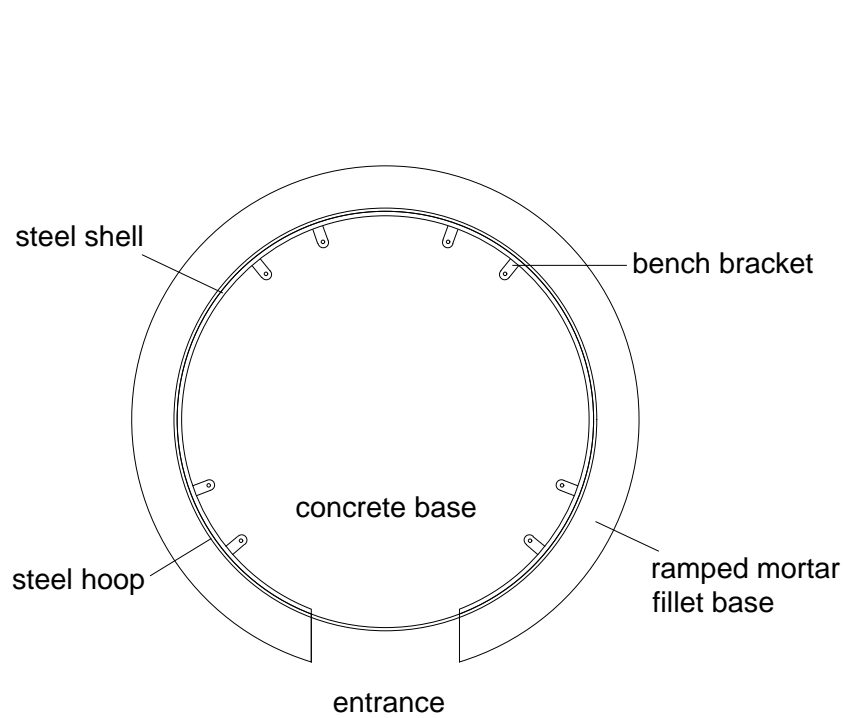
Example of a Consul shelter constructed by a firm called 'Vickers-Armstrong', situated on Walney Island, Burrow-in-Furness (http://www.users.globalnet.co.uk/~rwbarnes/defence/barrow_walney_consol-.htm)



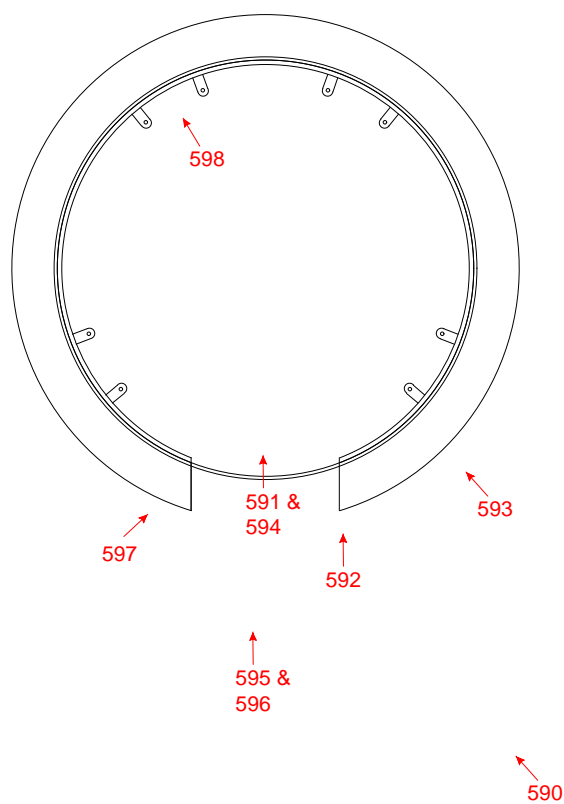
Example of a Consul shelter used as a fire watch shelter, displayed at the Eden Camp, Malton, North Yorkshire (<https://www.militaryimages.net/media/fire-watch-shelter.83535/>)



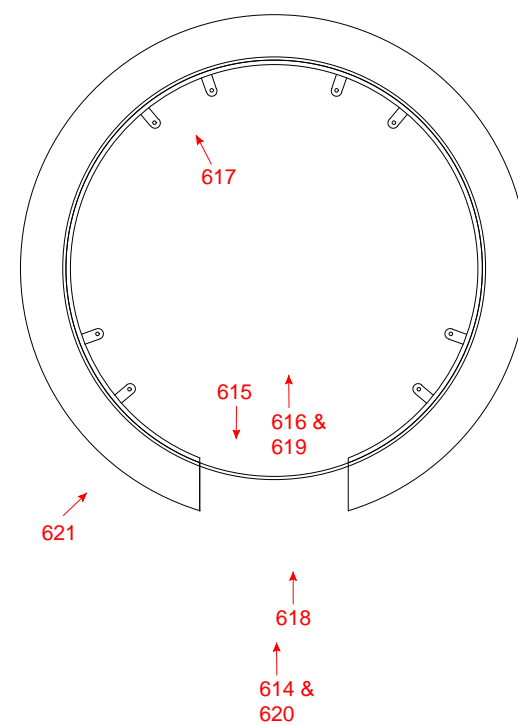
Example of a Consul shelter displayed at the Amberley Working Museum, Amberley, West Sussex (<https://www.tracesofwar.com/sights/15273/One-Person-Air-Raid-Shelter-Amberley-Museum.htm>)

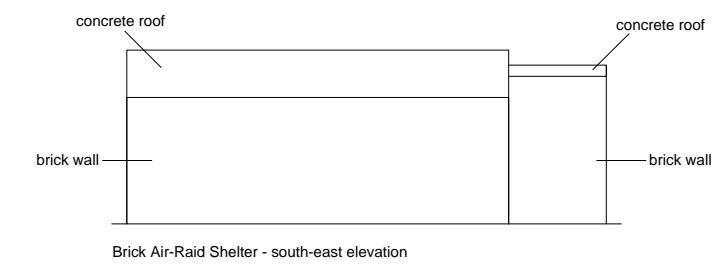
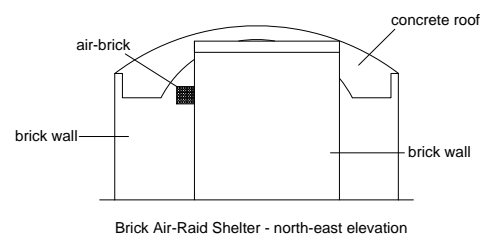
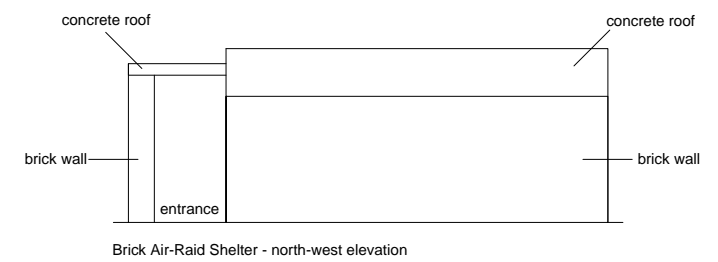
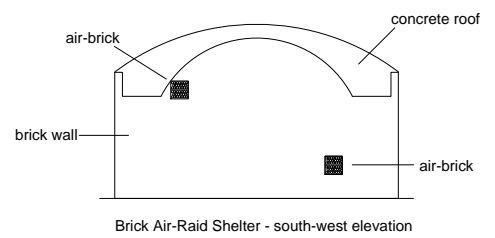
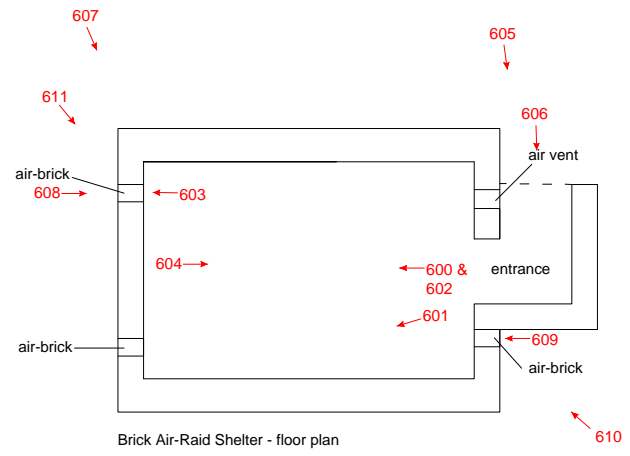


