

North Hooe Mine, Bere Alston, Devon

Historic Building Recording



SOUTHWEST ARCHAEOLOGY

Heritage Consultancy

North Hooe Mine, Bere Alston, Devon

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Executive Summary

This report presents the results of a programme of historic building recording undertaken at the non-designated North Hooe Mine in Bere Alston, Devon. This work was undertaken in advance of the conservation of the mine buildings, which are in a poor and declining state.

This report builds on existing surveys and provides a point-in-time record of the mine site prior to consolidation works taking place. Based on what is currently known, North Hooe Mine was established in 1838x42 as part of Tamar Consols, together with the renewed exploitation of South Hooe Mine and prospection elsewhere. This took place under the direction of the notable 19th century metallurgist-assayer Percival Norton Johnson. In 1855 the site was closed and the materials sold to support renewed investment at South Hooe. All the buildings are shown as roofless and derelict on the early 1880 OS maps, indicating they had been plundered for their materials. Attempts were made to revitalise the mine – a cross cut from South Ward mine in 1874, and a failed dewatering in the early 1900s – but the only significant addition to the site was a pump house dating to the c.1960s.

The principal surviving remains are those of the pumping engine house and the smithy, both of which are swathed in vegetation and in a poor structural state. Nonetheless, and despite a lack of statutory protection, the site is significant for its historical-illustrative and associative value, and the fact it embodies key attributes of the Cornwall and West Devon Mining Landscape World Heritage Site.

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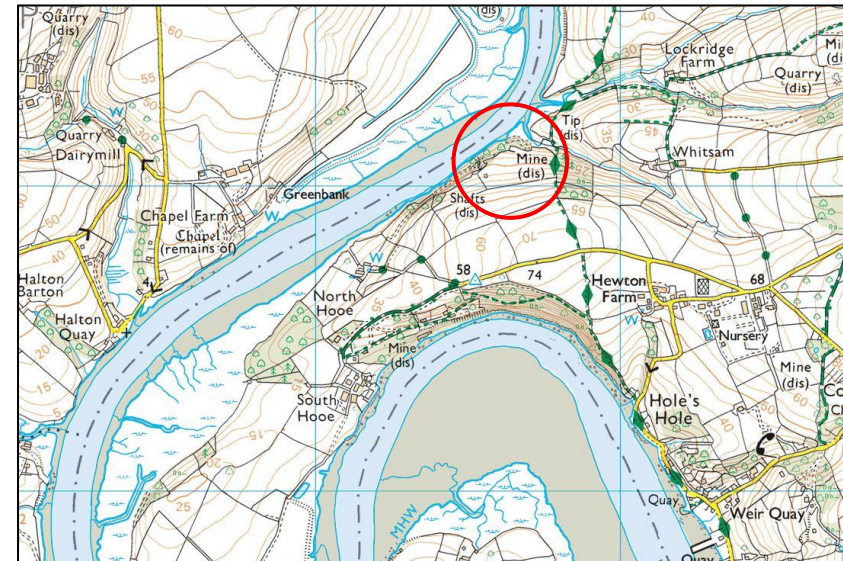


FIGURE 1: LOCATION MAP (THE MINE IS INDICATED).

1 INTRODUCTION

1.1 PROJECT BACKGROUND

South West Archaeology Ltd. (SWARCH) was instructed by Nova Surveyors Ltd. (the Agent), on behalf of a private client (the Client), to undertake historic building recording at North Hooe Mine, Bere Alston, Devon. This work was undertaken in advance of the conservation of the North Hooe engine house, which is in a poor and declining state. The site lies in the Cornwall and West Devon Mining Landscape World Heritage Site.

1.2 SCOPE AND CONTEXT

Nova Surveyors Ltd. commissioned SWARCH to build on existing reports for the site¹ and undertake a desk-based assessment with Level 1 Historic England ground survey. This was to include a gazetteer of structures within the historic mining complex, and a statement of significance. This work was taking place in the context of the grant-funded consolidation of some of the ruinous mine buildings.

1.3 METHODOLOGY

The North Hooe mine site was visited on the 28th of April 2024 by C. Wingrave and B. Morris. All the visible elements of the mine (including the engine house, boiler house, smithy, pump house, balance bob pit, crusher house, tramway, retaining walls, wharf, spoil heaps, dressing floors, leat/track, etc.) were located, compared to existing descriptions and the descriptions updated as necessary. Each structure was inspected and recorded on proforma record sheets. Photographs, with scales, were taken of exterior elevations and internal details.

1.4 LIMITATIONS AND CAVEATS

The site is largely located within and on the edge of woodland. Parts of the site had been cleared but as the site was inspected in the late spring, the lush vegetation will have concealed detail. The engine house in particular is swathed in ivy and much of the upper stonework was very effectively concealed.

Note that recommendations for vegetation clearance and consolidation of walls and openings would need to take ecological considerations into account (e.g. bats; reptiles), upon which the author is not qualified to comment. Similarly, the removal of vegetation (ivy etc.) will facilitate necessary repairs to the structures, but impact both the authenticity of the structures, and their aesthetic appearance, as ruins.

1.5 LOCATION, TOPOGRAPHY, GEOLOGY, SOILS

The mine site is located on the western side of the Bere Peninsular, and the northern side of a particularly dramatic incised bend in the course of the River Tamar. Most of the structures are located on the break of slope above a very steep, wooded slope that drops down to the river, at about 20m AOD. The soils here are the well-drained fine loamy and fine silty soils over rock of the Denbigh 1 Association². These overlie the slates of the Tavy Formation, with a narrow east-west dyke of lamprophyre just to the south³.

1.6 PREVIOUS WORK

The mine has been considered – albeit briefly and unfavourably relative to South Hooe Mine – by various mining authors⁴. North Hooe was assessed in 1996, 1998 and 2006 as part of a wider study of Devon engine houses⁵, industrial sites in the Tamar Valley⁶, and as part of the work of the Tamar Mining Group⁷. A conservation assessment was produced for the site in 2023⁸. A series of relevant reports have been produced for South Hooe Mine, in advance of and following conservation works there⁹. Finally, a synthetic account of silver mining on the Bere Peninsula was produced in 2009¹⁰.

1.7 HISTORICAL BACKGROUND

This synopsis has been compiled from multiple sources¹¹.

There has been silver mining in the parishes of Bere Alston and Bere Ferrers since the medieval period¹². The mines at North and South Hooe worked the same roughly north-south silver-lead lode, to which Hancock Mine, on the northern bank of the Tamar, was later added. South Hooe (in the 14th century as ‘South Mine’ and ‘Old Mine’) appears to have

been worked into the 15th century but works there only resumed in the later 18th century as mining technology improved and facilitated deep hard-rock mining.

It is not entirely clear, given the fluidity of mining companies, the extent to which the lode at North Hooe was mined from the shaft at South Hooe, but no buildings are shown at North Hooe until *after* the 1844 tithe map was drawn up. At that time the field containing the mine (*Lower Drayton*, the land use being recorded as arable) formed part of the tenement of Hewton (with the caveat that if the tithe map was based on an earlier estate map, potentially extant buildings may not have been depicted)

However, there were seven fields here that formed a single block called *Drayton* (North Drayton, West Drayton, Middle Drayton, Lower Drayton, Drayton Orchard, Great Drayton) and -ton (*tūn*) is a settlement place-name element, implying there is a lost medieval settlement here.

Between 1811-1824 the South Hooe silver lead mine was worked profitably by the *Beeralston Mining Company* but, there being only the single shaft, the lack of adequate ventilation hindered production. In 1824, when mining labour was in great demand, the mine was forced to close.

South Hooe was purchased by the newly formed *Tamar Silver Lead Company* in 1835, and in 1838 a 21-year lease was signed between Lord Mount Edgcumbe (the landowner) and Joseph Grout, Edward Wilkinson, Peter Stainsby, Richard Thomas, and Percival Norton Johnson, for mining rights in South Hooe, North Hooe, Hewton, Drayton Wood, and Hanging Cliffe Wood. Given that description, it seems most likely the complex at North Hooe Mine was established in or around 1838; Hamilton-Jenkin states it was opened in 1842¹³.

South Hooe reopened in 1842 as part of *Tamar Consols*, under the direction of Percival Norton Johnson (Director 1838-1854; Managing Director 1841-1855). He installed a system of fan ventilation and was thus able to re-open the mine. The 1842 AGM noted the company had

three pumping engines (2×50" and 1×42"), and these are probably South Hooe, North Hooe and Wheal Hancock.

To facilitate production, in 1845 the company acquired two smelters at Weir Quay. The site, which had been operating since the 1830s, was refurbished and became a leading centre for smelting lead and silver ores. The works were sold in 1853, against Johnson's advice.

Peak production in refined silver was reached in 1852, when 40,200oz of silver was produced, South Hooe being the more productive mine. North Hooe reached peak production in 1846, with 1200 tons of silver-lead ore being recorded.

Due to irregularities in the company accounts, a special committee was convened in 1855 to investigate, and as a result the directors were forced to resign and the company was wound up. The new manager reported in March that considerable investment would be required at South Hooe Mine, and recommended the machinery and buildings at North Hooe and Wheal Hancock should be sold to generate the necessary money. Production at South Hooe continued, and continued to be profitable, but the company was wound up in 1863/64 and its assets sold. Attempts were made in 1874 to drive a cross cut from South Ward Mine to intersect with the 60-fathom level of North Hooe Mine¹⁴.

The final years of the mine are poorly documented, but in 1878 a new company, possibly called *Tamar Silver Mine*, was formed, sold again in 1880 and rebranded *Tamar Silver Lead & Fluorspar Co.* 1882 is last year for which production is recorded. North Hooe Mine may have been worked at the same time, but the 1882 OS map shows all the buildings there as roofless.

Contra other accounts, therefore, it would appear North Hooe Mine was established in the years 1838×42, operated in conjunction with South Hooe and Wheal Hancock, and closed in 1855. There is very little in the published accounts to suggest it was operating any later than 1855, and thus the *mining abandonment plans* (MAPs) probably date to the period 1855×67. Given how completely Wheal Hancock disappears, it is

possible that so much of North Hooe survives due to its relative inaccessibility.

