

# HS2 WPO29 – Historic Environment Works – Saltley Trading Estate – Enabling Works North Contract

## Final Report for Historic Building Recording Survey

Document no.: 1EW04-LMJ-EV-REP-NS03\_NLog-029001

Revision	Author	Checked by	Approved by	Date	Reason for revision
P01	Paul Gwilliam, Megan Ridsdale and Magnus Kirby			20/06/2018	Issued for acceptance
Co2	Paul Gwilliam, Megan Ridsdale and Magnus Kirby	Melissa Melikian	Melissa Melikian	08.01.2019	Issued for acceptance after comments received

DOCUMENT OWNER: ROB EARLY

SECURITY CLASSIFICATION: OFFICIAL

Handling instructions: Uncontrolled when printed

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# 1 Executive Summary

- 1.1.1 This report describes the historic building recording carried out at Saltley Trading Estate (NGR: 409491 288525). The on-site recording was undertaken between the 22nd May 2018 and the 25th May 2018.
- 1.1.2 The building recording addressed a group of undesignated buildings at the former Saltley Railway Carriage & Wagon Works, on Dorset Road in Birmingham, to the east of the existing railway mainlines. The site was originally part of an extensive complex of associated industrial buildings that extended from the railway to Arley Road in the east, from Saltley Viaduct in the south to Aston Church Road in the north. The majority of buildings that were present in the Saltley Works in the early 20th century appear to have survived, although some are later, particularly at the northern end of the site which is now a major business park.
- 1.1.3 The building recording on selective buildings on the site was required in order to create an historical record prior to demolition. The objective of the investigation was to gain information about the historical resource in order to support an assessment of its character, extent, knowledge value, preservation and potential to contribute to Specific Objectives set out in the Generic Written Scheme of Investigation: Historic Environment Research and Delivery Strategy (Doc No: HS2-HS2-EV-STR-000-000015). The outcome of the investigation may be used to inform future decision-making on the requirement for further investigation at the Site, and a strategy for achieving it.

# 2 Site Location, Extent and Condition

- 2.1.1 The buildings under investigation are within the Saltley Trading Estate and comprise three large undesignated industrial sheds to the north-east of the city of Birmingham. The sheds are located between Pennine Way to the east, Dorset Way to the north and west and Pembroke Way to the south (Figure 1: Appendix A).
- 2.1.2 There are presently three large buildings on the site which are the subject of this report (A-C, Figure 2). The buildings are all constructed of brick with corrugated metal roofs containing large skylights, forming three long, linear, gabled structures arranged in blocks.
- 2.1.3 The buildings were in good state of repair and were watertight at the time of survey.

## 3 Site Background

- 3.1.1 As previously stated, the group of buildings under investigation were part of the former Saltley Railway Carriage & Wagon Works. They have been labelled A, B and C for the purposes of the survey (Figure 2: Appendix A), and an additional figure has been created to show the current and previous occupants of the buildings (Figure 3: Appendix A).
- 3.1.2 The buildings are all constructed of brick with corrugated metal roofs containing large skylights and date to the late 19th and early 20th century with later 20<sup>th</sup> century additions. The buildings were probably built or rebuilt after the creation of the Metropolitan Amalgamated Railway Carriage and Wagon Company Ltd in 1902, although some fabric may be earlier, however what is clear is that later phases of development has altered their significance as historic structures.
- 3.1.3 The Saltley Works was founded in 1847 by Joseph Wright, a stagecoach manufacturer from London who expanded his business into the construction of railway carriages at this early stage in the development of the national railway network. Wright's first Birmingham workshops were at a 6-acre site at Landor Street, c.1km to the south, leased in 1845 (Grace's Guide 2018a). The business quickly outgrew these premises, necessitating the move in 1847 to the new 50-acre site, on to the land now occupied by Saltley Business Park, becoming the main manufacturer of carriages for London North Western Railway (Richards 1962). The business took the name Metropolitan Railway Carriage and Wagon Company Ltd in 1862 (National Archives Ref: RAIL 791/312), after which the works were redeveloped. However, the carriage plant at Saltley was moved in 1877 to Wolverton, resulting in a noticeable migration of workers (Larkin *et al.* 1988; Richards 1962). In 1902, the company became part of the Metropolitan Amalgamated Railway Carriage and Wagon Company Ltd (Metropolitan Company; Grace's Guide 2018b). The purpose of this amalgamation was to centralise the rolling stock industry and to focus the skill and experience from many firms, which all gradually moved to Saltley, and other factories closed down.
- 3.1.4 During World War I, the Metropolitan Company became the dominant producer of British tanks, for which Saltley was one of the main production works, as well as continuing to produce rolling stock (Grace's Guide 2018c).
- 3.1.5 In 1919 shares in the Metropolitan Company were acquired by Vickers Ltd, an engineering company that in 1911 had massively expanded their operations into aircraft manufacture and rail. In 1929 Vickers and Cammell Laird merged their rolling stock interests to form the Metropolitan-Cammell Carriage and Wagon Co Ltd with its head office at Saltley (Grace's Guide 2018d). The outstation locomotive shop within Saltley, that undertook repairs, was closed and moved to Derby (Larkin *et al.* 1988). However, the company remained successful during and

after the Second World War. The Saltley Works were closed in 1962, with all subsequent railway works moved to nearby Washwood Heath.

- 3.1.6 By 1974, the site had been renamed Saltley Trading Estate, with parts being used as warehouses, depots (including for the Post Office) and a vehicle servicing depot. The current survey has identified that significant alterations on site have meant that within the buildings surveyed, only fragmentary remains of pre 20<sup>th</sup> century buildings may survive.

## 4 Aims and Objectives

4.1.1 The main aims of the building recording were to:

- Define the scope of the building recording;
- Outline the aims of the recording and how it will contribute to the specific objectives of the GWSI: HERDS;
- Describe the methodology employed; and set out the proposed deliverables and reporting mechanisms.

4.1.2 Further aims of the building recording were to:

- Search and collate archival sources relating to the design, construction, use and alteration of the buildings over time, including its use for the manufacture of tanks in the First World War;
- Record the extent and character of the different elements of the site, assess the extent and completeness of building remains and any features of significance;
- Develop a process flow to explain the manufacturing processes undertaken on the site and how the buildings were impacted or adapted through technological and material change;
- Assess the significance of the assets and their setting;
- Assess the extent of building remains;
- Suggest measures, if appropriate and feasible, for the conservation, repair or management of identified significant assets and their setting;
- Suggest provisions to be made during demolition, where applicable, for the salvage of material of architectural or historic significance which has the potential for reuse;

- Compile a lasting record of the buildings and their setting, including an assessment of the impact of the Midland Works on its immediate townscape; and
- Contribute to the delivery of GWSI: HERDS Specific Objectives as specified in Section 4.1.3 below.

4.1.3 Through delivery of the works set out in the methodology, the building recording aimed to create knowledge and outputs that would contribute to the following specific objectives in the following ways:

Table 1: Contribution to Specific Objectives

Specific Objective	Contribution
KC43: Investigate the link between the development of the railways and broader changes in the historic landscape, such as urban settlement expansion and the decline of the canal network.	The building recording should help characterise the site and allow comparisons to be made with the siting and amenities of other carriage works in the area, as well as developing a better understanding of the role and impact of Saltley Works in the development of the townscape and layout of Saltley settlement.
KC48: Methods of using digital technology and social media to engage with public, communities and volunteers should be explored in a manner that enables parties to contribute to research and interpretation as well as enabling easy access to knowledge and ultimately archives.	The building recording should help establish a sound basis for further engagement with local people and former employees as part of additional research (to be dealt with under a separate Project Plan), that will capture important oral historical evidence for the use of the site, and its relationship with the local community in the past and into the future.
KC54: Identify key changes in the technology of railway infrastructure and how these changes influenced the distribution of goods or the movement of people. Can we recognise changing public perceptions of railway infrastructure and associated buildings over time?	Prior to the removal of these assets, it is anticipated that building recording will provide an opportunity to assess their character and record their significance. To date, railway and technology historians have strongly focussed on the development and production of engines and very little research has been undertaken into the manufacture of carriages and wagons. This work should help develop a better understanding of this important aspect of railway infrastructure by focussing on a purpose-built site for the manufacture of carriages for goods and people.

## 5 Methodology

5.1.1 This historic building recording survey follows the detailed methodology laid out in the Project Plan (HS2 WPO29 Historic Environment Works – Saltley Trading Estate – Enabling Works North Contract: Project Plan for Historic Building Recording; Doc No: 1EW04-LMJ-EV-PLN-NS03\_NLog-029000) and the Location Specific Written Scheme of Investigation (LSWSI)(HS2 WPO29 – Historic Environment Works – Washwood Heath Depot and Saltley Trading Estate – Enabling Works North Contract: Location Specific Written Scheme of Investigation for Historic Building Recording (Doc No: 1EW04-LMJ-EV-MST-NS03\_NLo8-029000 Co2). The Project Plan

covers the methodology for all parts of the investigation, including archive research (Section 4.3.1 – 4.3.2; and the on-site historic building recording survey (Section 4.3.3 – 4.3.4). The methodology adheres to a basic Level 2 Survey of the buildings as defined by the Historic England 2016 publication 'Understanding Historic Buildings: A Guide to Good Reporting Practice'. This included a general and detailed written and photographic record of the exterior and interior of the buildings. As defined in the Location Specific Written Scheme of Investigation, the drawn record was restricted to a general client-provided site plan of the buildings to allow the position and direction of photographs to be recorded.

5.1.2 Before and after the on-site historic building recording, a number of archive sources were consulted relating to the design, construction, use and alteration made to the buildings. The following archives sources were consulted:

- Warwickshire County Records Office (visited 23.05.2018)
- Birmingham City Archives (visited 23 and 29.05.2018)
- National Archives (visited 26.10.2018)
- Metropolitan-Cammell Website ([www.metcam.co.uk](http://www.metcam.co.uk))
  - Alstom Website ([www.alstom.com](http://www.alstom.com)) accessed via Metropolitan-Cammell Website
- Railway archives ([www.railwaysarchive.co.uk](http://www.railwaysarchive.co.uk))

5.1.3 The Warwickshire Country Records Office had some plans relating to the building of the canal which passed land occupied by the former works.

5.1.4 A search of the Metropolitan-Cammell Archive website produced plans of Saltley Works from 1948 and 1949. Unfortunately, the quality of the reproduction is poor (Plates 6 and 7 respectively: Appendix B). No sources were found during a search of the Alstom Website. The archives presented at the Birmingham City Archives include two boxes regarding the sale of the Saltley Works premises. Unfortunately, only one of these plans showed the buildings under investigation (Plate 8: Appendix B) and, as a result, the rest of these plans have not been illustrated.

5.1.5 A thorough search was also undertaken of the Archaeology Data Service for grey literature concentrating on Saltley Works, Metropolitan Amalgamated Railway Carriage and Wagon Company, and Carriage and Wagon but produced no results.

5.1.6 A detailed map regression was also undertaken on all publicly accessible Ordnance Survey maps from the First Edition onwards. Three maps, which conclusively shows the land occupied by Saltley Works and predate the Ordnance Survey were identified these were:

- Tomlinson Map of Saltley Manor of 1760

- Tithe map of 1833
- Birmingham Borough Map of Fullarton of 1866

- 5.1.7 The research has been unable to source copies for reproduction, but they have been considered within the text of this report. A map has been included of the general Washwood and Saltley area which was produced in 1832, however the exact location of the site is unclear (Plate 1: Appendix B).
- 5.1.8 A written record was made of the exterior and interior of the buildings subject to survey using AOC *pro forma* recording sheets which were filled out for each building or large internal space. Where groups of spaces were wholly modern, a single record sheet was completed. Comments were made on the condition, construction and materials used, as well as architectural features and any evidence for phasing or past industrial activity. Modern interventions were also noted. Some sketches were made of specific features or internal arrangements to aid in the final reporting.
- 5.1.9 The photographic survey comprised a general and detailed record of the exterior and interior of the buildings subject to survey in colour digital using a digital SLR camera (Nikon D300) in both .JPG and .RAW format. General views of the exterior of each building were taken which comprised both oblique views and head-on elevation views (as was possible within the constraints of the site). This was accompanied by a detailed photographic record of specific elements such as windows, doors, blocked openings, architectural and structural detail, evidence for phasing and/or function, modern interventions and anything else pertinent to the historic record. Internally, a general photographic record was undertaken of each room or internal space within the buildings to sufficiently create good overall coverage of the space. As with the exterior, detailed photographs were undertaken of specific elements such as windows, doors, blocked openings, architectural and structural detail and evidence for phasing and function. A tripod was used in all areas of poor light and a flash gun employed to illuminate dark areas. A 2m and/or a 1m ranging pole was also placed in all shots where access and health and safety allowed. A running register of photographs was taken on site and this has been included in Appendix C, together with a site plan of the buildings showing the position and direction of each photograph and a reproduction of each photograph taken.
- 5.1.10 As outlined in the Location Specific Written Scheme of Investigation, the drawn record was restricted to working with a client-supplied site plan. However, as part of the requirement to create a visual record of the position and direction of the photographs, additional measurements were taken on site by hand using a DISTO® laser distance measurer and hand tapes to enhance this record, producing basic measured sketch plans on site. These were then reproduced and form part of the photographic record (Appendix C).
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## 6 Results

### 6.1 Archival Research

- 6.1.1 Three maps which predate the Ordnance Survey map done in 1886, show land prior to the construction of Saltley Works, the earliest of these shows the Manor of Saltley and the land to the east of the River Rea in 1760. The land is depicted as a series of irregular-shaped enclosed fields illustrated as Broad Meadow. Further to the west, the land opens out onto Crane Moor and it's here that the geo-referenced map shown on Birmingham's historic mapping website shows the land on which Saltley Works was constructed.
- 6.1.2 A plan of the proposed canal to the west of the site from 1832 (Plate 1: Appendix B) shows the London to Birmingham Railway line. Saltley and Saltley Hall are shown on the map but there is very little detail of the surrounding landscape other than scattered field boundaries to the south side of the line.
- 6.1.3 By 1833, a tithe map of Aston Parish shows that Crane Moor has been enclosed, and most of the irregular-shaped fields to the west of the river have merged forming larger regular-shaped enclosures. A bridge is also depicted crossing the river to the south of the large field that would later become the site for Saltley Works. A road is shown running east to west and crosses the bridge. There is also a small hamlet depicted on the west side of the road.
- 6.1.4 By 1866 the borough map shows new roads have been created, including Metropolitan Road (now Gate Street) and Clayton Road to the south of the site, The High Street is also shown on the map along with what is now Washwood Heath Road to the west. Very few houses are depicted but there are what appear to be long rectilinear building to the west of the Railway line and south of open land that was later to become Saltley Works. A significant addition to the earlier maps is the Midland Railway line which is shown in 1866, along with Saltley Station to the south of the railway crossing of the river. The canal reservoir and the canal are also shown on the map from 1866.
- 6.1.5 The First Edition Ordnance Survey map (Plate 2: Appendix B) details the Saltley Works premises in 1886 as a group of differently-sized buildings with railway sidings entering the site from the north with the main railway line to the west. The map marks a small reservoir on the east side of the works. There is housing situated to the south-east of the premises that becomes sparser further east. To the north-west of the Saltley premises are the canal, a canal reservoir and the River Rea. There is then a group of additional industrial buildings to the north-west, on the opposite side of the canal reservoir next to Nechells Park.