SOUTH-EASTERN SENTRY POST



NORTH-EASTERN SENTRY POST





© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 31
Project Ref: 171102	July 2018	Brick Air-Raid Shelter - Existing floor plan and elevations (1:100)	
Report Ref: 2017537	Drawn by: HS		



© Archaeology South-East		Land at Courtlands Farm, Park Road, Banstead, Surrey	Fig. 32
Project Ref: 171102	July 2018	Aerial photograph of the site as existing (2018)	
Report Ref: 2017537	Drawn by: HS		

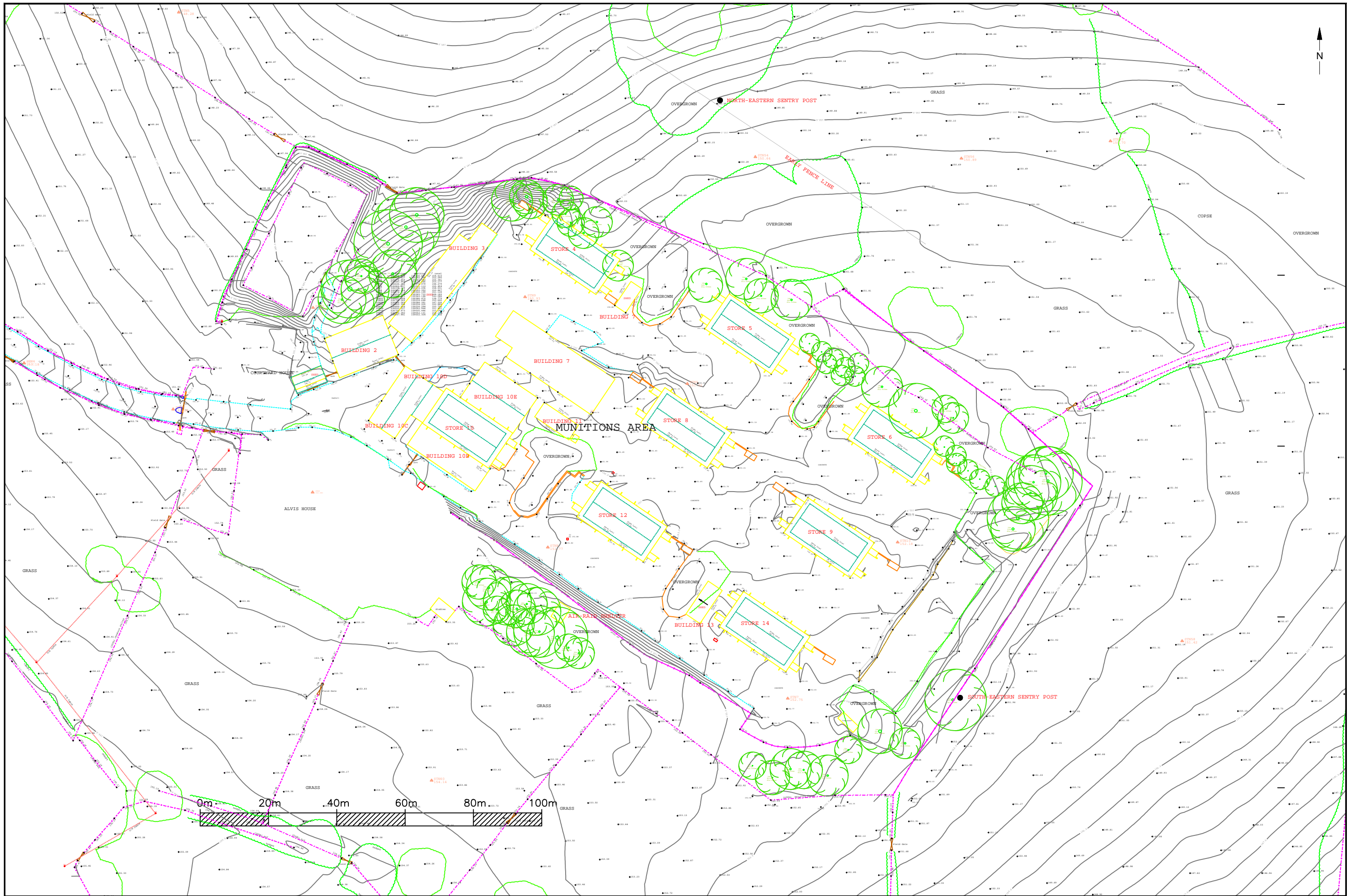




Plate 1: View of original perimeter fence line along the north-east site boundary, facing north (613)



Plate 2: Detail of an original perimeter fence bracket along the site's north-eastern boundary, facing east (612)



Plate 3: Detail of an original perimeter fence bracket along the site's north-eastern boundary, facing north-east (622)



Plate 4: Detail of the later 20th century perimeter fence defining the modern site compound, facing west along the south-west boundary (599)



Plate 5: Store 4 – View of the south-west and south-east elevations, facing north-east (229)



Plate 6: Store 4 – View of the open walkway separating the internal store structure from the outer blast walls, facing south-east (50)



Plate 7: Store 4 – View of the open walkway separating the north-east elevation of the store from the outer blast wall, facing north (81)



Plate 8: Store 4 - View of the earth blast barrier banking the north-east elevation of the outer blast wall, facing north (73)



Plate 9: Store 4 – View of the north-west and south-west elevation of the outer blast wall, facing south-east (233)



Plate 10: Store 4 – View of the northern loading bay, note the removed western platform, facing south-east (235)



Plate 11: Store 4 – Detail of the concrete buttresses supporting each outer blast barrier wall, facing south-east (135)



Plate 12: Store 4 – Detail of the covered entrance providing access into the walkway perimeter surrounding the inner structure, facing south-east (237)



Plate 13: Store 4 – Detail of the integral locking bar for the gates within each covered entrance, facing east (334)



Plate 14: Store 4 – Detail of the Revo light fitting serving each covered entrance, facing west (64)



Plate 15: Store 4 – View of the northern loading bay entrance and conveyor base, serving the eastern half of the store, facing south-east (55)



Plate 16: Store 4 – View of the southern loading bay serving the eastern half of the structure, note the complete nature of the bases and end platform (with brackets), facing south-east (277)



Plate 17: Store 4 / Building 15 – View of the southern loading platform serving the western half of the store, note the lamp post detailing, facing east (282)



Plate 18: Store 4 – View of the inner walkway separating the north-east elevation of the inner structure and outer blast wall, facing south-east (270)



Plate 19: Store 4 – Detail of the roof and walkway separating the inner structure and outer blast walls, facing west (74)



Plate 20: Store 4 – Detail of the current window detailing within the southern end of the store's south-west elevation, facing south-west (248)



Plate 21: Store 4 – Detail of the blocked opening within the store's south-west elevations, facing south-east (254)



Plate 22: Store 4 – Detail of the internal door and window arrangement within each store (western door to southern loading bay), facing south-east (258)



Plate 23: Store 4- Detail of the roof arrangement and central vents, facing east (187)



Plate 24: Store 4 – View of the light fittings serving each loading bay (south-east elevation), facing north-east (299)



Plate 25: Store 4 – Detail of the doorways serving each store (eastern doorway serving the southern loading bay), facing north-west (60)