The Engine Shaft at the pumping engine house was sunk to 110 fathoms, with levels every 10 fathoms below the 30-fathom level (indicated on the MAPs). As the engine house lay on the route of the leat feeding South Hooe Mine, water from the mine could have been raised to the leat to supplement its flow; alternatively – and perhaps more likely – most of the time water was raised to adit level and discharged straight into the Tamar.

The ore raised would presumably have been treated on the dressing floors and crushed in the crusher house, before being transported off site via the quay to the smelting works at Weir Quay (5km downstream). Coal would be imported via the same quay. The mine was re-investigated in 1901-02 (or 1906 – accounts differ); during the latter phase the mine was not fully dewatered and no extraction took place.

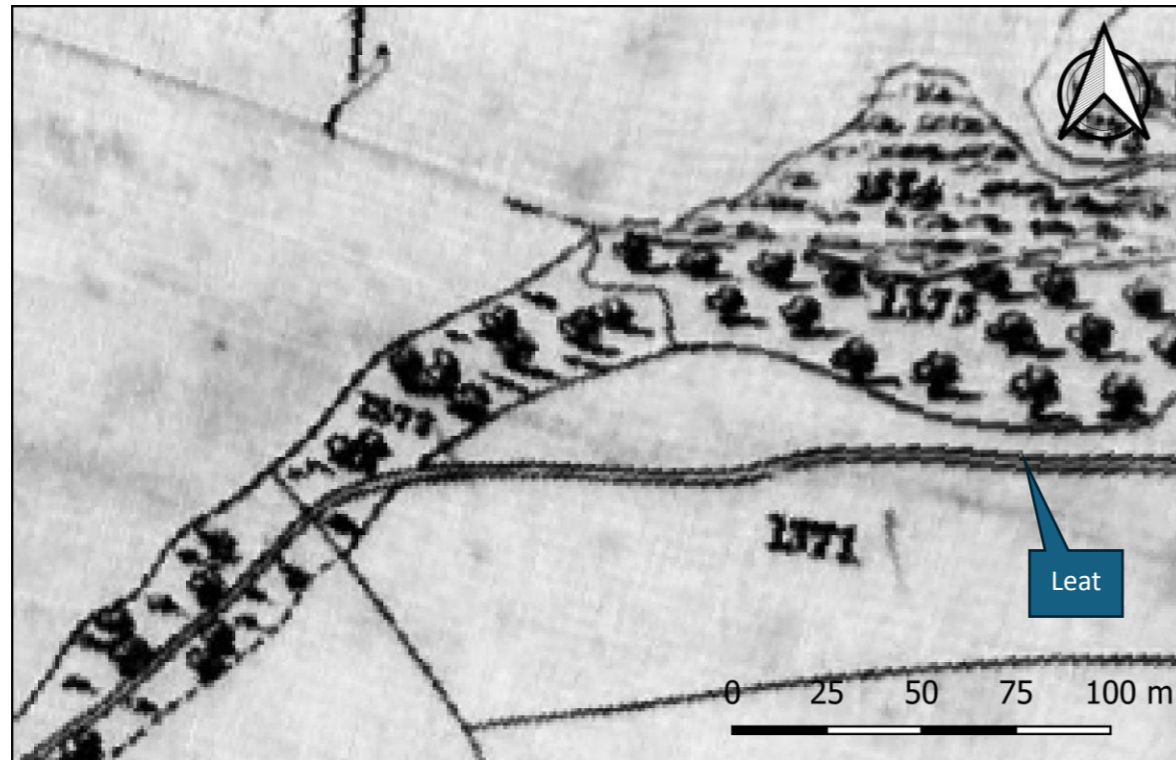


FIGURE 2: EXTRACT FROM THE 1844 BERE FERRERS TITHE MAP (TNA). THE LEAT FEEDING SOUTH HOOE IS SHOWN, BUT THERE IS NO SIGN OF THE MINE BUILDINGS AT NORTH HOOE.

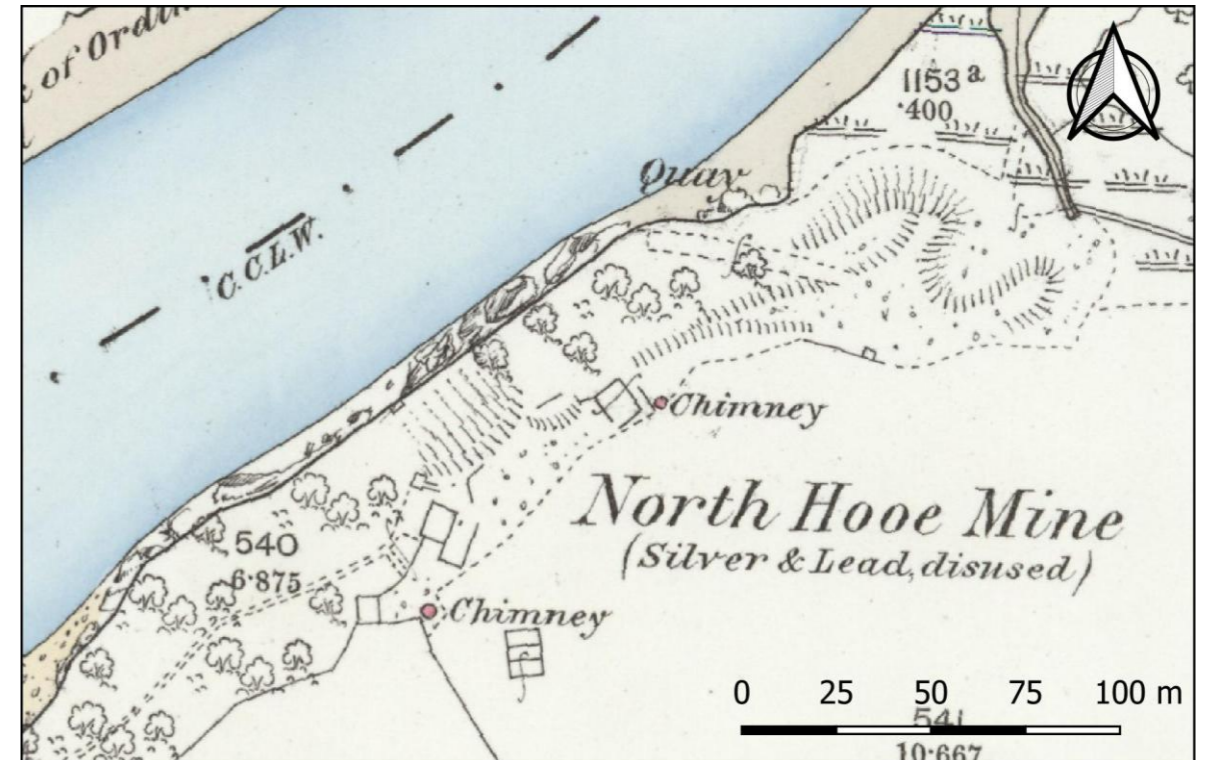


FIGURE 4: EXTRACT FROM THE 1893 ORDNANCE SURVEY 1:25" MAP (SURVEYED 1881-82) (CORNWALL SHEET XXXVIII.1). REPRODUCED WITH THE PERMISSION OF THE NATIONAL LIBRARY OF SCOTLAND. ALL THE STRUCTURES ARE SHOWN AS ROOFLESS.

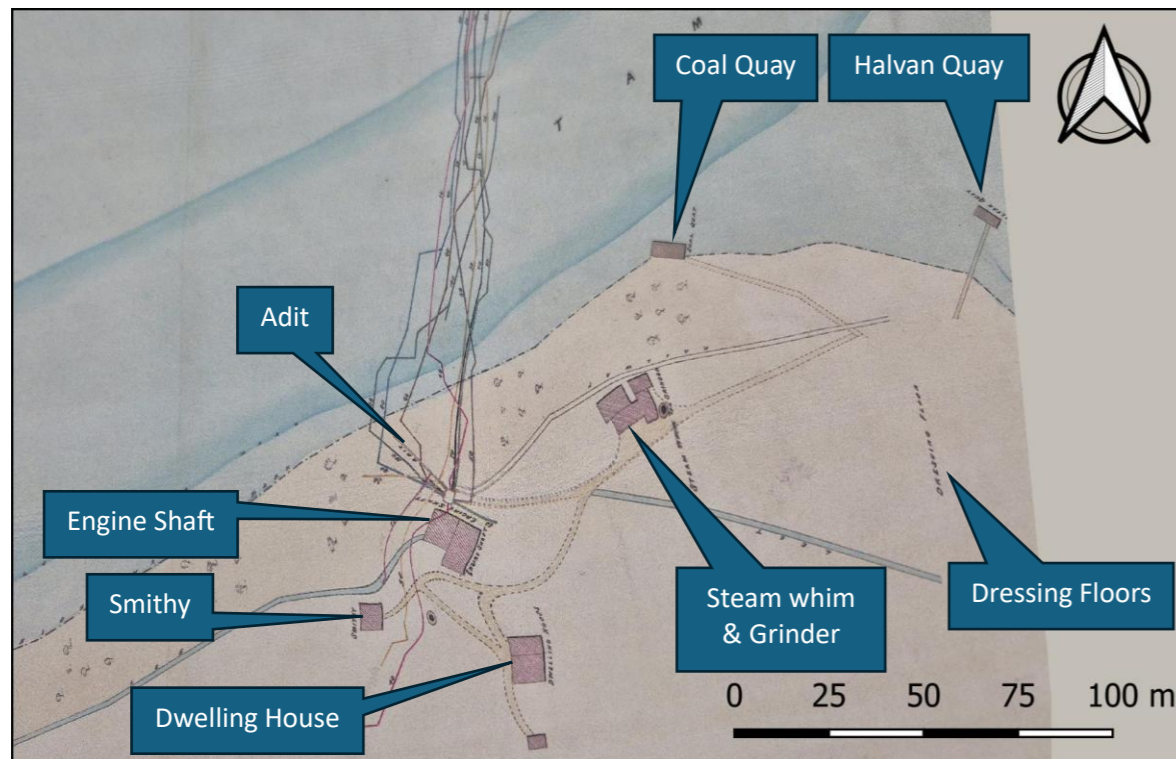


FIGURE 3: EXTRACT FROM THE MINE ABANDONMENT PLAN (c.1860s?) (DHC: R264). THE BUILDINGS ARE LABELLED.