- 6.1.6 The Second Edition Ordnance Survey map (Plate 3: Appendix B) shows the Saltley Works Premises in 1902. The layout of the buildings is similar to what is depicted on the 1886 map, although new buildings have now been erected to the east. The building to the immediate south-east of the development area has been either enlarged or replaced and the adjacent reservoir position has changed and now covers a much wider area. A new larger gasholder has also been built to the north-west as part of the City Gas Works and additional railway sidings entering from the north are connected to the new buildings on the east. The group of industrial buildings to the north (next to Nechells Park) have also been extended. More houses have been erected surrounding the premises to the east and north and the River Rea has been partially culverted.
- 6.1.7 The Third Edition Ordnance Survey map (Plate 4: Appendix B) details the Saltley Works Premises in 1913. The complex has extended to Arley Road to the east, with infilling of structures with several of the smaller buildings noted on earlier maps amalgamated into larger structures, including Sheds A-C. The small reservoir has been relocated to the north. Further development has taken place within Nechells Park and the group of industrial buildings to the north-west have been extended.
- 6.1.8 Complementary to the above maps, are two photographs from the 1920s which show Saltey Works in operation. An oblique view from the north-west in 1920 (Plate 10: Appendix B) shows new density of structures mentioned above, including large factory chimneys to the south of Shed C which appears shorter in length than it does today. Another photograph from 1928 (Plate 11: Appendix B) is centred on the engine house in the middle of the complex and appears to show that Shed B had also been re-roofed, however, this maybe the result of the angle of the photograph and the reflection of light off the skylights.
- 6.1.9 The Fourth Edition Ordnance Survey map of 1937 (Plate 5: Appendix B) shows a similar outline of the Saltley Works as depicted on the earlier maps. The central buildings have been merged into one, although there are still some surrounding individual buildings.
- 6.1.10 A search at the Warwickshire County Records Office did not locate any documents of relevance to the proposed area of development. However, their archives included various plans regarding the Saltley and Washwood Heath land with proposed plans of the Midland Railway line. The Warwickshire County Records Office Archives contained a package marked 'Plans December1939' relating to the proposed line of a canal and the line of the London and Birmingham railway through Mrs Mills' land. This package contained:
- A covering letter addressed to HEL in Warwick from K. Greenway of Warwick 1939;



- A copy of land intended to be purchased for improving the line of the road into the Garrison Farm, Aston, 1935;
- An 1832 plan showing the canal course in the hamlet of Saltley and Washwood, with section and reference showing its passage through Mrs Jane Mills' land;
- A plan of a larger area, showing the site for engine and feeder or heading to engine, the line of the railway, other watercourses, Saltley Hall, and cross section, c.1832 (Plate 1: Appendix B).

- 6.1.11 The online archive from the Metropolitan-Cammell Website ([metcam.co.uk](http://metcam.co.uk)) contains two plans of the Saltley Site; one dated 1948 (Plate 6: Appendix B) and one dated 1949 (Plate 7: Appendix B) both of these plans show spaces within the Saltley complex with their individual functions, although the reproduction of these plans is poor, it is possible to show that in 1948 Shed C is being used as a press shop, Shed A is a machine shop and Shed B is a smithy. No functions are ascribed to the individual buildings on the later plan from 1949, however, the overall plan form of buildings under investigation do not change. The two plans show the same northward projection of the central bay in Shed C which was identified on the Ordnance Survey maps, they also show a large complex of buildings attached to the southern end of Shed B. Both plans show that Shed A is connected to the building complex to the west via a small link building.
- 6.1.12 During the 1960s plans showing the Saltley Works were created pre-empting the later sale of the site. The plans of the site include The British Waterways Canal and the neighbouring railway lines and sidings to and from the Works. These plans, showing the works as a whole ascribe individual buildings in relation to the administrative centre of the works and floor functions, including, in 1964, dimensions and dates of when individual rooms, within the wider building complex, were vacated. These lists demonstrate the distribution of activities within Works when it was still in operation and before it was final closed, and the buildings reused within the modern trading estate. Unfortunately, the only plan to show the buildings under investigation was from 1962 (Plate 8: Appendix B). The plan shows the central bay of Shed C extended northwards, while Shed B is extended to the south and is connected to another group of buildings, no key was found to this plan.
- 6.1.13 A photograph from 1989 (Plate 11: Appendix B), shows the eastern edge of the Saltley Works as a trading estate, with new development and expansion to the north-west. It also shows the trading estate in relation to Washwood Heath Depot, which by then resembles the former Saltley Works, but on a much grander scale.

## 6.2 Building Descriptions

- 6.2.1 A descriptive record has been compiled for each of the buildings on site, highlighting features of interest. The information is taken from the room datasheets produced on site. The full photographic record of the site is reproduced in Appendix C.
- 6.2.2 In order to orientate the reader, the site has been divided into a number of individual blocks, identified by a letter (A-C) (Figure 2: Appendix A). An additional figure shows the current occupancy of the buildings at the time of survey (Figure 3: Appendix A).
- 6.2.3 Photographs contained within Appendix C are referred to throughout the text in parentheses using the prefix 'Saltley' e.g. Photo Saltley023. A selection of illustrative images are reproduced and embedded within the text as plates for quick reference, and a copy of all photographs has been included in Appendix C.

### *Shed A*

- 6.2.4 Shed A at the northern end of the site is aligned on a roughly north to south axis and currently stands empty, and measures 153m by 63m. Signage around the building shows that it was formerly occupied by Anixter, a company which specialises in communications and security products. The shed is brick-built and four bays wide, with two modern flat roofed office blocks attached to the north gable end (Plate 1): One of office blocks is a two-storey projecting 7.1m beyond the northern limits of the shed, while the other is a single-storey unit, 16.5m wide and projects 5.1m. The north external elevation of Shed A has been built using modern brick laid in a stretcher bond with roller shutter doors inserted.
- 6.2.5 The east elevation of Shed A fronts a modern estate road. The elevation has roller shutter doors and there are regular-spaced stepped brick pilasters (Photo Saltley058) along its length. Two horizontally mounted I-sectioned steel beams have been added to the elevation and sit on top of the brick pilasters (Photo Saltley059). Changes in the colour of the brickwork below these I-sectioned steel beams along the east elevation appear to indicate the presence of blocked window openings on two levels (Photo Saltley059). These blocked openings can also be seen internally (Photo Saltley040).



Plate 1: Shed A with modern offices attached to the north elevation

- 6.2.6 The west elevation of the building retains its early 20<sup>th</sup> century facade with regular spaced panels in the brickwork which have a sloping brick sill. None of the panels appear to have any openings which are contemporary with the building's construction in the early part of the 20<sup>th</sup> century (Photo Saltley068).
- 6.2.7 Internally, Shed A is divided into four bays across its width by a central brick-built wall with rows of paired steel stanchions either side running the full length of the building (Photo Saltley039). This kind of paired stanchion design can be seen in the railway sheds at Washwood Heath 1.5km to the north east, and in engine sheds on the current bombardier site in Crewe which were constructed around 1903 (Lanpro, 2017). The steel stanchions support rainwater valleys and a horizontally-mounted I-sectioned steel beam in each of the four bays. At the eastern side of the shed, this I-sectioned steel beam acts as a rail for an overhead crane (Photo Saltley054). Again, this arrangement can be seen in the sheds at Washwood and Crewe. Internally, the central brick wall of the shed is not continuous along its full length but open in the central portion of the building (Plate 2). Steel stations support the opening but these have been encased in brick.
- 6.2.8 The roof trusses over the building are of a lightweight steel construction and support a galvanised roof covering with skylights to each pitch. It was noted during the survey that the steel rail for the overhead crane and some of the stanchions had a foundry Mark 'EARL OF DUDLEY STEEL'. (Photo Saltley045). The Earl of Dudley Round Oak Iron & Steel Works Ltd was created in the early part of the 19<sup>th</sup> century but renamed after the company was reorganisation in the late 1890s. The company traded under the name The Earl of Dudley Round Oak Iron &

Steel Works Ltd until 1936 when its changed its name to become Round Oak Steel Works Ltd (Black Country History, 2012), it is likely therefore, that the presence of this foundry mark on the steel sanctions dates the steel superstructure of the shed to the late 19<sup>th</sup> or more likely the early part of the 20<sup>th</sup> century.

- 6.2.9 The floor is constructed from cast concrete and in the eastern half of the building there are the remains of a weighbridge (Photo Saltley046). The east wall of the shed also contains a horizontal-mounted I-sectioned steel beam; however, this is not continuous and is broken by the inserted roller shutter doors (Photo Saltley053).



Plate 2: Internal view of Shed A

- 6.2.10 At the northern end of Shed A there is a modern office block which can be accessed from the main body of the shed. Internally, the space is subdivided by modern stud walling (Photo Saltley050). Within the body of the main shed there are smaller brick-built office spaces, one of which contains an electricity substation. The brickwork construction of this sub station mark this out as a late 20<sup>th</sup> century insertion. The sub-station is currently the main entry point in to the building (Photo Saltley042).
- 6.2.11 The building material to survive from the early part of the 20<sup>th</sup> century in Shed A is primarily found in the walls, which are typically constructed from English bonded brick and survive in their most complete form in the west elevation (Photo Saltley065). The wall forming the east elevation does appear to have undergone rebuilding and the northern gable end is wholly modern in appearance. There are no original doors or windows in any of the walls to Shed A, however; the steel superstructure is probably a survival from the early part of the 20<sup>th</sup> century.

## Shed B

- 6.2.12 Shed B is attached to the southern end of Shed A and aligned along the same roughly north to south axis and measures 132m by 63m. The shed is currently occupied by Mercedes-Benz who use the space to service and sell commercial vehicles (Plate 3). The majority of the building is externally clad by metal sheeting along its west elevation (Plate 4), and the east elevation is shared with Shed A and has been re-clad using modern brick (Photo Saltley026).
- 6.2.13 Internally, shed B is divided into four bays across its width by steel stanchions which are encased with modern lagging material to provide protection against moving vehicles. The roof structure over the shed is supported by lightweight steel trusses (Photo Saltley016), and the steel stanchions, support rain water valleys. In the central open part of the shed the steel stanchions are thinner in section than those in Shed A, however close examination of these was not possible because of the cladding. The lightweight trusses over the shed appear to be modern along with the roof covering.

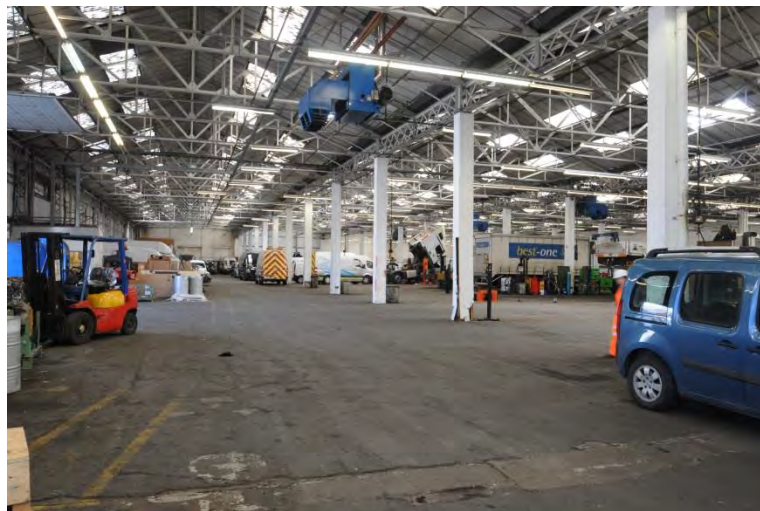


Plate 3: Internal view of Shed B showing its modern usage

- 6.2.14 At the western side of the shed, the brick outer wall has been reduced in height and modern steel stanchions added to support the roof and hold the exterior cladding (Photo Saltley019). The brick work in this reduced-height wall is painted, however the bonding of the wall differs internally and externally, suggesting two phases of construction. Internally the brick work has an English bond while externally the modern engineered bricks are laid using a stretcher bond. It is possible therefore that the internal face of the reduced-height west wall of Shed B is part of the early 20<sup>th</sup> century building. There are a number of modern office buildings within the shed

however, at the northern side of the shed is a single-storey brick-built storage unit with reinforced metal doors (Photo Saltley014).

- 6.2.15 There are blocked openings evident along the east wall of the Shed B which still retain their original brick pilasters. There are also inserted steel beams in the east wall of Shed B, possibly acting as lintels to former openings (Photo Saltley023). Currently the internal walls of the shed are painted white, however, it is possible to see that the brickwork beneath is laid with an English bond which matches that of the west side. The same bonding can also be seen in the south and north gable ends of Shed B (Photo Saltley015) suggesting that these wall date from the early part of the 20<sup>th</sup> century. Unfortunately, no early window or doors openings could be identified in any of the elevations, all of which were probably removed when the building was re-clad in the later part of the 20<sup>th</sup> century.
- 6.2.16 At the southern end of Shed B, a shed with a double pitched roof aligned on an east to west axis has been added (Photo Saltley028). This single bay shed is currently occupied by Midland Truck and Van Hire and used for storage space (Plate 5). Externally, this part of the building has been clad to match Shed B to the north. Internally there is a modern steel superstructure supporting a galvanised roof structure which has skylights to each pitch (Photo Saltley031). The space is entirely modern and the only early part of the structure is the north wall which also doubles as the south gable end of the Shed B. This elevation contains recessed panels at first-floor level formed by English-bonded brickwork. (Photo Saltley032).
- 6.2.17 The original, early 20th century material, surveying within Shed B is confined to the English bonded brick walls, at the gable ends and along the east (Photo Saltley025) and in the reduced height wall of the west elevation. There are no original openings within the shed, however regularly-spaced metal bars have been introduced along the length of the east wall (Photo Saltley022) which may indicate the presence of earlier openings.





