



CRYSTAL PALACE STATION
Crystal Palace Station Road
London SE19

London Borough of Bromley

Standing building survey report

May 2009

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Site Code: CYX08

National Grid Reference: 534123 170548

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Summary

Museum of London Archaeology was commissioned by Transport for London to analyse and record a standing building at Crystal Palace Station, Crystal Palace Station Road, London SE19. Crystal Palace Station is grade II listed, and is in a conservation area. Some elements within the building were to be demolished or altered prior to its refurbishment as part of the extension of the East London Line, and the archaeological investigation and a subsequent report were required as a condition of planning consent for the redevelopment. The investigation took place in April 2009.

Crystal Palace Railway Station was opened in 1854 to serve the large number of visitors who came to visit the Crystal Palace, which had been relocated from Hyde Park, where it had housed the Great Exhibition of 1851. The station was extended in 1857 and was partially rebuilt in the 1870s; the main ticket hall was relocated to a new building immediately to the south in 1986.

This report combines documentary and archive research into the history of the building, along with the results of the archaeological survey; both a wider view of the fabric of the whole building and the specific areas due to be altered for the refurbishment of the station. Revisions to the scope of works were made by Transport for London in April 2009 whilst fieldwork was underway, altering the specific areas to be recorded. Both the original and the new areas are included in this report thereby enabling the planning conditions to be discharged.

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1 Introduction

1.1 Site background

Crystal Palace Station, also known as Crystal Palace Low Level Station, is located in Crystal Palace Station Road, on the south-western side of Crystal Palace Park, in the London Borough of Bromley. The centre of the building lies at Ordnance Survey National Grid reference 534123 170548 (*Fig 1*). Crystal Palace Station was built by the London and Brighton Railway Company and opened in 1854 to take visitors to the newly-relocated Crystal Palace, which had been moved from its original site in Hyde Park, where it had housed the Great Exhibition of 1851, to Sydenham Hill, about 200 m to the north-west of the station. An Act of Parliament of 1853 enabled the London and Brighton Railway Company to construct a branch line to convey trains from the existing railway line between London Bridge and Croydon. The station was extended in 1857 and the entrance building was rebuilt in the 1870s. In 1865, the London, Chatham & Dover Railway opened a competing station to the west. In order to differentiate the two stations, the newer station – which has since been substantially demolished – was known as the Crystal Palace High Level Station, while the older station was referred to as the Low Level Station. In the 1980s, new entrance hall and ticket office were constructed on the south side of the Low Level Station's 1870s entrance building.

The building is statutorily listed as a building of special architectural or historic interest, at Grade II, and is located adjacent to, but not within, an Archaeological Priority Zone. A conservation strategy report (Lau 2007) has been prepared, outlining the history of the site. Apart from this and the listing description (below), there are no known publications focused specifically on Crystal Palace Station.

1.2 Proposed development

The proposed development involves the demolition of the 1980s entrance hall and ticket office. The 19th-century ticket hall will be restored to its former function, and the timber ticket office, which is currently situated in the disused ticket hall, will be removed, as will some doors and dividing walls at the southern end of the building. A lift and pedestrian walkway will be constructed on the eastern side of the station, on the site of a former staircase that led down to the southern side of the main building's island platform. Passenger lifts will be constructed at the western end of the existing footbridge (adjacent to Platform 1), and within the main building, on Platforms 3 and 4 and on the south side of the original island platform (now disused and not numbered). A canopy will be constructed over Platforms 3 and 4 in approximately the same position as a roof which extended over the platforms until its removal in 1905.

1.3 Origin and scope of the report

An application is being made to the London Borough of Bromley for listed building consent. A programme of archaeological investigation and recording has been recommended by English Heritage (GLAAS), advising the Conservation Officer and Planning Department of the London Borough of Bromley.

In 2008, Transport for London prepared a brief for building recording at Crystal Palace Station. Subsequently, Museum of London Archaeology (MOL Archaeology) was commissioned by Transport for London to carry out a standing building survey in accordance with a site-specific project design prepared by MOL Archaeology in April 2009. The site-specific project design presents the mitigation strategy necessary to support the listed building consent for the redevelopment at the site.

The planning policy and legislation relevant to this investigation have been described in the site-specific project design document (MOL Archaeology 2009), wherein the specific relevance of *Planning Policy Guidance: Planning and the Historic Environment* (PPG15) is noted. PPG15 (Department of the Environment 1994) sets out the Secretary of State's policy on the visible remains of historic buildings, spaces and structures, and provides recommendations many of which have been integrated into local development plans.

Survey work was completed in April 2009, and the results of the survey are presented in this report. The survey, and this report, are intended to supplement and not duplicate the contents of the conservation strategy report (Lau 2007).

Within the limitations imposed by dealing with historical maps and other materials, the information in this document is, to the best knowledge of the authors and MOL Archaeology, correct at the time of writing. Further archaeological investigation or more information about the nature of the present building may require changes to all or parts of the document.

1.4 Specific research aims

The overall aim of the programme of archaeological work was to secure 'preservation by record' of those aspects of the site and the existing building that are of archaeological, architectural and historic interest. As a survey designed to generate an analytical record of the building, the work corresponds to the form of record and reporting at Level 3 in the specifications *Understanding historic buildings: A guide to good recording practice* recommended by English Heritage (EH 2006).

The archaeological investigation has concentrated on elucidating the successive major builds and episodes of construction evident in the fabric of the building, and also evident in available documentary sources. For these purposes, and in light of the specific removals planned as part of the proposed development, the following features were recorded:

- the timber ticket office within the existing 19th-century ticket hall;
- the external doors to the building that are to be replaced;
- the internal walls that are to be demolished;
- the fireplaces that are to be blocked up;
- the 1980s aluminium, glass and brick entrance hall and ticket office (which are to be demolished);
- the side walls of Platforms 3 and 4, where a new roof canopy extending over the tracks will be installed; and
- the areas where new passenger lifts will be constructed.

1.5 Summary description of the building

Crystal Palace Station comprises a large agglomeration of structures including old and new entrance buildings to west and platforms to the east. The statutory listing of the building includes the following description.

Mid C19. Built for visitors to the Crystal Palace and consequently in the grand manner. It consists of 2 pavilions with a linking portion of 1 storey. The left side portion [to the north] is in French Chateau style. 2 storeys stock brick. Very steep hipped slate roof with decorative ironwork along ridge. Stone moulded eaves cornice 2 double round headed windows. The central section, containing the ticket office, is of 1 storey stock brick having 2 round-windows. The central section, containing the ticket office, is of 1 storey stock brick having 2 round-headed door cases with fanlights and 4 windows. Canopy now missing. The right hand portion [to the south] is of 2 storeys stock brick with hipped slate roof. Modillion cornice. 3 sashes, round headed on the ground floor.
(English Heritage, 1973)

It must be noted that the above description mentioned details that were no longer correct at the time of investigation. The slate roof of the northern pavilion had been removed and much later replaced by a sheet-metal-clad mansard roof. Also, a new canopy had since been erected along the façade of the central section. Therefore an updating of the listing description is highly recommended. The following summary description has been prepared to describe the station as it was found during the investigation.

Because the tracks run in short tunnels underneath the entrance buildings, the platforms are situated at a lower level than the entrances and are presently reached by stairs. The tracks and platforms are organised into two main groups. Both platform groups extend east to west, although they extend outward from the entrance area at different angles due to the different alignments of the tracks they serve. The southern group of tracks extends westward from the station in a more southerly direction, ultimately running south toward Norwood. The northern group of tracks extends westward and ultimately sweeps north towards Sydenham.

The northern group of tracks runs between two long east–west walls which originally supported the roof of a large train shed covering these tracks. The walls were part of the first Crystal Palace Station, and are constructed mainly of red brick. At their western ends, the former train shed walls abut the east side of the main station building. The latter comprises a large brick shed covering the westernmost portion of the northern group of platforms, with stairs leading up to the extant 19th-century entrance building. The tracks exit the west side of the main station building through the vaulted supports for the entrance building and for the northern end of Station Road. The main building is divided in two by a spine wall running east to west.

The extant 19th-century entrance building sits well above the level of the tracks, abutting the upper portion of the main building's western end. It was built in 1877, replacing an earlier entrance building which opened in 1854, and is constructed of yellow London stock brick with red-brick string courses and stone dressings. The building comprises two-storey pavilions linked by a long, single-storey structure which contains the former ticket hall. The roof of the central, single-storey ticket hall building is supported on rib-like structural members (roof principles) of cast iron perforated in foliage patterns. Within the hall, a roofed timber booth was constructed against the building's west wall, to serve as a ticket office. Doors on the eastern side of the former ticket hall lead to a landing within the main station building, over the northern group of tracks. Stairs up from the platforms meet the landing at its northern and southern ends (in the north-western and south-western corners of the main building), as well as in the middle (on either side of the main building's spine

wall). The main doors to the 19th-century entrance building are in the western wall of the ticket office, leading to Station Road.

On the ground floor, the northern pavilion – hereafter referred to as the ‘north tower’, in conformity with the conservation strategy report (Lau 2007) – contained a refreshment room in addition to a two-aisled ramp that formerly led to a covered walkway linking the station with the Crystal Palace. The 1st floor of the north tower was divided into several rooms, including a dining room and kitchen. At the time of investigation, the upstairs rooms were empty and undergoing refurbishment. The southern pavilion’s ground floor is divided into several rooms which are presently undergoing refurbishment. The first floor of the southern pavilion – hereafter referred to as the ‘south tower’ – was not accessible, but is presently in use as an office.

The first floors and basements of the station buildings were not within the scope of this investigation.

At the southern end of the 19th-century entrance building, on the east side of Station Road, is the new entrance hall and ticket office, opened in 1986. The entrance hall, constructed of aluminium and plate glass links the new yellow stock brick booking office to the south side of the old entrance building’s south tower. A staircase leads down from the 1980s entrance area to a covered, iron footbridge over the southern group of platforms. Like the northern group of platforms, these platforms pass below the entrance buildings, exiting the station to the west. The iron footbridge crosses north to south over the southern group of platforms and leads, at its northern end, into the main station building. Stairs lead down from the footbridge to either side of the southern platforms. The southernmost of these platforms is partially sheltered by a canopy.

1.6 Conventions used in this report

Although the station is oriented approximately north-west to south-east, for simplicity of description this is taken to be west to east.

In this report, the following terms are used to refer to the station buildings:

- **main building** – the roofed area over the west end of the northern group of platforms
- **(former) train shed walls** – the walls running eastward from the main building, to the north and south of the northern group of platforms. These walls formerly supported a roof, and were thus part of the original train shed, together with the main building – although this longer, eastern portion of the train shed was not as tall as the main building.
- **(19th-century) entrance building** – the brick structure at the western end of the station comprising two tower buildings linked by a hall
- **north tower** – the northern pavilion of the 19th-century entrance building, referred to as a ‘tower’ in conformity with the conservation strategy report (Lau 2007)
- **south tower** – the southern pavilion of the 19th-century entrance building, referred to as a ‘tower’ in conformity with the conservation strategy report (Lau 2007)
- **ticket hall** – the single-story hall in the centre of the 19th-century entrance building, linking the north and south towers
- **timber ticket office** – the timber booth constructed within the ticket hall

- **(1980s) entrance area** – the present entrance to the station, comprising the entrance hall and the brick ticket office
- **(1980s) entrance hall** – the aluminium and glass structure linking the south tower (to the north) to the brick ticket office (to the south)
- **brick ticket office** – the 1980s ticket office constructed south of the 19th-century entrance building and linked to that building by the aluminium and glass entrance hall
- **(iron) footbridge** – the bridge over the southern group of platforms
- **Platform 1** – the southernmost platform of the southern group of platforms (the platform numbers used in this report are those current at the time of writing)
- **Platform 2** – the northernmost platform of the southern group of platforms
- **Platform 3** – the southernmost platform of the northern group of platforms, within the main building and the area enclosed by the former train shed walls
- **Platform 4** – the northernmost platform of the northern group of platforms
- **island platform** – the platform between Platforms 3 and 4. In fact, the island platform is divided down the middle into two platforms. At the time of the investigation, the island platform was truncated a short distance east of the main building, although it formerly extended the length of the original train shed (and was divided along its entire length into a northern platform and a southern platform).
- **roof principals** – in the ticket hall, the primary structural members spanning the hall to support and give shape to the roof

2 Historical Background

In 2007, a conservation strategy for Crystal Palace Station was prepared by Scott Brownrigg (Lau 2007), outlining the history of the site together with reproductions of relevant historic maps, plans, and photographs. This conservation strategy report was based on an examination of materials housed at the London Transport Museum, the National Railway Museum, the London Metropolitan Archives, the National Monument Record at Swindon, the library of the Royal Institute of British Architects, the Bromley History Centre, the Surrey History Centre, and the Network Rail Archive, in addition to previously-published secondary sources listed in the report's bibliography.

The archaeological investigation reported in the present document was intended to complement the conservation strategy report without duplicating it, and therefore this discussion of the building's historical development is drawn largely from the evidence and analysis presented in the 2007 document, with added analytical comments and observations.

After the 1851 Great Exhibition in Hyde Park, the Crystal Palace – an enormous exhibition building constructed of cast-iron and glass – was moved to the top of Sydenham Hill, where it was to serve as the focus of new landscaped pleasure grounds. The relocated Crystal Palace, which was approximately 1.5 times the size of the original (Piggott 2004, 40), was opened to the public in 1854 and, in the same year, the new Crystal Palace railway station was opened adjacent to the grounds on a new line of the Brighton and South Coast Railway – later the London, Brighton and South Coast Railway. The station was designed by the engineer Jacob Hood and was intended to handle the large volume of visitors expected to be travelling to the Crystal Palace.

The original station comprised a long train shed building with a smaller entrance building abutting the shed to the west. The entrance building sat on higher ground than the tracks and train shed and was accessed by stairs leading up from the platforms at their western ends. The western end of the train shed, over the stairs and abutting the entrance building, was much taller than the balance of the shed, and featured an eastern wall that was solid above the level of the lower-roofed shed, with arched openings over the tracks to allow trains to enter. The taller western portion of the original train shed remains today and is referred to in this report as the 'main building'. Only the outer walls of the rest of the train shed remain.

The entire train shed formerly featured an east-to-west spine wall rising above a central island platform. Within the taller, western portion of the structure (ie, within what is now referred to as the main building) the spine wall was solid at platform level, but, within the long eastern portion of the train shed, the spine wall was arcaded. However, the openings of the arcade were closed by metal railings in order to maintain the division of the island platform along its entire length.

In addition to the northern and southern sides of the island platform, there were platforms near – but not abutting – the outer walls of the train shed, with tracks laid between the platforms and the walls. These outermost tracks, entering the station from the west, did not pass entirely through the train shed. Beyond the western termini of these outer tracks, that is, just within what is now referred to as the main building, the outer platforms widened to meet the train shed walls and there were stairs running up to the entrance building. In all, there were four tracks within the train shed: one along each of the train shed walls and one on either side of the divided island platform. Only the two tracks surrounding the island platform passed

through the entire length of the station and continued beyond. A total of four stairways led up from the platforms to the entrance building at the western end of the train shed: one stairway along each of the train shed's side walls and one on either side of the spine wall (ie, on each side of the divided island platform).

The original entrance building itself is poorly documented. The conservation report (Lau 2007, 6) states that '[n]o photographs of the original building have been discovered, however *The Builder* (26 May 1877) describes the building as being of timber construction.' It is known that this building was connected to the Crystal Palace some 200m to the north-west by a tall and imposing, glazed passage.

The conservation report also notes that an act of parliament was passed on 5 August 1853 to enable the railway from Crystal Palace to be extended westward beyond the station, to Wandsworth. This development took place less than a month after the passing of the act – on 8 July 1853 (Lau 2007, 5) – enabling the running of track from the north-east to Sydenham Hill, which necessitated the construction of Crystal Palace Station. Thus, although Lau reports that '[t]o achieve this [the extension of the railway toward Wandsworth], the new line was extended through the back wall [of the station]', it seems probable that the station was, in fact, originally designed to facilitate this. We suggest that the station was originally constructed with the openings and underpass structure necessary to extend the line westward to Wandsworth. In other words, the station was built such that the middle two tracks could be run through archways in the western wall of the main building, below the street-level entrance building, which was supported by vaulting. This is, in general, the arrangement that remains at present, although the original entrance building has been replaced. Although we suggest that the station was built to allow for the westward extension to Wandsworth, the extension itself was not opened until 1856 (Lau 2007, 7).