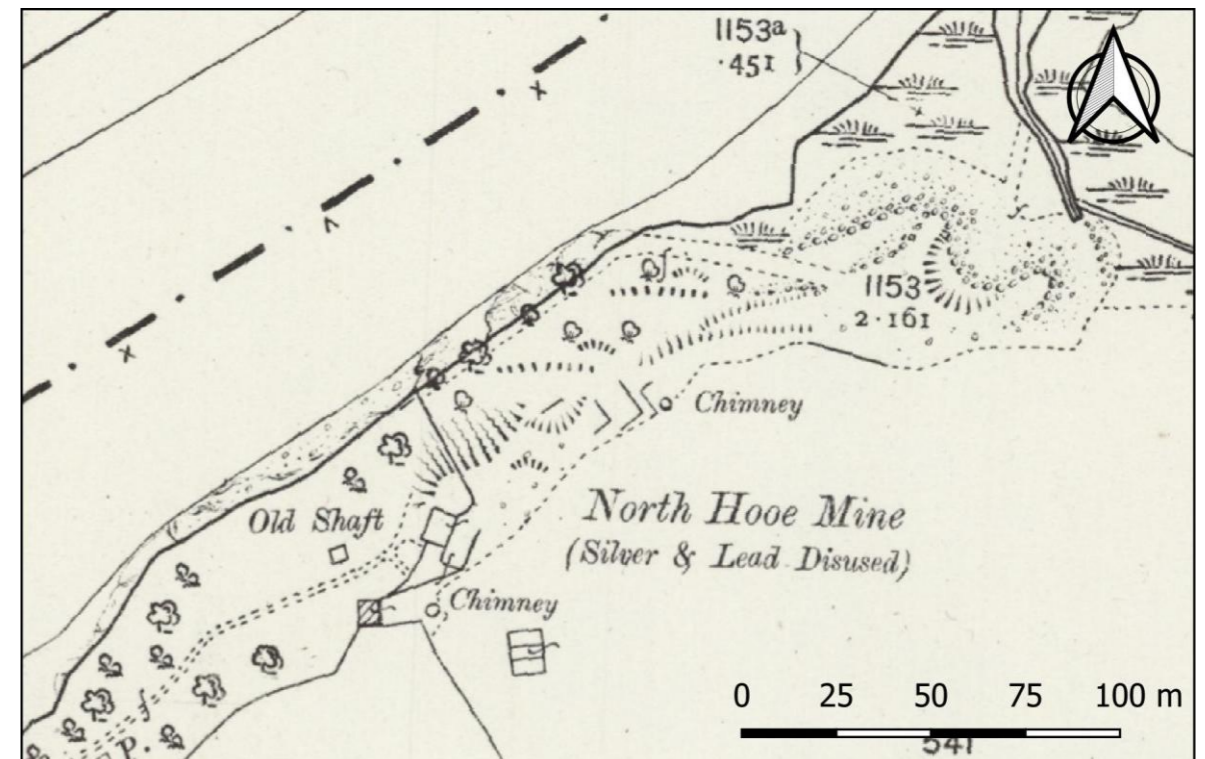


FIGURE 5: EXTRACT FROM THE 1906 ORDNANCE SURVEY 1:25" MAP (REVISED 1905) (DEVON SHEET CXI.10). REPRODUCED WITH THE PERMISSION OF THE NATIONAL LIBRARY OF SCOTLAND. NOTE THE 'OLD SHAFT' HAS APPEARED TO THE WEST OF THE ENGINE HOUSE, AND THE SMITHY IS SHOWN AS ROOFED.

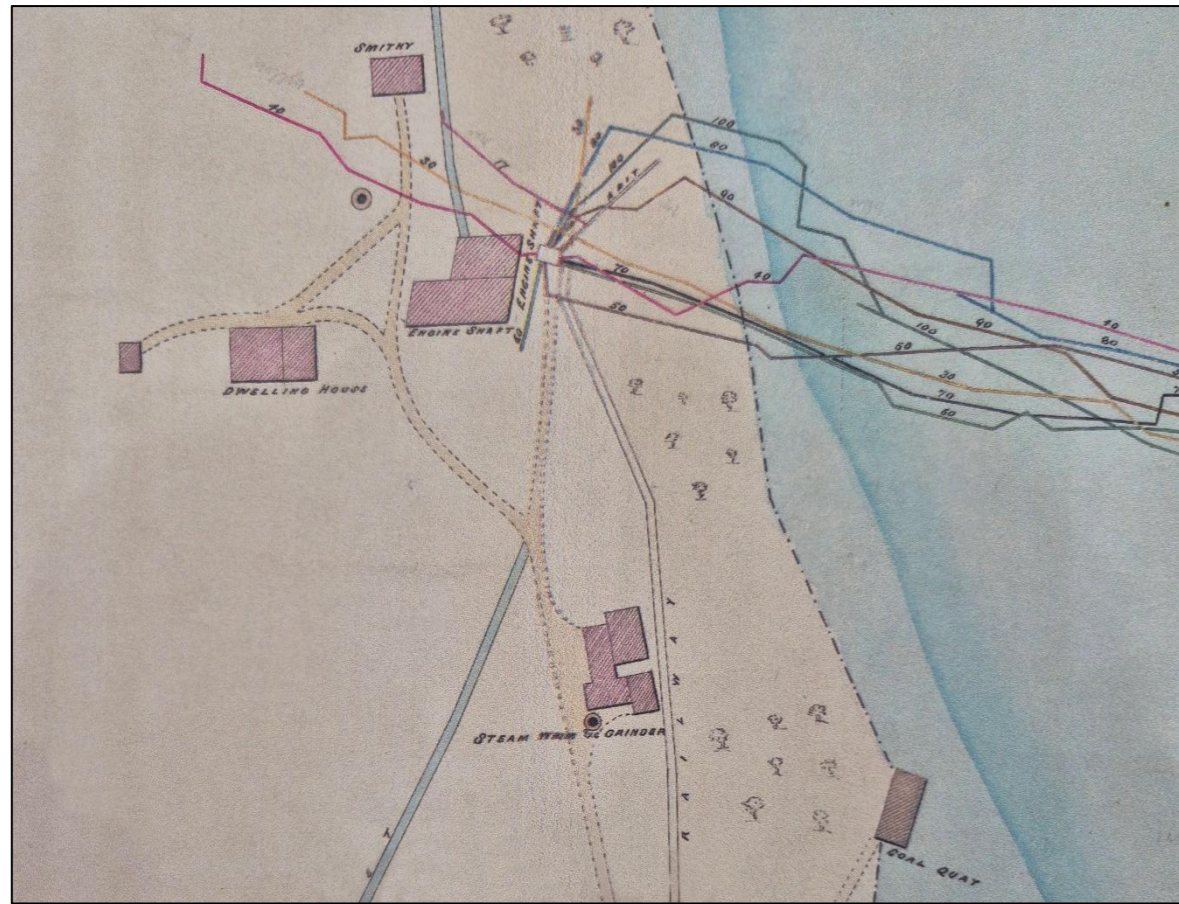


FIGURE 6: EXTRACT FROM THE C.1855x67 MINE ABANDONMENT MAP; NORTH TO THE LEFT (DHC: R264).

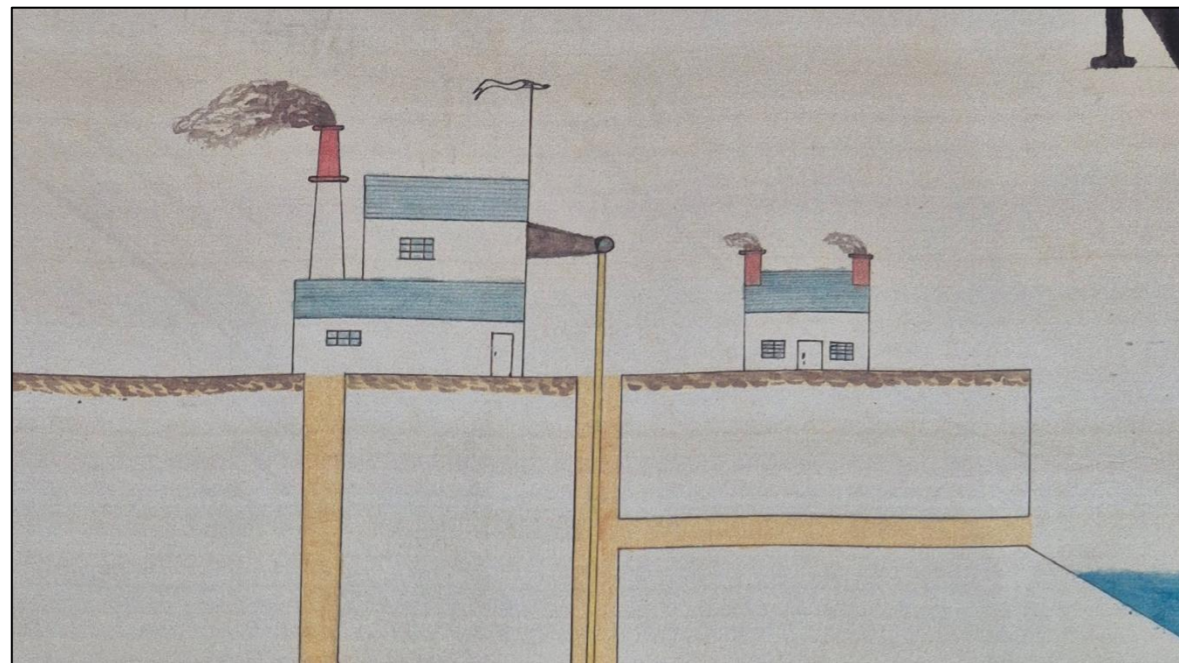


FIGURE 7: DETAIL OF THE MINE BUILDINGS FROM A COPY OF FIGURE 7/8. THE DEPICTION HERE IS CLEARLY STYLISTED, AS THE CHIMNEY IS SHOWN NEXT TO THE ENGINE HOUSE, RATHER THAN AT A DISTANCE (DHC: R264). NOTE THE ADIT IS SHOWN, AND TWO SHAFTS.