Plate 4: Shed B currently occupied by Mercedes Benz Truck and Van



Plate 5: Showing the interior of Midland Truck and Van Hire at the southern end of Shed B

### *Shed C*

- 6.2.18 Shed C lies at the west side of the site and is a stand-alone brick-built shed divided across its width into three bays, measuring 87m in length by 36m wide. Shed C is further divided across its length into two separate units: The southern end is entirely modern, and within the context of the modern trading estate, identified as Units 3. While at northern end of Shed C the internally space is identified as Unit 5, which also encompasses a modern office block (Figure 3: Appendix A).
- 6.2.19 When viewed from the outside, change in the brickwork and a fully-height straight joint in the west elevationshows at the intersection of Units 5 and 3 clearly demonstrates that the Unit 3 part of shed C is a modern addition to the south gable of Unit 5, of Shed C (Photo Saltley007).
- 6.2.20 The west elevation of Unit 3 has two modern roller shutter doors and two pedestrian doorways (Photo Saltley006). The east and south elevations are devoid of features (Photo Saltley008). The bonding of the brick work changes from common bonded bricks internally to stretcher bonded brick externally, despite this the brick work has a modern appearance.
- 6.2.21 Internally Unit 3 of Shed C has steel stanchions supporting lightweight steel trusses and a galvanised roof covering with skylights to each pitch (Plate 6). There are two modern roller shutter doors and a single-storey internal toilet block along the west elevation of the shed. At the southern end of the shed there is a modern two-storey office unit (Photo Saltley003). The floor is constructed from cast concrete and the internal walls have brick pilasters for strengthening.





Plate 6: Interior of Unit 3 (Shed C) showing the modern stanchions

- 6.2.22 Although the north and east-facing elevations of Unit 5 of Shed C are mainly obscured behind modern cladding or later extensions, the west elevation of the unit retains element of an earlier façade (Photo Saltley011). In this elevation there are regularly spaced window openings topped by brick voussoirs picked out in a darker coloured brick. Each of the window openings has a projecting brick sill and the openings are fitted with wooden window frames. Two of the window openings have been blocked and a modern doorway inserted. Each of the window openings is framed by a recessed panel in the brickwork. Running parallel with the west elevation of Unit5, between Sheds C and A, there are the remains of a railway track in the current road surface (Photo Saltley012).
- 6.2.23 The doorways and window openings in the east elevation appear to be an early feature. If, as the historic mapping indicates, Shed C survived the rebuilding of site when it became the Amalgamated Railway Carriage and Wagon Company in 1902, then it is likely that these doorways and window openings date from the late 19<sup>th</sup> century.
- 6.2.24 The modern office block attached to the west side of Unit 5 of Shed C is constructed over two floors (Photo Saltley010). Internally, the space is divided into smaller office spaces (Photo Saltley071), with a rear corridor running the full length of the building. At either end of the office block there are stairs to the first floor, and there is a centrally-placed staircase which is now blocked (Photo Saltley072).
- 6.2.25 Internally, Unit 5 of Shed C, has a roof structure supported on two rows of paired I-sectioned steel stanchions (Plate 7), which are much thinner in section than those in Shed A. The space is lit from above by skylights to each roof pitch, and by window openings in the east wall which

have fixed light wooden frames (Photo Saltley078). The windows openings all have segmental arched heads formed from 2 lays of brick voussoirs. In the west wall the early window openings have been blocked and partially rendered over (Photo Saltley077), two doors have also been inserted to the modern offices beyond. Between each of the window openings on either side of the shed, there are full-height brick pilasters. (Photo Saltley077). The floor is constructed of cast concrete and retains no railway lines. All the walls are constructed using English-bonded brick both internally and externally.



Plate 7 Interior of Unit 5 (Shed C) with the blocked window openings to the left of the picture

- 6.2.26 Original building fabric within Unit 5 of Shed C is confined to the walls and the segmental arched opening in the east and west walls. There is a break in the brickwork of the north wall (Photo Saltley075) showing the possible location of the extension to the central bay which is show on the historic mapping, and on the plans created in the 1940 (Plates 6 and 7: Appendix B). The roof has been recovered using modern galvanised steel sheeting, and the floor is covered with modern concrete.

## 7 Site Significance

- 7.1.1 This assessment classifies the significance of heritage assets according to five value thresholds, defined as very high/high, medium, low and negligible. Table 1 summarises the relative value (significance) of cultural heritage assets based on the approach outlined in ICOMOS guidance (2011).

7.1.2 As the buildings subject to assessment at Saltley are non-designated heritage assets, and do not lie within a conservation area, their significance may be considered to be low and equate to 'local importance' (see Table 1 below).

Table 1: Definition of Heritage Significance

Heritage value (significance)	Definition / Criteria
Very High	<ul style="list-style-type: none"> <li>• World Heritage Sites</li> </ul>
High	<ul style="list-style-type: none"> <li>• Scheduled Monuments</li> <li>• Grade I &amp; II* Listed Buildings</li> <li>• Registered Historic Parks and Gardens</li> <li>• Registered Battlefields</li> </ul>
Medium	<ul style="list-style-type: none"> <li>• Archaeological sites and areas of distinctive regional importance</li> <li>• Grade II Listed Buildings</li> <li>• Conservation Areas</li> </ul>
Low	<ul style="list-style-type: none"> <li>• Archaeological assets and areas of local importance</li> <li>• Unlisted buildings and townscapes of some historic or architectural interest</li> </ul>
Negligible	<ul style="list-style-type: none"> <li>• Sites of former archaeological features</li> <li>• Artefact find-spots</li> <li>• Unlisted buildings of little or no historic or architectural interest</li> <li>• Poorly preserved examples of particular types of feature.</li> </ul>

7.1.3 Historic England's 'The Setting of Heritage Assets Good Practice Advice in Planning Note 3 – 2nd edition, 2018' offers guidance on managing change within the settings of heritage assets, the guidance is set against the background of the National Planning Policy Framework (NPPF). It is important to note that within the guidance given by Historic England, setting is not a heritage asset, nor a heritage designation. Its importance lies in what it contributes to the significance of the heritage asset. (Historic England, 2018)

- 7.1.4 Further guidance on setting is provided by HS2's in document HS2-HS2-EV-STD-000-000037. In that guidance, the principal considerations to be taken into account when analysing the significance of setting are grouped under the following headings:
- Non-visual elements, atmosphere and character
  - Associations and social
  - Quality of survival
- 7.1.5 As part of the National Heritage Protection Plan, NHPP 4B3 (Transport and Communication), a statement of significance was commissioned by Historic England in 2014 for all types of railway buildings and associated structures. The draft document produced did not have a specific category for wagon and carriage works, however, research done for the heritage plan, noted that 'between 1850 and 1900, the gradually coalescing of railway companies slowly evolved various standardised designs of straight shed, classically with panelled exterior brickwork and longitudinal or transverse ridge-and-furrow roofs' (Kinchin-Smith, 2014). From an architectural and design perspective, the original 19<sup>th</sup> century eastern wall of Unit 5, Shed C (Saltley 012) and the west elevation of Shed A (Saltley 067) fits neatly within that category of railway building and therefore retain some aesthetic value.
- 7.1.6 The roofs of sheds A, B and C have been replaced, so any remaining 19<sup>th</sup> century fabric is limited to the survival of original 19<sup>th</sup> century walls, unfortunately modern cladding has been added on Building B and on the north gable end of Unit 5 (Saltley 034) replacing or obscuring this original fabric. The railway lines that lie between Sheds A and C (Saltley 007) are also a visual reminder that the site had an association with the nearby railway, however, the presence of railway lines on a site does not necessarily indicate the manufacture of railway wagons and carriages; branch lines were often introduced into large industrial complexes such as mills to deliver large quantities of raw materials.
- 7.1.7 Sheds A, B and C form part of a much larger complex of buildings and the impact of their loss may have an effect on the overall significance of the site as an integrated system performing a specific function. However, the complex's original fabric has already been eroded by later alterations to the buildings. The modern roadways introduced around the site have made Sheds A, B and C isolated from the rest of a complex that once would have been connected by railway lines and crossing points.
- 7.1.8 The buildings on site have historic value as part of a larger complex associated with Birmingham's industrial past. The complex also bears witness to a wider expansion of railway infrastructure across the country, which took place in the late 19<sup>th</sup> and early 20<sup>th</sup> century. As a

major employer in the area during most of the 20<sup>th</sup> century, using skilled labourers and craftsmen, the site would have had a significant impact on the surrounding townscape and this is evidenced by historic mapping (Plates 1 to 4) which shows a growth in the number of houses, presumably to house workers, as the site expanded. However, the historical research has shown that when the works were redeveloped, and carriage production at Saltley was moved in 1877 to Wolverton, this resulted in a noticeable migration of workers (Larkin et al. 1988; Richards 1962).

- 7.1.9 The site ceased operations in the 1960s, so only residual memories of the site as a wagon and carriage works will remain within the local community, and further loss or alteration on the site will have limited effect on people's collective memories.

### *Architectural or design*

- 7.1.10 As previously stated sheds A and C have walls which are typical of railway sheds designed and built between 1850 and 1900, however, most of this is hidden from the public realm on other buildings on trading estate by modern extensions and re cladding. As original brick surfaces may be hidden behind it is difficult for the observer to gain a coherence appreciation of a unity of design. In this respect, the piecemeal changes which have occurred to the individual buildings surrounding Sheds A, B and C have an overall detrimental effect on that appreciation.

### *Associations and social*

- 7.1.11 As sheds A, B and C are part of a former large industrial complex, the significance of the former wagon and carriage works to Birmingham's industrial and social past is an important consideration, however, the legibility of the site has been compromised by later alterations and adaptations and therefore the rest of the trading estate offers no enhancement to the heritage values of the historic assets under consideration.

### *Quality of survival*

- 7.1.12 The early 19<sup>th</sup> century manufacturing complex at Saltley was not the only examples of a wagon and carriage works to be built during the late 19<sup>th</sup> century and early part of the 20<sup>th</sup> century such as in Derby, and Bristol. However, the early buildings on the Saltley site may be considered to be a dwindling historic resource and have some rarity value. The changes made to the trading estate, however, have now obscured much of the site's original fabric and a full appreciation of the quality of that fabric is no longer possible.
- 7.1.13 The contribution of the site's setting to the building's significance lies predominately in the survival of part of the original layout of the buildings but this is best appreciated from the air, as a group of sheds and buildings next to a major railway line. From the ground this is less tangible and the cumulative effect of changes since its conversion to a trading estate in the

1960's has meant that readability of the original form and functions of the buildings has all but gone.

- 7.1.14 The setting of the former wagon and carriage works has changed over time, illustrated by the historic mapping (Plates 1 to 4), in the latter part of the 19<sup>th</sup> century the wagon carriage works was bounded by open fields to the east and west. By 1901, the historic map (Plate 2) shows the modern street plans have been formed, however significant changes occurred after 1937 (Plate 4) with the introduction of a dual carriageway (A67) to the west, infilling the former canal reservoir. Today the trading estate is surrounded by modern development of mainly low-rise housing and industrial units, which add little to the setting of the industrial estate and to the buildings within.

## 8 Discussion

- 8.1.1 The only elements of the original external building fabric which survive in any detail are within the west elevation of Shed A and the east elevation of Shed C. The presence of a railway track between these two elevations (Photo Saltley 012) means that this part of the site has remained relatively well preserved since the early part of the 20<sup>th</sup> century and been mostly saved from the effects of later interventions when the site was made into a trading estate in the 1970s.
- 8.1.2 The early mapping of the site in 1886 (Plates 2: Appendix B) shows railway sidings and tracks entering the site from the main line to the north. At this point, the current space occupied by Shed A is depicted as a series of structures orientated east to west; Shed C straddles the incoming railway sidings from the north, and Shed B may be present, but only as part of a larger structure. It is not until the Ordnance Survey map of 1913 (Plate 4: Appendix B) that the three sheds are clearly shown in their current plan form. From this we can conclude that the only building to possibly survive the re-modelling of the factory in the early part of the 20<sup>th</sup> century was Shed C when the works became the Metropolitan Amalgamated Railway Carriage and Wagon Company. The brickwork in the east elevation therefore may be of a late 19<sup>th</sup> date.
- 8.1.3 The 1948 drawing of the site from the Metropolitan-Cammell website (Plates 6: Appendix B) is the only plan to show the function of each of the rooms in any detail, although the quality of the reproduction is poor, and the detail is lost. It is possible to determine that Shed A in 1948 was a machine shop, Shed B a smithy and Shed C a press shop. The plan also shows that the centre of the three bays of Shed C extended northwards.
- 8.1.4 The presence of a machine shop in Shed A next to a smithy in B and a press shop in Shed C, suggests that this part for the works was probably being used in the middle of the 20<sup>th</sup> century to turn and mould metal part for the rolling stock. Further east, in buildings which now form



part of the modern trading estate, there was a wood mill and a paint shop depicted on the plan from 1948, and at the very eastern side of the site there was a bodyshop. The location of these building suggests that raw materials entered the site via the railway siding to the west, and the finished goods left via the body shop to the east.

- 8.1.5 As the plans from 1948 and 1949 do not show any railway lines, and assuming that the sheds retained similar functions, then a process flow from east to west would be possible. The plan of the site from 1962 (Plate 8: Appendix B) shows railway lines leading north from the west side of the site back to the main line. More crucially, however, the plan also shows a transverser mechanism which answers the question of how partially finished wagons and carriages were moved around the site. A similar mechanism can be seen in railway sheds at Crewe (Lanpro, 2017).
- 8.1.6 As part of the National Heritage Plan NHPP 4B3: Transport and Communication a statement of significance was commissioned by Historic England in 2014 for all types of railway buildings and associated structures. In that document it notes that between 1850 and 1900 the gradually coalescing railway companies slowly evolved various standardised designs of straight shed, classically with panelled exterior brickwork and longitudinal or transverse ridge-and-furrow roofs. The presence of brick panelling on the east elevation of Shed C and the west elevation of Shed A seem to typically fit this pattern, potentially marking these buildings early 19<sup>th</sup> century structures, of later examples 20<sup>th</sup> century examples built to the same design.
- 8.1.7 The width of the steel stanchions in Shed C do not match those in Shed A, however, this can be explained by the presence of an overhead crane in shed C which utilises the horizontally-mounted steel beam as a runner. At the top of each of the stanchions in Sheds A and C there is a bracket supporting this beam, however, the bracket is only present on one side of the stanchion in Shed C indicated that a crane could have only operated in the central bay. The horizontal-mounted beam has also been removed which was possible done when the northern extension to the central bay was present. In Shed A the thicker stanchions allowed runner to be mounted to both side of a stanchion so overhead cranes could be used in each bay across the width of the shed.