Plate 26: Store 4 – Detail of the internal door face serving the inner structure of each store (western doorway serving the southern loading bay), facing south-east (217)



Plate 27: Store 4 – Detail of the internal door face serving the inner structure of each store (eastern doorway serving the northern loading bay), facing south-west (43)



Plate 28: Store 4 – Padlock hook located adjacent to each doorway (south-east elevation), facing west (220)



Plate 29: Store 4 – Detail of the 'Vent Class A' stencilling found on the external face of each store door (western door serving the northern loading bay), facing south-east (266)



Plate 30: Store 4 – Internal view of the western passage and storage bays, facing north-west (1)



Plate 31: Store 4 – View of a typical bay arrangement (western half), facing north-east (264)



Plate 32: Store 4 – View of the roof arrangement above each bay (western half), facing north-east (196)



Plate 33: Store 4 – View of the store's roof construction above the western passage, facing south-east (263)



Plate 34: Store 4 – Detail of the storage bay signage, note the phasing (western half), facing south-east (257)



Plate 35: Store 4 – Detail of the floor sockets within each bay (eastern half), facing south (261)



Plate 36: Store 4 – Detail of the floor sockets within each bay (eastern half), facing south-west (251)



Plate 37: Store 4 – View of the store's western passage and bay divisions, facing south-east (245)



Plate 38: Store 4 – Detail of the roof and light detailing above each bay (eastern half), facing south-west (249)



Plate 39: Store 4 – Detail of the electric fuse box found within Bins 1 and 10 within each store (eastern half), facing north-west (30)



Plate 40: Store 4 – Detail of one of the removed conveyor bases within the western passage, facing west (250)



Plate 41: Store 4 – View of the western loading platform serving the southern loading bay, facing north (230)



Plate 42: Store 4 – Detail of the western loading platform serving the southern loading bay, facing north (86)



Plate 43: Store 4 – Detail of the displaced light fitting formerly situated on the eastern loading platform serving the southern loading bay, facing east (285)



Plate 44: Store 4 – Detail of the eastern loading platform serving the northern loading bay, note retained conveyor base, facing north (281)



Plate 45: Store 4 – Detail of the conveyor system brackets on the western loading platform serving the southern loading bay, facing north (283)



Plate 46: Store 4 – Detail of the retained conveyor bases within the southern loading bay (eastern side), note the metal brackets, facing north-east (66)



Plate 47: Store 4 – Detail of the painted marking on the western door serving the northern loading bay, facing south-east (265)



Plate 48: Store 4 – Later 20th century alterations to the south-east walkway, comprising low level concrete block walls, facing south-west (11)



Plate 49: Store 4 – Detail of the continuous cill running the length of the store's south-west elevation, facing south-east (241)



Plate 50: Store 4 – Missing window casements within the northern window in the eastern passage, facing north-east (201)



Plate 51: Store 4 – Displaced window casements within Bin 1 (eastern half), facing north-west (274)



Plate 52: Store 4 – View of the internal bay signage and floor sockets (western half), facing north (247)



Plate 53: Store 4 – Bay signage detail, note the phasing (eastern half), facing north-west (280)



Plate 54: Store 5 – View of the north-west and south-west elevations of the outer blast wall, note the retained earth barrier along the north-west elevation, facing south-east (290)



Plate 55: Store 5 – View of the south-west and south-east elevations of the outer blast wall, facing north (294)



Plate 56: Bund separating Stores 4 and 5, facing north-west (289)



Plate 57: Store 5 – View of the south-west elevation, facing north-east (293)



Plate 58: Store 5 – Light fixture retained on the eastern loading platform serving the northern bay, facing north-east (291)



Plate 59: Store 5 – Detail of the conveyor base retained within the western doorway serving the northern loading bay, note the brackets, facing north-east (311)



Plate 60: Store 5 – Detail of the 'R' signage on the western door serving the northern loading bay, facing north-east (312)



Plate 61: Store 5 – Detail of the 'H' signage on the western door serving the southern loading bay, facing south-east (318)



Plate 62: Store 5 – View of the internal western passage, note the high-set openings within the south-west elevation, facing south-east (317)



Plate 63: Store 5 – View of the western walkway, note the absence of a cill and high-set openings, facing north-west (297)



Plate 64: Store 5 – View of the western passage, facing north-west (301)



Plate 65: Store 5 – View of the western passage and bay arrangement, facing south-east (314)



Plate 66: Store 5 – Detail of the inserted stud framework used to create lower ceilings in a number of the bays (Bin 10), facing north-east (308)



Plate 67: Store 5 – Detail of the metal brackets framing the containment wall of Bin 6, facing east (305)



Plate 68: Store 5 – Soot staining left on the walls of Bin 4, facing west (322)



Plate 69: Store 5 –Detail of the phased bay signage visible throughout the structure (Bin 17), facing east (303)



Plate 70: Store 5 – Detail of the paintwork visible at the southern end of the eastern passage (north-east wall), facing north-east (320)



Plate 71: Store 5 – Detail of the degraded timber notice board within the northern end of the western passage, facing west (306)



Plate 72: Store 5 – Detail of the brackets formerly securing the timber notice board within the northern end of the western passage, facing south-west (313)



Plate 73: Store 6 – General view of the north-west and south-west elevations, facing east (328)



Plate 74: Store 6 – View of the bund separating Stores 5 and 6, facing north-east (325)



Plate 75: Store 6 – View of the northern loading bay, facing north-east (327)



Plate 76: Store 6 – Detail of the removed loading platform to the western side of the northern bay, facing east (328)



Plate 77: Store 6 – Detail of the retained footings of a former conveyor base situated in the western passage, facing south (346)



Plate 78: Store 6 – View of the eastern platform serving the southern loading bay, facing east (333)



Plate 79: Store 6 – Detail of the doorway complete with conveyor base, set within the northern end of the western passage, facing south-east (351)



Plate 80: Store 6 – Detail of the blocked windows visible within the north-east elevation, facing north-west (359)



Plate 81: Store 6 – Evidence of the blocked window opening within the store's north-east wall, facing east (357)



Plate 82: Store 6 – Detail of the drainage channels built into the inner walkway (south-east elevation), facing south (347)



Plate 83: Store 6 – View of the western passage, facing north-west (338)



Plate 84: Store 6 – View of the eastern passage, facing north-west (349)



Plate 85: Store 6 – View of Bin 3, facing south (353)



Plate 86: Store 6 – Detail of the phased bay signage visible throughout the store (Bin 18), facing south-east (339)



Plate 87: Store 6 – Detail of the signage visible on a number of the containment bay walls (Bin 9), facing north-west (340)



Plate 88: Store 6 – Detail of the retained timber notice board within the northern end of the western passage, entitled 'Fire Orders', facing south-west (341)



Plate 89: Store 6 – Detail of the retained timber notice board within the northern end of the western passage, entitled 'Fire Orders', facing south-west (342)



Plate 90: View of the area formerly occupied by the bund separating Stores 8 and 9 (removed after 1990), facing north-east (412)



Plate 91: Store 8 – View of the north-west and south-west elevations, facing east (419)



Plate 92: Store 8 – View of the southern loading bay, facing north (411)



Plate 93: Store 8 – View of the northern loading bay, facing south-east (420)



Plate 94: Store 8 – Detail of a retained conveyor base serving the eastern side of the southern loading platform, note the bracket fixing, facing south-east (414)



Plate 95: Store 8 – Detail of the conveyor bases retained within the western entrance serving the southern loading bay, facing north-west (409)



Plate 96: Store 8 – View of the eastern platform serving the southern loading bay, note the brackets and lamp fixture, facing west (413)



Plate 97: Store 8 – Detail of the fire hydrant signage visible on the south-west wall of the western entrance serving the southern loading bay, facing west (416)