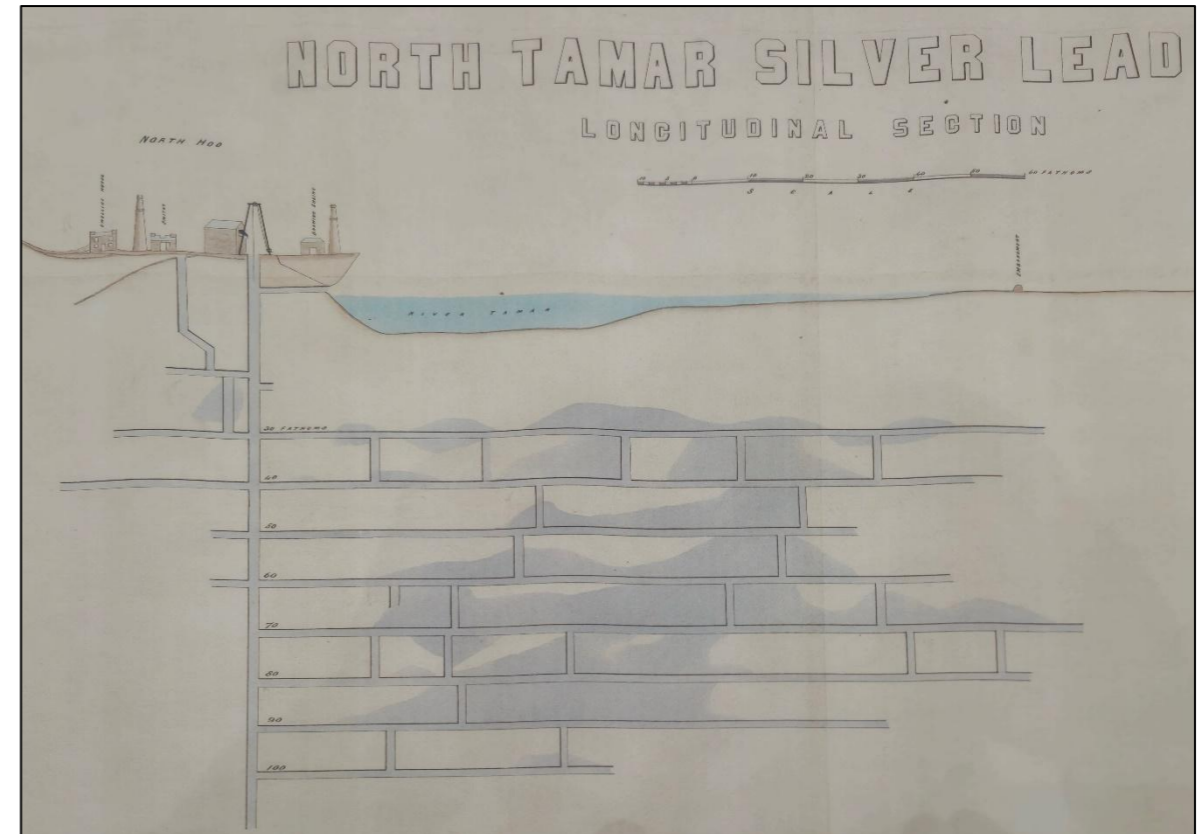


FIGURE 8: LONGITUDINAL SECTION FROM THE MINE ABANDONMENT PLANS (DHC: R264).



FIGURE 9: DETAIL OF THE BUILDINGS IN FIGURE 7. THE DEPICTION OF THESE BUILDINGS APPEARS MORE PLAUSIBLE, AS THEY APPEAR IN THE CORRECT ORDER IN RELATION TO ONE ANOTHER (DHC: R264). THE BROWN LINE RUNNING FROM THE BOTTOM LEFT CORNER MAY BE A REPRESENTATION OF THE LEAT.

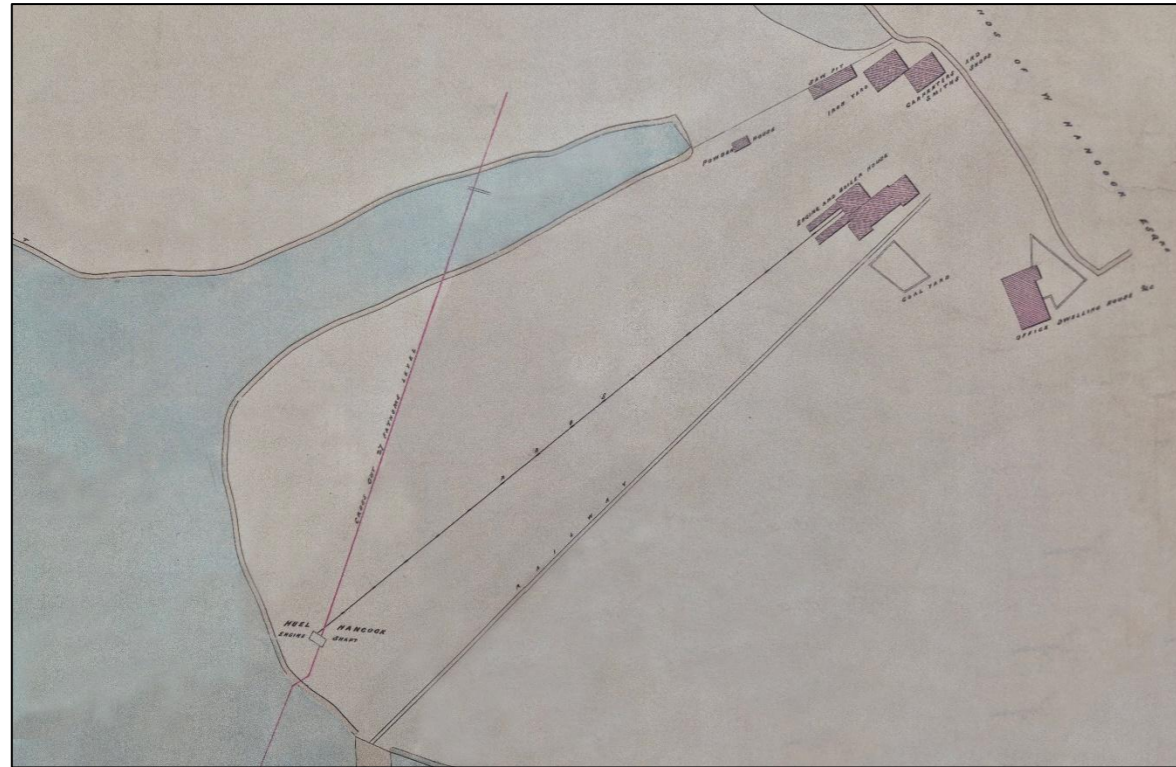


FIGURE 10: EXTRACT FROM THE c.1855×67 MINE ABANDONMENT MAP; NORTH TO THE LEFT (DHC: R264). THIS SECTION SHOWS WHEEL HANCOCK, LOCATED ON THE WEST SIDE OF THE TAMAR. SOME OF THESE STRUCTURES ARE SHOWN ON THE 1ST EDITION OS MAPS (AND LABELLED *HAYMARSH*), BUT THE MINE CAPTAIN'S HOUSE (BOTTOM RIGHT) IS LAST SHOWN ON MID-20TH CENTURY MAPS AS A RUIN, AND NONE NOW SURVIVE.

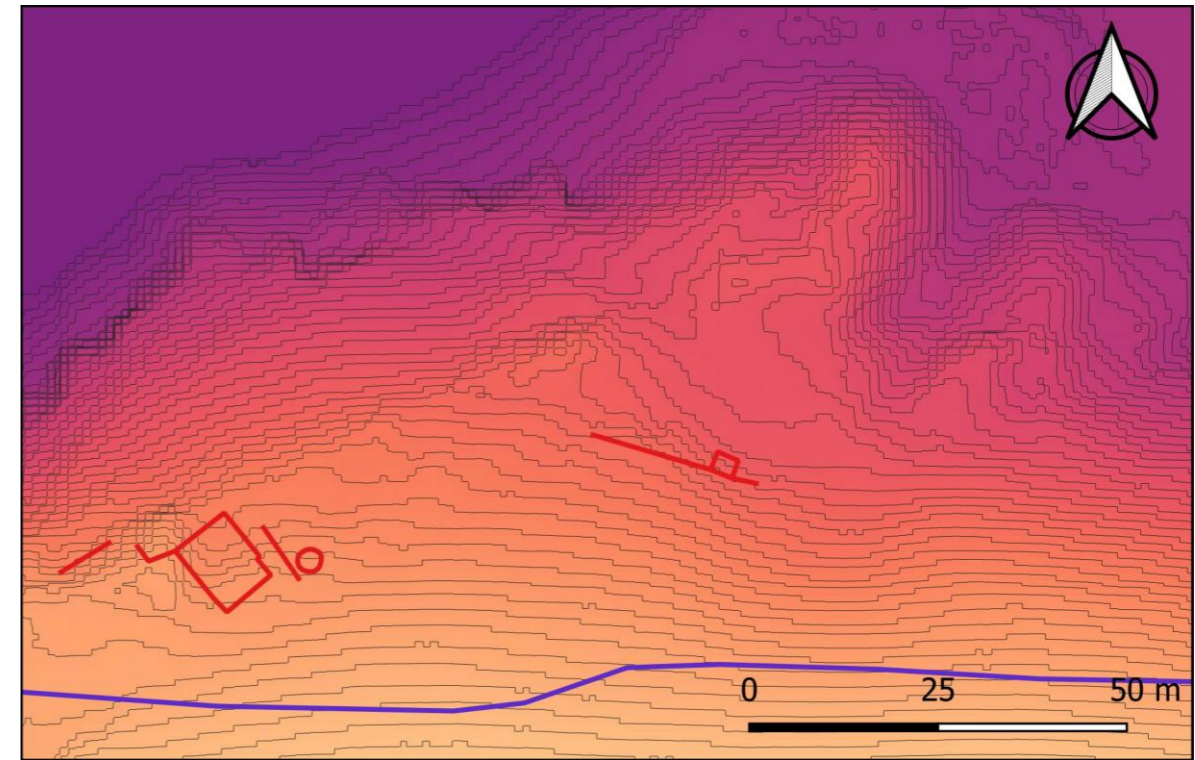


FIGURE 12: IMAGE BASED ON 1M DTM LIDAR DATA, SHOWING THE NORTH-EASTERN END OF THE SITE (DATA USED UNDER THE OGL V3.0). CONTOURS EVERY 0.5M. THE RED LINES SHOW THE STRUCTURES ON THE 1ST EDITION OS 1@25" MAP; THE LEAT SHOWN ON THE 1844 TITHE MAP IS SHOWN AS A BLUE LINE. MAPS GEORECTIFIED USING QGI 0.v3.16; THE LIDAR DATA PRESENTED AS A SIMPLE COLOUR RAMP (DARK-TO-LIGHT) OF -20 TO +30M AOD.