In 1857, a new line was run from the tunnel west of Crystal Palace station, passing just to the south of the train shed. The existing cutting west of the station, was widened to allow the new line to pass to the south. Two new platforms were constructed to the south of the train shed, flanking the new tracks and corresponding to the present Platforms 1 and 2.

In 1865, the London, Chatham and Dover Railway Company (LCD) opened a rival station at Crystal Palace to the west of the London, Brighton and South Coast station and closer to the Crystal Palace building. This new station was known as the High Level Station, while the older station became known as the Low Level Station.

In 1877, the London, Brighton and South Coast Railway (LBSC) decided to replace the original entrance building of the Crystal Palace Low Level Station at a cost of £13,000. Lau (2007, 8) states that the redevelopment was overseen by Frederick Dale Bannister, the successor of Jacob Hood (the designer of the original station) as the railway company's engineer. Bannister was apparently assisted in this by Whitley Mannering, and the signatures of both of these men can be discerned on drawings reproduced in the 2007 conservation report. These drawings are dated 1875 and also bear the inscription of 'Dove Brothers', most probably in reference to the well-known Islington-based building firm (whose extensive archival material is available at various London libraries and archives).

Lau also states that:

The architect was HR Gough. Research into HR Gough's career at the RIBA Library has not revealed any information about his career and he does not appear to have carried out any other work for the London, Brighton and South Coast Railway. It would therefore seem that the Crystal Palace Low Level Station was one of his most important buildings.
(Lau 2007, 8)

As an avenue of future research, we suggest that it would be worth investigating whether or not HR Gough was, in fact, Hugh Roumieu Gough, the son of Alexander Dick Gough. Hugh Roumieu Gough (1842 or 3 to 1904) was the architect of various London churches including St Paul's, Hammersmith, and parts of St Stephen's in Gloucester Road, South Kensington, among other sorts of work. A biography file for Hugh Roumieu Gough is available at the Royal Institute of British Architects (RIBA), although the information value of its contents has not been ascertained.

The new station survives today as the 19th-century entrance building, with north and south towers on either end of the long ticket hall. This building has been described briefly in Section 1 above, and further details are added in Section 3, below. To these details can be added the functions of the interior spaces as indicated on drawings from 1875 (as presented in Lau 2007, vol. 2), although it should be borne in mind that these are preliminary drawings and the building was in fact constructed slightly differently. Nonetheless, the arrangement of the interior as observed during this investigation accords well with most of the 1875 drawings, although with notable differences that are mentioned below where relevant.

According to the drawings presented in the conservation strategy report (Lau 2007), the ground floor of the north tower contained a refreshment room with a large bar and a staircase leading up to a dining room on the first floor. To the east of the refreshment room, and running the north–south length of the tower, was a double corridor (ie, a rectangular space divided down its long axis an open-arched, load-bearing wall). The entirety of the floor surface of the north tower's ground floor sloped upward from the level of the ticket hall floor on the south to the ground level outside the building to the north. A cloak room shown in the drawings was to have been housed in a small block (no longer extant) built on to the north end of the eastern wall of the tower, accessible through a single door in the north-east corner of the ground floor. One of the archways in the east wall of the north tower also communicated with this cloak room, and it may be that it was through this archway that the public was served.

The 1875 plan of the ground floor shows that the bar of the north tower's refreshment room was to have been located near the west wall of the building, and the refreshment room was to be separated from the double corridor by doorways set within the arches of another load-bearing internal wall. The ground floor of the north tower was joined to the central ticket hall by three arches, one of which led to the refreshment room, and one to each division of the corridor. At the northern end of the corridor, two arches led to a small, low lobby building that linked the station entrance building to the existing glazed passage to Crystal Palace.

It would seem sensible to view the ground floor of the north tower, or at least the broad double corridor, as a funnel intended to move large numbers of visitors swiftly into and out of the link to Crystal Palace; however, the actual arrangements did not seem to favour efficiency and speed. The cloak room, which would need to be accessed both when coming and going by those dining upstairs, was placed in the corner of the corridor nearest the doors to the Crystal Palace link, thus preventing the corridor from being strictly divided into traffic lanes. Also, the refreshment bar was oriented to attract customers as they passed by. In addition, there was a second refreshment area to the south, better placed to serve travellers in the ticket hall. These arrangements give the impression of a place designed to encourage milling about and spending money in the station, rather than any attempt to facilitate rapid movement between Crystal Palace and the trains.

On the 1875 plan, the spaces of the north tower's first floor are labelled as a dining room (presumably a commercial operation), a kitchen, a scullery, a larder and a

bedroom. The presence of scullery and larder lend credence to the notion that the dining room was for regular use by the public or for pre-booked functions. This is also supported by the dividing up of the basement vaults into beer and wine cellars (according to the 1875 plans). Perhaps the bedroom was for the use of an employee associated with the running of the dining and refreshment areas.

The 1875 drawings show that the ground floor of the south tower was to be divided into several smaller spaces including a refreshment room, a ladies' waiting room with a separate ladies lavatory, a first-class ladies' waiting room with its own separate lavatory, and a telegraph room. The first floor of the south tower was to be an apartment for the station master and was to be accessible only from outside, by the single, central door in the south wall of the building. At the time of investigation, there was also an internal door leading from the south-westernmost room of the south tower's ground floor to the ground floor landing of the stairs up to the first floor.

According to the 1875 plans, the south tower's refreshment room communicated by archways with the ticket hall to the north, as well as with a corridor to the east leading southwards from the ticket hall to the ladies' waiting areas. The 1875 drawing of the ground floor of the entrance building depicts servery bars across these archways such that the refreshment room was actually to be accessible only to staff who would serve the public over the counters. The public would therefore order and pay for their refreshments while standing within the ticket hall or in the corridor to the first class and general ladies' waiting areas.

The telegraph room was to be accessible from the ticket hall through a single doorway much smaller than the arch in which it was set.

The 1875 drawings show a fireplace in the centre of each long wall of the ticket hall, the one to the east being much larger and more elaborate than the one to the west. The eastern one was to be within the public area of the hall, between the archways leading to the train shed. The one to the west was to be within a timber ticket office which was planned to be smaller than the one present at the time of investigation. It is noteworthy that the 1875 drawings show, for the east wall of the ticket hall, a remarkably different design than that which appears to have been constructed. The east wall (and not the west wall) was to have featured eight archways arranged in two groups of four, each group on either side of the central fireplace. These arches would be supported on columns, except for the wider central pier below the chimney. As shown on the drawings themselves, this would have put them relative to the roof principals in such a way that only the inner three principals would have been aligned over columns or the central pier, while the outer two principals would have been aligned over the crowns of arches. Accordingly, and as shown on an 1875 elevation drawing, the beams would have sprung from corbels bonded into the walls rather than resting on imposts above piers (as they ultimately were constructed to do). This difference between the 1875 drawings and the building as apparently constructed implies a late change in design for structural reasons, possibly suggested by the builder.

Lau (2007, 9) states that the footbridge over the Platforms 1 and 2 (the southern group of platforms) – presumably that present at the time of writing – was constructed during the same period as the construction of the new entrance building, although it was later given a new roof.

In 1905, the roof of the train shed was replaced and the eastern portion of the train shed was significantly rearranged. The timber truss roofs covering the lower portion of the shed, and bearing on the outer walls and the arcaded spine wall, were removed along with the spine wall itself. (The spine wall of the taller western building – the main building – was not removed.) As well, the island platform which this spine wall divided was also removed from the lower shed area, leaving only a stub

projecting eastward a short distance from the taller building. Thereafter, there were only two full-length platforms in the lower portion of the train shed, each served by two tracks. As part of this rearrangement, canopies were erected over the two remaining platforms, each canopy supported on a single row of columns down the centre of the platform. The footbridge over the southern group of platforms was also re-roofed at this time.

Much later, in 1954, a substantial portion of the lower, eastern side of the staircase from the southern central platform, within the main building, was demolished to make room for a small equipment store (which now appears to be an electricity substation), thus removing any purpose for the upper portion of the staircase. This latter portion of the staircase was presumably left in place because of the space in use below it. The new building was a small brick block built near the eastern end of the southern central platform, within the main building.

At some point, Lau does not say when, the timber ticket office within the ticket hall was expanded. The main feature of this expansion was that the office was extended northward to take in one leaf of the double door in the western wall of the ticket hall, providing direct access from the timber ticket office to area in front of the station. An undated drawing reproduced in the conservation report (Lau 2007, vol. 2, 30) shows details of this work, and also shows that the doors that originally hung in the arches of the ticket hall's western wall were panelled differently than those present at the time of writing.

In 1963, a multi-bay porte-cochère which had extended westward from the ticket hall was removed. Also in the 1960s, the Crystal Palace link, and the lobby building that was adjacent to the north side of the north tower were removed (the Crystal Palace itself having been destroyed by fire in 1936). The cloak room block abutting the north tower is not shown in the conservation report on any drawings dating later than 1921, and it may have been removed when the lobby building was demolished or at some earlier date.

It is worth noting that a drawing of proposed improvements from 1965 (Lau 2007, vol. 2, 45) shows the eastern wall of the ticket hall, in terms of arches, piers, and roof beams, largely as they are now but with kiosks on either side of the central pier. The kiosks are shown as extending eastward through the east wall of the ticket hall, below the central arches, where there is presently solid brickwork. The conservation report's caption for this drawing states that the works shown were never carried out, but it is unclear from the reproduction which aspects of the drawing were existing and which were proposed. The kiosks as shown would have had fronts flush with the interior face of the wall and would have been constructed out onto the landing over the platforms within the main building.

A 1971 photograph reproduced in the conservation report (Lau 2007, vol. 2, 56) seems to show a kiosk in the south-east quadrant of the ticket hall, in the position of another kiosk shown on the 1965 drawing (ie, not one of the central kiosks. While this is not proof that the 1965 drawing showed only existing kiosks, it does establish that at least some kiosks were built within the ticket hall. Thus it may be that alterations to the east wall of the hall, including removal of the central chimney breast, were related in some fashion to kiosk construction, although other explanations are no doubt possible.

In the 1970s and 80s, the canopies over the platforms in the former train shed were removed, as were canopies over the southern group of platforms. However, the canopy over the southernmost platform, presently Platform 1, was replaced. In 1976, the mansard roof of the north tower was also removed, together with that tower's chimneys. In 1986, a glass and green-coloured aluminium entrance hall was built to

the south of the south tower. The design of this entrance hall was intended as an homage to the original Crystal Palace.

In 2000–2001 numerous improvements to the station were undertaken, including the construction of a new mansard roof for the north tower, the rebuilding of the main building's roof (ie, the roof over the western end of the northern group of platforms), the construction of a new canopy on the western face of the ticket hall, the re-roofing of the footbridge over the southern group of platforms, general re-pointing of brickwork, and the underpinning of the north tower. Internally, work was begun on re-plastering and on the replacement of doors.

3 Analytical Description of the Standing Building

3.1 The standing building survey

The refurbishment of the station and the insertion of wheelchair access to the platforms required certain specific alterations to the ground floor layout of the 19th century entrance building, the main building, access to the platforms along with the demolition of the 1980s entrance area (comprising the aluminium and glass entrance hall and the brick ticket office). These areas were located on site and allocated a task number (Table 1). Whilst the survey fieldwork was underway the plans for the refurbishment were superseded by a revised refurbishment scheme proposed by the client. Some areas that had already been recorded were no longer due for demolition in the revised plan, whilst new areas previously unaltered were subsequently due to be affected. These were also assigned a task number and included in the survey. The table below lists the task numbers of all areas included in both the original brief and the revised plan.

Table 1: Specific areas within the station recorded during the survey, including those of the revised refurbishment plan, identified as numbered tasks

Task Number	Description	Comment	Section of this report
1	External doors in west wall of ticket hall	To be replaced	3.3.1
2	1980s entrance hall (aluminium and glass) to south of 19th-century entrance building	Constructed 1986, to be demolished	3.3.2
3	Brick ticket office to S of [2]	1986, to be demolished	3.3.3
4	S external wall of 19th-century entrance building (south tower) within 1980s entrance hall	Doorway to replace set of (blocked) windows to E	3.3.4
5	Window to S in E wall of south tower	Doorway originally to replace window (cf [26], interior to W of this window). Now window will remain	3.3.5
6	Stairs to E of [2] and [3] down to level of footbridge [8]	1986? to be demolished	3.3.6
7	CCTV camera pole and base to S of [6]	Since 1986; to be removed	3.3.7
8	Footbridge over southern group of tracks, with at each end stairs to SE down to Platforms 1 and 2, and connecting through [12] stairs in main building down to Platform 3	Originally planned to be demolished, now remaining	3.3.8

Task Number	Description	Comment	Section of this report
9	Top of N and S walls of former train shed on main line	New roof to be erected on these walls	3.3.9
10	S stairs down to E on to southern central platform (surviving W end of original island platform); stair scar visible on S face of central W–E wall of station building	Shallow ramps to be erected above level of stairs running from W to E, from head of stairs to a lift down to southern side of island platform	3.3.10
11	Brick wall and metal fence along S edge of Platform 1 and foot of stairs [8]	Originally to have been demolished, now to remain	3.3.11
12	Doorway in S wall of main building, connecting N end of [8] with intermediate landing in stairs down to Platform 3, inside main building	Originally to have been blocked, now remaining open	3.3.12
13	Railings at head of central two stairs down to central platforms (within main building)	To be removed	3.3.13
14	Doors to extreme S in E wall of ticket hall	To be replaced	3.3.14
15	Doors to S in E wall of ticket hall	To be replaced	3.3.14
16	Doors to N in E wall of ticket hall	To be replaced	3.3.14
17	Doors to extreme N in E wall of ticket hall	To be replaced	3.3.14
18	Timber ticket office in W part of ticket hall	To be removed	3.3.15
19	Brick partition wall in N arch in S wall of ticket hall	To be removed	3.3.16
20	Glazed timber partition in S arch in S wall of ticket hall, and similar partition to E	To be removed	3.3.17
21	Wall running from north to south and guichet to S, between [19] and [25]	To be demolished	3.3.18
22	Doors and door frames to S of ticket hall, within south tower, leading to [23] and [25]	To be removed	3.3.19
23	Ground floor central western room of south tower, with green-glazed tile fire surround, S of [22]	To have partitions inserted and openings made in E and S walls	3.3.20
24	Ground floor south-western room of south tower, south of [23]	To have partitions inserted and opening made in N wall (see [23])	3.3.21
25	Ground floor central eastern room of south tower, S of [21] and [22]	To have partitions inserted and S wall removed (see [4])	3.3.22

Task Number	Description	Comment	Section of this report
26	Ground floor south-eastern room of south tower, S of [25]	To have N wall removed (see [25]) and doorway to replace window in E wall (see [5])	0
27	North-facing façade of platform-level rooms on Platform 3, below staircase on Platform 3	Originally unaffected, but to be obscured by lift in revised proposal	3.3.24
28	Wall at ticket-hall level on landing between Platforms 3 and 4 in main building	Originally unaffected but to be cut through for access to lift on Platform 3	3.3.25

3.2 Detailed description of the ticket hall and north and south towers

The surviving original station building takes the form of a brick built central hall (old ticket hall) open to the roof, with a two storey brick built pavilion at both the north and south ends (the north and south towers). Yellow London stock bricks are used in Flemish bond with red brick string courses and sandstone dressings. Each pavilion or tower is divided internally into smaller rooms on both the ground and 1st floors. There is no direct access between the 1st floors of the two towers. In basic plan the building could be considered to resemble a medieval double-ended hall house without the screen passage, which may indeed have been intentional. However, externally, the general appearance of the building is far more in keeping with the Victorian fervour for classical architecture.

3.2.1 North tower

The north tower is the larger of the two with a mansard roof featuring railings around its crown, at the break in slope (*Fig 8*). Below roof level is a parapet with two sets of five arched openings on the western façade above and in line with the sets of windows below (*Fig 9*). The arched openings in the parapet are absent on the other three sides of the building where the parapet is constructed of solid brick.