## 9 Conclusions

- 9.1.1 The presence of modern cladding on Shed B and the modern extensions added to both Sheds B and C have compromised the readability of these early 20<sup>th</sup> century industrial buildings and as historic railway structures. It is also clear that, since closure in the 1960s when production moving to Washwood, the historic significance of the site as a whole, as a former large specialist

industrial complex, has been diminished by later piecemeal alterations and adaptations. These changes also have also had a negative impact on the setting of the Sheds A, B and C.

- 9.1.2 The siting of the former wagon and carriage works on open land next to an important railway line and a canal and within a mile and a half of Joseph Wright’s coach works seems a well-considered plan by Wright as he would have been able to utilise a local skilled labour force and draw in raw materials via the canal and the rail networks. However, by the time that the site was being constructed in 1847 the canals were in decline. Ironically sites such as Saltley would have contributed to the decline of the canals producing wagons to move goods by rail, taking valuable tonnage away from the canals that had all but come to an end by the start of the First World War.
- 9.1.3 It is well understood that during the 19<sup>th</sup> century, industrialists and their companies constructed homes to house workers. Early examples of this happening with railway companies can be seen at Wolverton, Crewe and Swindon where the rail companies built rows of terraced houses for workers (Kinchin-Smith, 2014). Although little is known of Wright’s contributions to this, other than that he funded the building of the church in the small working-class village which emerged around his original site (Chinn, 2014). It is unclear, however, whether the increase in houses seen on the mapping between 1886 and 1902 (Plates 2 and 3: Appendix B) is a direct result of an expansion of the site, and there has been no link established in any of the archives visited to suggest that the newly formed Metropolitan Amalgamated Railway Carriage and Wagon Company were responsible for their construction. It may be possible that the increase in the number of houses surrounding the site may just have been part of a need to create new homes to accommodate the increasing number of people in cities that took place in the late 19<sup>th</sup> and early 20<sup>th</sup> centuries.
- 9.1.4 Three objectives outlined in the *Generic Written Scheme of Investigation: Historic Environment Research and Delivery System* (HS2-HS2-EV-STR-000-000015) were identified by the Project Plan for Historic Building Recording as being relevant to the investigation of the Site (KC43, KC48 and KC54). The results of the Historic Building Recording have made a contribution to these aims as described in Table 16.

Table 2: Contributions to Specific Objectives (results)

Specific Objective	Contribution
KC43: Investigate the link between the development of the railways and broader changes in the historic landscape, such as urban settlement expansion and the decline of the canal network.	The building recording characterised the site and allowed comparisons to be made with the siting and amenities of other carriage works in the area, as well as developing a better understanding of the role and impact of Saltley Works in the development of the townscape and layout of Saltley settlement.



<p>KC48: Methods of using digital technology and social media to engage with public, communities and volunteers should be explored in a manner that enables parties to contribute to research and interpretation as well as enabling easy access to knowledge and ultimately archives.</p>	<p>The building recording has helped to establish a sound basis for further engagement with local people and former employees as part of additional research.</p>
<p>KC54: Identify key changes in the technology of railway infrastructure and how these changes influenced the distribution of goods or the movement of people. Can we recognise changing public perceptions of railway infrastructure and associated buildings over time?</p>	<p>The building recording has provided an opportunity to assess the character and record the significance of these buildings prior to demolition. This work has helped develop a better understanding of this important aspect of railway infrastructure by focussing on a purpose-built site for the manufacture of carriages for goods and people.</p>

## 10 Evaluation of Methodology

- 10.1.1 No access was possible to the exteriors of some parts of the buildings (shown on Figure 3: Appendix A). Despite this, it has been possible to fully characterise the buildings and produce an adequate descriptive and photographic record.
- 10.1.2 There is a high confidence rating that the scale, scope and methodology were appropriate to record the historic fabric of the buildings present on the site.

## 11 Recommendations

- 11.1.1 As these buildings were present at the turn of the 20th century, and manufacturing continued into the early 1960s, it is likely that people who worked here are still alive and a community engagement project would be valuable to compile an oral history of the site to fill the gaps in our understanding of process flow through the site.
- 11.1.2 These buildings were once part of a much larger industrial complex which symbolised an important period in the development of a modern rail network in this country. New construction methods and outsourcing has seen a steady decline in the need for such buildings which have reused their practical value other than for storage. Their significance as historic structures, however, remains but the cumulative effect of alterations to them and the rest of the site has reduced this significance. As these buildings stand there are no practical recommendations to be made for the salvage, retention and or the preservation of any features within the current structures.

## 12 Archive Deposition

- 12.1.1 The archive, consisting of a digital and physical archive of records, drawings and photographs, will be collated in line with the London Archaeological Archive and Research Centre (LAARC), Museum of London, *Standards for the Deposition of Archaeological Archives*.<sup>1</sup> HS2 Ltd will assure the delivery of the archive.
- 12.1.2 The archive will be prepared in accordance with UKIC guidelines for the preparation of excavation archives for long-term storage<sup>2</sup> and the Archaeological Archives Forum.<sup>3</sup>

## 13 Acknowledgements

- 13.1.1 Connect Archaeology wishes to thank Gurkha Security for assisting the team to work safely and unhindered, and the current occupants of the properties for allowing access.

## 14 References

### 14.1 Bibliographic

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<sup>1</sup> <http://www.museumoflondon.org.uk/collections-research/laarc/standards-deposition>

<sup>2</sup> United Kingdom Institute for Conservation 1990, Guidance for Archaeological Conservation Practice.

<sup>3</sup> Brown Duncan H & AAF 2011, . Archaeological Archives: a guide to best practice in creation, completion, transfer and collection.

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## 14.2 Cartographic

Tomlinson Map of Saltley Manor, 1760  
([https://localview.birmingham.gov.uk/Planning/Sites/HLC\\_Maps/](https://localview.birmingham.gov.uk/Planning/Sites/HLC_Maps/))

Aston Parish Tithe Map Fowler, 1833  
([https://localview.birmingham.gov.uk/Planning/Sites/HLC\\_Maps/](https://localview.birmingham.gov.uk/Planning/Sites/HLC_Maps/))

Birmingham borough map Fullarton, 1866

[https://localview.birmingham.gov.uk/Planning/Sites/HLC\\_Maps/](https://localview.birmingham.gov.uk/Planning/Sites/HLC_Maps/)

Ordnance Survey (1886) Ordnance Survey 1st Edition 25 inch map. Warwickshire Sheet XIV.2

Ordnance Survey (1902) Ordnance Survey 2nd Edition 25 inch map. Warwickshire Sheet XIV.2

Ordnance Survey (1913) Ordnance Survey 3rd Edition 25 inch map. Warwickshire Sheet XIV.2

Ordnance Survey (1937) Ordnance Survey 4th Edition 25 inch map. Warwickshire Sheet XIV.2

# 15 Oasis Form

## OASIS ID: hs2conne2-321619

### Project details

Project name	HS2 WP29a Saltley Trading Estate Historic Building Recording Survey
Short description of the project	A historic building recording carried out at Saltley Trading Estate. The on-site recording was undertaken between the 22nd May 2018 and the 25th May 2018. The building recording addressed a group of undesignated buildings at the former Saltley Railway Carriage and Wagon Works, on Dorset Road to the east of the existing railway mainlines. The site was originally part of an extensive complex of associated industrial buildings that extended from the railway to Arley Road in the east, and from Saltley Viaduct in the south to Aston Church Road in the north. The majority of the buildings that were present in the Saltley Works in the mid 20th century appear to have survived, although some were later (particularly to the north). The area is now a business park. The historic building recording was required in order to create an historical record of the site prior to commencement of its demolition.
Project dates	Start: 22-05-2018 End: 25-05-2018
Previous/future work	No / No
Type of project	Building Recording
Site status	None
Current Land use	Industry and Commerce 1 - Industrial
Monument type	INDUSTRIAL Modern
Significant Finds	NONE None
Methods & techniques	"Photographic Survey", "Survey/Recording Of Fabric/Structure"
Prompt	Required by HS2

### Project location

Country	England
Site location	WEST MIDLANDS BIRMINGHAM BIRMINGHAM HS2 WP029 Saltley Trading Estate Historic Building Recording
Postcode	B8 1BL
Study area	0 Square metres
Site coordinates	SP 09491 88525 52.494184032786 -1.860191012675 52 29 39 N 001 51 36 W Point

Revision: Co2

### Project creators

Name of Organisation	Connect Archaeology
Project brief originator	LM-JV
Project design originator	Connect Archaeology / LM-JV
Project director/manager	Melissa Melikian
Project supervisor	Paul Gwilliam

### Project archives

Physical Archive Exists?	No
Digital Archive recipient	Unknown
Digital Contents	"none"
Digital Media available	"Images raster / digital photography", "Survey", "Text"
Paper Archive recipient	Unknown
Paper Contents	"none"
Paper Media available	"Diary", "Drawing", "Notebook - Excavation", "Research", "General Notes", "Photograph", "Plan", "Report", "Unpublished Text"

### Project bibliography 1

Publication type	Grey literature (unpublished document/manuscript)
Title	HS2 WP029 - Historic Environment Works - Saltley Trading Estate - Enabling Works North Contract Final Report for Historic Building Recording Survey
Author(s)/Editor(s)	Gwilliam, P, Ridsdale, M and Kirby, M
Date	2018
Issuer or publisher	LM-JV
Place of issue or publication	LM-JV
Entered by	Nuala C. Woodley (nuala.woodley@aocarchaeology.com)

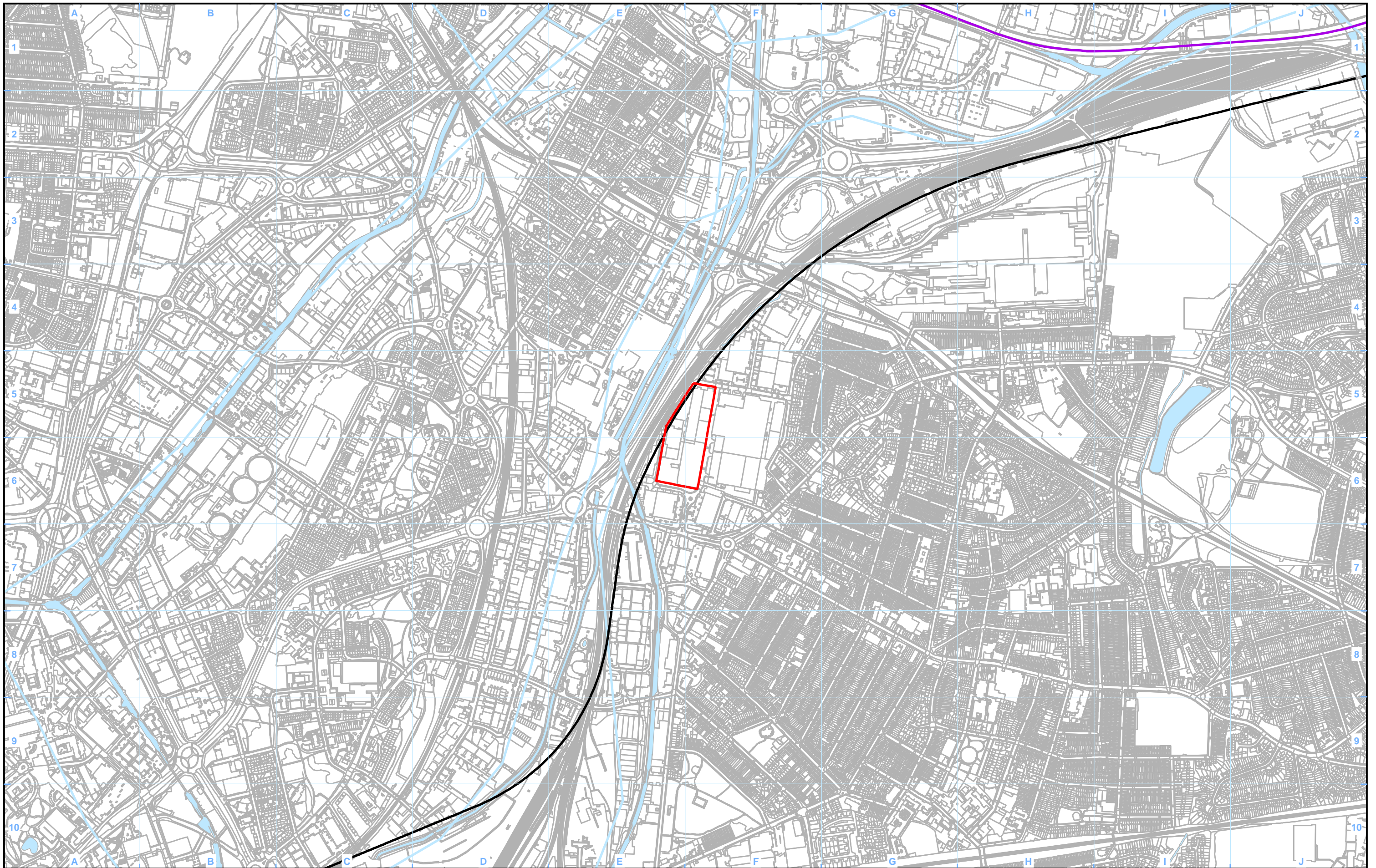


Entered on            5 July 2018

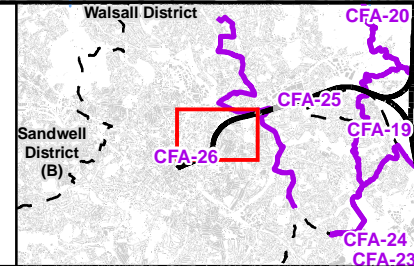


# Appendix A: Figures





- Legend**
- Route in tunnel
  - Route on surface
  - Site Extent
  - Study Area
  - Community forum boundary
  - District/Borough boundary
  - Watercourse
  - Water Body



Map Number Appendix A - Figure 1

Map Name Saltley Business Park Location Plan

Community Forum Area CFA26  
Washwood Heath to Curzon Street

Registered in England. Registration number 06791686.  
Registered office: Eland House, Bressenden Place,  
London SW1E 5DU.