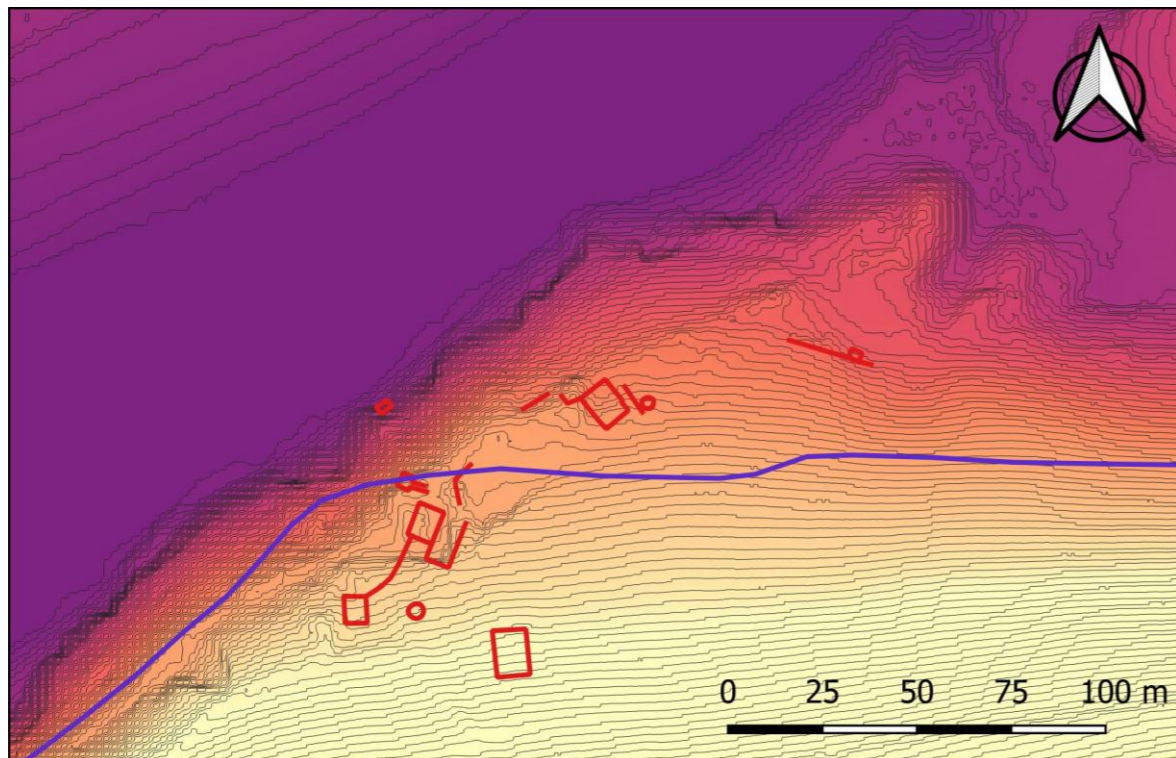


FIGURE 11: IMAGE BASED ON 1M DTM LIDAR DATA, SHOWING THE SITE (DATA USED UNDER THE OGL V3.0). CONTOURS EVERY 0.5M. THE RED LINES SHOW THE STRUCTURES ON THE 1ST EDITION OS 1:25" MAP; THE LEAT SHOWN ON THE 1844 TITHE MAP IS SHOWN AS A BLUE LINE. MAPS GEORECTIFIED USING QGISV3.16; THE LIDAR DATA PRESENTED AS A SIMPLE COLOUR RAMP (DARK-TO-LIGHT) OF -20 TO +30M AOD.

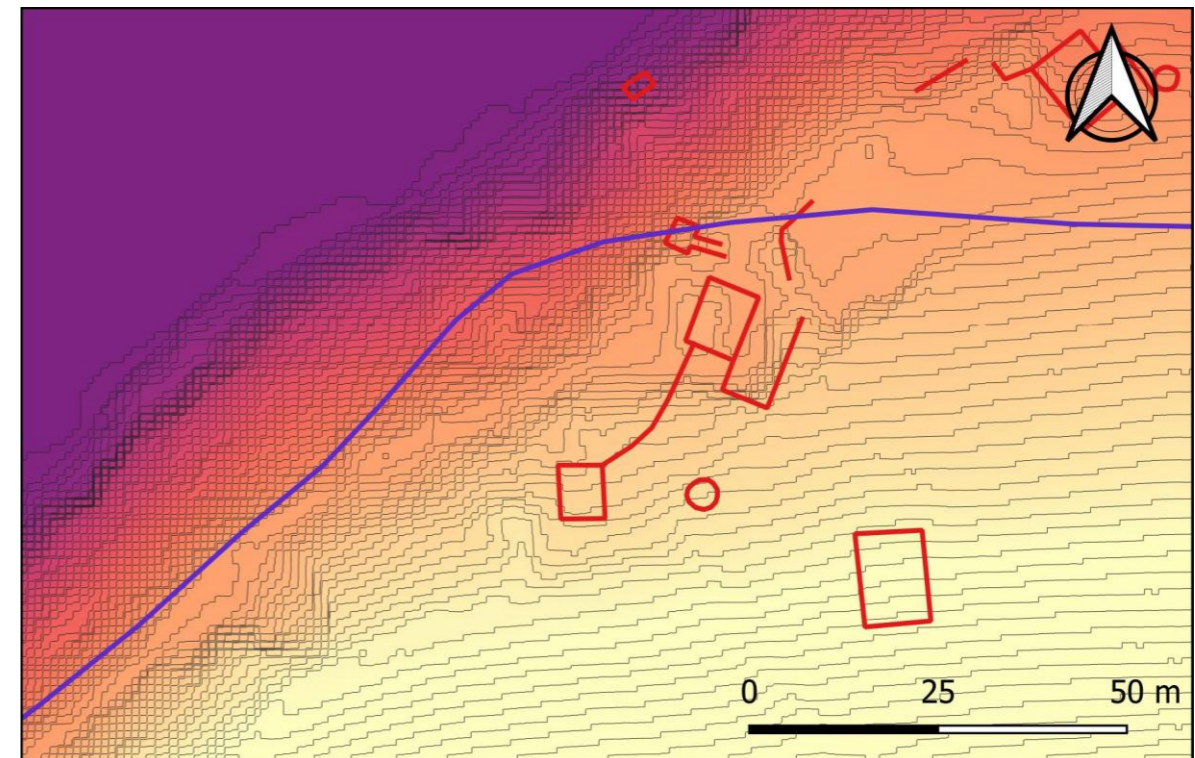


FIGURE 13: IMAGE BASED ON 1M DTM LIDAR DATA, SHOWING THE SOUTH-WESTERN END OF THE SITE (DATA USED UNDER THE OGL V3.0). CONTOURS EVERY 0.5M. THE RED LINES SHOW THE STRUCTURES ON THE 1ST EDITION OS 1:25" MAP; THE LEAT SHOWN ON THE 1844 TITHE MAP IS SHOWN AS A BLUE LINE. MAPS GEORECTIFIED USING QGISV3.16; THE LIDAR DATA PRESENTED AS A SIMPLE COLOUR RAMP (DARK-TO-LIGHT) OF -20 TO +30M AOD.

2 HISTORIC BUILDING RECORDING

2.1 ENGINE HOUSE

The engine house is located on a wide platform cut back into the slope, also occupied by the boiler house and the later pump room, addressed by the access track from the south-west and overlooking the Tamar. The engine house is covered in ivy, which is particularly thick to the upper external walls and conceals detail.

The engine house is *almost* rectangular, being 9.10-9.21m long by 6.25-6.35m wide externally. Internally, it is 6.66-6.72m long by 4.53-5.54m wide. Its long axis is orientated north-east to south-west and its bob wall faces north-east. The bob wall is 1.5m thick; the other three walls are 0.8m thick. There is a shallow external plinth, and a shallow internal plinth to the bob wall. The walls are constructed of greenish-blue-grey platey slatestone rubble, with larger stones to the corners but no clearly distinguished quoins. The stonework is roughly brought to courses, in a coarse, brittle, off-white lime mortar. The three other walls survive to the height of the bob wall (c.6.5m with some variation), though the MAP shows them continuing up and carrying a pitched slate roof. They have presumably been reduced or have collapsed, though in the latter case the interior is therefore strangely free of debris.

Unusually, but paralleled by the mine buildings at South Hooe Mine and Gawton Mine, almost all the openings have (or had) flat, timber lintels; only a narrow window opening in the north-west side elevation has a tight segmental arch of slatestone. Where they survive, the lintels are of heavy, squared timbers, probably pitch pine.



FIGURE 14: THE NORTH-EAST ELEVATION (BOB WALL); FROM THE NORTH-EAST (SCALE 2M).

The north-east facing elevation contains the central plug door; this was originally c.3.4m high, externally 1.3m wide, 1.85m wide to the splayed interior. The base has been broken down by c.0.15m. The heavy squared (0.3m) timber lintels, which extend 1.1m to the left and 0.95m to the right of the opening, have failed and the stonework above, to both external and internal face, has failed and dropped. To the right and slightly above the plug door is a shallow very shallow socket, cut back into the face of the stonework.



FIGURE 15: THE SOUTH-EAST ELEVATION; FROM THE SOUTH-EAST (SCALE 2M).