The north façade is much plainer than the western façade without the ornamentation around windows, plinth and lower string courses, which pass around the corner from the western façade before terminating (*Fig 8*). A 1st floor sash window with a brick lintel is situated in the centre of the northern façade. At ground floor level at the eastern end of the north façade are two tall openings with rounded arches. These have large timber double-leaf doors, the timbers being angled in a herringbone pattern (a style used elsewhere in the ticket hall, see sections 3.3.15 and 3.3.14). The brickwork around these two doors is noticeably different from that elsewhere, being of high fired whitish-yellow brick. Furthermore there is a distinct change in the colour of the brickwork of the northern façade, just below the 1st floor window. The lower portion is lighter and less weathered, indicating the likely presence of a former building or construction adjacent to the north tower that reached to (or slightly above) the level of the 1st floor.

The parapet rests upon a stone cornice that runs continuously around all four sides of the building (*Fig 9*). Above the two sets of windows in the western façade the cornice rests upon two sets of four brackets, interspersed with protruding roundels. The west facing façade of the station entrance building and the north tower are the more elaborately decorated fronts, with two sets of two-light windows on each floor dividing the Tower into two bays. The 1st floor windows are formed from four round arches with a dropped keystone in each. The spandrels of the two arches contain decorative roundels of green marble. The window surrounds are formed of pilasters with composite capitals as the impost of the arches.

The ground floor two-light windows in the North Tower are of a similar scale as those on the 1st floor, but both sit within a larger rounded arch with the space between containing a circular window or oculus, surrounded by two three-sided glass panels. The effect is similar to the tracery found in the upper portions of window arches in medieval stone architecture. Again the arches and architraves are constructed of yellow sandstone and two green marble roundels are included either side at the top of the large arches.

As mentioned above the western façade of the building is the most elaborately decorated and the plinth at the base of the western facade turns round the north west corner of the north tower before terminating, as do protruding moulded yellow sandstone stringcourse at 1st floor and 2nd floor levels, and a third moulded coping on a plinth at ground floor level (compare *Fig 8* and *Fig 9*)

The western façade of the 19th century ticket hall has a glass canopy attached externally, supported by decorated steel columns (*Fig 10*). This is a modern replacement for the more extensive original porte-cochère that was removed in the 1970s. However it rests upon original stone corbels built into the brickwork of the western façade of the ticket hall. Each bay of this recent addition is therefore the same width (north to south) as the original. The former 19th century porte-cochère extended further to the west, its pillars resting upon stone blocks that top the wall on the western edge of the station approach, overlooking the tracks far below. Each of these stone blocks remains in line with the present pillars of the canopy and the corbels in the ticket hall wall. Scars are visible on the upper faces of these blocks showing where the original pillars were removed. Along with the modern canopy three glass lamps with decorative brackets have recently been added to the façade.

The western façade of the ticket hall has two double leaf doors with transom lights in segmental-headed arches, with two single round arched sash windows between. At both the north and south ends of the façade is a single sash window with a segmental arch. At the top of the western façade above the glass canopy there is a diminutive chimney stack, no more than three or four brick courses in height, centrally placed above and between the two arched windows (*Fig 10*). A red brick sill course runs level with moulded plinth in the tower, while the west wall of the booking hall has its own plinth at a slightly lower level.

Internally, the ticket hall has a hipped, north-south-ridged timber roof with a clerestory lantern along the length of the ridge. The roof is supported on cast-iron principals made up of bolted-together segments. The composite roof principals, constructed from bolted-together segments, are designed to function as arch-braced rafters for the main roof and as beams to support the clerestory lantern above. The segments of the roof principals have two or more flanges and their webs are perforated in a foliate pattern. The cast-iron roof principals descend to rest on stone imposts supported by brick pilasters on all four walls of the hall. One of these imposts, situated centrally in the eastern wall of the ticket hall, was formerly above a fireplace and chimney breast. The chimney breast has been removed and the impost is presently supported on brick corbelling, which is not original. The impost itself is

decorated with acanthus leaves and is the only decorated impost in the building (*Fig 11*). It therefore seems probably that this decoration was originally made to adorn the fireplace that was formerly present below. A possibly similar but smaller chimney breast, situated in a narrow piece of wall between windows, exists in the timber ticket office in the central pilaster of the hall's western wall. Externally, tiny chimneys were noted, at the tops of the ticket hall's western and eastern walls corresponding to both these fireplaces, and it seems apparent that in the case of the main ticket hall, chimneys were considered to be detrimental to the overall aesthetic design, or perhaps awkward to fit in around the iron ribbed structure of the building with its pilasters and therefore made as small as possible (*Fig 10*).

The south tower differs from the north in height and decoration (*Fig 12*). Its hipped slated roof, though similarly decorated with iron railings and finials as the north tower roof, is of a more gentle pitch with two chimney stacks to the west and a third on the eastern side. There is no parapet but a cornice surrounds the top of the building supported by a continuous series of Corinthian modillion brackets. Both 1st and second floors have three windows on the western facade, those of the upper floor having a stone surround that is both shouldered and eared. The central window on the 1st floor of the western façade is the longest of the three; the other two have higher sills above a circular relief decoration. The three windows on the ground floor are single-light windows, similar in decoration to the double-light 1st floor window on the north tower. A plinth at the base of the south tower has a moulded stone coping like that on the north tower.

The southern façade of the south tower is abutted by the 1980s entrance hall. Windows in this façade on both the 1st and ground floors have been bricked up (*Fig 12*). Internally these blocked windows have been plastered over and show no trace. Externally the windows surrounds are evident on both floors, with wooden panels filling the window space. On the ground floor a single central doorway beneath a round stone arch leads to a staircase up to the first floor; the impost of the arch are decorated with acanthus leaves (*Fig 13*). Either side of this central doorway are a series of three stone arches which contain narrow windows, subsequently blocked. The outer two arches in each set are larger and contain two window openings, whilst the smaller central arch contains only one.

As described in section 3.1 the original proposal for the development of Crystal Palace Station required the demolition or alteration of specific areas within the old station building, the new ticket hall area, Platforms 3 and 4, and the footbridge to Platforms 1 and 2 (Table 1). These areas or tasks were identified and numbered at the beginning of the survey and recorded to an appropriate level. Whilst recording was in progress an alternative proposal was submitted by the client, which changed some of the areas to be affected by the station redevelopment. As recording had begun and some tasks were close to completion (in particular the footbridge), it was thought reasonable to finish recording these areas (tasks 5, 8, 11 and 12) and add the new areas to the survey (tasks 27 and 28).

3.3 The survey

The section below is a description of every numbered task (see *Fig 3* for location).

3.3.1 Task 1

External doors in west wall of booking hall (Fig 14 and Fig 15)

Two double doors were situated in the west wall of the old ticket hall within rounded arches formed by a segmental lintel of fine and thin red bricks, the highest point of

the intrados edge of the arch being approximately 4.11m in height. Above the tops of the doors was a rounded transom light formed from four panels of glass, filling the gap below the arch. In the southernmost of the two doorways the doors remained unaltered, although possibly not original (*Fig 14*). These were painted red externally and white internally. Both sides of the doors were decorated with three sets of wooden panels with moulded surrounds. Long middle panels with shorter top and bottom panels elongate the door and set the lock rail, handle and letterbox below waist level. The southern set of doors were normal two-leaf doors, opening inwards.

The northern set of doors had been significantly altered to accommodate the wooden ticket office (task 18). The doors themselves were only four panelled rather than six, and the four panels resembled the lower four panels of the southern door in size and arrangement. The northern doors were thus shorter than their southern counterparts, although the transom of the northern doorway remained at the same height. The space between the doors and the transom was filled by a fascia panelled to resemble the upper panels of the southern set of doors, as if the northern set had simply been cut down to size and their upper portions fixed permanently to the frame (*Fig 15*). A wooden mullion had been inserted between the northern doors, slightly to the south of centre. The southern of the two doors in this northern doorway was hung from this central mullion rather than from its outer edge and a slightly narrower hinge style was used on this door to accommodate the mullion. These alterations enabled external access to the wooden ticket office through the southern leaf of the northern doorway, while the northern leaf of the doorway continued to give access to the ticket hall.

3.3.2 Task 2

1980s entrance hall (Fig 16 and Fig 17)

A metal and glass shelter covers the present entrance hall and platform entry turnstiles. It is formed from a tall central arch running west to east forming a tunnel vault, flanked by a flat roofed aisle on the north side and corresponding shorter areas to the south, where the flat roof only covers the entrance in the south-west corner and the exit to the platforms in the south east corner. The present brick built ticket office (task 3) occupies the majority of the southern edge of the ticket hall. In appearance the western façade is intended to recall the former Crystal Palace with a symmetrical elevation (*Fig 16*), despite the presence of the brick built ticket office inserted into the southern edge. The roof structure is supported by square upright posts with straight trusses spanning the base of the vault (*Fig 17*).

3.3.3 Task 3

Present brick built ticket office (Fig 18 and Fig 19)

The present ticket office is a single storey brick built building situated to the south of the entrance hall (*Fig 18*). Internally it has a single room operating as the ticket office (*Fig 19*) with a kitchen area to the south west and a toilet to the south-east. Two booking windows in the north wall open out on to the entrance hall, while a door provides access in the south east corner of the building, where a small lobby connects the toilet to the ticket office. An internal skin wall of cement blocks is visible throughout the building to which the external brickwork forms a facing. The south western wall of the ticket offices curves round to connect to an electricity sub station. Behind the ticket office is a walled open area marked by the substation and southern boundary wall.

3.3.4 Task 4

South external wall of the south tower, within the entrance hall (Fig 13, Fig 20 and Fig 21)

The southern façade of the south tower is abutted by the 1980s entrance hall, with the ground floor façade providing the northern edge of the entrance hall. Windows in this façade are small and narrow in comparison with those on the western façade and all have been bricked up. Internally within the southern most, ground floor rooms of the south tower these blocked windows have been plastered over and show no trace. Externally the stone window surrounds, sills and arches are evident with wooden panels replacing the glass in the sash windows (Figs 13, 20). These have been painted with scenes as if looking through a glass window to a darkened Victorian interior complete with oil lamps, hanging pictures and curtains as well as figures such as a seated figure reading a newspaper and a girl looking back at the viewer through the window. (The set of three arches and their corresponding windows to the east of the south tower's southern wall are due to be converted to a doorway).

A single central doorway beneath a round stone arch gives access to a staircase up to the first floor of the south tower and is flanked by door jambs decorated with acanthus leaves. Either side of the central doorway are a series of three stone arches which contain the blocked narrow windows. The outer two arches in each set are larger and contain two window openings, whilst the smaller central arch contains only one.

3.3.5 Task 5

Window to the south in east wall of south tower (will remain in revised plan) (Fig 22)

Task 5 was a large sash window situated in the eastern wall of the south eastern ground floor room of the south tower. Internally the sash window measured 1.55m from floor level, 2.66m tall and the top of the window architrave measured 4.26m from the floor, the ceiling height in the south eastern room being 4.858m. The wall thickness through the east wall of the south tower was approximately 0.36m.

Externally the window, which appears in Fig 25 immediately above the roof of the footbridge, was without the ornamentation found in the western façade and with a flat arched lintel of red brick. The difference in styles between the highly elaborate windows of the western façade and the plainer window of task 5 was continued in the most conspicuous east-facing windows of the building, indicating the reservation of adornment for the front of the building.

Externally, below the window was a stone topped buttress that formed the north side of the train tunnel beneath the south tower, abutting the south wall of the main building and main train shed. (In the initial refurbishment plans the top of the buttress was to be removed and the large sash window converted to a door. In the revised refurbishment plans the window and buttress are to remain untouched).

3.3.6 Task 6

Stairs to footbridge (Fig 23)

To the east of the 1980s entrance hall and brick built access (Tasks 2 and 3) is a concrete staircase with mild steel railings that gives access from the entrance hall to the footbridge and station platforms. It takes the form of a dog legged stair, and ramp the stairs from two successive flights above and the ramp runs back to the north down to Platform 1; it appears to post date the footbridge, with which it

connects. Iron railings and a handrail run along both the inner and outer edge, with pattern of balusters being arranged in arched pairs, continuing the arched theme of the 1980s entrance hall. Traces of blue paint are evident on the handrail, whilst the balusters are painted white.

3.3.7 Task 7

CCTV camera pole and base (Fig 24)

Adjacent and to the south of the mid landing level of the stairs to the footbridge are two CCTV cameras mounted to a post. One camera points upwards to the top of the stairs towards the entrance hall, whilst the other points downwards towards the footbridge. The post is set in a square concrete base, which is partially surrounded (to the south) by two courses of cement bricks. The base of the post is hinged to allow the post to be lowered to the south, for maintenance.

3.3.8 Task 8

Footbridge (Fig 3, Fig 4 and Fig 25)

The footbridge over the branch line tracks (Platforms 1 and 2) connects with the concrete staircase (Task 6) at its southern end. At each end of the bridge a staircase leads down in a south eastwards direction to Platforms 1 and 2. At its north end the bridge leads through an open doorway (task 12) into the main building onto the staircase to Platform 3. (The initial plans for station refurbishment included the demolition of the footbridge. However in the revised scheme they are retained).

At the western ends of both Platforms 1 and 2 are two H-shaped iron frames set upright in concrete at right angles, formed from straight girders riveted together. These provide the main support for the steel framework of the footbridge and the top of each staircase. The footbridge and platform stairs are covered with a pitched roof of corrugated iron which is supported by vertical steel joists. These are fixed to the main frame of the structure. There is an angled bend at the top of each vertical support so that it forms a rafter. Further support is given to the roof by steel purlins bolted on to the rafters and the ends of the rafters themselves are bolted on to a central beam that runs beneath the ridge in the corrugated roof. The sides of the footbridge and staircases are covered with steel panels riveted to the framework of the structure and painted blue. Internally the supporting posts are painted blue, whilst the reverse of the panels are painted grey. Tubular steel handrails are fixed to the inside of the framework of the staircases and painted green.

3.3.9 Task 9

Top of north and south train shed walls, Platforms 3 and 4 (Fig 26 and Fig 27)

The north and south walls of the early train shed are formed (Fig 26). The walls are built of red stock bricks with the wall and pilasters in English bond. At the top of each pilaster the brick arch rests upon a moulded impost stone. Above the arches the wall continues with two courses of projecting bricks. Above this there is a change in the brickwork as four courses of yellow bricks in English bond are topped by slabs of concrete. These upper courses of yellow brick and concrete slabs appear to be later additions to the top of the walls. The round-headed arches of the arcades are in grey brick. The south wall looks different, also externally. The south wall is very high, because of the slope of the ground.

At the eastern end of both north and south walls was a large pier built of red brick in English bond; the outer east face of each pier contains a round-headed niche,

framed with stressed quoin in grey bricks over a projecting plinth (*Fig 27*). In the north wall this solid pier incorporated the outer face of the wall, which was impossible to see from the platform side. However, glimpses of this exterior face could be seen from the north through the trees of the park. The buttress was topped by a large pyramidal capstone and, in an echo of the decoration of the façade of the north and south towers, this was supported by carved stone brackets above a stone stringcourse.

(The refurbishment scheme for the station includes the erection of a new roof spanning Platforms 3 and 4. It will be supported centrally by columns between the two tracks and also rest on the top of both walls).

3.3.10 Task 10.

Stairs down to east side of island platform, main building (Fig 28)

From the level of the entrance and ticket hall in the main building, wide staircases give access to Platforms 3 and 4. There was formerly also a central pair of platforms – the divided northern and southern sides of the central island – accessed by staircases on either side of the west-east spine wall of the main building. Only the western ends of these platforms, within the main building, remain, although they extend a short distance eastward from the main building as the stub of an island platform. With the removal of most of the island platform, these staircases became redundant, unless they were used for access to the spaces below the stairs. At present, both of the central stairways are blocked off by iron railings at their head. The staircase along the south side of the spine wall has had its lower portion removed in order to accommodate a small, brick built equipment store (which now appears to be an electricity substation) constructed within the main building at platform level. The scar of the removed portion of stairs is visible on the southern side of the spine wall (*Fig 28*). All staircases in the main building are of the same type with a solid brick built retaining wall topped by a moulded decoration, interspersed with brick pillars and moulded capstones. Along the spine wall a wide concave recess in the brickwork indicates the former presence of a handrail. In the design for the main building and platforms advantage was taken of the space below the stairs to provide rooms at platform level. These rooms still exist in all four sets of staircases in the main building, including those of Task 10 although the windows and doorways are boarded up throughout and access to these was not obtained.