Plate 98: Store 9 – Detail of the girder situated adjacent to the western entrance of the southern loading bay, facing north-east (418)



Plate 99: Store 8 – View of the western passage and bay arrangement, facing south-east (426)



Plate 100: Store 8 – View of the western passage, facing north-west (434)



Plate 101: Store 8 – View of the eastern passage, facing south-east (437)



Plate 102: View of the eastern passage and bay arrangement, facing north-west (440)



Plate 103: Store 8 – Detail of the fire damaged south-west wall within the western passage, note the concrete blocks used to fill the former openings, facing south (431)



Plate 104: Store 8 – Detail of the bay divisions, note the absence of floor sockets, facing west (442)



Plate 105: Store 8 – Detail of the fire damaged timber notice board within Bin 15 (western half), note the 'Fire Orders' title still visible, facing north (432)



Plate 106: Store 8 – Detail of the degraded signage to the north of the former location of the notice board, upon the store's south-western elevation, facing west (441)



Plate 107: Store 8 – View of the bay roof arrangement and external vents situated above the central void, facing south-west (439)



Plate 108: Store 9 – View of the south-east and north-east elevations, facing west (360)



Plate 109: Store 9 – View of the south-east and south-west elevations, facing north (364)



Plate 110: Store 9 – View of the south-west elevation, facing north-east (399)



Plate 111: Store 9 – View of the northern loading bay, facing south-east (400)



Plate 112: Store 9 – View of the southern loading bay, facing north-west (370)



Plate 113: Store 9 – Detail of the retained lamp post upon the eastern platform in the southern loading bay, facing east (371)



Plate 114: Store 9 – Detail of the retained lamp post on the eastern platform of the southern loading bay, facing east (373)



Plate 115: Store 9 – Detail of the displaced girder within the southern loading bay, facing south-east (372)



Plate 116: Store 9 – Retained conveyor track on the eastern platform within the northern loading bay, facing north (406)



Plate 117: Store 9 – Detail of the retained conveyor base situated within the eastern door serving the northern loading bay, facing north-west (396)



Plate 118: Store 9 – Detail of the fire hydrant signage found on the south-west wall of the western entrance serving the southern loading bay, facing north-west (377)



Plate 119: Store 9 – Detail of the fire hydrant signage found on the south-west wall of the western entrance serving the southern loading bay, facing south-west (376)



Plate 120: Store 9 – View of the western walkway, facing north-west (379)



Plate 121: Store 9 – View of the western passage, facing north-west (381)



Plate 122: Store 9 – View of the western passage, facing south-east (390)



Plate 123: Store 9 – View of the eastern passage, facing north-west (393)



Plate 124: Store 9 – View of the eastern passage, facing south-east (397)



Plate 125 Store 9 – Detail of the bay signage within the eastern half of the store, facing north-west (395)



Plate 126: Store 9 – View of the bay arrangement (Bin 9), facing west (394)



Plate 127: Store 9 – Detail of the painted floor bay divisions (western half), facing north (383)



Plate 128: Store 9 – Detail of the timber notice board, entitled 'Regulations', situated at the northern end of the western passage, facing north-west (384)



Plate 129: Store 9 – Detail of the timber notice board, entitled 'Regulations', situated at the northern end of the western passage, facing north-west (389)



Plate 130: Store 9 – Detail of the timber notice board, situated at the northern end of the western passage, facing south-west (385)



Plate 131: Store 9 – Detail of the timber notice board, situated at the northern end of the western passage, facing south-west (386)



Plate 132: Store 9 – Detail of the timber notice board, situated at the northern end of the western passage, facing south-west (388)



Plate 133: Store 10 – View of the south-east elevation, facing north (8)



Plate 134: Store 10 – View of the northern end of the north-east elevation, note the encroachment of later 20th century structures on all sides, facing south (562)



Plate 135: View of the bund separating Stores 10 and 12, facing east (514)



Plate 136: Store 10 – Detail of the eastern doorway serving the northern loading bay, facing south-east (565)



Plate 137: Store 10 – View of the western passage, facing north-west (522)



Plate 138: Store 10 –View of the eastern passage, facing south-east (528)



Plate 139: Store 10 – Detail of the clear bay signage and white-washed walls within the eastern passage, facing north-west (526)



Plate 140: Store 10 – View of the internal bay arrangement (eastern half), facing south-west (523)



Plate 141: Store 10 – Detail of the retained light fitting within Bin 9, facing south-west (529)



Plate 142: Store 12 – View of the north-east and south-east elevations, facing west (445)



Plate 143: Store 12 – View of the south-east and south-west elevations, facing north (487)



Plate 144: Store 12 – View of the northern loading bay, facing north-east (505)



Plate 145: Store 12 – Detail of the retained lamp fitting upon the eastern platform in the northern loading bay, facing north-east (508)



Plate 146: Store 12 – Detail of the lamp fitting serving the western entrance of the southern loading bay, facing south-west (500)



Plate 147: Store 12 – Detail of the fire hydrant signage situated on the south-west wall of the western entrance serving the southern loading bay, facing west (492)



Plate 148: Store 12 – Detail of the door signage retained on the eastern doorway serving the southern loading bay, facing north-west (507)



Plate 149: Store 12 – Detail of the door signage retained on the western doorway serving the northern loading bay, facing north-west (510)



Plate 150: Store 12 – Detail of the door hook found on all doors within this store (western door serving the southern loading bay), facing north-west (495)



Plate 151: Store 12 – View of the western walkway, note the removal of the window from the south-west elevation of the inner structure, facing north-west (494)



Plate 152: Store 12 – View of the western passage, facing north-west (497)



Plate 153: Store 12 – View of the western passage, facing north-west (498)



Plate 154: Store 14 – View of the north-east and south-east elevations, facing west (453)



Plate 155: Store 14 / Building 13 – View of the northern loading bay, note the retained earth blast barrier, facing north-east (483)



Plate 156: Store 14 – View of the southern loading bay, facing north-west (454)



Plate 157: View of the bund separating Store 12 and 14, facing east (485)



Plate 158: Store 14 – View of the eastern entrance serving the northern loading bay, note the retained bund walls from the removed bund separating Store 8 and 9, facing south-west (448)



Plate 159: Store 14 – Detail of the eastern door signage serving the southern loading bay, facing north-west (458)



Plate 160: Store 14 – Detail of the fire hydrant signage on the south-west wall of the western entrance serving the southern loading bay, facing west (476)



Plate 161: Store 12 – View of the eastern passage, facing south-east (470)



Plate 162: Store 14 – View of the western passage, facing north-west (479)



Plate 163: Store 14 – View of the alterations made within each of the eastern bays to accommodate the store's later use as a piggery, facing south-west (469)



Plate 164: Store 14 – Detail of the troughs situated within each of the eastern bays, facing south (465)



Plate 165: Store 14 – Detail of the brackets fitted to the end of each of the eastern bay containment end walls, facing south-west (467)



Plate 166: Store 14 – Detail of one of the retained light fittings within a number of the bays (eastern half), facing south (463)



Plate 167: Detail of the feature situated along the site's south-eastern end, aligned south-west – north-east, facing south-west (366)