The south-east facing elevation adjoined the boiler house. The east end of the elevation is the boiler door, 1.12m wide and c.2.25m high, above which the stonework has been lost. The splay of one side of a window is just visible at first-floor level; the combination of a failed boiler house door with a window above explaining why this section of the wall has failed, and why the corresponding section in the north-west elevation has survived. There is a second ground-floor opening, originally c.0.5m square for the steam pipe, but this has been deliberately and crudely expanded, both above and below, perhaps in an abortive attempt to mimic the facing window opening in the north-west elevation; the opening is now 2.07m high and up to 1.28m wide. There are at least five sockets for joists in this elevation; the two larger sockets are at a level just below the window and carried the roof of the boiler house. To the west corner there are three iron pegs and a hinge driven into the stonework.



FIGURE 16: THE SOUTH-WEST ELEVATION (CYLINDER DOOR); FROM THE SOUTH-WEST (SCALE 2M).

The south-west facing elevation contained the cylinder door. It is likely this also had a flat timber lintel, as all the stonework above the opening has collapsed. The cylinder door was 1.95m wide with a base c.1m above the external ground level. The level of its threshold has since been reduced by c.0.25m. To the south side of the opening the slot for a short length of timber lacing is visible. At first-floor height, the west splay of a central window is visible in the surviving stonework.

Given that the distance between the wall of the engine house and the back of the rock-cut terrace is only c.2.4m, it seems likely the engine/bob arm was directed into the engine house down the slope behind and carried across to the cylinder door on a ramp.



FIGURE 17: THE NORTH-WEST ELEVATION; FROM THE NORTH-WEST (SCALE 2M). THE BUILDING IN THE FOREGROUND IS THE C20 PUMP HOUSE.

The north-west facing elevation contains two openings at plinth level: a doorway 1.1m wide and 2.26m high, with surviving timber lintel, opposite the boiler house door; and a tall, narrow window 0.64m wide and 1.99m high, with a tight segmental slatestone arch. The lintel is failing and the stonework above the lintel is dropping. The window lacks an internal splay, so it is possible the doorway is actually a window, as the opening opposite the boiler house door tends to be (note the first-floor windows and the plug door *are* splayed to the interior). Otherwise, there are four joist sockets in a line just above the plinth, two with traces of decayed wood, and a fifth joist socket is visible at first-floor level at the southern end of the elevation.

Unsurprisingly, the internal elevations reflect the layout of the exterior, with the following additional notes. In the north-east wall an extensive section of stonework around the plug door has dropped and partly fallen out. The south-east wall steps back slightly at first-floor level, with two large joist sockets to the south side. Some short lengths of timber lacing are visible. At the base of the wall, along most of its length and up to c.0.3m high, the stonework is rather ragged. The south-west elevation has two large joist sockets at first-floor height, abutting the joining walls. The north-west elevation again steps in slightly at first-floor level, with two large joist sockets to the southern end matching those in the opposing wall. There is again timber lacing, and the stonework over the doorway has lost its internal face. At the base of the wall, along most of its length and up to c.0.3m high, the stonework is rather ragged.



FIGURE 18: THE INTERNAL ELEVATION OF THE NORTH-EAST WALL; FROM THE SOUTH-WEST (SCALE 2M).



FIGURE 19: THE INTERNAL ELEVATION OF THE SOUTH-EAST WALL; FROM THE NORTH-WEST (SCALE 2M).



FIGURE 21: THE INTERNAL ELEVATION OF THE NORTH-WEST WALL; FROM THE SOUTH-EAST (SCALE 2M).

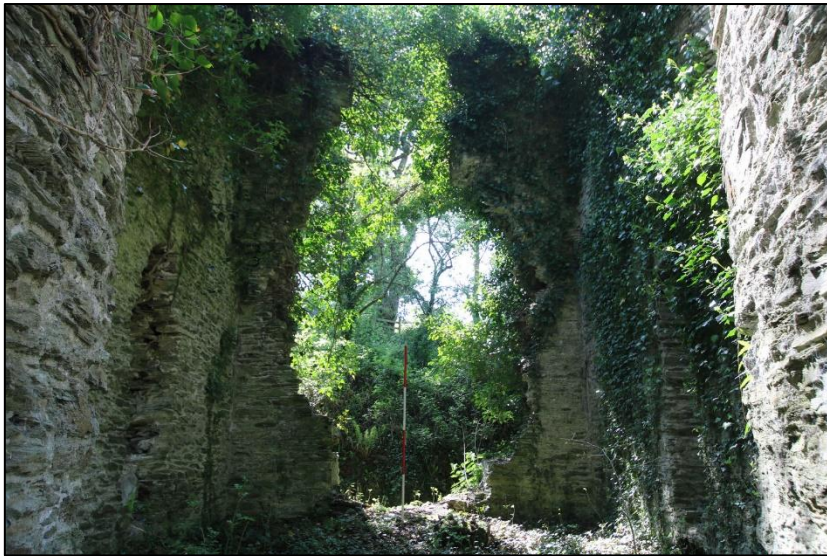


FIGURE 20: THE INTERNAL ELEVATION OF THE SOUTH-WEST WALL; FROM THE NORTH-EAST (SCALE 2M).

The cataract pit has been infilled and – if the ragged stonework to the base of the north-west and south-east walls is any guide, the driver's floor has been reduced in height by c.0.3m. The lack of rubble to the interior, despite the reduction in the height of the engine house and the evident loss of stonework, particularly to the south-east elevation, would suggest the interior has been cleared, perhaps more than once. (The rubble may have found its way into the balance-bob pit?)

2.1.1 CONSERVATION MANAGEMENT

The engine house is in poor and declining condition. The principal issue is the failure of the timber lintels over the openings, which has led to the partial or complete collapse of the masonry above. It appears likely this has been aggravated by deliberate intervention (i.e. the loss of the walls above bob height, and steam pipe opening). The solution is fairly straightforward: the careful removal of vegetation, the replacement of lintels, and the consolidation and partial reinstatement of the stonework.



FIGURE 22: NORTH-WEST AND NORTH-ELEVATIONS; FROM THE NNE (SCALE 2M).



FIGURE 24: SOUTH-WEST AND NORTH-WEST ELEVATIONS; FROM THE WEST (SCALE 2M).



FIGURE 23: NORTH-EAST AND SOUTH-EAST ELEVATIONS; FROM THE EAST (SCALE 2M).

2.2 THE BOILER HOUSE

Located immediately to the south of the engine house is a deep terrace cut into the slope; this defines the footprint of the lost boiler house. The terrace is c.12.7m long and c.6m wide, cut back c.2.5m into the slope. The floor of the terrace is essentially level but rises slightly to the back of the cut. Along the south-east side of the terrace, towards the back of the cut and perched on top of the exposed rock face, is a stretch of mortared stonework c.6.8m long; this appears to be the only surviving fragment of walling.

At the back of the cut (i.e. the south-west end) the end of a covered flue is visible; while largely concealed by vegetation, the flue appears to be c.0.4m square with flat stone lintels. Due to the thick vegetation, the site of the chimney associated with the flue was not located.

Note the MAP shows this structure in plan but not in profile.

2.2.1 CONSERVATION MANAGEMENT

Not much remains of the boiler house; it is in poor but stable condition. Its surviving fragment of wall and the entrance to the flue should be cleared of ivy and consolidated.



FIGURE 25: THE ROCK-CUT TERRACE FOR THE BOILER HOUSE; FROM THE NORTH-EAST (SCALE 2M).



FIGURE 26: THE SURVIVING STRETCH OF BOILER HOUSE WALL; FROM THE NORTH-WEST (SCALE 2M).

2.3 BALANCE BOB

Immediately below the shaft and the engine house to the north are the two walls of the balance bob pit. These two walls, each 0.75m wide, run parallel to one another and 2.42m apart. The build of these walls is similar to that of the engine house, being of greenish-blue-grey platey slatestone rubble, with no clearly distinguished quoins, roughly brought to courses, in a coarse, brittle, off-white lime mortar. The space between the two walls appears to have been mostly infilled, perhaps with rubble from the interior of the engine house.

The western wall is almost entirely buried, with only the downslope end, which stands to c.1.56m tall, projecting from the slope. 0.6m below the top of this wall is a horizontal slot running the width of the wall for a timber 0.2×0.2m across. Across the gap between the two walls a low section of mortared stonework has been inserted; this only survives up to a height of c.0.8m.

The eastern wall, due to the fall of the ground and the angle of the slope, is 3.1m tall, and its north-eastern face is visible running c.4.7m back into the hillside. 2.1m below the top of this wall is another horizontal slot running the width of the wall and 0.2×0.2m across. Built up against the north-east face of the eastern balance bob wall, and 2.3m back from the end of the wall, is a battered section of walling that abuts it at a c.45° angle. The lower face of this wall has been lost; the loss being either from erosion of the slope below it, or perhaps due to a missing timber element.

2.3.1 CONSERVATION MANAGEMENT

The walls of the balance bob pit are in fair and stable condition, though the loss of the timber lacing should be corrected. The battered fragment of wall abutting it to the east is in poor but stable condition. Again, these walls should be cleared of vegetation and consolidated.



FIGURE 27: THE END OF THE BALANCE BOB PIT WALLS; FROM THE NORTH-WEST (SCALE 2M).



LEFT FIGURE 28: THE WESTERN BALANCE BOB PIT WALL; FROM THE WEST (SCALE 2M).

RIGHT FIGURE 29: THE EASTERN BALANCE BOB PIT WALL; FROM THE NORTH (SCALE 2M).



FIGURE 30: THE SIDE OF THE EASTERN BALANCE BOB PIT WALL; FROM THE NORTH (SCALE 2M).

2.4 THE SMITHY

The smithy is located west and upslope from the engine house. It is built into the slope but lacks an obvious terrace; unlike the engine house, material from the collapsed walls is present here and lies in heaps against the surviving walls. To the upslope side there is a terrace or recess cut into the slope; this steps back from the south-west corner, and it bounded on its downslope side by a stone-faced hedgebank that appears to abut the south-east corner.

The smithy is a single- or 1½-storey rectangular structure measuring 8.07×5.81m externally and 6.93×4.82m internally. Its long axis is orientated roughly north-to-south. The walls are all c.0.55m wide, of greenish-blue-grey platey slatestone rubble, with larger stones to the corners but no clearly distinguished quoins. The stonework is roughly brought to courses, in a coarse, brittle, off-white lime mortar. The MAP indicates there were chimneys to the north and south gable, but no trace of these survive.