3.3.11 Task 11

Brick wall, tactile paving and metal fence, Platform 1 (Fig 29)

At the base of the steps from the footbridge to Platform 1 are 14 square, tactile paving slabs. To the south is a small section of wall constructed of red stock bricks in English bond, which closes the gap between the staircase to the footbridge and the west end of buildings on Platform 1. Behind this to the south is a metal fence, painted blue, which borders the base of the bank to the south of Platform 1. (These items were originally to be demolished but will remain in the revised scheme).

3.3.12 Task 12

Doorway in south wall of main building connecting footbridge to Platform 3 (Fig 30)

At the north end of the footbridge (Task 8) is an open doorway through the south wall of the southern side of the main building. It has a segmental arch and the southern side is surrounded by a recess in the brickwork, indicating the former presence of a heavy duty door, possibly of steel. Also on the southern side, two

stone blocks have been inserted into the brickwork, below the springer of the segmental arch. This doorway was initially due to be bricked up as part of the refurbishment of the station. However, in the revised scheme it is unaffected and will remain open.

3.3.13 Task 13

Railings at head of stairs to former island platform (Fig 31)

The disused steps to the former central platforms (discussed in Task 10, section 3.3.10), are currently sealed off by two sections of iron railings. These railings appear to be a relatively recent addition and consist of main upright posts approximately 2.94m in height, square in horizontal section, but with rounded corners. The top of each post is decorated with a single ball finial. Between the posts are vertical railings linked at the top by two horizontal cross bars, between which is a row of iron rings and a further bar at the bottom forms the base of the railing.

3.3.14 Tasks 14, 15, 16 and 17.

Entrance doors to platforms in eastern wall of 19th century ticket hall (Fig 32 and Fig 33)

Tasks 14 to 17 are the four doors in the eastern wall of the 19th century ticket hall and originally provided access between the platforms and the entrance building. They are arranged in two groups of two, with tasks 14 and 15 to the south (14 being the southernmost) with 16 and 17 to the north (17 being the northernmost). All four doors have been boarded over apart from 15, which has been fitted with a modern metal folding door. Each door is situated within a rounded brick arch. Internally the brick arches over tasks 14-17 are wider than others in the main building. The distance between the intrados and extrados edges of the arches over the four doors being 0.98m, whereas the arches in the northern, western and southern walls have a thickness of only 0.39m. Within the brickwork of the internal arches of all four doors are two symmetrical cavities that have been filled or boarded over. These small openings, measuring approximately 0.20m in height occur either side of the arch and respect the first course of bricks on the intrados edge. Below each filled cavity, spaced at intervals down the wall are two small metal brackets. It is possible the brackets and cavities were formerly used for securing signage over each doorway, indicating directions to platforms for instance. Generally the archways are still in good condition, with only one obvious repair to Task 14, where the impost below the arch on the internal northern side has been replaced.

Of the four doorways 14 and 16 have been boarded over with panels or planks in a herringbone pattern, comparable to that of the timber ticket office in the ticket hall (Task 18). The northernmost door, Task 17 is boarded over in a more temporary way, with unpainted boards. A scar remains around the door indicating that the white painted herringbone panels may have covered this door also. Within each doorway the round space formed by the arch is covered with a plain board painted white.

3.3.15 Task 18

Timber ticket office (Fig 34, Fig 35, Fig 36, Fig 37 and Fig 38)

The timber ticket office is situated along the western wall in the ticket hall in the main building. It is rectilinear in plan with the north east and south east corners removed, producing a six sided shape (Fig 34). Entrance to the ticket office from the outside is through the southern door of the pair of north doors to the ticket hall, which have been modified for the purpose (see Task 1, section 3.3.1). A second door is located

at the south end of the ticket office, which gives access to the ticket hall. The ticket office is of timber frame and panel construction, using vertical timber studs with bevelled corners, with the panels between slotting into grooves in opposite faces of each stud. The panels are formed from planks slotted in at an alternating angle in each panel. This forms a herringbone pattern. Further decoration is given to the external side of the ticket office by a moulded wooden skirting board at the base; more ornamentation is introduced at the top of each panel by a wooden arch linking each stud and a moulded wooden cornice runs around the top of the structure. Much of the ticket office appears to be slotted together using a tongue and groove technique. However in some areas, particularly internally and within an area of damage to the external skirting, the heads of nails or tacks are visible.

The ticket office has its own roof, which slopes up from the top of the walls on all sides but is flat at the top adjacent to the west wall of the ticket hall (*Fig 34*). The herringbone pattern is continued in the sloping areas of the roof (*Fig 35*). These areas are supported by rafters slotted into a wall plate at the top of the internal walls. Roof lights are also present in the sloping northern, eastern and southern areas of the roof which would have increased the amount of natural light into the ticket office, the only other windows being two in the western wall of the ticket hall (see 3.2).

The flat area of roof is formed from continuous boards, rather than the herringbone panels and two hatches subsequently blocked were present in that roof. These would presumably have opened out and may have provided ventilation rather than light. Further support is given to the structure by two truss rods supporting vertical iron rods (*Fig 35, 37*). The truss rod is mounted into the internal edge of the ticket hall's west wall and inserted into the bottom of the timber wall plate. The truss supports an iron rod that connects to the eastern most beam of the flat roofed area.

In the south eastern, eastern and north eastern walls of the ticket office are a series of three ticket office sales windows or hatches (*Fig 35*). Each hatch is situated between two studs with doors opening inwards into the ticket office and a small protruding shelf, with rounded corners on both the interior and exterior. With the opening having a width of 0.47m and a height of 0.74m in all three, the hatches are relatively small. A rebate is situated within the sides of the opening that corresponds to a 5mm deep groove in the opening's base, presumably for a grill or partition that could be slid downwards from above when the office was closed. A second set of grooves or rebates exist on the outer side of the first. It is suspected that the outer window was fixed and the inner window slid up or had a semicircular opening its bottom edge. Internally the service hatches are abutted on either side by timber jambs which are more substantial and project further than the vertical studs used in the wall construction. A series of three small pulley wheels were noted with two around the hatch in the south eastern corner of the ticket office (*Fig 36*) and a third above the west side of the south door from the office to the ticket hall. Their exact function is uncertain but the orientation of each pulley wheel suggests a cord ran vertically from below, on the west side of the south door, horizontally over the top of the door to the south east corner, vertically again to the right internal jamb of the hatch and then down again towards the hatch and service counter. No other pulleys were noted around the other two hatches.

A long desk or counter is fitted below the hatches onto the east and south eastern walls within the ticket office and a smaller second to the north eastern wall, both with drawers and cupboards below (*Fig 37*). A further counter, which is taller and wider than those below the hatches runs from west to east towards the north end of the ticket office. This divides the office into two areas with the smaller area to the north and the remaining area, roughly 2/3rds of the ticket office space, to the south. This counter has a swing top at its eastern end, to allow for access.

The ticket office incorporates the central part of the west wall of the ticket hall and has been built to fit into the arches and pilasters of this wall's interior (*Fig 38*). Two windows are situated to either side of a pilaster that supports one of the iron principals of the ticket hall roof. The sides of this stepped out at the level of the window sills and a small fireplace subsequently blocked was noted towards its base. This fireplace corresponds to the small diminutive chimney stack noted at the top of the western façade of the ticket hall (see section 3.2 and *Fig 10*).

Wooden boards make up the majority of the floor in the ticket office, apart from the northern area, on the northern side of the swing top counter. Here the floor, as in the rest of the ticket hall is of concrete. This may indicate an enlargement of the ticket office, with the original office corresponding to the area of wooden floorboards. Later the enlargement of the office space took in one of the two northern double doors of the ticket hall, as discussed above (and in section 3.3.1).

3.3.16 Task 19

Brick partition wall within south east arch of the ticket hall (Fig 39)

Internally both north and south ends of the ticket hall ended in a series of three brick arches. Those at the southern end formed two separate enclosed rooms either side of a central space where the central arch had been left open. These two rooms occupy the north west and north east corners of the ground floor of the south tower.

Task 19 was the bricked up north-facing arch on the eastern side at the south end. This formed a west to east wall, the north wall of the room in the south tower's north east corner (*Fig 39*). Here the arch had been filled with a wall of darkish sandy-yellow coloured bricks in stretcher bond, with occasional header bricks that acted as closers, either side of a central doorway. The doorway had a simple wooden frame and a concrete lintel. Above the doorway and filling the curve of the arch was a rounded window. Internally within the room was a small buttress either side of the doorway. The space enclosed by this partition (and that of Task 21 section 3.3.18) formed the north eastern ground floor room of the south tower. The east to west running southern wall of this room is also due for demolition in the refurbishment plans. The eastern wall of this room contained an arched window with a substantial reveal, indicating the thickness of this wall which was approximately 0.72m in this area. This wall was the external wall between the ticket hall and the train shed.

3.3.17 Task 20

Glazed timber partitions, south end of ticket hall (Fig 40)

Task 20 was located to the west of Task 19 and comprised two glazed partitions, one east facing and the other north facing, that sealed off the south western area at the southern end of the ticket hall to form a room. The east-facing partition was a large window, arched in its upper section within the brick arch, and a timber panel below to the ground. The plinths at the base of the uprights of the arch had been cut to accommodate the timber and glass partition, suggesting it was a later addition.

The north facing partition was of a similar build but included a set of double doors that opened into the room to the south. The upper parts of the doors were windowed, whilst the lower portion had a single panel in each door with bevelled edges.

The timber and glass partitions enclosed a space that formed the north western room of the south tower. Within the room small areas of wainscot panelling survived in the north east and south west corners, whereas elsewhere much of the room had been stripped back to the brickwork. Also a fireplace, later blocked, was situated in the south wall of the room.

3.3.18 Task 21

West facing side wall and ticket desk with room, south end of 19th century ticket hall (Fig 39)

Task 21 was the second brick infill wall that together with Task 19 enclosed the south eastern corner of the ticket hall to form a separate room. It was situated directly opposite the window partition within the arch of task 20. The bricks used for the infill were of a similar type to those used in the original build in the interior of the ticket hall, being a light yellow in colour and of the same dimensions. A plinth survived intact across the entire base of the infill wall, However there was no arch present over the top of the brick infill. It may be that there was no arch here in the original design for the building, although this seems unlikely when the general symmetry of the building is taken into account.

A ticket sales window or guichet was situated within the wall at a height of 1m above the floor. It comprised a timber counter with two square depressions to enable transactions to take place below a window, the glass of which had subsequently been removed and replaced by a board, sealing the opening. The opening itself was 1.92m wide and framed by a thin architrave of moulded wood. Above the opening was a concrete lintel 2.4m in length. At the level of the guichet opening and to its south there was a possible change in the brickwork where an area had been repointed.

3.3.19 Task 22

Doors and doorframes leading from the ticket hall into the central rooms of the south tower (Fig 41)

Task 22 included both of the two doors and door frames located at the south end of the ticket hall. They led from the open space between Tasks 20 and 21, into remaining ground floor rooms of the south tower. Both doors and their frame's were identical and had bottom, middle and top panels with bevelled edges on the north side, whilst the southern side the panels had moulded edges. The bevelled panels matched those mentioned in the panels of the glazed timber partition (Task 20), whilst the moulded panels were more in keeping with door panels, skirting boards, coving and picture rails seen in the ground floor rooms of the south tower (Tasks 23 - 26).

3.3.20 Task 23

Central western room, ground floor of the south tower with green glazed fireplace (Fig 42)

The westernmost of the two doors described in Task 22 led into a room in the north west ground floor corner of the south tower. In the north wall was a chimney breast (shared with the room to the north, Task 20), with a fireplace surrounded by green glazed square tiles, with an edging of black glazed tiles. The fireplace projected only 0.07m from the chimney breast and had a relatively narrow opening, 0.38m wide with an angled reveal, reducing the grate to only 0.18m wide.

In the opposite wall to the fireplace a doorway with a moulded timber architrave led into the south eastern ground floor room of the south tower. The doorway was set between two buttresses projecting 0.12m from the wall. These formed an arch high up below the ceiling over the doorway. This feature was repeated on the other side of the door within the room to the south (see Task 24 below).

3.3.21 Task 24

Far south western room, ground floor, south tower with green wall paint (Fig 43)

Through the arched doorway from the room described in Task 23 was the south western most ground floor room of the south tower. Much recent re-plastering had occurred on the southern wall of this room; however the north wall retained prominent traces of the room's former colour scheme, in the form of dark green paint from the floor up to a height of 1.24m. A gap then occurred till a band of the same dark green paint 0.035m wide topped the design at a height of 1.32m. To a height of 2.26m the wall appeared to be white, although this colouring had the appearance of having been removed, perhaps scraped off, and possibly once covered the green paint. The base colour of the wall, below both the white and green was a dark mustard yellow colour that filled the rest of the wall up to ceiling height. The green painted areas survived well in places but gradually deteriorated towards the east end of the north wall.

The east wall of this room contained a locked door which led into the lobby and stairwell to the 1st floor of the south tower, used as private offices and out of the remit of the survey. The door was surrounded by a moulded timber frame, much thinner than those of Task 22 and other doors in the ground floor of the south tower, which suggests it may have been a later addition.

3.3.22 Task 25

Central eastern room, ground floor of the south tower with an angled fireplace and windowed partition wall (Fig 6, Fig 44 and Fig 45)

To the south of the room described in Tasks 19 and 21 was a room accessed by the easternmost of the two doors described in Task 22. Within the eastern wall were two arched windows with reveals (Fig 44), matching that described at the end of Task 19. Both windows indicated the thickness of the external eastern wall to the main train shed continued at 0.72m.

Both windows were at 1.55m from the floor, a considerable height, which beneath the northernmost of the two contained a fireplace. The unusual occurrence of a fireplace beneath a window clearly led to atypical aspects in its design. The fireplace surround had been removed, although the (later) skirting board that ran around the room respected it (Fig 6 and Fig 45). In height the fireplace was 0.90m with a width of 0.90m. A stone slab fronted the fireplace, set into the floor, whilst internally the fireplace was brick lined and an area of sooty deposit survived at the back. A flue ran at an angle to the south and presumably continued within the wall between the two windows. However, the internal sides of the fireplace appeared to have been rebuilt at a later stage, using more modern red bricks and a sandy mortar. This is so odd it suggests either an after thought or else incompetence on the part of the architect.

The south wall of this room was also recorded as it is included for demolition in the refurbishment scheme (Fig 46). A central doorway, with a moulded timber architrave led into the south easternmost ground floor room of the south tower. The panelled door and architrave was the same as the two doors of Task 22, with the same dimensions. On either side of the door was an internal window, 2.3m above floor level (Fig 6). Neither window was capable of being opened, but both were set within a reveal on this northern side. In plan this west to east wall was on the same alignment as the southern wall of the main train shed. Two picture rails ran around the room the highest at 2.63m with the one below at 1.87m.

3.3.23 Task 26

Far SE room & windowed partition wall (other side) (Fig 22)

Task 26 was situated within the ground floor room in the south east corner of the south tower. Here the southern façade of the partition wall, described above in Task 25, was noted. On this southern side the windows were without the reveal. To the east, the external wall of the south tower contained the window described in Task 5. The external wall in this portion of the south tower was thinner at 0.40m, than the thickness noted elsewhere (0.72m, Tasks 19/21, 25). The southern wall of this room was the internal side of that discussed in Task 4. No evidence of the narrow blocked up windows was observed on this side, the wall being plastered over.

3.3.24 Task 27

North facing façade below staircase on Platform 3 (Fig 7 and Fig 47)

Task 27 was situated on Platform 3 within the main train shed and comprised the north facing and east facing elevations, at platform level, below the brick built staircase down to the platform. As with the steps to all of the platforms within the main building, use had been made of the space beneath to form rooms accessible from the platforms. The north facing elevation on Platform 3 (*Fig 7*) was built of yellow stock bricks in English bond and contained two round-arched doorways, centrally placed directly below the intermediate landing in the centre of the platform steps. A series of five small arched windows were situated to the west, below a projecting string course. All five windows and the two central doorways had been boarded over, the boards painted and with mouldings suggesting panelling for the doors and frames for the windows. Above the five small windows were two arches brick filled, the westernmost being the larger.

The east facing elevation had an arched doorway at platform level with a large arched opening for a round arched window above, both again boarded over with decorated panels. A projecting stringcourse ran the length of the elevation below the round arched window.