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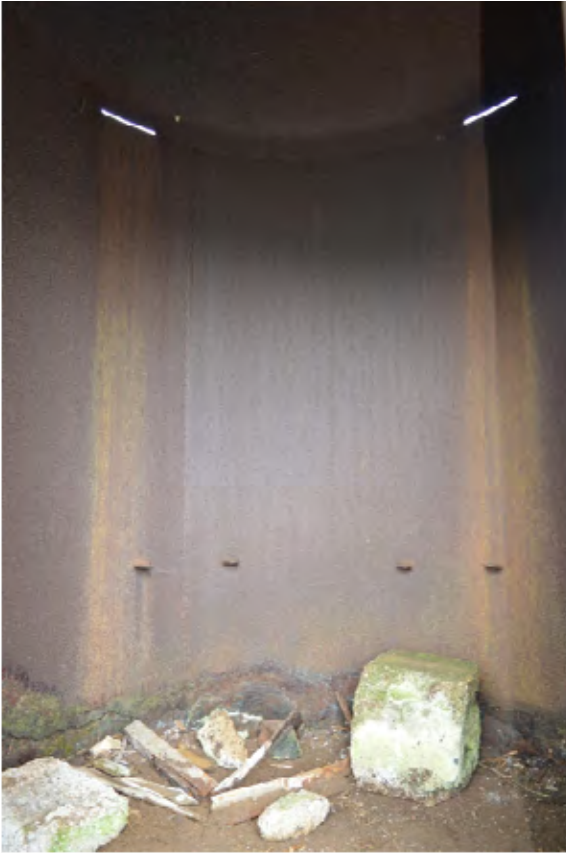


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Appendix 1: OASIS Form

OASIS ID: ARCHAEO6-323101

Project details

Project name LAND AT COURTLANDS FARM, PARK ROAD, BANSTEAD, SURREY, SM7 3EF

Short description of the project Between December 2017 and July 2018 Archaeology South-East (a division of the Centre for Applied Archaeology, UCL) carried out a historic buildings record of Courtlands Farm, Park Road, Banstead, Surrey, SM7 3EF. The work was commissioned by HA and DB Kitchin Ltd and requested by Reigate and Banstead Borough Council, to be addressed as a condition placed on planning consent relating to the removal of hardstanding and buildings and the erection of nine detached houses and retained 'bunker' with associated access, parking and landscaping (planning ref. 16/01013/F). The site identified as Equipment Ammunition Depot Magazine (EAM) No. 116 was built as part of the countrywide anti-aircraft defence preparation works established in the lead-up to the Second World War. The ammunition depot at Banstead formed one of three depots built between 1938 and 1939 to store and supply ammunition for the anti-aircraft batteries surrounding and defending London during war time, including the Battle of Britain, which was the first major Nazi defeat and a crucial turning point in the conflict, and the Blitz. The Banstead site supplied the South West London sector from Dulwich to Raynes Park and included the South East region for a short period in 1940. The site officially closed as an active storage / distribution centre in 1953 and was auctioned-off in 1966. After, it was utilised for agricultural, storage and workshop use. The site is now redundant of use.

Project dates Start: 11-12-2017 End: 20-07-2018

Previous/future work Yes / No

Any associated project reference codes 171102 - Contracting Unit No.

Any associated project reference codes CFB17 - Sitecode

Type of project Building Recording

Site status Local Authority Designated Archaeological Area

Monument type WWII AMMUNITION STORE Modern

Significant Finds NONE None

Project location

Country England

Site location SURREY REIGATE AND BANSTEAD BANSTEAD SURREY REIGATE AND BANSTEAD BANSTEAD Courtlands Farm, Park Road, Banstead, Surrey, SM7 3EF

Postcode SM7 3EF

Study area 18.21 Hectares

Site coordinates 526799 159153 526799 00 00 N 159153 00 00 E Point

Project creators

Name of Organisation Archaeology South-East

Project brief originator HA and DB Kitchin Ltd

Project design originator Archaeology South-East

Project supervisor Hannah Samuels
Type of sponsor/funding body HA and DB Kitchin Ltd

Project archives

Physical Archive Exists? No
Digital Archive recipient CFB17
Digital Contents "none"
Digital Media available "Text","Images raster / digital photography"
Paper Archive recipient CFB17
Paper Contents "none"
Paper Media available "Aerial Photograph","Correspondence","Notebook - Excavation',' Research',' General Notes","Plan","Report","Section","Unpublished Text"

Project bibliography 1

Publication type Grey literature (unpublished document/manuscript)
Title LAND AT COURTLANDS FARM, PARK ROAD, BANSTEAD, SURREY, SM7 3EF - HISTORIC BUILDING RECORD LEVEL 4
Author(s)/Editor(s) Samuels, H.
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Place of issue or publication Archaeology South-East
Entered by Hannah Samuels (hannah.green@ucl.ac.uk)
Entered on 20 July 2018

Appendix 2 Index of Digital Photographs



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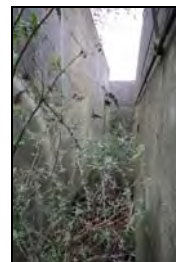
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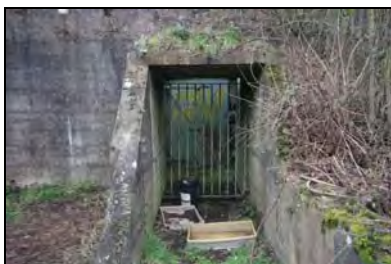
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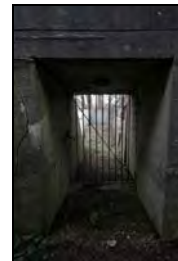
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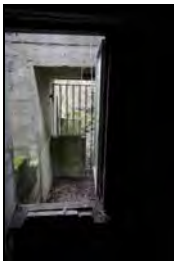
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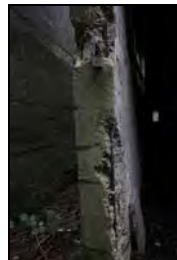
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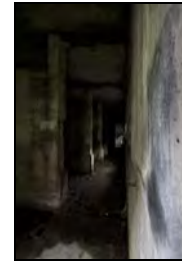
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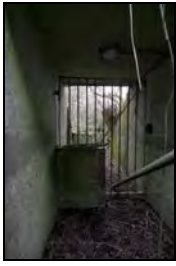
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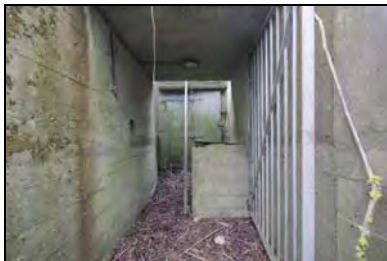
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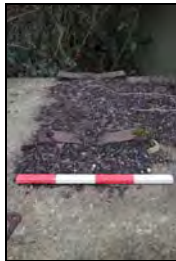
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Bund separating Store 4 and 5 (2015)



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Bund separating Store 4 and 5 (2015)



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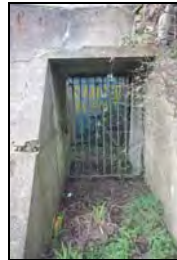
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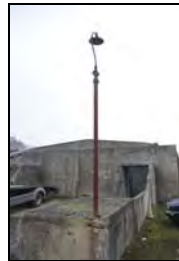
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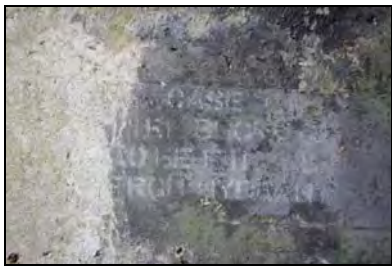
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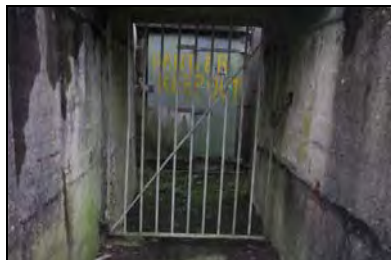
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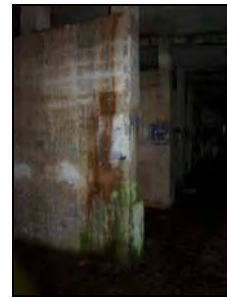
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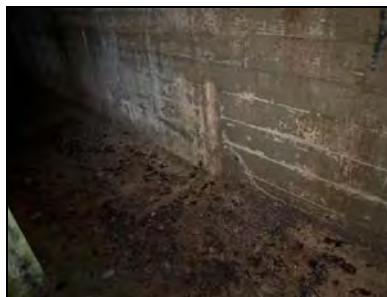
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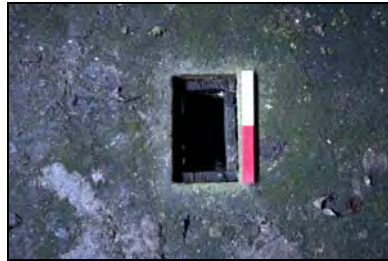
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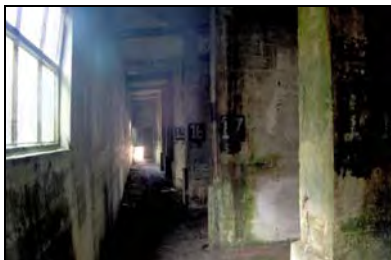
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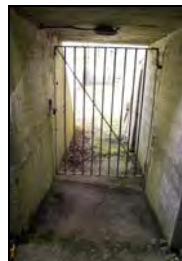
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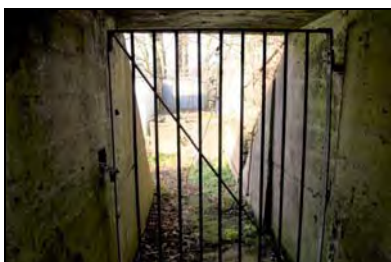
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CFB17-0267
Store 4 (2017)