The east ('front') elevation has largely collapsed; the MAP would suggest this had a central door with window to the north, and, once the timber elements failed, the unsupported masonry would have been vulnerable to loss. The west ('back') has also partly collapsed; to the north side of the collapse enough survives to indicate the presence of a door here, with timber lacing.

The south gable survives to first-floor height, and to the west side the internal face has been collapsed. The north gable survives to full height, though swathed in ivy. There is a central window with missing timber lintel, what survives presenting as a ragged hole c.1m across and c.1.8m high. The loss of both reveals and the sill would imply this was deliberately – and crudely – widened. To the east of the window are two sockets (putlogs?), the upper one blocked. Below, and below the internal floor level, there is another irregular forced opening c.1.0m wide and c.0.45m high, of unknown purpose (for bringing something – pipework? – into the building?).

The floor is obscured. In the debris there are roof slates and a sheet of corrugated steel sheeting (note that the structure is shown as roofless on the 1st edition OS map, but roofed on the 2nd edition map, 20 years later).



FIGURE 31: THE SMITHY; FROM THE EAST (SCALE 2M).

2.4.1 CONSERVATION MANAGEMENT

The walls of the smithy are in poor and declining condition, again partly, it would seem, through deliberate intervention (in the 1900s?). The walls should be cleared of vegetation and consolidated.



FIGURE 32: THE NORTH-EAST CORNER; FROM THE ENE (SCALE 2M).



FIGURE 33: THE NORTH GABLE; FROM THE NORTH (SCALE 2M).



FIGURE 34: THE NORTH-WEST CORNER; FROM THE NORTH-WEST (SCALE 2M).



FIGURE 36: THE SOUTH GABLE WALL; FROM THE SOUTH (SCALE 2M).



FIGURE 35: THE SMITHY; FROM THE WEST (NO SCALES).



FIGURE 37: THE SOUTH-EAST CORNER; FROM THE SSE (SCALE 2M).



FIGURE 38: THE INTERIOR OF THE SMITHY; FROM THE SOUTH (SCALE 2M).



FIGURE 40: THE INTERIOR; FROM THE NORTH (SCALE 2M).



FIGURE 39: THE INTERIOR; FROM THE EAST (SCALE 2M).

2.5 THE SHAFT

Located NNW of the engine house and main pumping shaft is a second (uncapped) shaft. As observed from a safe distance, the shaft appears to be rock-cut almost to the surface, a sub-rectangular vertical hole c.3m across. It is surrounded by a clear and obvious berm 1-2m wide and up to 1m high on the downslope (north-east) side.

This shaft is first shown on the 2nd edition OS map, so either it was opened during the aborted early 1900s phase of works or – more likely – a capping had failed and the shaft was newly exposed.

2.5.1 CONSERVATION MANAGEMENT

The shaft appears to be stable and in no risk of deteriorating. However, the fencing around the shaft is in very poor condition and should be reviewed.



FIGURE 41: THE UNCAPPED SHAFT; FROM THE ENE (NO SCALE).

2.6 THE PUMP HOUSE

Located immediately adjacent to the north-west elevation of the engine house is a small single-storey C20th building containing C20th pumping equipment. On a concrete raft foundation, the structure is 3.27x2.65m across and built of a single skin of concrete blockwork bound with a hard cement mortar; the blocks used are 18"x9"x4" (440x215x100mm). The structure has a mono-pitch roof, sloping very slightly to the north-west, of corrugated asbestos-cement sheets supported on simple sawn pine joists. The joists have failed and the roof has partly caved in.

The north-east and south-west walls are blind. The north-west wall has a central window opening 0.66m high and 0.55m wide, with a timber frame set with a steel-framed two-light awning casement. The south-east wall, facing the engine house, has an off-centre doorway 0.93m wide. The simple timber doorframe is nailed to the blockwork. The *ex-situ* door, located inside the building, is ledged and braced pine plank door painted a light green; there were no obvious latches or lock fittings.

Inside the building are four main sets of machinery. To the east corner, set on a plinth of concrete blocks, is a Lister FR2 engine ('freedom range' manufactured 1954-1964). To the north corner is a Worthington Simpson 1½" pump (according to Flowserve, the successor company, this pump was sold in 1968). To the west corner is a Broom and Wade N5 compressor (dating to before 1968, as thereafter the company was called *Broomwade*). If all the equipment was contemporary, it would indicate the pump house was built c.1968; if the equipment was periodically updated, it would indicate a date range of 1954x1968.

This pump house will be contemporary with some of the equipment located immediately in front of the engine house, over the shaft. The shaft is capped by with concrete, on top of which is a galvanised steel tank 1.82x0.9m across and 0.8m high (approx. capacity 280 gallons). The tank is served by 2" steel piping. This presumably implies the water-filled shaft was used as a spring, but perhaps for agricultural use only – as even in the 1950s sourcing drinking water from a silver and lead mine should have raised questions.



FIGURE 42: NORTH-EAST ELEVATION; FROM THE NORTH-EAST (SCALE 2M).



FIGURE 44: SOUTH-WEST AND SOUTH-EAST ELEVATIONS; FROM THE SOUTH (SCALE 2M).



FIGURE 43: NORTH-WEST ELEVATION; FROM THE NORTH-WEST (SCALE 2M).



FIGURE 45: INTERIOR, SOUTH SIDE; FROM THE SOUTH-WEST (NO SCALE).



FIGURE 46: INTERIOR, NORTH SIDE; FROM THE SOUTH (NO SCALE).



FIGURE 47: THE LISTER ENGINE; FROM THE SOUTH (NO SCALE).



FIGURE 48: THE LISTER ENGINE AND WORTHINGTON-SIMPSON PUMP; FROM THE SSE.

2.6.1 CONSERVATION MANAGEMENT

The walls of the structure are sound, but the roof has collapsed into the building, affecting the machinery inside. As and when the roof is made sound, the building could be considered in fair and stable condition.

2.7 CONCRETE PLINTHS

To the south of the galvanised steel water tank, and in line with the concrete shaft capping and balance bob walls, at three concrete plinths. Two of the plinths are next to each other, adjacent to the concrete cap to the shaft; the third plinth is 1.8m to the south-east.

Each plinth is of mass concrete construction and forma raised 'step' c.0.2m high. The paired plinths are 1.33×1.2m across; the upper surface to the south-east side is slightly recessed, 0.93×0.94m across. Along the north-west side of this recess is a slot 0.1m wide containing the rotted remains of a timber. Set into the recess are the stubs of iron fixings. There is a concrete skirt 0.34m wide along the north-eastern side of the two plinths, terminating at its south-east end with a probable post socket 0.30×0.27m across.

The plinth 1.8m to the south-east is 1.26×1.5m+ across and was largely concealed by the vegetation.

Further to the east, adjacent to the gateway into the next field, is a large *ex situ* block of concrete and a mass of stone bonded with cement.

The three plinths are likely to belong to the aborted c.1900 repurposing of the engine house and shaft, indicating the location of the fixed machinery at the shaft head (pumps etc.).

2.7.1 CONSERVATION MANAGEMENT

The concrete plinths are likely to date to the early 1900s, and the quality of the concrete is unlikely to be particularly high. Nonetheless, they appear to be in fair and stable condition. No interventions are required.



FIGURE 49: THE PAIRED CONCRETE PLINTHS; FROM THE EAST (SCALE 2M).



FIGURE 50: THE *EX-SITU* CONCRETE/MASONRY AT THE GATEWAY; FROM THE SOUTH-WEST (SCALE 2M).

2.8 CAPTAIN'S HOUSE

The historic mapping indicates there was a structure, labelled the [mine] captain's house, located upslope of the engine house. No trace of this structure survives, although there is a very slightly raised area with stunted vegetation in the general location of the structure (note the 1949 RAF aerial photographs show a clump of vegetation at this location). The Client reports that stonework was visible here, but that it has since been cleared away.

The MAP shows the 'dwelling house' as having a central door, flanking windows at ground- and first-floor level, and gable-end chimneys.

2.8.1 CONSERVATION MANAGEMENT

The captain's house survives solely as a below-ground archaeological feature; it can be considered to be in a stable condition. No interventions are required.



FIGURE 51: THE SITE OF THE CAPTAIN'S HOUSE; FROM THE ESE (SCALE 2M).

2.9 CRUSHER HOUSE

The crusher house was located along the slope to the east of the engine house. The MAP indicates the crusher house ('steam whim and grinder') was a relatively complex building comprising a T-shaped structure with two separate blocks added to the north-west side, with a chimney.

The terrace on which this building was constructed is visible, as is one face of a short (c.5.5m) length of stone wall, with a return (c.2m long) to the south. The relative intelligibility of the terrace would imply the building was dismantled or was perhaps built of less durable materials (i.e. timber and corrugated steel sheets, for instance).

The terrace is built up on the downslope side and retained by a stone-faced bank. This stone facing has partially collapsed, but it remains clear that two phases are present. The initial phase consists of a battered wall of pitched coursed stonework c.1.3m high. This was subsequently raised with a very crude stone wall or poorly-sorted stone blocks.

The base of the chimney could not be located.

2.9.1 CONSERVATION REQUIREMENTS

The crusher house has been almost totally lost; only its terrace and a short section of wall survive, the rest as a buried archaeological feature. The surviving visible fragments of wall are in poor condition, but can be considered to be stable, assuming the shrubs/roots are not removed. No interventions are required.

The retaining wall to the north-west has suffered a partial collapse fairly recently and can be considered to be in a poor and declining condition. That structure would benefit from partial rebuilding.



FIGURE 52: THE CRUSHER HOUSE TERRACE; FROM THE WEST (NO SCALE).



FIGURE 53: THE EAST SIDE OF THE TERRACE, INDICATING THE WALLS; FROM THE NORTH-WEST (SCALE 2M).