3.3.25 Task 28

Wall at ticket hall level on landing between Platforms 3 and 4 (Fig 48)

Task 28 was situated at the level of the 19th century ticket hall, within the main building. Here the four doors from the ticket hall (Tasks 14-17) led on to an elevated walkway from which access steps led to Platforms 3 and 4 (as well as originally to the central platforms (Task 13). Leading south to the steps to Platform 3 was a brick wall of yellow stock bricks in Flemish bond, with brick piers topped with a white painted capstone (*Fig 7*). The top of the wall was capped by a rounded coping, painted white, that ran along the length of the wall and down each of the staircases to the platforms.

4 Archiving and Publication

Information on the results of the survey will be made publicly available by means of a database in digital form, stored under the site code KOA 08 to permit inclusion of the site data in any future academic researches into the development of London.

The site archive containing original records and finds will be stored in accordance with the terms of the Method Statement (MOL Archaeology, 2008) with the Museum of London within 12 months of the end of the survey.

A summary of the results of the work will be submitted to the Greater London SMR and NAR (using the appropriate archaeological report forms), and for publication in the form of an article in the *London Archaeologist*.

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6 Bibliography

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7 Appendix 1: Photographs Taken on Site

Table 2 Photographs taken by the MOLA standing buildings team throughout the survey

Image Number (DSCN)	Description	Direction of View
7659	Exterior of the north end of the station	South
7660	Exterior of the west facade	North-east
7661	Exterior of the west facade	South-east
7662	Exterior of the south end of the station	East
7663	Detail of window in west façade of southern end of station	East
7664	Interior of old ticket hall	South
7665	Interior of old ticket hall	North
7666	Detail of north door to old ticket hall in western facade	East
7667	Exterior of the north end of the station	South
7668	Exterior of the north end of the station	South
7669	Detail of open hatch in old ticket office (Task 18)	South-east
7670	Detail of open hatch in old ticket office (Task 18)	South-east
7671	Detail of pulleys around hatch in old ticket office (Task 18)	South-east
7672	Interior of old ticket office (Task 18)	North
7673	Exterior of old ticket office (Task 18)	South-west
7674	Exterior of old ticket office (Task 18)	North-west
7675	Exterior of old ticket office (Task 18)	North-west
7676	Old ticket office and old ticket hall	North
7677	Wood and glass door , old ticket hall (Task 20)	South
7678	Wood and glass partition (Task 20)	South-west
7679	Arch at southern end of old ticket hall	South
7680	Two doorways at southern end of old ticket hall to south wing (Task 22)	South
7681	Side wall and former ticket desk (Task 21)	North-east
7682	Detail of wooden partition and archway in old ticket hall (Task 20)	South-west
7683	Detail of archway and side wall in old ticket hall (Task 21)	North-east
7684	Brick infill, old ticket hall (Task 19)	South
7685	Former ticket office interior and side wall (Task 21)	North-west
7686	Fireplace and window in former 2nd class ladies waiting room (Task 23)	West
7687	Door in southern wall of ladies 2nd class waiting room (Task 23)	South
7688	Fireplace in 2nd class ladies waiting room (Task 23)	North
7689	North wall of 2nd class ladies toilet (Task 24)	North
7690	North wall of 2nd class ladies toilet (Task 24)	North-east
7691	Detail of green paint in 2nd class ladies toilet (Task 24)	North
7692	Window and doorway 2nd class ladies toilet (Task 24)	North-west
7693	Doorway from 2nd class ladies toilet to waiting room (Tasks 24 and 23)North-west	

Image Number (DSCN)	Description	Direction of View
7694	Windowed partition wall between ladies 1st class waiting room and toilet (Task 25)	South
7695	Detail of window in partition wall between ladies 1st class waiting room and toilet (Task 25)	South
7696	Detail of fireplace in east wall ladies 1st class waiting room (Task 25)	South-east
7697	Detail of fireplace in east wall ladies 1st class waiting room (Task 25)	North-east
7698	Detail of fireplace in east wall ladies 1st class waiting room (Task 25)	South-east
7699	Detail of fireplace in east wall ladies 1st class waiting room (Task 25)	South-east
7700	Windowed partition wall between ladies 1st class waiting room and toilet (Task 25)	North
7701	Windowed partition wall between ladies 1st class waiting room and toilet (Task 25), and east facing window (Task 26)	East
7702	Doorway from 1st class ladies waiting room to old ticket hall (Task 22)	North
7703	Ornate iron staircase support in north wing.	North
7704	Detail of tiled floor in north wing	Down
7705	Detail in interior of window in north wing	West
7706	Staircase to 1st floor, north wing	East
7707	Detail of staircase to 1st floor, north wing	South-east
7708	Detail of newel post of staircase to 1st floor, north wing	North-west
7709	Detail of newel post of staircase to 1st floor, north wing	North-west
7710	1st floor landing	West
7711	1st floor corridor	South
7712	View of north wall of train shed from 1st floor window	East
7713	Ironwork detail, staircase to 1st floor, north wing	West
7714	Ironwork detail, staircase to 1st floor, north wing	South
7715	Detail of plinth and chamfered brickwork in old ticket hall	North
7716	Detail of iron roof principle in east side of old ticket hall	North
7717	Detail of iron roof principle in east side of old ticket hall	North
7718	Detail of iron roof principle in east side of old ticket hall	South
7719	Doorway from old ticket hall to station platform (Task 14)	East
7720	Blocked fireplace in old ticket office. (Task 18)	West
7721	Blocked fireplace in old ticket office. (Task 18)	West
7722	Blocked fireplace in old ticket office. (Task 18)	West
7723	Detail of radiator panel next to blocked fireplace in old ticket office. (Task 18)	South-west
7724	Detail of radiator panel next to blocked fireplace in old ticket office. (Task 18)	South-west
7725	Detail of wooden panelling in old ticket office(Task 18)	North-east
7726	Internal, detail of door to old ticket office from outside (Task 1)	West
7727	Detail of door to old ticket office from outside (Task 1)	West
7728	Detail of door to old ticket office from outside (Task 1)	West
7729	Open hatch to old ticket office (Task 18)	East
7730	Desk in old ticket office (Task 18)	North
7731	Counter in old ticket office (Task 18)	South-west
7732	Counter in old ticket office (Task 18)	South-west

Image Number (DSCN)	Description	Direction of View
7733	Plinth in old ticket hall and doorway to old ticket office.	West
7734	Plinth in old ticket hall and doorway to old ticket office.	West
7735	Detail of pulleys around hatch in old ticket office (Task 18)	South-east
7736	Detail of pulleys around hatch in old ticket office (Task 18)	South
7737	Detail of pulleys around door in old ticket office (Task 18)	South
7738	Detail of pulleys around door in old ticket office (Task 18)	South
7739	Interior of door to old ticket office with area of truncated plinth and buttress (Task 18)	South-west
7740	Exterior of southern end of old ticket office, to the right of the door (Task 18)	North
7741	Closed hatch to old ticket office (Task 18)	West
7742	Closed hatch to old ticket office (Task 18)	West
7743	Blocked fireplace in west wall of north wing, ground floor	West
7744	Blocked fireplace in west wall of north wing, ground floor	West
7745	Roof between old ticket hall and train shed	South
7746	Internal, southern entrance doors to old ticket hall (Task 1)	West
7747	Internal, northern entrance door to old ticket hall (Task 1)	West
7748	Decorated corbel over site of former fireplace (?) in old ticket hall	East
7749	Detail of wooden panelling. (Task 20)	North-east
7750	Glass doors to refreshment area, north wing ground floor	South-west
7751	East facing windows, north wing, ground floor	South
7752	Detail of decorated iron roof principals in old ticket hall	South
7753	North end of old ticket hall	Up
7754	Truncated buttress adjacent to old ticket office	West
7755	Iron brackets and wood filled void above door from old ticket hall to platforms (Task 14)	Up
7756	Exterior of north wing	East
7757	Exterior of façade of old ticket hall	East
7758	Exterior of façade of old ticket hall, with small chimney	North-east
7759	Exterior, south wing	North-east
7760	Detail, south wing (blocked windows) abutting 1980's ticket area (Task 2)	North-east
7761	Door to 1st floor in southern wall of south wing, 1980's ticket hall	North
7762	Two narrow blocked windows in southern façade of south wing (Task 4)	North
7763	Area to the south of 1980's ticket hall (Task 3)	West
7764	Canopy over footbridge and Platform 1 (Tasks 6 and 8)	East
7765	View towards main train shed walls (Task 9)	East
7766	View towards main train shed walls (Task 9)	East
7767	View over Platforms 1 and 2	North-east
7768	1980's ticket hall (Task 2)	South
7769	1980's ticket hall, "The Birdcage" (Task 2)	West
7770	1980's ticket hall, "The Birdcage" (Task 2)	South
7771	1980's ticket hall, "The Birdcage" (Task 2)	East
7772	Approach to 1980's ticket hall, ticket machines and substation (Task 3)	East
7773	Approach to 1980's ticket hall, ticket machines and substation (Task 3)	North

Image Number (DSCN)	Description	Direction of View
7774	Approach to 1980's ticket hall, ticket machines. (Task 3)	North
7775	Window in southern wing	East
7776	Window in west façade of old ticket hall	East
7777	Door to old ticket hall, southern end (Task 1)	East
7778	Door to old ticket hall, northern end (Task 1)	East
7779	Window in north wing	East
7780	Exterior of the north end of the station	South
7781	Exterior of the north end of the station	South
7782	North wall of main train shed	South
7783	North wall of main train shed and north wing	South
7784	Area of former glass walkway to Palace, with path on right	South-east
7785	Path along line of former walkway to palace	West
7786	Exterior of the station	South-east
7787	Doors to platform from old ticket hall (Tasks 14 and 15)	East
7788	South wing with large chimney from the roof	South
7789	South wing with large chimney from the roof	South
7790	Roof of the main train shed	East
7791	View over the main train shed	South-east
7792	Clerestory windows of old ticket hall from the roof	North
7793	South bay from the roof	South
7794	South bay from the roof	South
7795	Interior of the 1980's ticket office (Task 2)	West
7796	Kitchen of 1980's ticket office (Task 2)	South
7797	Interior of the 1980's ticket office (Task 2)	North-east
7798	Detail of fireplace & angled flue in 1st class ladies waiting room (Task 25)	South-east
7799	Detail of fireplace in 1st class ladies waiting room (Task 25)	North-east
7800	Detail of fireplace in 1st class ladies waiting room (Task 25)	East
7801	Detail of east facing window in 1st class ladies waiting room (Task 25)	South-east
7802	Old ticket hall	North
7803	Open doors at north end of north wing	North-east
7804	Open doors at north end of north wing	South
7805	Open doors at north end of north wing	South
7806	View into north wing through open doors	South
7807	Blocked doorway in north east corner of north wing (cloakroom?)	North-east
7808	West facing window in 2nd class ladies toilet, showing decorated Corinthian capital (Task 26)	North-west
7809	Detail of surviving paint work in 2nd class ladies toilet (Task 26)	North
7810	Detail of surviving paint work in 2nd class ladies toilet (Task 26)	North
7811	West facing window in 2nd class ladies toilet, showing decorated Corinthian capital (Task 26)	North-west
7812	Stairway to Platform 3	North
7813	Below stairs façade with boarded windows to island platform	North
7814	View to below stairs façade to Platform 3 (Task 27)	South
7815	Railings on old ticket hall level to Platform 3 and island platform	East
7816	Landing at old ticket hall level between Platforms 3 and 4	North
7817	Platform 4	West

Image Number (DSCN)	Description	Direction of View
7818	Platform 4 with station in background	West
7819	Platforms 3 and 4 with station in background	West
7820	Entrance to main train shed from Platform 4	West
7821	Entrance to main train shed from Platform 4	South-west
7822	Entrance to main train shed from Platform 4	South-west
7823	Island platform from Platform 4	South-west
7824	Façade below stairs to north side island platform	South-west
7825	West end of Platform 4	West
7826	Platform 4, bottom of stairs	North-east
7827	Main train shed from Platform 4	South
7828	Façade below stairs to north side of island platform	South-west
7829	Opening within tunnel, Platform 4 and Island platform	South-west
7830	Opening within tunnel, Platform 4 and Island platform	South-west
7831	Landing at old ticket hall level between Platforms 3 and 4	South
7832	Handrail and concave space in brickwork, Platform 4	East
7833	Platform 4	East
7834	Stairs to island platform, concave gap for handrail	East
7835	Stairs to island platform, concave gap for handrail	East
7836	Platform 3	East
7837	Platform 3	East
7838	Doorway from Platform 3 to Platform 2	South-east
7839	Doorway from Platform 3 to Platform 2	North
7840	East facing window and buttress (Task 5)	West
7841	Doorway beneath cutting under 1980's ticket hall	West
7842	Internal space within cutting under 1980's ticket hall	South
7843	Internal space within cutting under 1980's ticket hall	West
7844	Vaulted ceiling in internal space within cutting under 1980's ticket hall	Up
7845	Internal space within cutting under 1980's ticket hall	West
7846	Change in level in internal space within cutting under 1980's ticket hall	South-west
7847	Change in level in internal space within cutting under 1980's ticket hall	South-west
7848	Footbridge from west end of Platform 1 (Task 8)	North
7849	Tactile paving, Platform 1 (Task 11)	East
7850	Bank to the south of Platform 1	East
7851	Bank to the south of Platform 1	East
7852	Base of CCTV camera pole (Task 7)	South
7853	CCTV camera on pole	South
7854	Internal, roof of 1980's ticket hall, "the Birdcage"	West
7855	Internal, roof of 1980's ticket hall, "the Birdcage"	East
7856	Cement block repair below window in southern room off old ticket hall. (Task20)	West
7867	View of southern room off old ticket hall, showing glass and wood partitions (Task20)	North-east
7858	Blocked chimney breast in southern room off old ticket hall (Task 20)	

Table 3 Photographs taken by MOLA photographer on 23rd and 24th of April 2009

Image Number	Location	Description	Date
09009001 09009002 09009003	Exterior: to N of 19th-century entrance building, across roadway, on slope adjacent to path	Looking S, showing station from distance (especially N elevation of 19th-century entrance building, with oblique view of façade)	23/04/2009
09009004	Exterior: to NW, from above mouth of tunnel to W of station	Looking SE, showing station façade with underpasses below	23/04/2009
09009005 9009006	Exterior: to N of 19th-century entrance building	Looking S, showing detail of N face where building opened onto former covered way to exhibition grounds	23/04/2009
09009007	Exterior: to W of station	Looking E at N doors in W wall of 19th-century entrance building (doors closed)	23/04/2009
09009008	Exterior: to W of station	Looking E at N doors in W wall of 19th-century entrance building (with door to timber ticket office open)	23/04/2009
09009009 09009010 09009011	Exterior: to SW of station	Looking NE at aluminium and glass (1980s) entrance building to S of 19th-century entrance building	23/04/2009
09009012 09009013 09009014 09009015 09009016	Within 1980s entrance building	Looking NE at S face of 19th-century entrance building (south tower)	23/04/2009
09009017 09009018	Within 1980s entrance building	Looking SW at brick ticket office	23/04/2009
09009019	Within 1980s entrance building	Looking SW towards brick ticket office: working shot	23/04/2009
09009020	Within 1980s entrance building	Looking N toward door in S face of 19th-century entrance building (south tower): working shot	23/04/2009
09009021 09009022 09009023	Within 1980s entrance building	Looking N, showing detail of painting in blocked windows E of door on S face of 19th-century entrance building (south tower)	23/04/2009
09009024	Within 1980s entrance building	Looking W toward W wall	23/04/2009
09009025 09009026	Exterior: to SE of 1980s entrance building	Looking N toward S side of 19th-century entrance building (south tower) and SE portion of 1980s entrance building	23/04/2009
09009027	Exterior: to SE of 1980s entrance building	Looking W toward brick ticket office	23/04/2009
09009028	Exterior: SE of 1980s entrance building, at head of stairs to platforms	Looking S, down stairs	23/04/2009