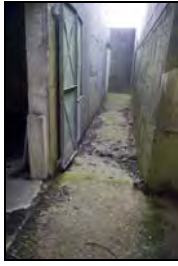
CFB17-0268
Store 4 (2017)



CFB17-0269
Store 4 (2017)



CFB17-0270
Store 4 (2017)



CFB17-0271
Store 4 (2017)



CFB17-0272
Store 4 (2017)



CFB17-0273
Store 4 (2017)



CFB17-0274
Store 4 (2017)



CFB17-0275
Store 4 (2017)



CFB17-0276
Store 4 (2017)



CFB17-0277
Store 4 (2017)



CFB17-0278
Store 4 (2017)



CFB17-0279
Store 4 (2017)



CFB17-0280
Store 4 (2017)



CFB17-0281
Store 4 (2017)



CFB17-0282
Store 4 / Building 15 (2017)



CFB17-0283
Store 4 (2017)



CFB17-0284
Store 4 (2017)



CFB17-0285
Store 4 (2017)



CFB17-0286
Store 4 (2017)



CFB17-0287
Store 4 (2017)



CFB17-0288
Bund separating Stores 4 and 5 (2017)



CFB17-0289
Bund separating Stores 4 and 5 (2017)



CFB17-0290
Store 5 (2017)



CFB17-0291
Store 5 (2017)



CFB17-0292
Store 5 (2017)



CFB17-0293
Store 5 (2017)



CFB17-0294
Store 5 (2017)



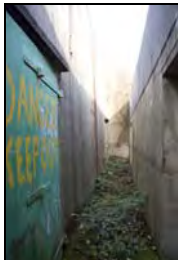
CFB17-0295
Store 5 (2017)



CFB17-0296
Store 5 (2017)



CFB17-0297
Store 5 (2017)



CFB17-0298
Store 5 (2017)



CFB17-0299
Store 5 (2017)



CFB17-0300
Store 5 (2017)



CFB17-0301
Store 5 (2017)



CFB17-0302
Store 5 (2017)



CFB17-0303
Store 5 (2017)



CFB17-0304
Store 5 (2017)



CFB17-0305
Store 5 (2017)



CFB17-0306
Store 5 (2017)



CFB17-0307
Store 5 (2017)



CFB17-0308
Store 5 (2017)



CFB17-0309
Store 5 (2017)



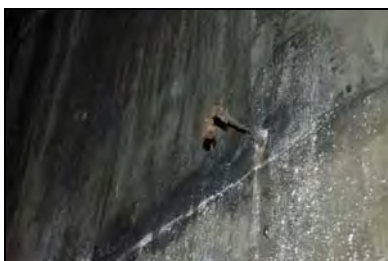
CFB17-0310
Store 5 (2017)



CFB17-0311
Store 5 (2017)



CFB17-0312
Store 5 (2017)



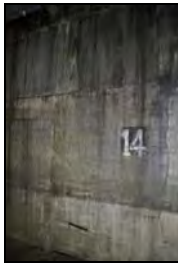
CFB17-0313
Store 5 (2017)



CFB17-0314
Store 5 (2017)



CFB17-0315
Store 5 (2017)



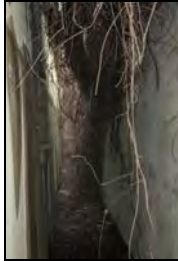
CFB17-0316
Store 5 (2017)



CFB17-0317
Store 5 (2017)



CFB17-0318
Store 5 (2017)



CFB17-0319
Store 5 (2017)



CFB17-0320
Store 5 (2017)



CFB17-0321
Store 5 (2017)



CFB17-0322
Store 5 (2017)



CFB17-0323
Store 5 (2017)



CFB17-0324
Bund separating Store 5 andf 6 (2017)



CFB17-0325
Bund separating Store 5 andf 6 (2017)



CFB17-0326
Stores 8 and 12 (2017)



CFB17-0327
Store 6 (2017)



CFB17-0328
Store 6 (2017)



CFB17-0329
Store 6 (2017)



CFB17-0330
Store 6 (2017)



CFB17-0331
Store 6 (2017)



CFB17-0332
Store 6 (2017)



CFB17-0333
Store 6 (2017)



CFB17-0334
Store 6 (2017)



CFB17-0335
Store 6 (2017)



CFB17-0336
Store 6 (2017)



CFB17-0337
Store 6 (2017)



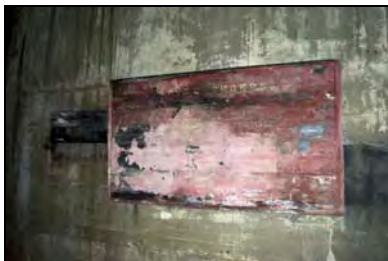
CFB17-0338
Store 6 (2017)



CFB17-0339
Store 6 (2017)



CFB17-0340
Store 6 (2017)



CFB17-0341
Store 6 (2017)



CFB17-0342
Store 6 (2017)



CFB17-0343
Store 6 (2017)



CFB17-0344
Store 6 (2017)



CFB17-0345
Store 6 (2017)



CFB17-0346
Store 6 (2017)



CFB17-0347
Store 6 (2017)



CFB17-0348
Store 6 (2017)



CFB17-0349
Store 6 (2017)



CFB17-0350
Store 6 (2017)



CFB17-0351
Store 6 (2017)



CFB17-0352
Store 6 (2017)



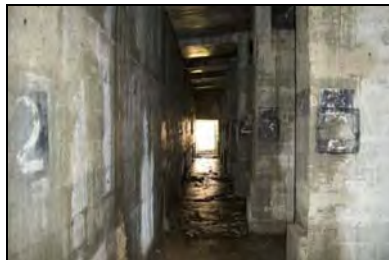
CFB17-0353
Store 6 (2017)



CFB17-0354
Store 6 (2017)



CFB17-0355
Store 6 (2017)



CFB17-0356
Store 6 (2017)



CFB17-0357
Store 6 (2017)



CFB17-0358
Store 6 (2017)



CFB17-0359
Store 6 (2017)



CFB17-0360
Stores 9 and 14 (2017)



CFB17-0361
South-eastern edge of the site (2017)



CFB17-0362
Southern site boundary (2017)



CFB17-0363
Southern site boundary (2017)



CFB17-0364
Stores 9 (foreground) and 8 (2017)



CFB17-0365
Feature along the southern site boundary (2017)