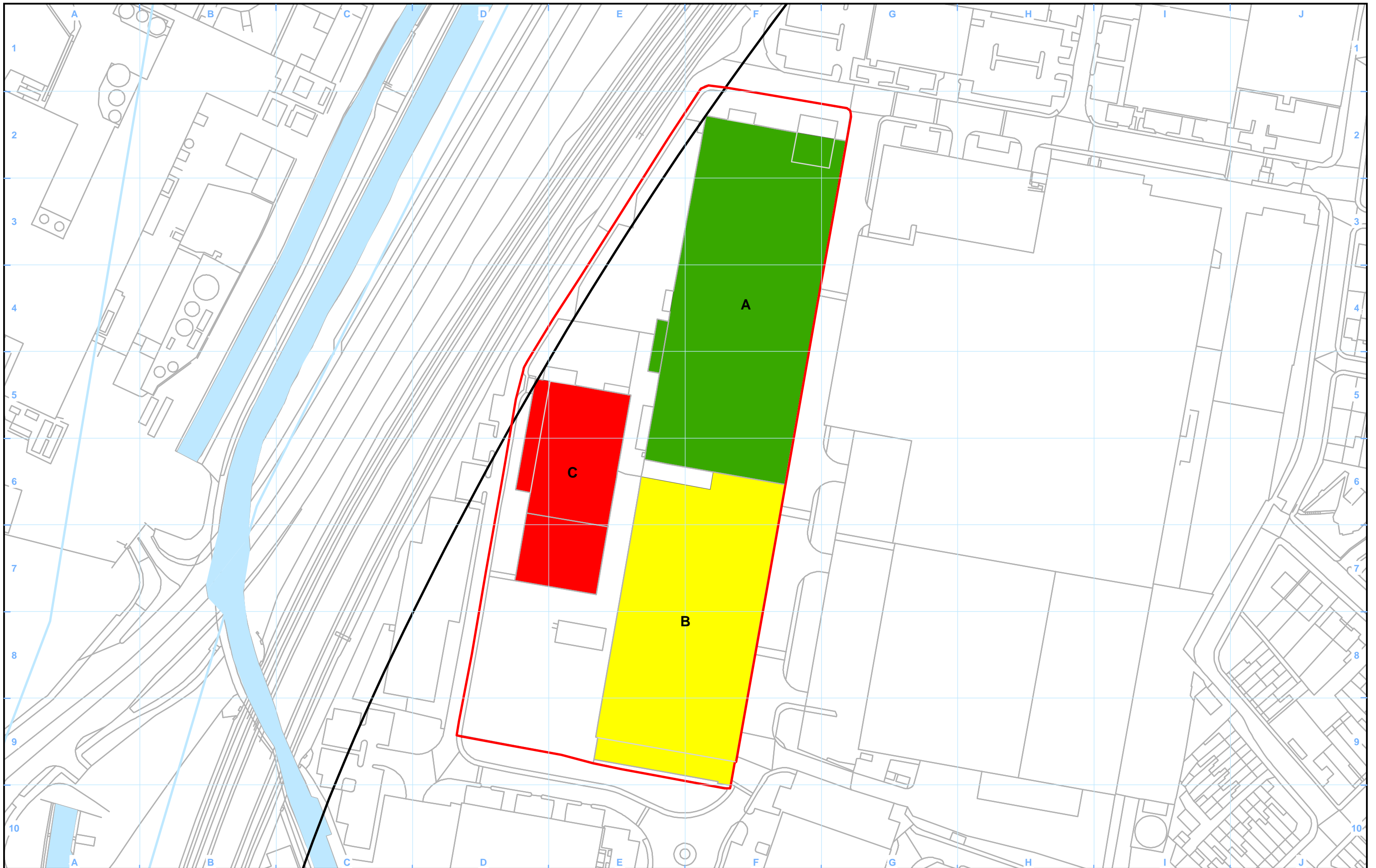
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Doc Number: 1EW04-LMJ-EV-REP-NS03\_NL09-029000

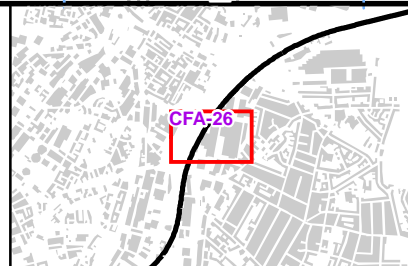
Date: 27/06/18

Scale at A3: 1:10,000





- Legend**
- Route on surface
  - Site Extent
  - Watercourse
  - Water Body



Map Number **Appendix A - Figure 2**

Map Name **Saltley Business Park  
Building Numbers**

Community Forum Area CFA26  
Washwood Heath to Curzon Street



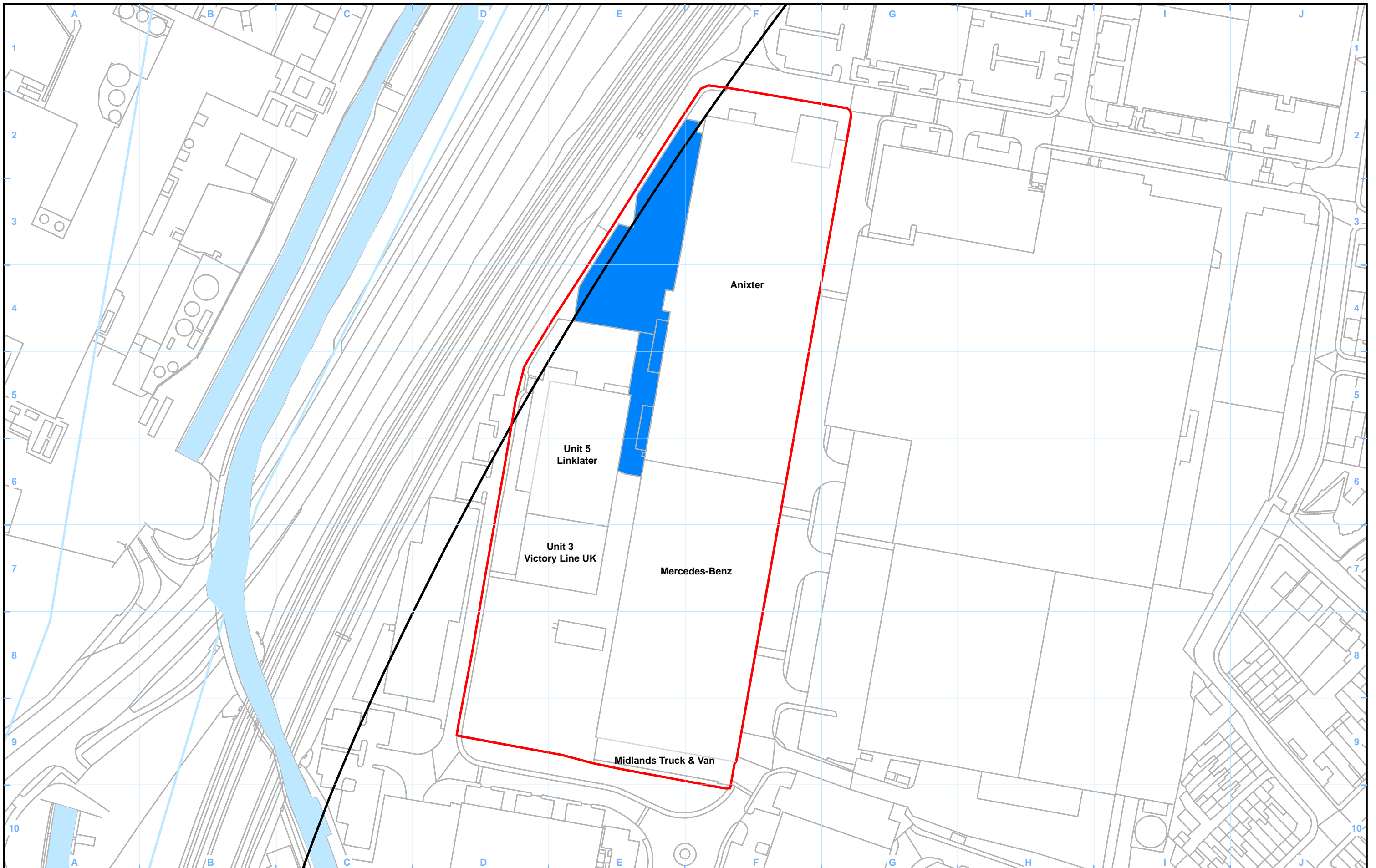
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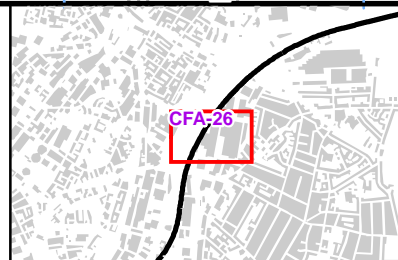
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Scale at A3: 1:1,500

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- Legend**
- Route on surface
  - Site Extent
  - No access possible
  - Watercourse
  - Water Body



Map Number **Appendix A - Figure 3**

Map Name **Saltley Business Park Building by Occupancy**

Community Forum Area CFA26  
Washwood Heath to Curzon Street

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Metres

**Date: 27/06/18**





Plate 1: Ordnance Survey mapping surveyed 1886

Ordnance Survey 1st Edition 25 inch map. Warwickshire Sheet XIV.2 (Birmingham). Surveyed: 1886. Published 1890. Southampton, Ordnance Survey.

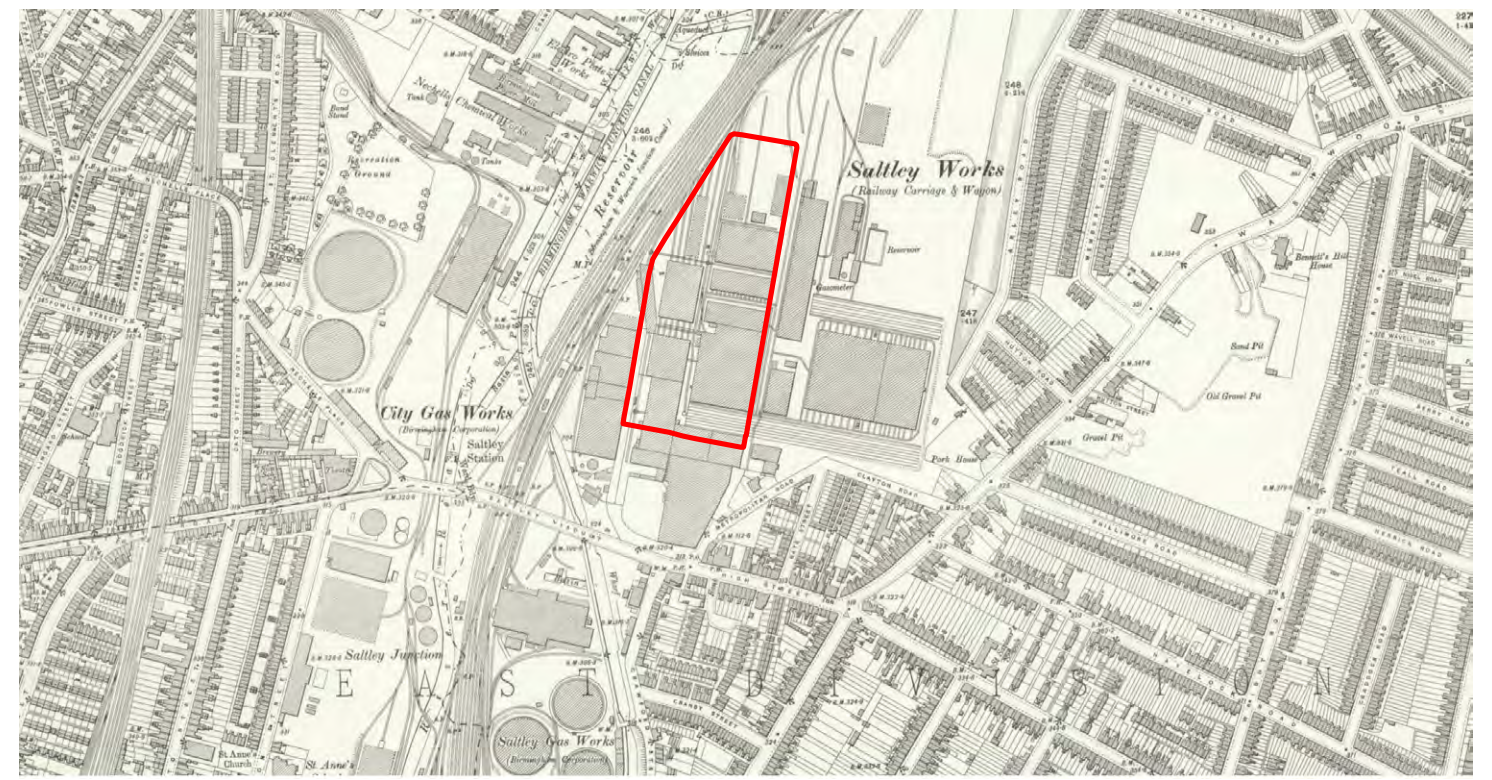


Plate 2: Ordnance Survey mapping surveyed 1902

Ordnance Survey 2nd Edition 25 inch map. Warwickshire Sheet XIV.2 (Birmingham). Revised: 1902. Published 1905. Southampton, Ordnance Survey.



Plate 3: Ordnance Survey mapping surveyed 1913

Ordnance Survey 3rd Edition 25 inch map. Warwickshire Sheet XIV.2 (Birmingham). Revised: 1913. Published 1917. Southampton, Ordnance Survey.



Plate 4: Ordnance Survey mapping surveyed 1937

Ordnance Survey 4th Edition 25 inch map. Warwickshire Sheet XIV.2 (Birmingham). Revised: 1937. Published 1945. Southampton, Ordnance Survey.