Image Number	Location	Description	Date
09009029	Exterior: SE of 1980s entrance building, at head of stairs to platforms	Looking E over iron footbridge toward Platforms 1 and 2. Note S wall of former train shed on left, where proposed new roof to be constructed	23/04/2009
09009030 09009031	Exterior: on landing at foot of first flight of stairs below 1980s entrance area (S of Platform 1)	Looking E along embankment to S of Platform 1	23/04/2009
09009032	Exterior: to S of iron footbridge, at foot of stairs to 1980s entrance area	Looking N toward iron footbridge	23/04/2009
09009033	Exterior: at S end of iron footbridge	Looking N, showing covered interior of iron footbridge	23/04/2009
09009034	Exterior: Platform 1 at foot of stairs to iron footbridge	Looking SW toward stairs to iron footbridge and boundary wall & fence on S side of platform	23/04/2009
09009035	Exterior: Platform 1	Looking NW toward main group of station buildings	23/04/2009
09009036 09009037	Exterior: Platform 1	Looking N toward area where new roof will cover Platforms 3 and 4, adjoining E end of main building	23/04/2009
09009038 09009041	Exterior: Platform 2	Looking W toward footbridge and main group of station buildings	23/04/2009
09009039 09009040	Exterior: Platform 2	Looking SW toward footbridge and main group of station buildings: working shot	23/04/2009
09009042	Main building – Platform 3	Looking E along platform	23/04/2009
09009043	Exterior: Platform 3	Looking N toward N wall of Platform 4 (near E end)	23/04/2009
09009044 09009045	Exterior: Platform 3	Looking W toward main group of station buildings	23/04/2009
09009046	Exterior: Platform 3	Looking NE toward pier at E end of N wall of Platform 4	23/04/2009
09009047	Exterior: Platform 3	Looking W toward central arches leading into main building	23/04/2009
09009048	Exterior: Platform 3	Looking W toward stub of island platform between Platforms 3 and 4, where masonry trials are ongoing	23/04/2009
09009049	Exterior: Platform 3	Looking NW toward island platform and spine wall of present train shed	23/04/2009
09009050	Main building – Platform 3	Looking W toward end of platform and stairs to ticket hall	23/04/2009
09009051	Main building – Platform 3	Looking NE toward present end of island platform and interior of arches at E end of main building	23/04/2009
09009052	Main building – Platform 3	Looking SW toward W end of Platform 3	23/04/2009
09009053	Main building – Stairs to Platform 3 from ticket hall – second landing from top	Looking NW up stairs, showing stairway parapet and E end of main building	23/04/2009

Image Number	Location	Description	Date
09009054	Main building – Stairs to Platform 3 from ticket hall – first landing from top	Looking NE, showing stairway parapet with rest of main building beyond	23/04/2009
09009056	Main building – upper landing at former entrance to ticket hall (presently serving as bridge between Platforms 3 and 4)	Looking S toward stairway in SW corner of main building (leading down to Platform 3)	23/04/2009
09009057	Main building – upper landing at former entrance to ticket hall (presently serving as bridge between Platforms 3 and 4)	Looking E toward former stairway to southern side of island platform	23/04/2009
09009058	Main building – upper landing at former entrance to ticket hall (presently serving as bridge between Platforms 3 and 4)	Looking NE toward railings and gates at head of stairs to central platforms (divided island platform)	23/04/2009
09009059	Main building – upper landing at former entrance to ticket hall (presently serving as bridge between Platforms 3 and 4)	Looking NW, showing metal fixture	23/04/2009
09009060	Main building – upper landing at former entrance to ticket hall (presently serving as bridge between Platforms 3 and 4)	Looking SW toward southern two doorways	23/04/2009
09009061	Main building – upper landing at former entrance to ticket hall (presently serving as bridge between Platforms 3 and 4)	Looking E toward stairway to northern side of island platform	23/04/2009
09009062	Exterior: Platform 4	Looking N toward pier at E end of N wall of platform	23/04/2009
09009063 09009064	Exterior: Platform 4	Looking NW toward niche in E face of pier at E end N wall of platform	23/04/2009
09009065	Exterior: Platform 4	Looking W along platform toward main group of station buildings	23/04/2009
09009066	Exterior: Platform 4	Looking NW into corner made by N wall of platform and E wall of main building	23/04/2009
09009067	Main building – upper landing at former entrance to ticket hall (presently serving as bridge between Platforms 3 and 4)	Looking SW toward northern two doorways to ticket hall	23/04/2009
09009068	Exterior: to W of 1980s entrance area	Looking S	24/04/2009
09009069	Timber ticket office	Looking SE, towards open hatch	24/04/2009
09009070	Timber ticket office	Looking SE, towards closed hatch	24/04/2009
09009071	Timber ticket office	Looking SE, up towards ceiling/roof	24/04/2009
09009072	Timber ticket office	Looking S, towards door from ticket office to ticket hall	24/04/2009
09009073	Timber ticket office	Looking N	24/04/2009
09009074	Timber ticket office	Looking W towards former fireplace	24/04/2009
09009075	Timber ticket office	Looking W towards entrance door (leading to exterior)	24/04/2009
09009076	Ticket hall	Looking SE	24/04/2009

Image Number	Location	Description	Date
09009077	Ticket hall	Looking SW	24/04/2009
09009078	Ticket hall	Looking SW	24/04/2009
09009079	Ticket hall	Looking NE	24/04/2009
09009080	Ticket hall	Looking N, showing timber ticket office	24/04/2009
09009081	Ticket hall	Looking N, showing timber ticket office	24/04/2009
09009082	Ticket hall	Looking NW, showing timber ticket office	24/04/2009
09009083	Ticket hall	Looking NW, showing timber ticket office	24/04/2009
09009084	Ticket hall	Looking NW, showing timber ticket office	24/04/2009
09009085	Ticket hall	Looking W, showing timber ticket office	24/04/2009
09009086	Ticket hall	Looking W, showing timber ticket office	24/04/2009
09009087	Ticket hall	Looking W, showing southern entrance doors in W wall	24/04/2009
09009088	Ticket hall	Looking SW	24/04/2009
09009089	Ticket hall	Looking SW	24/04/2009
09009090	Ticket hall	Looking W toward N-most entrance door in W wall	24/04/2009
09009091	Ticket hall	Looking S	24/04/2009
09009092	Ticket hall	Looking S	24/04/2009
09009093	Ticket hall	Looking E toward E wall: N-most doors to platforms	24/04/2009
09009094	Ticket hall	Looking E toward E wall: N-most doors to platforms	24/04/2009
09009095	Ticket hall	Looking SE toward E wall: N-most doors to platforms	24/04/2009
09009096	Ticket hall	Looking NE through arches into ground floor of north tower (ramps to N exit doors)	24/04/2009
09009097	Ticket hall	Looking N: working shot of survey at E wall	24/04/2009
09009098	Ticket hall	Looking NW toward NW corner of hall, showing arched openings to ground floor of N tower (to N of hall)	24/04/2009
09109001	South tower: ground floor – north-eastern room	Looking W	24/04/2009
09109002	South tower: ground floor – north-eastern room	Looking W	24/04/2009
09109003	South tower: ground floor – central eastern room	Looking E	24/04/2009
09109004	South tower: ground floor – central eastern room	Looking E	24/04/2009
09109005	South tower: ground floor – central eastern room	Looking SW	24/04/2009
09109006	South tower: ground floor – south-eastern room	Looking NE	24/04/2009
09109007	South tower: ground floor – west central room	Looking W	24/04/2009
09109008	South tower: ground floor –west central room	Looking SE	24/04/2009

Image Number	Location	Description	Date
09109009	South tower: ground floor – south-western room	Looking W	24/04/2009
09109010	Ticket hall	Looking NE toward E wall: northern doorways to platforms	24/04/2009
09109011	South tower: ground floor – south-western room	Looking W: working shot	24/04/2009
09109012	South tower: ground floor – south-western room	Looking W: working shot	24/04/2009
09109013	Ticket hall	Looking N, showing timber ticket office: working shot	24/04/2009
09109014	Ticket hall	Looking N, showing timber ticket office: working shot	24/04/2009
09109015	Ticket hall	Looking NW, showing timber ticket office: working shot	24/04/2009
09109016	Ticket hall	Looking NW, showing timber ticket office: working shot	24/04/2009
09109017	Exterior: Platform 2	Looking W: working shot	24/04/2009
09109018	Exterior: Platform 2	Looking W: working shot	24/04/2009
09109019	Exterior: Platform 2	Looking N: working shot	24/04/2009
09109020	Exterior: Platform 2	Looking N: working shot	24/04/2009
09109021	Exterior: Platform 2	Looking E: working shot	24/04/2009
09109022	Exterior: Platform 2	Looking E: working shot	24/04/2009
09109023	Exterior: Platform 2	Looking E: working shot	24/04/2009
09109024	Exterior: Platform 2	Looking E: working shot	24/04/2009
09109025	Exterior: Platform 2	Looking E: working shot	24/04/2009
09109026	Exterior: Platform 2	Looking W: working shot	24/04/2009
09109027	Exterior: Platform 2	Looking W: working shot	24/04/2009
09109028	Exterior: Platform 1	Looking N towards Platform 2	24/04/2009
09109029	Exterior: Platform 1	Looking N towards Platform 2	24/04/2009
09109030	Exterior: Platform 1	Looking N towards Platform 2	24/04/2009
09109031	Exterior: top of stairs to platforms S of 1980s entrance area	Looking NE towards Platform 2	24/04/2009
09109032	Exterior: top of stairs to platforms S of 1980s entrance area	Looking NE towards Platform 2	24/04/2009
09109033	Exterior: top of stairs to platforms S of 1980s entrance area	Looking NE towards Platforms 1 (under canopy at right) and 2 (on far side of tracks at centre left)	24/04/2009
09109034	Exterior: top of stairs to platforms S of 1980s entrance area	Looking NE towards Platform 2	24/04/2009
09109035	Exterior: W of ticket hall	Looking NE towards N doorway: working shot	24/04/2009
09109036	Exterior: W of ticket hall	Looking NE towards N doorway: working shot	24/04/2009
09109037	Exterior: W of ticket hall	Looking E working shot	24/04/2009
09109038	Exterior: W of ticket hall	Looking NE towards N doorway: working shot	24/04/2009
09109039	Exterior: W of ticket hall	Looking NE towards N doorway: working shot	24/04/2009
09109040	Exterior: roadway above tunnel to W of station	Looking E over railway cutting toward 19th-century entrance building, showing ticket hall and south tower supported by arches over tracks	24/04/2009

Image Number	Location	Description	Date
09109041	Exterior: roadway above tunnel to W of station	Looking E over railway cutting toward S part of 19th-century entrance building (ticket hall and south tower) and N part of 1980s entrance area (aluminium and glass entrance hall)	24/04/2009
09109042	Exterior: roadway above tunnel to W of station	Looking E over railway cutting toward 19th-century entrance building, showing ticket hall and south tower supported by arches over tracks	24/04/2009
09109043	Exterior: roadway above tunnel to W of station	Looking E over railway cutting toward 19th-century entrance building, showing ticket hall and south tower supported by arches over tracks	24/04/2009
09109044	Exterior: roadway above tunnel to W of station	Looking E over railway cutting toward 19th-century entrance building, showing ticket hall and south tower supported by arches over tracks	24/04/2009
09109045	Exterior: roadway above tunnel to W of station	Looking E over railway cutting toward 19th-century entrance building, showing ticket hall and south tower supported by arches over tracks	24/04/2009

8 Appendix 2: List of Working Drawings Made on Site During the Survey

Table 4 Working drawings made on site during the survey

Sheet number	Description	Date
1	Plan of roof frame of timber ticket office as seen from above Task 18	AW 21/4/09
2	Section/elevation through timber panelling of ticket office Task 18	AW 21/4/09
3	Analytical drawing of doors in west wall of 19th C ticket hall Task 1	MT 24/4/09
4	North facing section through timber ticket office and roof Task 18	MT 22/4/09
5	Notes & observations inside timber ticket office, fireplace etc Task 18	MT 23/4/09
6	West facing elevation of west wall within timber ticket office Task 18	MT 22/4/09
7	Section/elevation and plan view of southernmost hatch of timber ticket office Task 18	MT 22/4/09
8	Elevation of east facing wall at west end of Platform 3 Task 27	MT 24/4/09
9	Plan of timber ticket office Task 18	DS 21/4/09
10	Ground floor plan of south end of south tower	DS 22/04/09
11	Ground floor plan of north end of south tower	DS 22/4/09
12	North facing elevation and east facing elevation of Task 20	DS 22/4/09
13	Sketched elevation of doors to platforms from ticket hall, Tasks 14, 15, 16 and 17	DS 22/04/09
14	Sketched elevation of typical door, task 14	DS 22/4/09
15	North facing elevation of task 19	DS 23/4/09
16	West facing elevation, Task 21	DS 23/4/09
17	North facing elevation and detail of architrave moulding, Task 22	DS 23/4/09
18	West facing elevation, Task 25	DS 23/4/09
19	North facing elevation, windowed partition wall, Task 35	DS 23/4/09
20	Detail of door, picture rail and skirting board mouldings, Task 25	DS 23/4/09
21	Internal west facing elevation, Task 21	DS 23/4/09
22	South facing elevation of fireplace, Task 23	DS 24/4/09
23	South facing elevation, doorway, arch and window in section, Task 24	DS 24/4/09
24	North facing elevation, Platform 3, Task 27	DS 24/4/09
25	Section/elevation drawing of balustrade wall & brick posts, Task 28	DS 24/4/09

9 Appendix 3: OASIS Entry

OASIS ID: molas1-60224

Project details

Project name Crystal Palace Station

Short description of the project Museum of London Archaeology was commissioned by Transport for London to analyse and record parts of Crystal Palace Station, London SE19. The station is grade II listed, and is in a conservation area. Some elements within the building were to be demolished or altered prior to its refurbishment as part of the extension of the East London Line, and the archaeological investigation and a subsequent report were required as a condition of planning consent for the redevelopment. Investigation took place in April 2009. Crystal Palace Railway Station was opened in 1854 to serve the large number of visitors who came to visit the Crystal Palace, which had been relocated from Hyde Park. The station was extended in 1857 and was partially rebuilt in the 1870s. In 1986, the public entrance and ticket office were relocated to new facilities built adjacent to the south side of the 1870s entrance building. The standing building survey report combines the results of the archaeological survey with evidence from archival research and presents both a wider view of the main station buildings and a focused discussion of the specific areas due to be altered as part of the planned refurbishment.

Project dates Start: 23-10-2008 End: 02-06-2009

Previous/future work Not known / Not known

Type of project Building Recording

Site status Listed Building

Site status Conservation Area

Current Land use Transport and Utilities 2 - Other transport infrastructure

Monument type RAILWAY STATION Post Medieval

Monument type FOOTBRIDGE Post Medieval

Methods techniques & 'Annotated Sketch', 'Survey/Recording Of Fabric/Structure'

Prompt Listed Building Consent

Project location

Country England
Site location GREATER LONDON BROMLEY BROMLEY Crystal Palace Station
Postcode SE19
Study area 1.00 Hectares
Site coordinates TQ 34123 70548 51.4175505405 -0.07095781456710 51 25 03 N
000 04 15 W Point

Project creators

Name of MOL Archaeology
Organisation
Project originator brief Transport for London (TfL)
Project originator design MOL Archaeology
Project director/manager Elaine Eastbury
Project supervisor Andrew Westman
Type of Developer
sponsor/funding body
Name of Transport for London
sponsor/funding body

Project archives

Physical Archive No
Exists?

Digital recipient Archive LAARC

Paper recipient Archive LAARC

Project bibliography 1

Publication type Grey literature (unpublished document/manuscript)

Title Crystal Palace Station, Crystal Palace Station Road, London SE 19:
a standing building survey report

Author(s)/Editor(s) Sorapure, D.

Author(s)/Editor(s) Tetreau, M.