CFB17-0366
Feature along the southern site boundary (2017)



CFB17-0367
Feature along the southern site boundary (2017)



CFB17-0368
Feature along the southern site boundary (2017)



CFB17-0369
Road between Stores 9 (right) and 14 (left) (2017)



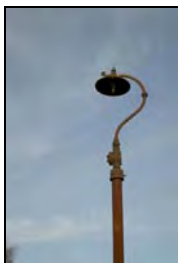
CFB17-0370
Store 9 (2017)



CFB17-0371
Store 9 (2017)



CFB17-0372
Store 9 (2017)



CFB17-0373
Store 9 (2017)



CFB17-0374
Store 9 (2017)



CFB17-0375
Store 9 (2017)



CFB17-0376
Store 9 (2017)



CFB17-0377
Store 9 (2017)



CFB17-0378
Store 9 (2017)



CFB17-0379
Store 9 (2017)



CFB17-0380
Store 9 (2017)



CFB17-0381
Store 9 (2017)



CFB17-0382
Store 9 (2017)



CFB17-0383
Store 9 (2017)



CFB17-0384
Store 9 (2017)



CFB17-0385
Store 9 (2017)



CFB17-0386
Store 9 (2017)



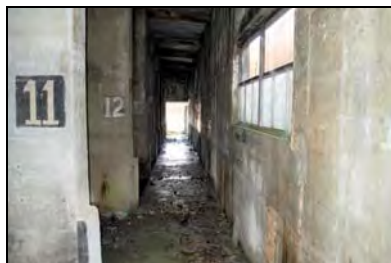
CFB17-0387
Store 9 (2017)



CFB17-0388
Store 9 (2017)



CFB17-0389
Store 9 (2017)



CFB17-0390
Store 9 (2017)



CFB17-0391
Store 9 (2017)



CFB17-0392
Store 9 (2017)



CFB17-0393
Store 9 (2017)



CFB17-0394
Store 9 (2017)



CFB17-0395
Store 9 (2017)



CFB17-0396
Store 9 (2017)



CFB17-0397
Store 9 (2017)



CFB17-0398
Store 9 (2017)



CFB17-0399
Store 9 (2017)



CFB17-0400
Store 9 (2017)



CFB17-0401
Road between Store 8 (right) and Store 12 (left) (2017)



CFB17-0402
Store 8 (2017)



CFB17-0403
Stores 9 (right) and 6 (left) (2017)



CFB17-0404
Store 14 (2017)



CFB17-0405
Store 9 (2017)



CFB17-0406
Store 9 (2017)



CFB17-0407
Store 9 (2017)



CFB17-0408
Store 8 (2017)



CFB17-0409
Store 8 (2017)



CFB17-0410
Store 8 (2017)



CFB17-0411
Store 8 (2017)



CFB17-0412
Area of former bund separating Stores 8 and 9 (removed) (2017)



CFB17-0413
Store 8 (2017)



CFB17-0414
Store 8 (2017)



CFB17-0415
Store 8 (2017)



CFB17-0416
Store 8 (2017)



CFB17-0417
Store 8 (2017)



CFB17-0418
Store 8 (2017)



CFB17-0419
Store 8 (2017)



CFB17-0420
Store 8 (2017)



CFB17-0421
Store 8 (2017)



CFB17-0422
Store 8 (2017)



CFB17-0423
Store 8 (2017)



CFB17-0424
Store 8 (2017)



CFB17-0425
Store 8 (2017)



CFB17-0426
Store 8 (2017)



CFB17-0427
Store 8 (2017)



CFB17-0428
Store 8 (2017)



CFB17-0429
Store 8 (2017)



CFB17-0430
Store 8 (2017)



CFB17-0431
Store 8 (2017)



CFB17-0432
Store 8 (2017)



CFB17-0433
Store 8 (2017)



CFB17-0434
Store 8 (2017)



CFB17-0435
Store 8 (2017)



CFB17-0436
Store 8 (2017)



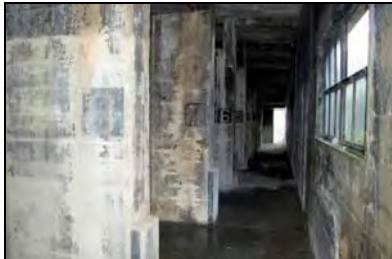
CFB17-0437
Store 8 (2017)



CFB17-0438
Store 8 (2017)



CFB17-0439
Store 8 (2017)



CFB17-0440
Store 8 (2017)



CFB17-0441
Store 8 (2017)



CFB17-0442
Store 8 (2017)



CFB17-0443
Store 12 (2017)



CFB17-0444
Bund separating Store 12 and 10 (2017)



CFB17-0445
Store 10 (2017)



CFB17-0446
Store 10 (2017)



CFB17-0447
Store 14 (2017)



CFB17-0448
Store 14 (2017)



CFB17-0449
Bund separating Stores 14 and 12 (2017)



CFB17-0450
Bund separating Stores 14 and 12 (2017)



CFB17-0451

Bund separating Stores 14 and 12 (2017)



CFB17-0452

Store 14 (2017)



CFB17-0453

Store 14 (2017)



CFB17-0454

Store 14 (2017)



CFB17-0455

Store 14 (2017)



CFB17-0456

Store 14 (2017)



CFB17-0457

Store 14 (2017)



CFB17-0458

Store 14 (2017)



CFB17-0459

Store 14 (2017)



CFB17-0460

Store 14 (2017)



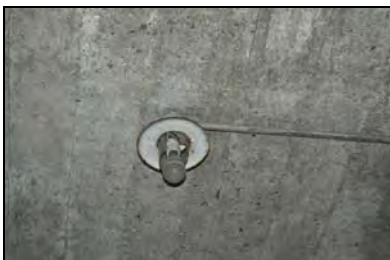
CFB17-0461

Store 14 (2017)



CFB17-0462

Store 14 (2017)



CFB17-0463

Store 14 (2017)



CFB17-0464

Store 14 (2017)



CFB17-0465

Store 14 (2017)



CFB17-0466
Store 14 (2017)



CFB17-0467
Store 14 (2017)



CFB17-0468
Store 14 (2017)



CFB17-0469
Store 14 (2017)



CFB17-0470
Store 14 (2017)



CFB17-0471
Store 14 (2017)



CFB17-0472
Store 14 (2017)



CFB17-0473
Store 14 (2017)



CFB17-0474
Store 14 (2017)



CFB17-0475
Store 14 (2017)



CFB17-0476
Store 14 (2017)



CFB17-0477
Store 14 (2017)



CFB17-0478
Store 14 (2017)



CFB17-0479
Store 14 (2017)



CFB17-0480
Store 14 (2017)



CFB17-0481
*Road to the south of Stores 14
(foreground), 9 and 6 (2017)*



CFB17-0482
Store 14 (2017)



CFB17-0483
Store 14 / Building 13 (2017)



CFB17-0484
Building 13 (2017)



CFB17-0485
Bund separating Store 12 and 14 (2017)



CFB17-0486
Bund separating Store 12 and 14 (2017)



CFB17-0487
Store 12 (2017)



CFB17-0488
Store 12 (2017)



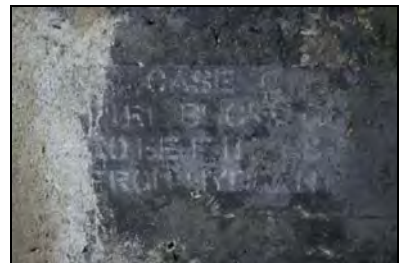
CFB17-0489
Stores 9 (foreground) and 6 (2017)