Map Number Appendix B - Plates 1-4

Map Name Saltley Business Park  
Map regression

Community Forum Area CFA26  
Washwood Heath to Curzon Street



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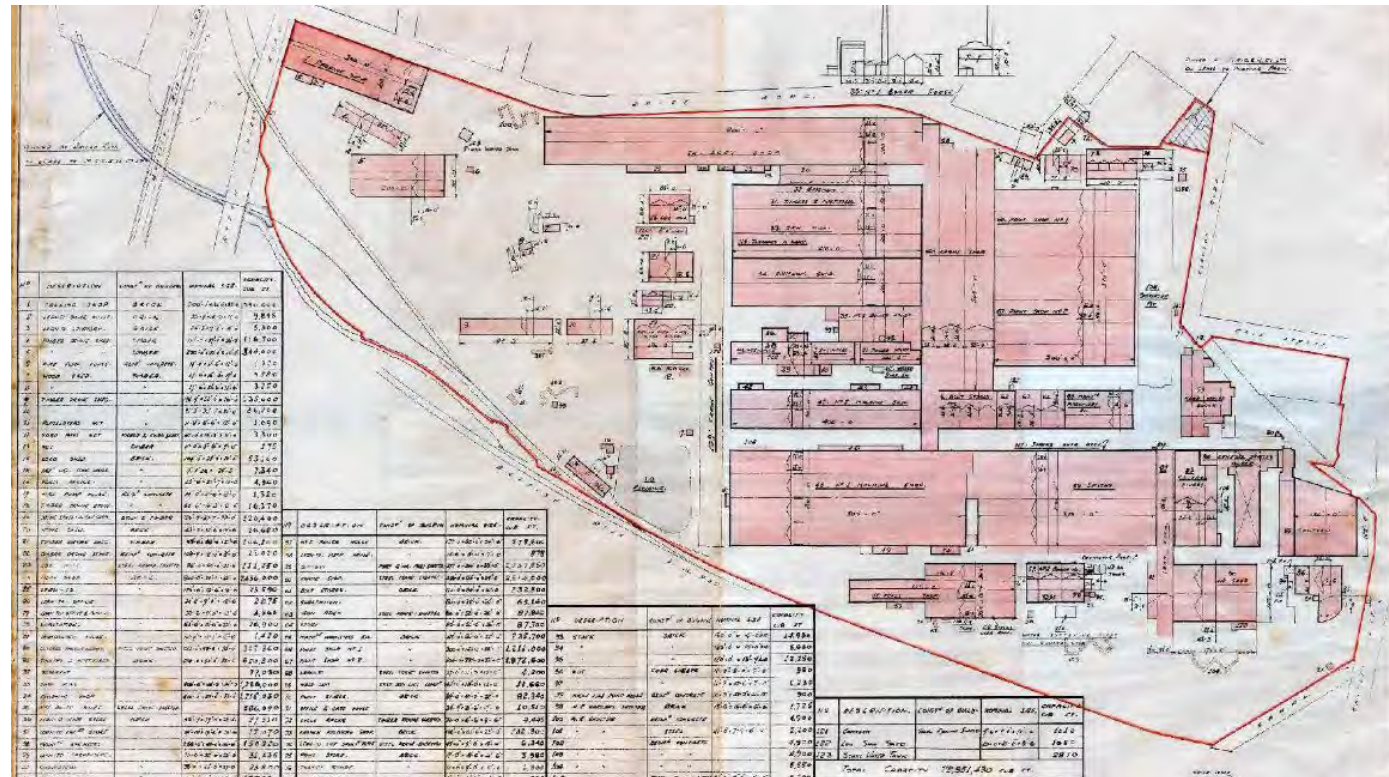


Plate 5: Plan of Saltley dated 1948

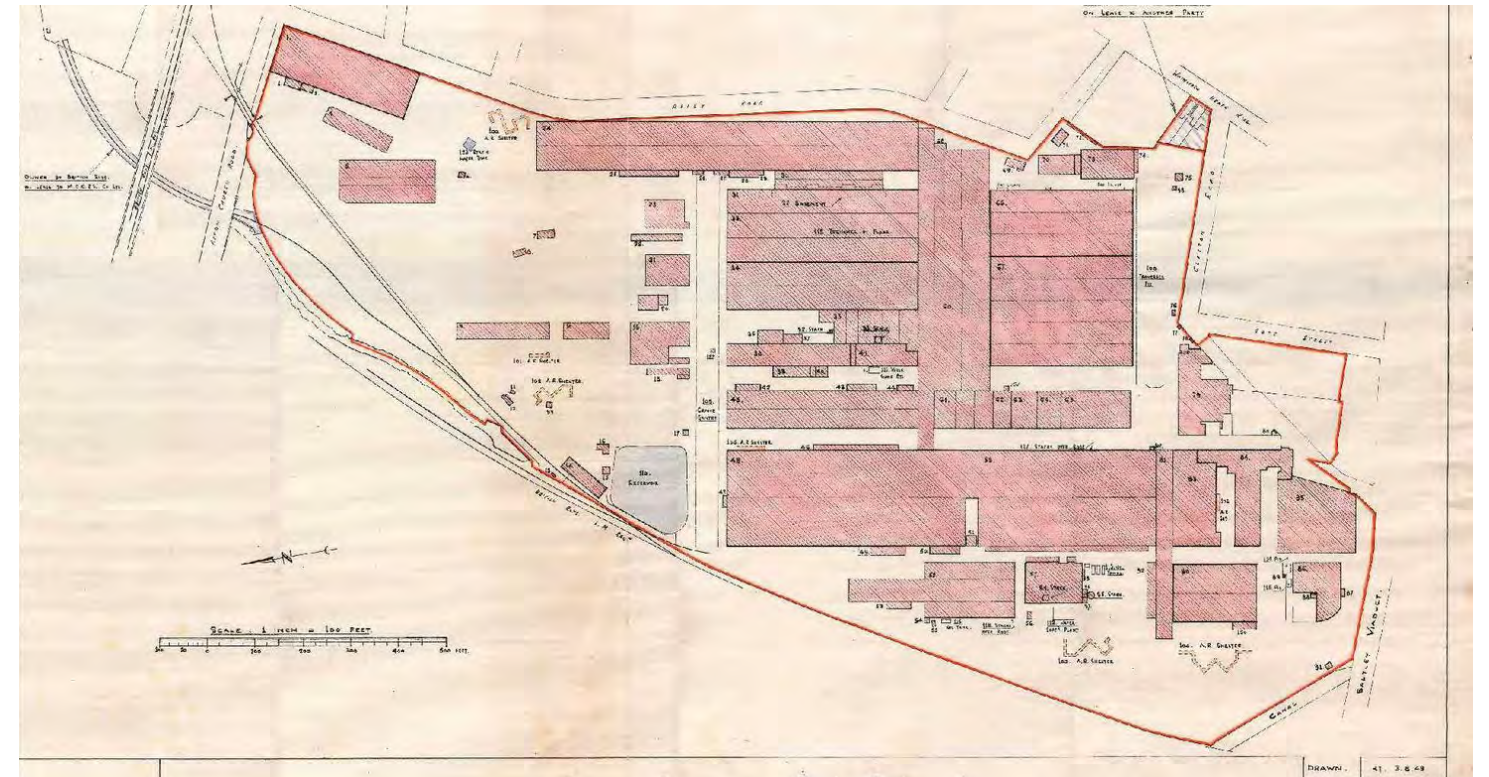


Plate 6: Plan of Saltley dated 1949

Map Number Appendix B - Plates 5-6

Map Name  
Saltley Business Park  
Map regression

Community Forum Area CFA26  
Washwood Heath to Curzon Street



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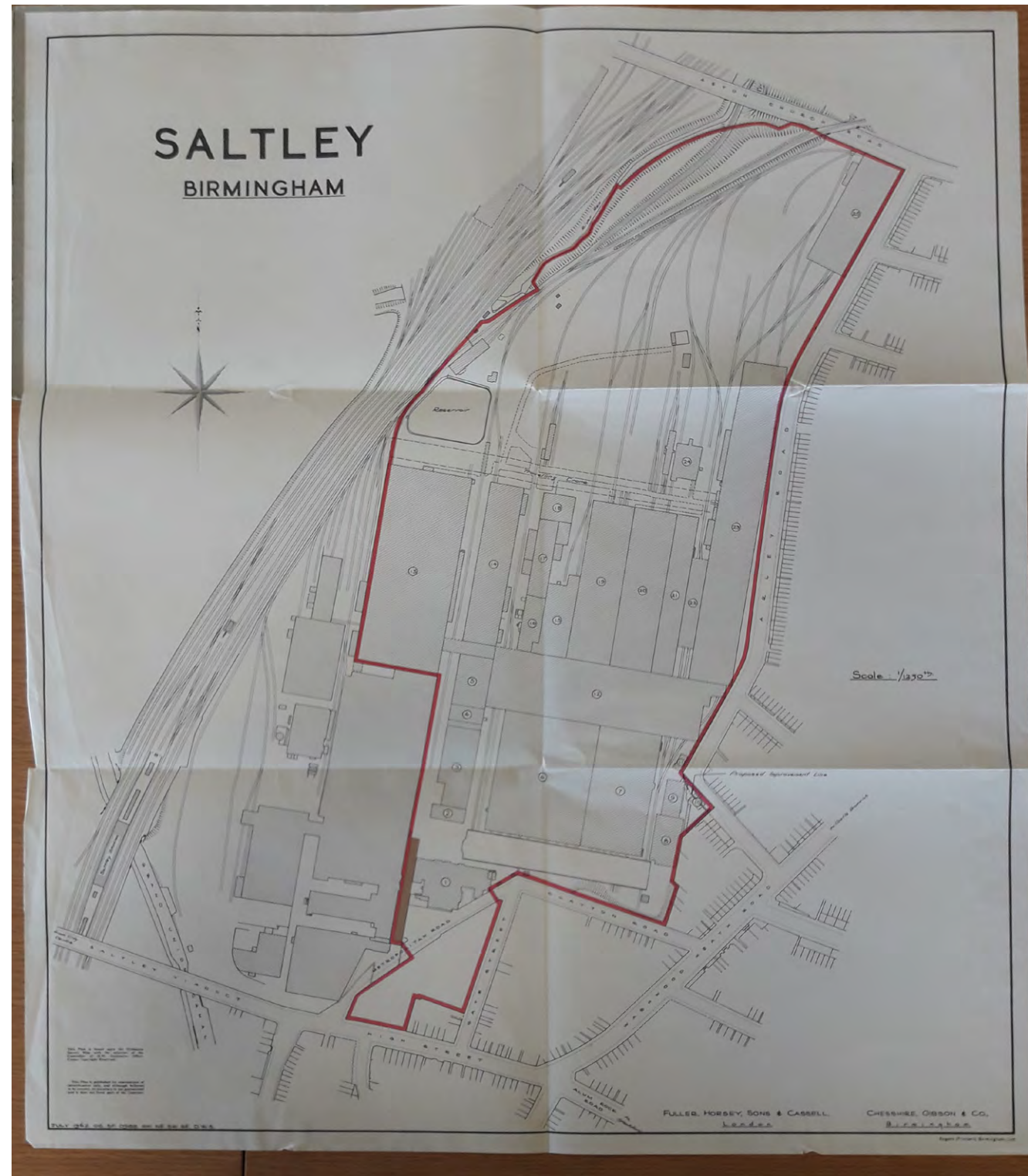


Plate 7: A 1962 plan by Fuller, Horsey, Sons & Cassell, London and Chessire, Gibson, Co, Birmingham  
Warwickshire county records Office

Map Number Appendix B - Plates 7

Map Name Saltley Business Park  
Map regression

Community Forum Area CFA26  
Washwood Heath to Curzon Street



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Date: 29/06/18



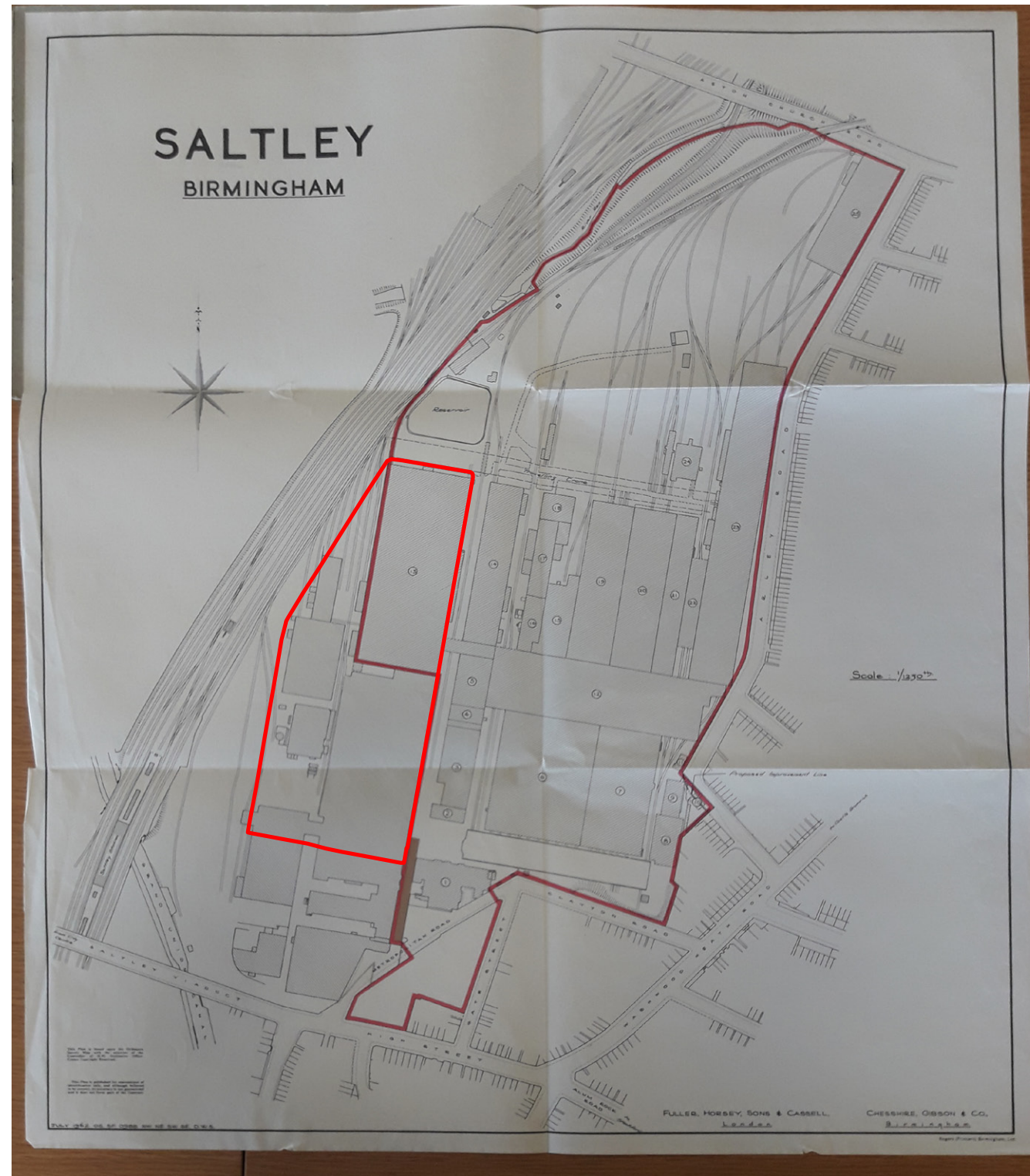


Plate 8: A 1962 plan by Fuller, Horsey, Sons & Cassell, London and Chessire, Gibson, Co, Birmingham  
Warwickshire county records Office

Map Number Appendix B - Plate 8

Map Name Saltley Business Park  
Map regression

Community Forum Area CFA26  
Washwood Heath to Curzon Street



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Plate 9: Saltley carriage works 1920, Ref EPW00137. © Historic England

Map Number Appendix B - Plate 9

Map Name Saltley Business Park  
Map regression

Community Forum Area CFA26  
Washwood Heath to Curzon Street



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Plate 10: Satley carriage works 1928, Ref EPW022293. © Historic England

Map Number Appendix B - Plate 10

Map Name  
Saltley Business Park  
Map regression

Community Forum Area CFA26  
Washwood Heath to Curzon Street



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Plate 11: Washwood heath 1989. © Historic England