Date 2009

Issuer or publisher MOL Archaeology

Place of issue or publication London

Description A4 client report

Entered by Michael Tetreau (mtetreau@museumoflondon.org.uk)

Entered on 2 June 2009

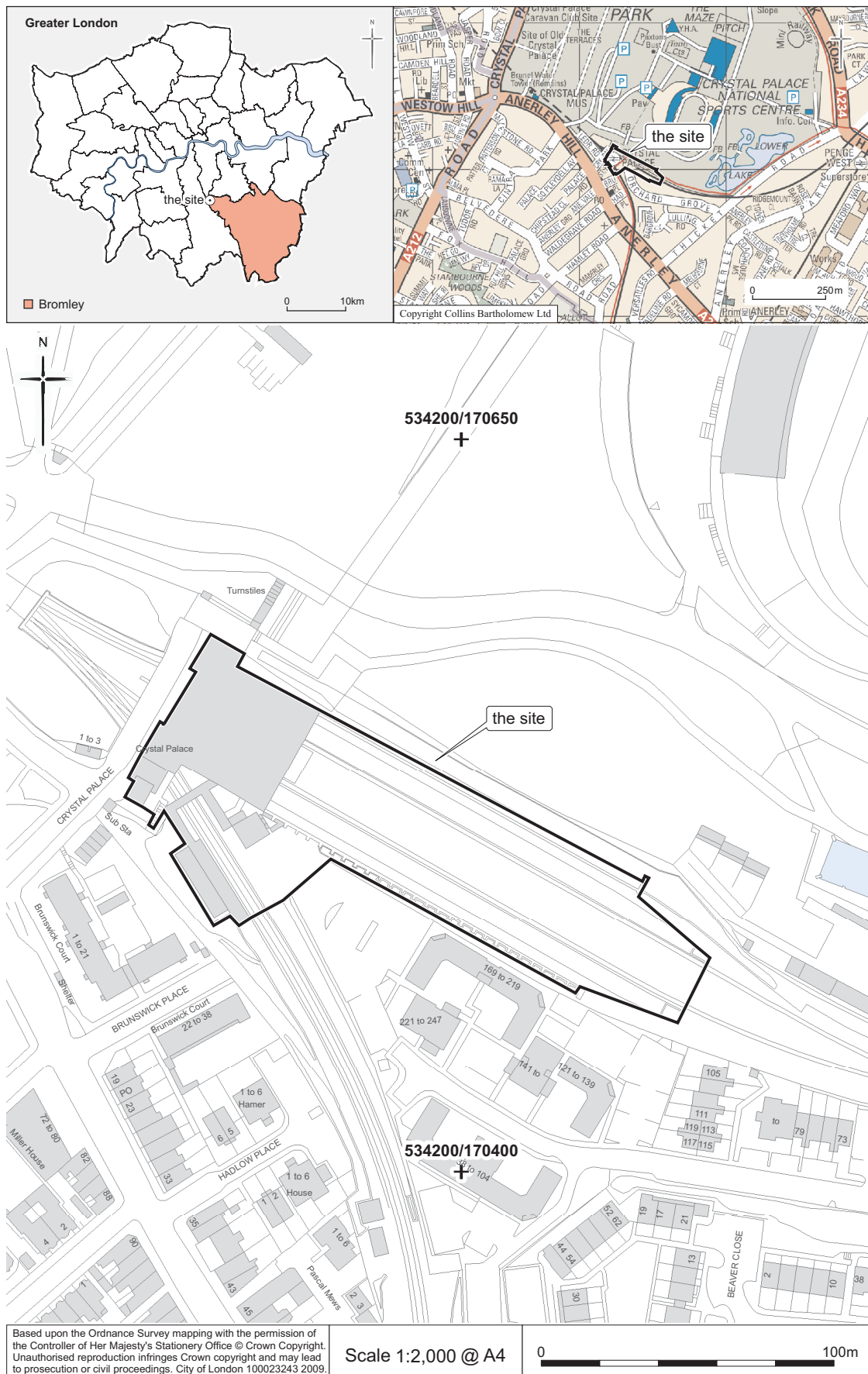


Fig 1 Site location

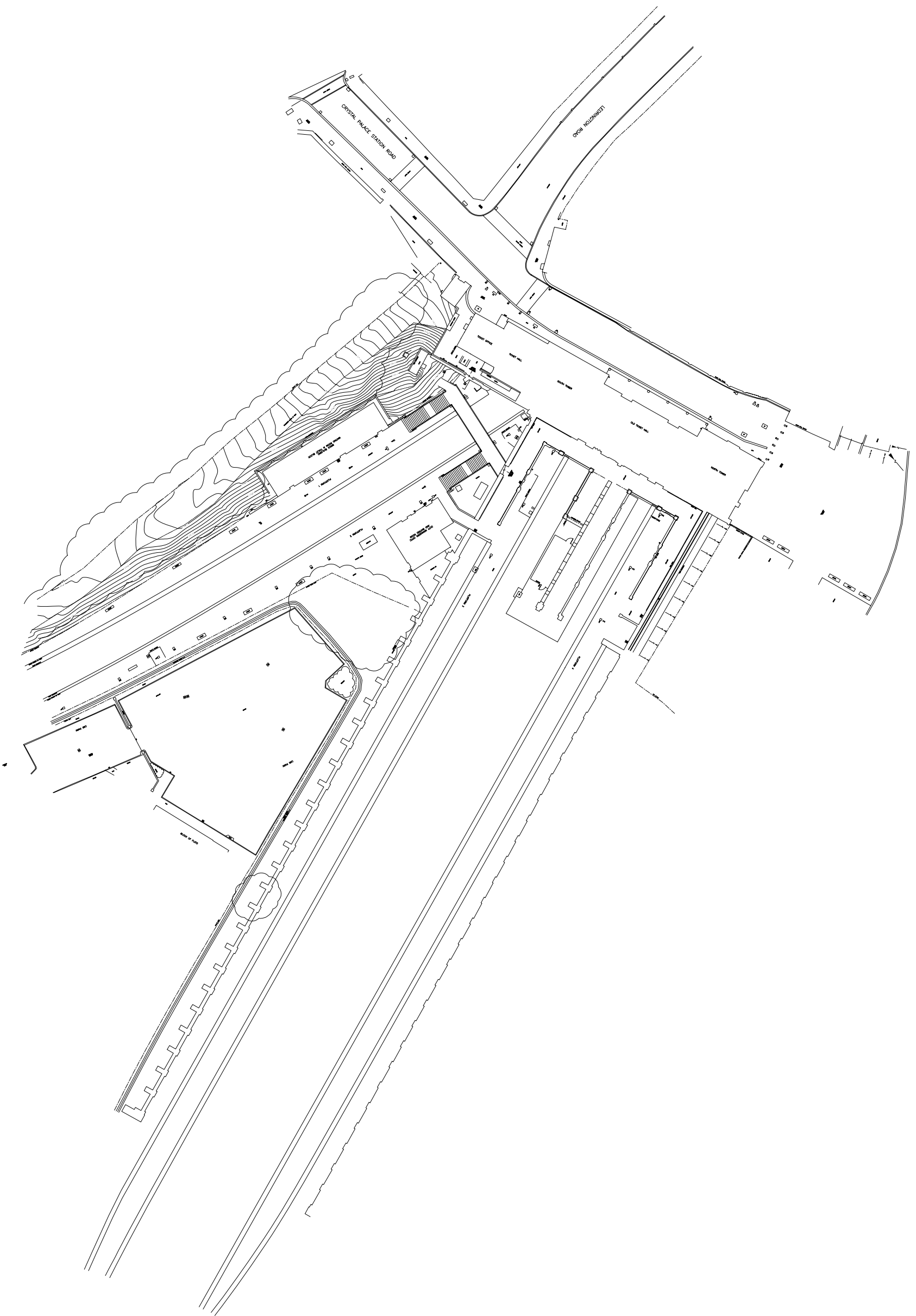


Fig 2 Site layout

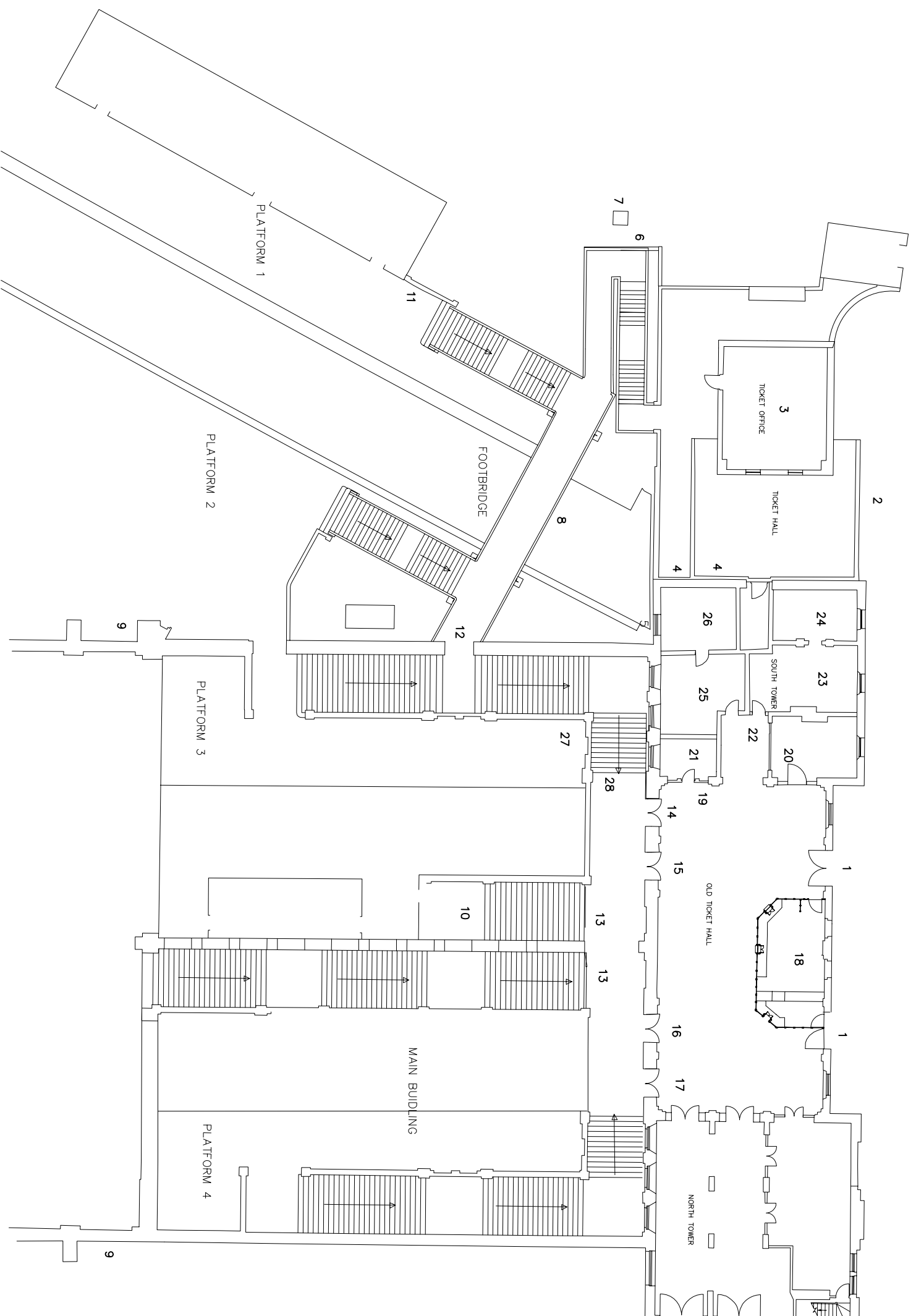


Fig 3 The site showing the location of the footbridge (task 8) and items recorded during the survey (see Table 1)

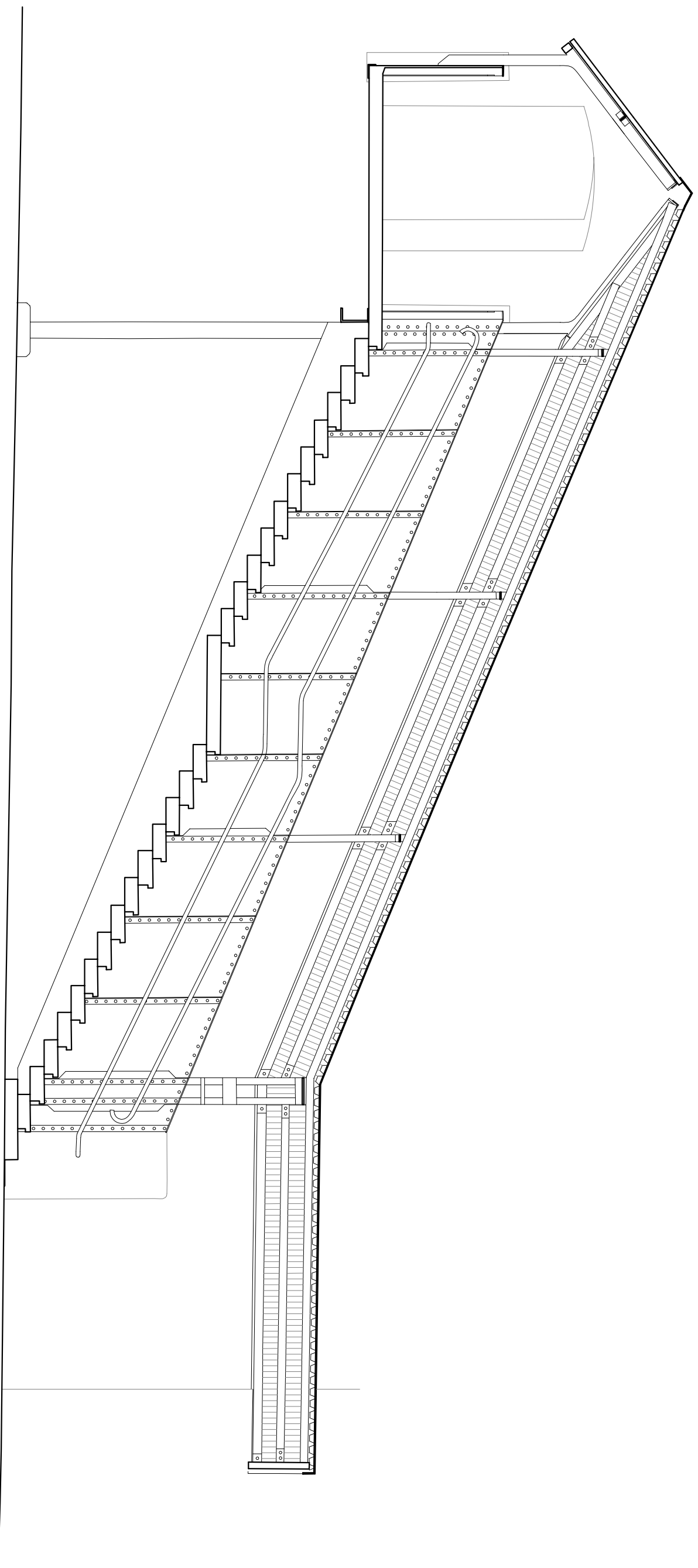


Fig 4 South-facing sectional elevation through the footbridge between platforms 1 and 2