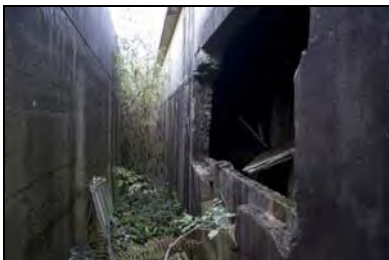
CFB17-0490
*Looking east across the centre of the site
from Store 12 (2017)*



CFB17-0491
Store 12 (2017)



CFB17-0492
Store 12 (2017)



CFB17-0493
Store 12 (2017)



CFB17-0494
Store 12 (2017)



CFB17-0495
Store 12 (2017)



CFB17-0496
Store 12 (2017)



CFB17-0497
Store 12 (2017)



CFB17-0498
Store 12 (2017)



CFB17-0499
Store 12 (2017)



CFB17-0500
Store 12 (2017)



CFB17-0501
Store 12 (2017)



CFB17-0502
Store 12 (2017)



CFB17-0503
Store 12 (2017)



CFB17-0504
Store 12 (2017)



CFB17-0505
Store 12 (2017)



CFB17-0506
Store 12 (2017)



CFB17-0507
Store 12 (2017)



CFB17-0508
Store 12 (2017)



CFB17-0509
Store 12 (2017)



CFB17-0510
Store 12 (2017)



CFB17-0511

Bund separating Store 10 and 12 (2017)



CFB17-0512

Bund separating Store 10 and 12 (2017)



CFB17-0513

Store 10 / Building 11 (2017)



CFB17-0514

Bund separating Store 10 and 12 (2017)



CFB17-0515

Store 10 / Building 10b (2017)



CFB17-0516

Store 10 (2017)



CFB17-0517

Store 10 / Building 10e (2017)



CFB17-0518

Store 10 (2017)



CFB17-0519

Store 10 (2017)



CFB17-0520

Store 10 (2017)



CFB17-0521

Store 10 (2017)



CFB17-0522

Store 10 (2017)



CFB17-0523

Store 10 (2017)



CFB17-0524

Store 10 (2017)



CFB17-0525

Store 10 (2017)



CFB17-0526
Store 10 (2017)



CFB17-0527
Store 10 (2017)



CFB17-0528
Store 10 (2017)



CFB17-0529
Store 10 (2017)



CFB17-0530
Store 10 (2017)



CFB17-0531
Buildings 10b and 10c (2017)



CFB17-0532
Store 10 / Buildings 10b (2017)



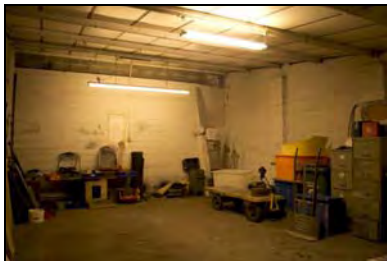
CFB17-0533
Store 10 / Buildings 10b (2017)



CFB17-0534
Buildings 10b and 10c (2017)



CFB17-0535
Building 10b (2017)



CFB17-0536
Building 10b (2017)



CFB17-0537
Building 10b (2017)



CFB17-0538
Building 10b (2017)



CFB17-0539
Building 10b (2017)



CFB17-0540
Building 10b (2017)



CFB17-0541
Building 10b (2017)



CFB17-0542
Building 10c / 10d (2017)



CFB17-0543
Building 1 (2017)



CFB17-0544
Building 10c / 10d (2017)



CFB17-0545
Building 2 (2017)



CFB17-0546
Building 2 (2017)



CFB17-0547
Building 2 (2017)



CFB17-0548
Building 3 (2017)



CFB17-0549
Building 3 (2017)



CFB17-0550
Building 3 (2017)



CFB17-0551
Building 2 (2017)



CFB17-0552
Building 2 (2017)



CFB17-0553
Buildings 2 and 3 (2017)



CFB17-0554
Building 3 (2017)



CFB17-0555
Building 3 (2017)



CFB17-0556
Building 3 (2017)



CFB17-0557
Building 3 (2017)



CFB17-0558
Building 3 (2017)



CFB17-0559
Buildings 7 (foreground) and 10e (2017)



CFB17-0560
Store 10 (centre) / Buildings 10d (right) and 10e (left) (2017)



CFB17-0561
Store 10 / Building 10e (left) (2017)



CFB17-0562
Store 10 (centre) / Buildings 10d (right) and 10e (left) (2017)



CFB17-0563
Buildings 10c and 10d (2017)



CFB17-0564
Store 10 (2017)



CFB17-0565
Store 10 (2017)



CFB17-0566
Building 10e (2017)



CFB17-0567
Building 10e (2017)



CFB17-0568
Building 7 (2017)



CFB17-0569
Building 10e (2017)



CFB17-0570
Building 10e (2017)



CFB17-0571
Building 11 (2017)



CFB17-0572
Building 11 (2017)



CFB17-0573
Building 7 (2017)



CFB17-0574
Building 7 (2017)



CFB17-0575
Building 7 (2017)



CFB17-0576
Building 7 (2017)



CFB17-0577
Building 7 (2017)



CFB17-0578
Building 7 (2017)



CFB17-0579
Building 7 (2017)



CFB17-0580
Building 7 (2017)



CFB17-0581
Building 7 (2017)



CFB17-0582
Building 7 (2017)



CFB17-0583
Store 10 (centre) / Buildings 10d (right) and 10e (left) (2017)



CFB17-0584
Building 7 (2017)



CFB17-0585
Building 7 (2017)



CFB17-0586
Building 7 (2017)



CFB17-0587
Store 8 (2017)



CFB17-0588
Road passing to the west of Stores 5 (foreground) and 6 (2017)



CFB17-0589
Store 4 (2014)



CFB17-0590
South-eastern sentry post



CFB17-0591
South-eastern sentry post



CFB17-0592
South-eastern sentry post



CFB17-0593
South-eastern sentry post



CFB17-0594
South-eastern sentry post



CFB17-0595
South-eastern sentry post



CFB17-0596
South-eastern sentry post



CFB17-0597
South-eastern sentry post



CFB17-0598
South-eastern sentry post



CFB17-0599
Modern perimeter fence (2018)



CFB17-0600
Air-raid shelter



CFB17-0601
Air-raid shelter



CFB17-0602
Air-raid shelter



CFB17-0603
Air-raid shelter



CFB17-0604
Air-raid shelter



CFB17-0605
Air-raid shelter



CFB17-0606
Air-raid shelter



CFB17-0607
Air-raid shelter



CFB17-0608
Air-raid shelter



CFB17-0609
Air-raid shelter



CFB17-0610
Air-raid shelter



CFB17-0611
Air-raid shelter



CFB17-0612
Detail of original perimeter fence (2018)



CFB17-0613
*Remnant of original perimeter fence
(2018)*



CFB17-0614
North-eastern sentry post



CFB17-0615
North-eastern sentry post



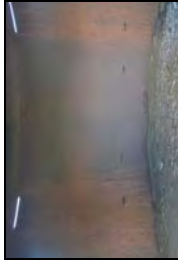
CFB17-0616
North-eastern sentry post



CFB17-0617
North-eastern sentry post



CFB17-0618
North-eastern sentry post



CFB17-0619
North-eastern sentry post



CFB17-0620
North-eastern sentry post



CFB17-0621
North-eastern sentry post



CFB17-0622
Detail of original perimeter fence (2018)

Sussex Office

Units 1 & 2
2 Chapel Place
Portslade
East Sussex BN41 1DR
tel: +44(0)1273 426830
email: fau@ucl.ac.uk
web: www.archaeologyse.co.uk

Essex Office

27 Eastways
Witham
Essex
CM8 3YQ
tel: +44(0)1376 331470
email: fau@ucl.ac.uk
web: www.archaeologyse.co.uk

London Office

Centre for Applied Archaeology
UCL Institute of Archaeology
31-34 Gordon Square
London WC1H 0PY
tel: +44(0)20 7679 4778
email: fau@ucl.ac.uk
web: www.ucl.ac.uk/caa