Map Number Appendix B - Plate11

Map Name Saltley Business Park  
Map regression

Community Forum Area CFA26  
Washwood Heath to Curzon Street



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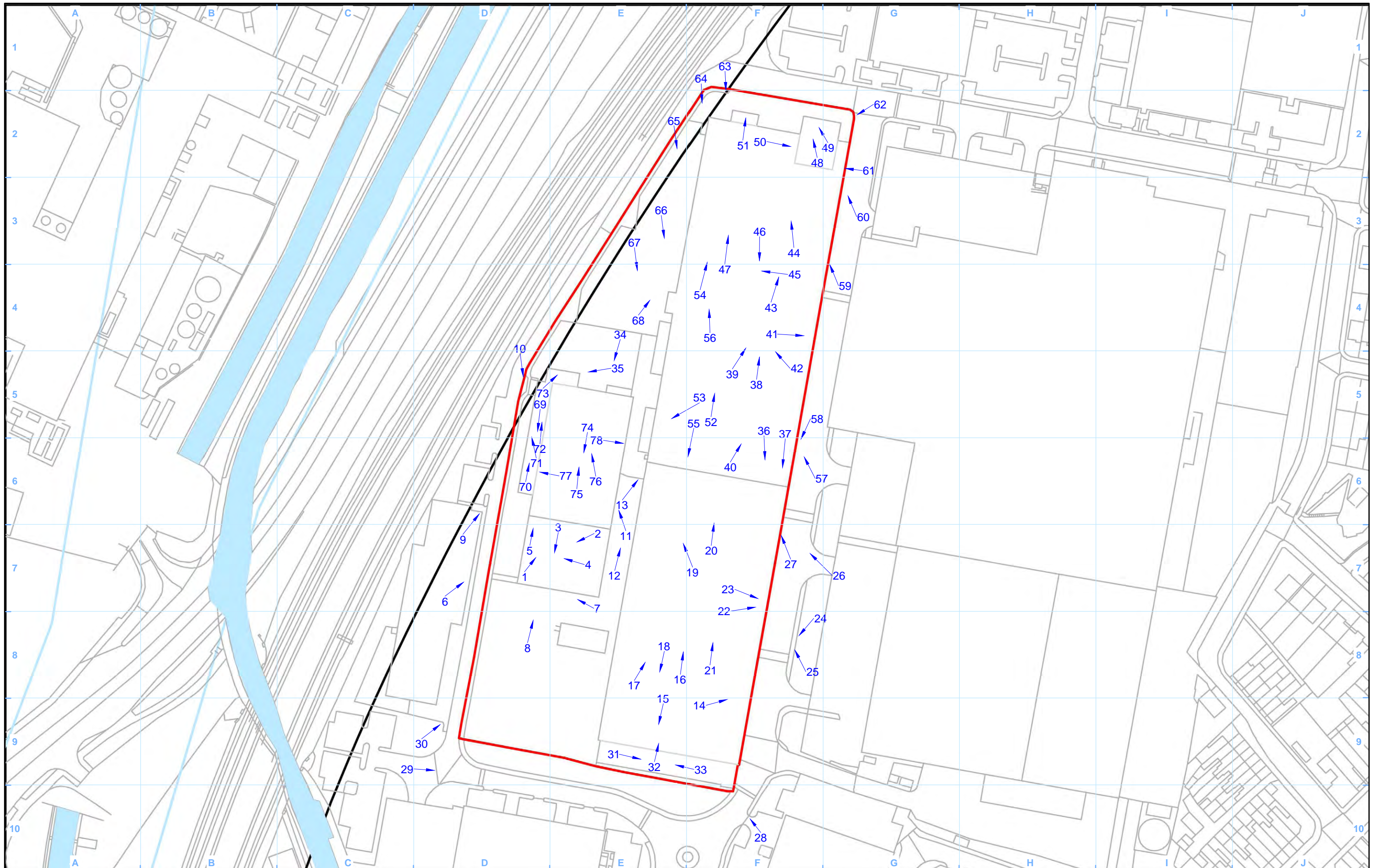
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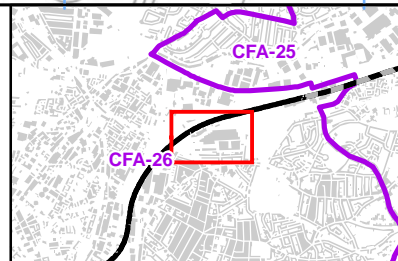
Doc Number: 1EW04-LMJ-EV-REP-NS03\_NL09-02900

Date: 29/06/18





**Legend**  
**HX** Building number  
**001** Photographic location



Map Number  
**Appendix C**  
 Map Name  
**Saltley Business Park  
 Photo Locations  
 Outside**  
 Community Forum Area CFA26  
 Washwood Heath to Curzon Street

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 Metres



# Appendix B: Plates





Photograph No. Saltley001

Description: General view of interior, north wall and north end of east wall

Site Location: Unit 3

Direction From: SW



Photograph No. Saltley002

Description: General view of interior, south wall with offices and south end of W wall

Site Location: Unit 3

Direction From: NE



Photograph No. Saltley003

Description: Brick and timber double story offices on internal south wall

Site Location: Unit 3

Direction From: NE



Photograph No. Saltley004

Description: Brick WCs on internal west wall

Site Location: Unit 3

Direction From: E





Photograph No. Saltley005  
Description: General view of office space interior  
Site Location: Unit 3  
Direction From: E



Photograph No. Saltley006  
Description: General view of exterior west elevation  
Site Location: Unit 3  
Direction From: WSW



Photograph No. Saltley007  
Description: General view of exterior east elevation, including rail lines running north-south  
Site Location: Unit 3  
Direction From: NNE



Photograph No. Saltley008  
Description: General view of exterior south gable wall  
Site Location: Unit 3  
Direction From: S





Photograph No. Saltley009  
Description: General view of exterior west elevation - offices  
Site Location: Unit 5  
Direction From: SW



Photograph No. Saltley010  
Description: General view of exterior west elevation - offices  
Site Location: Unit 5  
Direction From: NW



Photograph No. Saltley011

Description: Exterior east elevation at south end, showing blocked openings and windows

Site Location: Unit 5

Direction From: ENE



Photograph No. Saltley012

Description: General view of exterior east elevation at south end, with north-south aligned rail lines

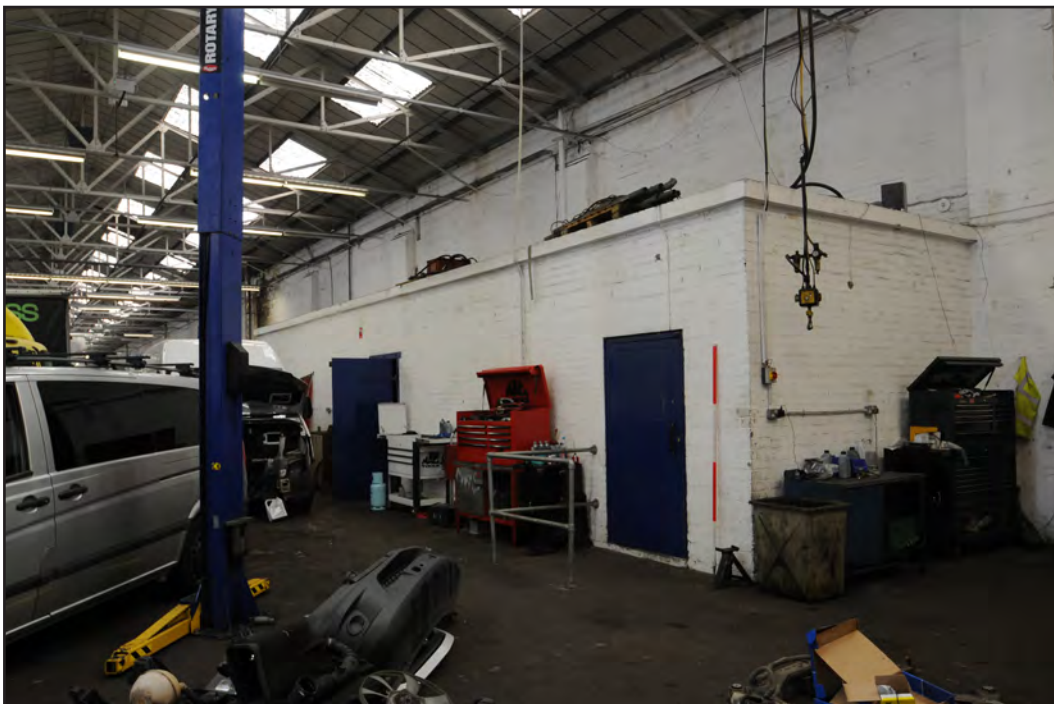
Site Location: Unit 5

Direction From: SSE





Photograph No. Saltley013  
Description: General view of exterior west elevation, north end  
Site Location: Mercedes-Benz  
Direction From: SW



Photograph No. Saltley014  
Description: Brick stores at south end of east internal elevation  
Site Location: Mercedes-Benz  
Direction From: SW



Photograph No. Saltley015  
Description: Detail of bricked up door on internal south gable wall, east end  
Site Location: Mercedes-Benz  
Direction From: NNE



Photograph No. Saltley016  
Description: General view of shed interior  
Site Location: Mercedes-Benz  
Direction From: SSE





Photograph No. Saltley017  
Description: General view of shed interior  
Site Location: Mercedes-Benz  
Direction From: SW



Photograph No. Saltley018  
Description: Office space at southeast corner of building interior  
Site Location: Mercedes-Benz  
Direction From: N



Photograph No. Saltley019

Description: Brick stores with 1st floor platform at northwest corner of building interior

Site Location: Mercedes-Benz

Direction From: ESE



Photograph No. Saltley020

Description: Detail of west bay being shorter than central and east bays, north interior gable end

Site Location: Mercedes-Benz

Direction From: SSW





Photograph No. Saltley021  
Description: Detail view of stanchion and roof trusses  
Site Location: Mercedes-Benz  
Direction From: NNE



Photograph No. Saltley022  
Description: Detail of blocked openings on central area of east wall  
Site Location: Mercedes-Benz  
Direction From: SW



Photograph No. Saltley023  
Description: Detail of blocked entrance at north end of east wall  
Site Location: Mercedes-Benz  
Direction From: W



Photograph No. Saltley024  
Description: General view of exterior east elevation, south end  
Site Location: Mercedes-Benz  
Direction From: NE





Photograph No. Saltley025  
Description: General view of exterior east elevation, central area  
Site Location: Mercedes-Benz  
Direction From: SE



Photograph No. Saltley026  
Description: General view of exterior east elevation, north end  
Site Location: Mercedes-Benz  
Direction From: SE



Photograph No. Saltley027

Description: Detail of steel doors at south end of exterior east elevation

Site Location: Mercedes-Benz

Direction From: NE



Photograph No. Saltley028

Description: General view of exterior south and east elevations

Site Location: Midlands

Direction From: SE





Photograph No. Saltley029  
Description: General view of south and west exterior elevations  
Site Location: Midlands  
Direction From: SW



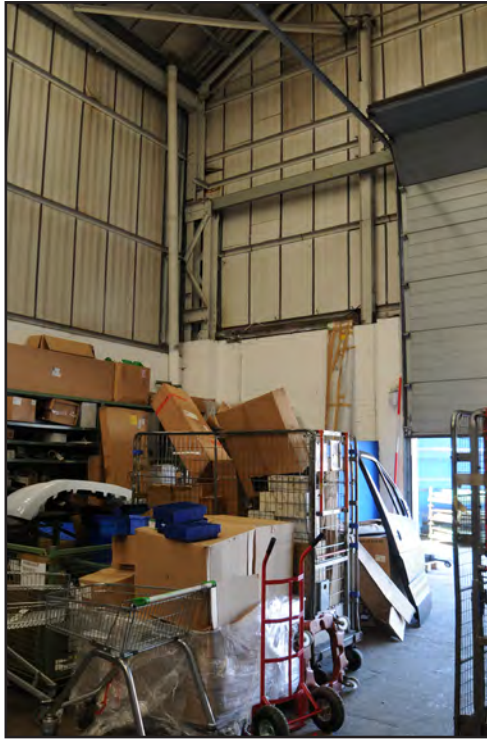
Photograph No. Saltley030  
Description: General view of exterior west elevations  
Site Location: Midlands/Mercedes  
Direction From: SW



Photograph No. Saltley031  
Description: General view of interior large space to east  
Site Location: Midlands  
Direction From: W



Photograph No. Saltley032  
Description: Exterior south elevation of Mercedes building with blocked opening, now  
Midland internal north elevation  
Site Location: Midlands/Mercedes  
Direction From: S



Photograph No. Saltley033

Description: Brick and metal sheet walls at Midland southwest internal corner

Site Location: Midlands

Direction From: NE



Photograph No. Saltley034

Description: Exterior north elevation with shed gable end and offices to east

Site Location: Unit 5

Direction From: N





Photograph No. Saltley035

Description: northeast corner of brick office and WCs at NW corner of building

Site Location: Unit 5

Direction From: NE



Photograph No. Saltley036

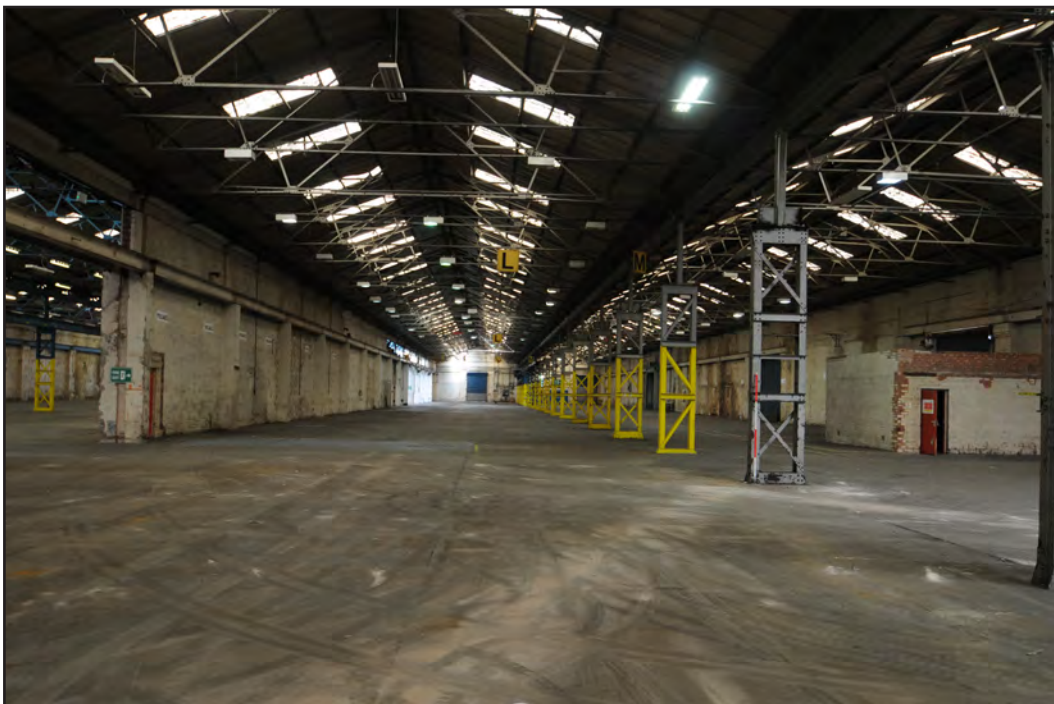
Description: southeast corner of shed with gable end and brick office space