Fig 5 Task 18. Plan of the timber ticket office within the ticket hall

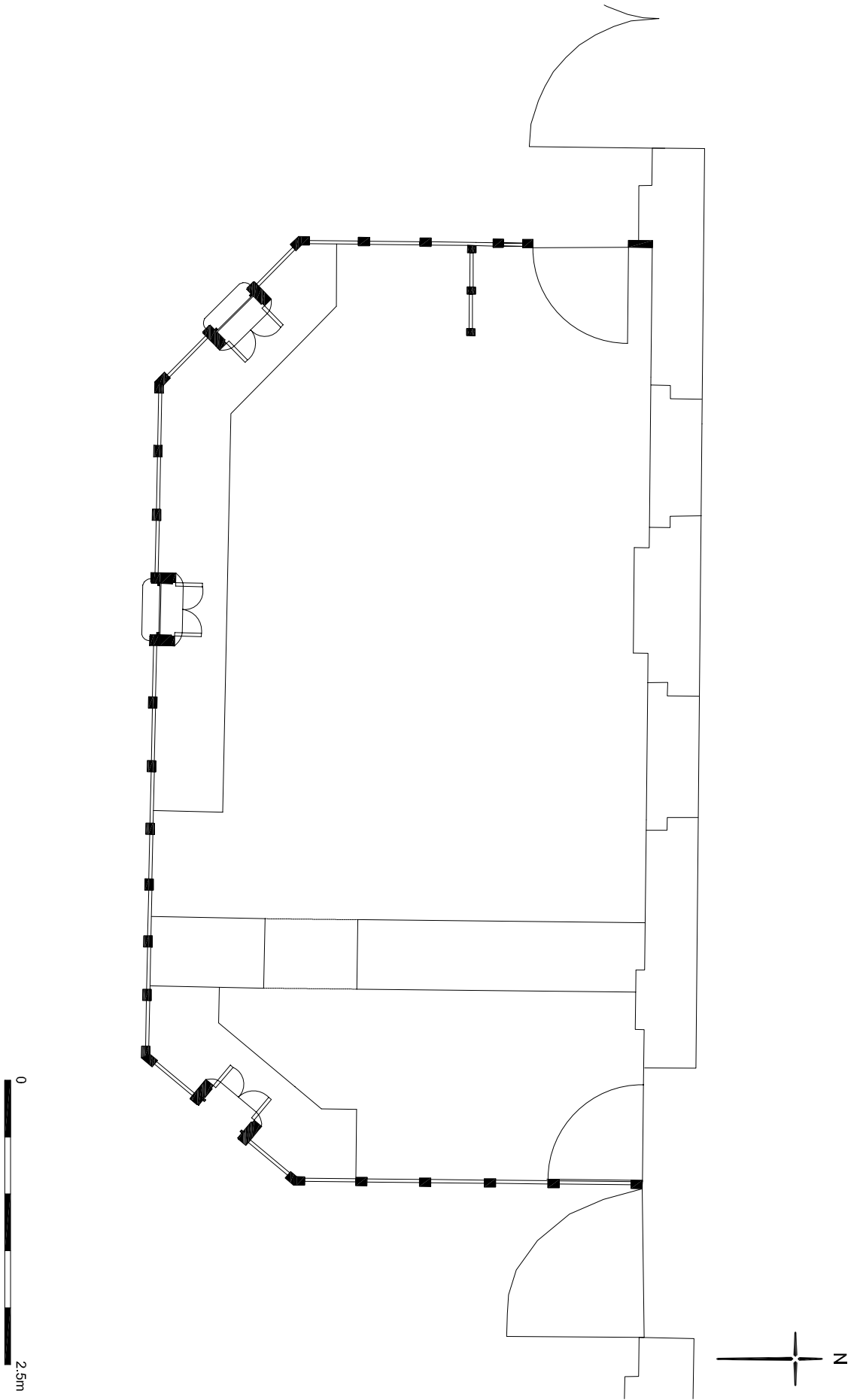


Fig 6 Task 25. North-facing elevation of partition wall in ground floor of south tower

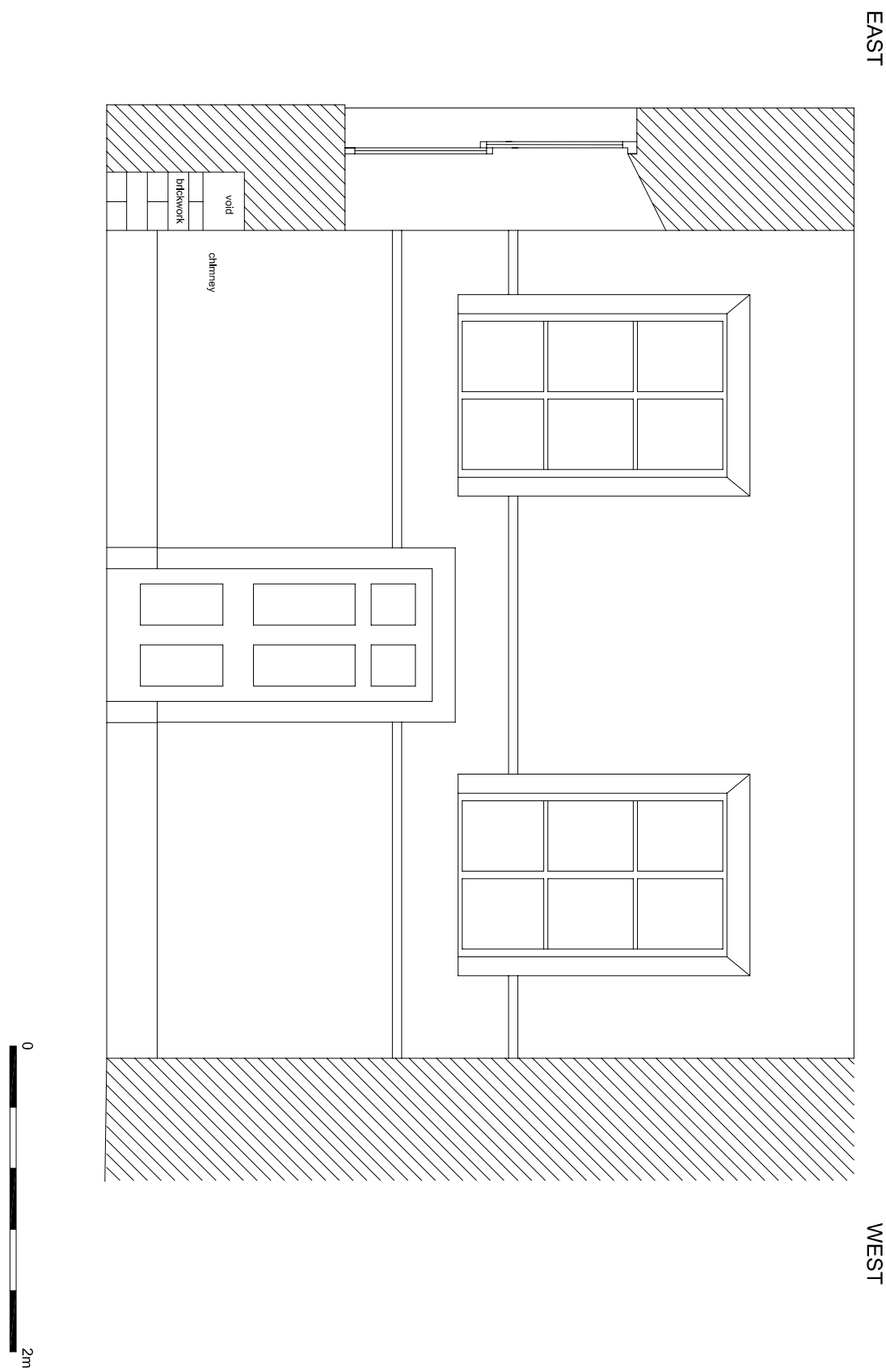


Fig 7 Task 27. North facing facade of platform-level rooms on platform 3, below staircase

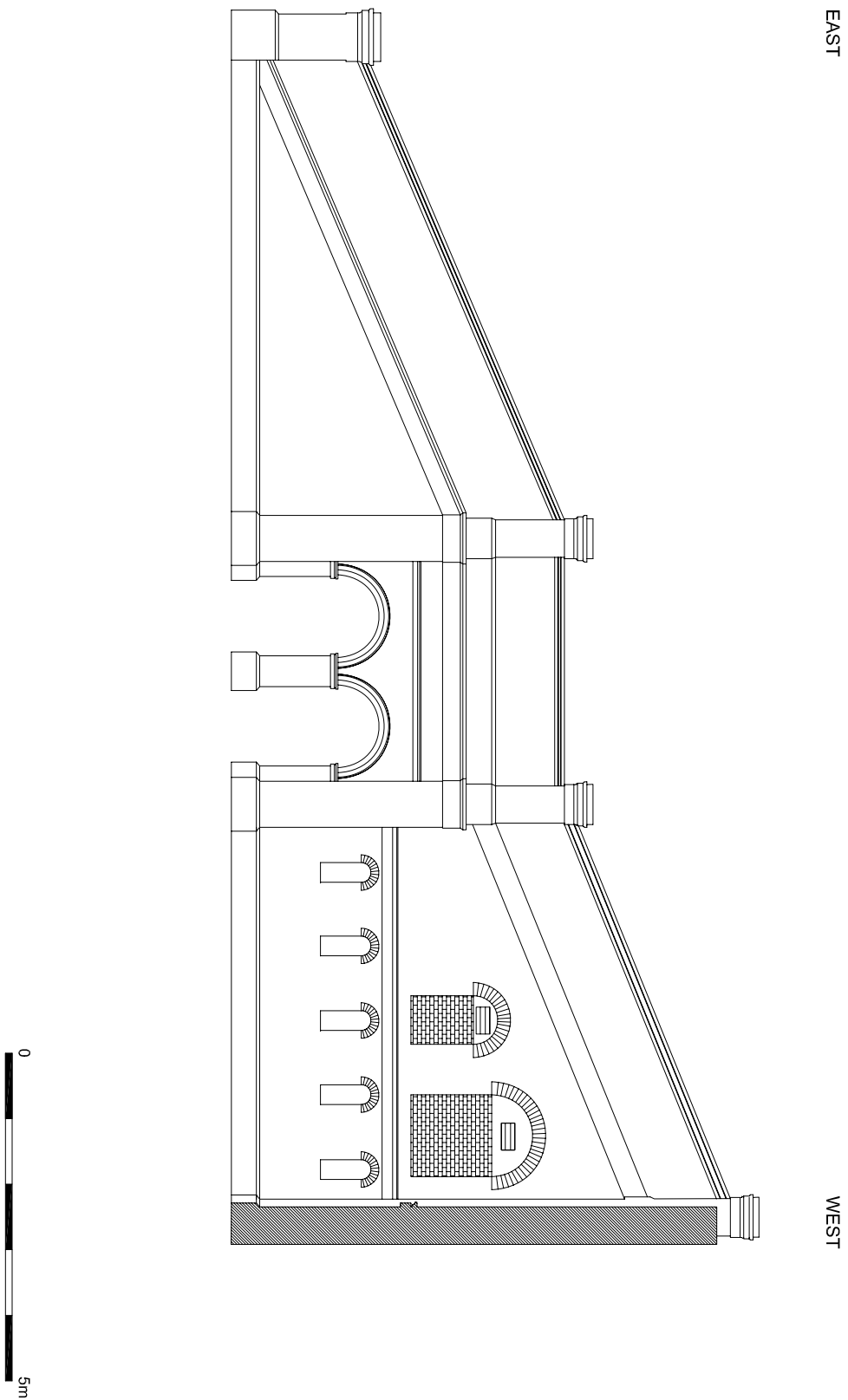




Fig 8 The north tower, north façade, looking south



Fig 9 The north tower, west façade, looking east



Fig 10 The western façade of the 19th century ticket hall, looking east



Fig 11 The decorated floating corbel, on the eastern wall of the 19th century ticket hall



Fig 12 The south tower, looking north east



Fig 13 Southern façade of the south tower, looking north



Fig 14 Task 1 the southernmost pair of doors in the western façade of the ticket hall, internal looking west



Fig 15 Task 1 the northernmost pair of doors in the western façade of the ticket hall, external looking east



Fig 16 Task 2 1980s entrance hall at the southern end of the 19th century entrance building



Fig 17 Task 2 internal view of the 1980s entrance hall, looking west

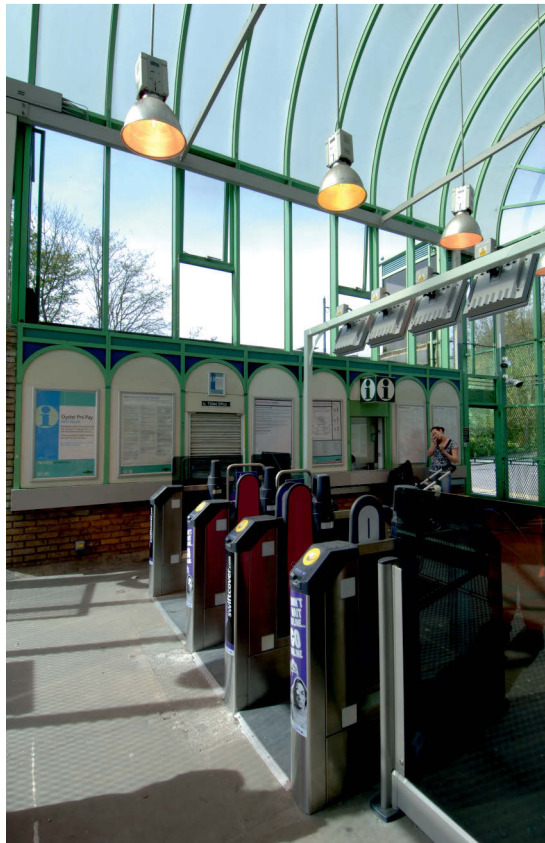


Fig 18 Task 3, present brick built ticket office, external view looking south west



Fig 19 Task 3 internal view of the present ticket office, looking north east



Fig 20 Task 4 south external wall of the south tower looking north, within the entrance hall showing boarded and painted windows



Fig 21 Task 4 south external wall of the south tower behind the entrance hall, looking north



Fig 22 Task 5 internal view of the window to the south in east wall of south tower, looking north east. (will remain in revised plan)



Fig 23 Task 6 stairs to footbridge looking north



Fig 24 Task 7 CCTV camera pole and base, looking south



Fig 25 Task 8 the Footbridge between Platforms 1 and 2 looking north west



Fig 26 Task 9 the northernmost wall on Platform 4, looking north



Fig 27 Task 9 detail of the buttress at the east end of the wall on Platform 4, looking north west



Fig 28 Task 10, stairs down to east side of island platform , main building, looking north west



Fig 29 Task 11, brick wall, tactile paving and metal fence, Platform 1, looking south west



Fig 30 Task 12 doorway in south wall of main building connecting the footbridge to Platform 3, looking north



Fig 31 Task 13 railings at the head of former stairs to island platform looking north east



Fig 32 Tasks 14, 15 16 and 17 internal view of doorways from ticket hall to Platforms 3 and 4, looking north east



Fig 33 Tasks 16 and 17 with Tasks 14 and 15 in the distance, external view of doorways from ticket hall to platforms, looking south



Fig 34 Task 18 the timber ticket office, looking west with hall entrance door



Fig 35 Task 18, internal view of the timber ticket office, looking south



Fig 36 Task 18, detail of pulleys around ticket hatch in south east wall of ticket office, looking south east



Fig 37 Task 18, internal view of the ticket office, looking north



Fig 38 Task 18, internal west wall of ticket hall building and chimney breast within ticket office, looking west



Fig 39 Tasks 19 and 21, bricked up arch in south east corner of the ticket hall, looking south east and the west facing side wall with ticket desk Task 21



Fig 40 Task 20 glazed timber partitions to room at south end of ticket hall

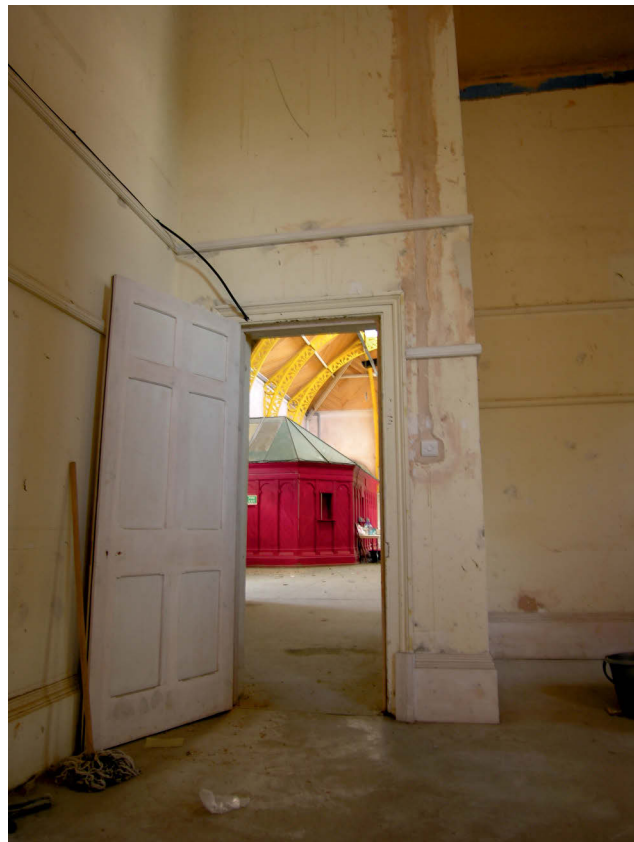


Fig 41 Task 22, easternmost of the two doors, looking north into the ticket hall



Fig 42 Task 23, north western ground floor room of the south tower, looking west



Fig 43 Task 24 far south western ground floor room in the south tower showing the green wall paint, looking west



Fig 44 Task 25, the east room of the south tower, looking east, showing the two windows and a fireplace beneath



Fig 45 Task 25, detail of the fireplace showing the angled flue, looking south east



Fig 46 Task 25, southern windowed partition wall, looking south west



Fig 47 Task 27, North facing façade below staircase on Platform 3, looking west



Fig 48 Task 28 wall at ticket hall level on the landing between Platforms 3 and 4, looking south