Site Location: Anixter

Direction From: NW

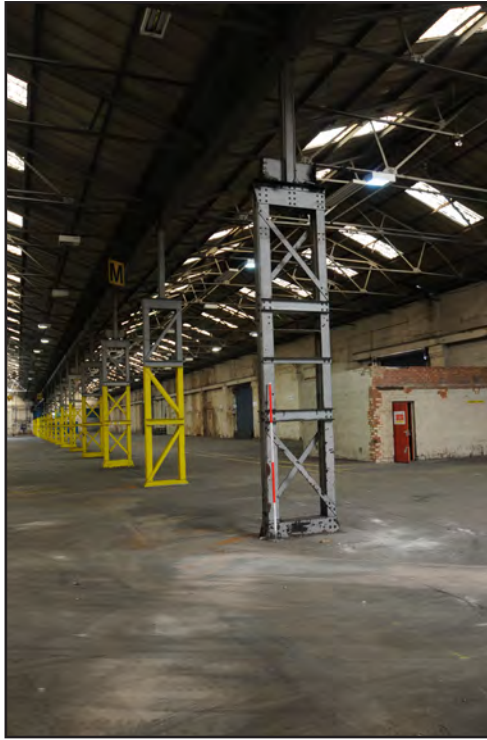


Photograph No. Saltley037  
Description: Interior view of brick office kitchen space  
Site Location: Anixter  
Direction From: N



Photograph No. Saltley038  
Description: General view of shed interior, two bays to east  
Site Location: Anixter  
Direction From: S





Photograph No. Saltley039  
Description: Detail of stanchion at southeast corner of building  
Site Location: Anixter  
Direction From: SW



Photograph No. Saltley040  
Description: southeast interior corner of sheds with brick offices and blocked openings  
Site Location: Anixter  
Direction From: WNW





Photograph No. Saltley041

Description: Brick electrical room and roller door at south end of interior east elevation

Site Location: Anixter

Direction From: SW



Photograph No. Saltley042

Description: Interior of electrical room

Site Location: Anixter

Direction From: SW



Photograph No. Saltley043

Description: Blocked openings on central area of interior east elevation

Site Location: Anixter

Direction From: SW



Photograph No. Saltley044

Description: northeast interior corner of shed with gable ends and modern office space

Site Location: Anixter

Direction From: SSE





Photograph No. Saltley045  
Description: EARL OF DUDLEY STEEL foundry mark on rail above stanchions at northeast interior corner of buildings  
Site Location: Anixter  
Direction From: E



Photograph No. Saltley046  
Description: Detail of weighbridge mechanism at northeast corner of interior  
Site Location: Anixter  
Direction From: N



Photograph No. Saltley047  
Description: General view of shed  
Site Location: Anixter  
Direction From: N



Photograph No. Saltley048  
Description: General view of office interior and entrance through to offices north of main building north elevation  
Site Location: Anixter  
Direction From: SW





Photograph No. Saltley049  
Description: General view of office space at northeast of building  
Site Location: Anixter  
Direction From: SW



Photograph No. Saltley050  
Description: General view of 1st floor office space  
Site Location: Anixter  
Direction From: SW





Photograph No. Saltley051  
Description: Kitchen as part of office space at northwest of building  
Site Location: Anixter  
Direction From: WSW



Photograph No. Saltley052  
Description: General view of west shed area  
Site Location: Anixter  
Direction From: N



Photograph No. Saltley053  
Description: Internal west wall, south end  
Site Location: Anixter  
Direction From: NE



Photograph No. Saltley054  
Description: Travelling crane in northwest area of shed  
Site Location: Anixter  
Direction From: S





Photograph No. Saltley055

Description: Gable end with blocked entrances and row of blocked openings above on interior south elevation

Site Location: Anixter

Direction From: N



Photograph No. Saltley056

Description: General view of west shed area

Site Location: Anixter

Direction From: S



Photograph No. Saltley057  
Description: Exterior east elevation, south end  
Site Location: Anixter  
Direction From: SE



Photograph No. Saltley058  
Description: Windows of office space at south end of exterior east elevation  
Site Location: Anixter  
Direction From: NNE





Photograph No. Saltley059  
Description: Exterior east elevation, central area  
Site Location: Anixter  
Direction From: SE



Photograph No. Saltley060  
Description: Exterior east elevation, north end of shed  
Site Location: Anixter  
Direction From: SE





Photograph No. Saltley061  
Description: Exterior east elevation, north end of shed and office area  
Site Location: Anixter  
Direction From: E



Photograph No. Saltley062  
Description: Exterior north elevation of shed and northeast office area  
Site Location: Anixter  
Direction From: NNE



Photograph No. Saltley063

Description: Exterior north elevation of shed, central area, and office space at northwest of building

Site Location: Anixter

Direction From: NW



Photograph No. Saltley064

Description: General view of building northwest exterior corner

Site Location: Anixter

Direction From: NW



Photograph No. Saltley065  
Description: Exterior west elevation, north end  
Site Location: Anixter  
Direction From: NNW



Photograph No. Saltley066  
Description: Exterior west elevation, central area  
Site Location: Anixter  
Direction From: NW





Photograph No. Saltleyo67  
Description: Exterior west elevation, south area  
Site Location: Anixter  
Direction From: NW



Photograph No. Saltleyo68  
Description: Exterior west elevation, central area  
Site Location: Anixter  
Direction From: SW



Photograph No. Saltley069  
Description: 1st floor office space  
Site Location: Unit 5  
Direction From: NNE



Photograph No. Saltley070  
Description: Stairway at southwest corner of building  
Site Location: Unit 5  
Direction From: SW



Photograph No. Saltley071  
Description: Ground floor office space  
Site Location: Unit 5  
Direction From: SSE



Photograph No. Saltley072  
Description: Corridor and truncated stairway  
Site Location: Unit 5  
Direction From: SSE





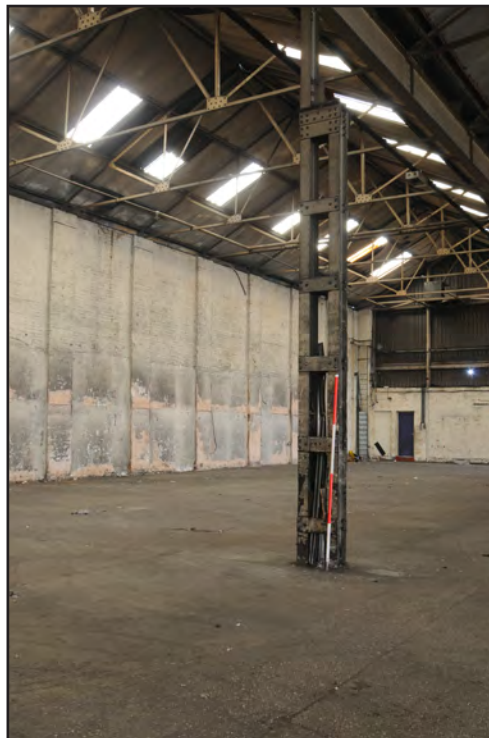
Photograph No. Saltley073  
Description: Stairway at northwest corner of building  
Site Location: Unit 5  
Direction From: SW



Photograph No. Saltley074  
Description: General view of shed interior  
Site Location: Unit 5  
Direction From: N



Photograph No. Saltley075  
Description: General view of shed interior  
Site Location: Unit 5  
Direction From: S



Photograph No. Saltley076  
Description: Detail of stanchion  
Site Location: Unit 5  
Direction From: SE





Photograph No. Saltley077

Description: Blocked openings and inserted door at centre of shed internal west elevation

Site Location: Unit 5

Direction From: E



Photograph No. Saltley078

Description: Blocked entrance and opening with inserted door at south end of shed internal east elevation

Site Location: Unit 5

Direction From: W



## Appendix C: Photographic register and full photographic record

Photographic Reference	Site Location	Description	Direction taken from
Saltley001	Unit 3	General view of interior, north wall and north end of east wall	SW
Saltley002	Unit 3	General view of interior, south wall with offices and south end of W wall	NE
Saltley003	Unit 3	Brick and timber double story offices on internal south wall	NE
Saltley004	Unit 3	Brick WCs on internal west wall	E
Saltley005	Unit 3	General view of office space interior	E
Saltley006	Unit 3	General view of exterior west elevation	WSW
Saltley007	Unit 3	General view of exterior east elevation, including rail lines running north-south	NNE
Saltley008	Unit 3	General view of exterior south gable wall	S
Saltley009	Unit 5	General view of exterior west elevation - offices	SW
Saltley010	Unit 5	General view of exterior west elevation - offices	NW
Saltley011	Unit 5	Exterior east elevation at south end, showing blocked openings and windows	ENE
Saltley012	Unit 5	General view of exterior east elevation at south end, with north-south aligned rail lines	SSE
Saltley013	Mercedes-Benz	General view of exterior west elevation, north end	SW
Saltley014	Mercedes-Benz	Brick stores at south end of east internal elevation	SW
Saltley015	Mercedes-Benz	Detail of bricked up door on internal south gable wall, east end	NNE
Saltley016	Mercedes-Benz	General view of shed interior	SSE
Saltley017	Mercedes-Benz	General view of shed interior	SW
Saltley018	Mercedes-Benz	Office space at southeast corner of building interior	N
Saltley019	Mercedes-Benz	Brick stores with 1st floor platform at northwest corner of building interior	ESE
Saltley020	Mercedes-Benz	Detail of west bay being shorter than central and east bays, north interior gable end	SSW
Saltley021	Mercedes-Benz	Detail view of stanchion and roof trusses	NNE
Saltley022	Mercedes-Benz	Detail of blocked openings on central area of east wall	SW
Saltley023	Mercedes-Benz	Detail of blocked entrance at north end of east wall	W
Saltley024	Mercedes-Benz	General view of exterior east elevation, south end	NE
Saltley025	Mercedes-Benz	General view of exterior east elevation, central area	SE
Saltley026	Mercedes-Benz	General view of exterior east elevation, north end	SE
Saltley027	Mercedes-Benz	Detail of steel doors at south end of exterior east elevation	NE
Saltley028	Midlands	General view of exterior south and east elevations	SE
Saltley029	Midlands	General view of south and west exterior elevations	SW

Photographic Reference	Site Location	Description	Direction taken from
Saltley030	Midlands/Mercedes	General view of exterior west elevations	SW
Saltley031	Midlands	General view of interior large space to east	W
Saltley032	Midlands/Mercedes	Exterior south elevation of Mercedes building with blocked opening, now Midland internal north elevation	S
Saltley033	Midlands	Brick and metal sheet walls at Midland southwest internal corner	NE
Saltley034	Unit 5	Exterior north elevation with shed gable end and offices to east	N
Saltley035	Unit 5	northeast corner of brick office and WCs at NW corner of building	NE
Saltley036	Anixter	southeast corner of shed with gable end and brick office space	NW
Saltley037	Anixter	Interior view of brick office kitchen space	N
Saltley038	Anixter	General view of shed interior, two bays to east	S
Saltley039	Anixter	Detail of stanchion at southeast corner of building	SW
Saltley040	Anixter	southeast interior corner of sheds with brick offices and blocked openings	WNW
Saltley041	Anixter	Brick electrical room and roller door at south end of interior east elevation	SW
Saltley042	Anixter	Interior of electrical room	SW
Saltley043	Anixter	Blocked openings on central area of interior east elevation	SW
Saltley044	Anixter	northeast interior corner of shed with gable ends and modern office space	SSE
Saltley045	Anixter	EARL OF DUDLEY STEEL foundry mark on rail above stanchions at northeast interior corner of buildings	E
Saltley046	Anixter	Detail of weighbridge mechanism at northeast corner of interior	N
Saltley047	Anixter	General view of shed	N
Saltley048	Anixter	General view of office interior and entrance through to offices north of main building north elevation	SW
Saltley049	Anixter	General view of office space at northeast of building	SW
Saltley050	Anixter	General view of 1st floor office space	SW
Saltley051	Anixter	Kitchen as part of office space at northwest of building	WSW
Saltley052	Anixter	General view of west shed area	N
Saltley053	Anixter	Internal west wall, south end	NE
Saltley054	Anixter	Travelling crane in northwest area of shed	S
Saltley055	Anixter	Gable end with blocked entrances and row of blocked openings above on interior south elevation	N
Saltley056	Anixter	General view of west shed area	S
Saltley057	Anixter	Exterior east elevation, south end	SE
Saltley058	Anixter	Windows of office space at south end of exterior east elevation	NNE

Photographic Reference	Site Location	Description	Direction taken from
Saltley059	Anixter	Exterior east elevation, central area	SE
Saltley060	Anixter	Exterior east elevation, north end of shed	SE
Saltley061	Anixter	Exterior east elevation, north end of shed and office area	E
Saltley062	Anixter	Exterior north elevation of shed and northeast office area	NNE
Saltley063	Anixter	Exterior north elevation of shed, central area, and office space at northwest of building	NW
Saltley064	Anixter	General view of building northwest exterior corner	NW
Saltley065	Anixter	Exterior west elevation, north end	NNW
Saltley066	Anixter	Exterior west elevation, central area	NW
Saltley067	Anixter	Exterior west elevation, south area	NW
Saltley068	Anixter	Exterior west elevation, central area	SW
Saltley069	Unit 5	1st floor office space	NNE
Saltley070	Unit 5	Stairway at southwest corner of building	SW
Saltley071	Unit 5	Ground floor office space	SSE
Saltley072	Unit 5	Corridor and truncated stairway	SSE
Saltley073	Unit 5	Stairway at northwest corner of building	SE
Saltley074	Unit 5	General view of shed interior	N
Saltley075	Unit 5	General view of shed interior	S
Saltley076	Unit 5	Detail of stanchion	SW
Saltley077	Unit 5	Blocked openings and inserted door at centre of shed internal west elevation	E
Saltley078	Unit 5	Blocked entrance and opening with inserted door at south end of shed internal east elevation	W