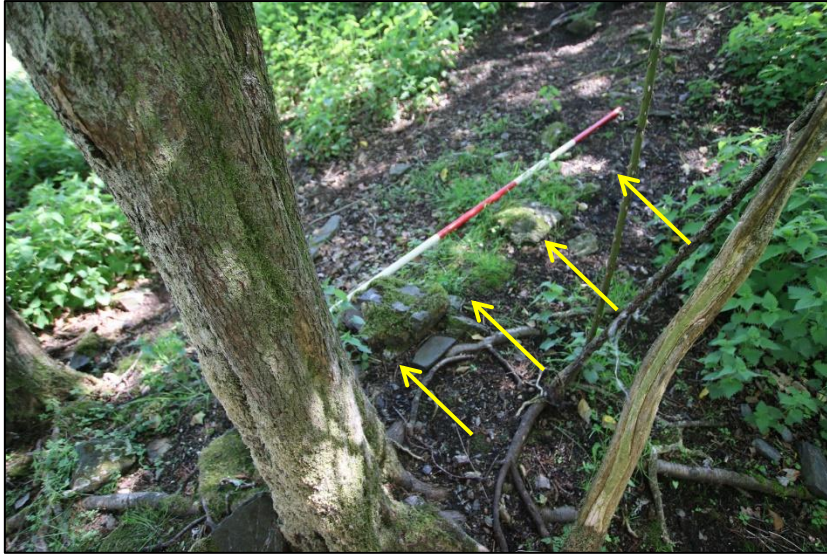


FIGURE 54: THE FRAGMENT OF WALL FACING (INDICATED); FROM THE NORTH-WEST (SCALE 2M).



FIGURE 56: AS ABOVE, THE RAISE INDICATED; FROM THE NNE (SCALE 2M).



FIGURE 55: THE RETAINING WALL BELOW THE CRUSHER HOUSE; FROM THE NORTH-EAST (SCALE 2M).



FIGURE 57: AS ABOVE, SHOWING DETAIL OF THE RAISE; FROM THE NORTH (SCALE 1M).

2.10 UNIDENTIFIED STRUCTURE EAST OF THE CRUSHER HOUSE

The 1st edition OS map (surveyed 1882) shows a straight boundary (a wall?) and a small rectangular structure to the east of the crusher house. In this approximate location a short length of battered stone wall was identified within the scrub/undergrowth, but it is unclear whether it belonged to the structure, or to the boundary.



LEFT FIGURE 58: THE WALL EAST OF THE CRUSHER HOUSE; FROM THE WNW (SCALE 2M).

RIGHT FIGURE 59: THE POSSIBLE TRAMWAY; FROM THE NORTH-EAST (SCALE 2M).

2.10.1 CONSERVATION MANAGEMENT

This unidentified structure survives as a low retaining wall within an area of scrub. It appears to be in poor but stable condition. No interventions are required.

2.11 TRAMWAY

The MAP shows the line of a tramway running from the engine house, north of the crusher house, to the vicinity of the spoil tips. The LiDAR data would suggest the line of this tramway between the engine house and the crusher house has been buried by later spoil/rubble, as a short length appears to survive just to the north-west of the crusher house. It comprises a narrow terrace with a line of stones along its outer edge, as if it is revetted. It is possible from this point it ran across the top of a linear bank, and the visibility diagnostic elements of the trackbed have been lost.

2.11.1 CONSERVATION MANAGEMENT

The tramway survives as a linear earthwork with some elements of stonework visible. It can be considered to be in a poor but stable condition. No interventions are required.



FIGURE 60: THE LINE OF THE POSSIBLE TRAMWAY; FROM THE SOUTH-EAST (SCALE 2M).

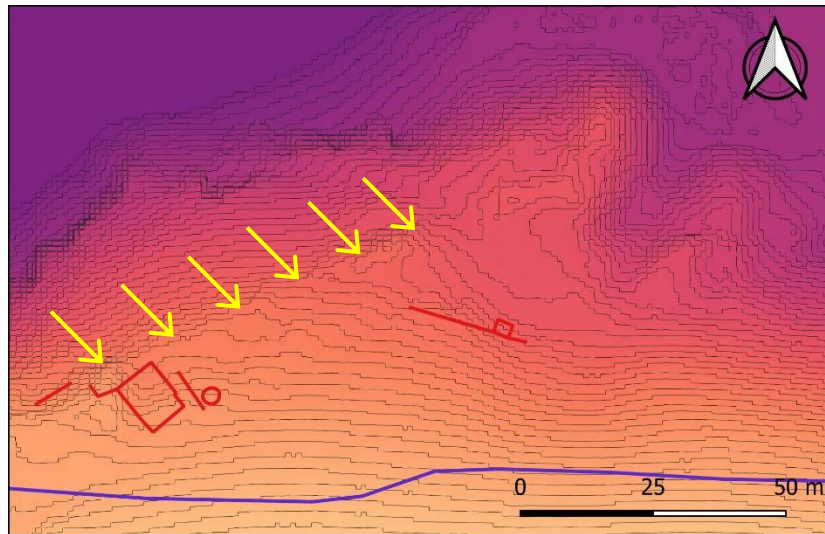


FIGURE 61: IMAGE BASED ON 1M DTM LIDAR DATA, SHOWING THE POSSIBLE LINE OF THE FORMER TRAMWAY (DATA USED UNDER THE OGL V3.0). CONTOURS EVERY 0.5M.

2.12 RAMP AND COAL QUAY

At the eastern end of the site there is a wide, well-defined inclined ramp orientated roughly east-west and dropping down to the level of the river, labelled on the MAP as the coal quay. The ramp is between 4-5m wide and cut into the south (upslope) side by up to 0.6m. Towards the top the south side is retained behind a low wall of loosely coursed pitched stone.

The ramp drops down to the level of the river. Near the base and, on the south-west (upslope) side, the rock has been quarried (or possibly been subject to mineral prospection) where a sub-rectangular hollow c.4.3xc.3.75m across and up to 0.4m deep has been cut down into the rock. This hollow presumably anchored the landward side of a wooden quay structure.

2.12.1 CONSERVATION MANAGEMENT

The ramp survives as a linear earthwork with some elements of stonework visible. It can be considered to be in a fair and stable condition. No interventions are required.



FIGURE 62: THE RAMP; FROM THE EAST, LOOKING DOWN TO THE RIVER (SCALE 2M).



FIGURE 63: AS ABOVE; FROM THE WEST, LOOKING BACK UP THE RAMP.



FIGURE 64: THE SUB-RECTANGULAR HOLLOW CUT INTO THE ROCK AT THE BASE OF THE RAMP; FROM THE EAST (SCALE 2M).

2.13 DRESSING FLOORS

There are two points on the site which present as broad and fairly level – and thus must be deliberately fashioned, cut back into the slope or built out with spoil or both. These are most readily identified by the DTM LiDAR data (see Figure 65).

The largest and most regular area is located just to the north-east of the boiler house. Here there is a broad, level, open space 15×25m across, quite clearly cut back into the slope to the rear.

The second main candidate is at the eastern end of the site, above the spoil heaps. This area is more irregular (more opportunistic? or perhaps more damaged?), sub-oval and 18×20m across. The 1949 RAF aerial photographs show this, and the adjacent spoil heap, as devoid of vegetation, so perhaps its current appearance is owed to a later phase of reworking?

2.13.1 CONSERVATION REQUIREMENTS

The dressing floors survive as a broad, level spaces cut back into the hillside. They can be considered to be in a good and stable condition. No interventions are required.

2.14 SPOIL HEAPS

At the eastern end of the site there are two clear spoil heaps. They extend out into the base of the shallow coombe to the north-east. The western spoil heap is larger, c.40×45m across, and better preserved. The eastern spoil heap is c.35×40m across and divisible into four clear benches. This vegetation of this spoil heap is disturbed and its rocky surface has been exposed; the Client reports mineral hunters frequent the site.

Mindat.org reports (2007):

The dumps contain large quantities of fluorite (estimated by Dines to be 1%) (clear, white, honey, green and blue colours) and some quartz, with quite good pieces of galena (which do not test positive for silver above 0.05% - 3 samples tested) and some sphalerite.

2.14.1 CONSERVATION MANAGEMENT

The spoil heaps present as substantial, vegetation-covered earthworks, with the exception of parts of the eastern spoil heap. Here the grass and scrub has been lost and the rocky spoil picked over by mineral-hunters, creating new artificial platforms and terraces. Otherwise, the two main heaps can be considered in fair and stable condition; it is unclear whether remediation of the damaged spoil heap would be feasible, or desirable, given that reprofiling the mound is unlikely to return it to an 'authentic' state.



FIGURE 66: THE DRESSING FLOOR TO THE EAST OF THE BOILER HOUSE; FROM THE EAST.

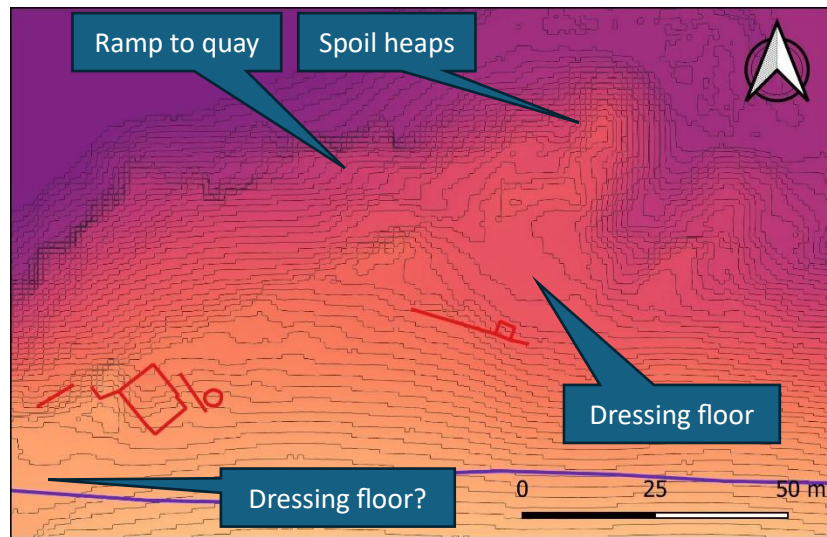


FIGURE 65: IMAGE BASED ON 1M DTM LiDAR DATA, SHOWING THE LINE OF THE TRACK (FORMER LEAT) (DATA USED UNDER THE OGL v3.0). CONTOURS EVERY 0.5M.



FIGURE 67: THE POSSIBLE DRESSING FLOOR ABOVE THE SPOIL HEAPS TO THE EAST; VIEWED FROM THE EAST (SCALE 2M).

2.15 THE LEAT

A leat, drawn off a stream just north of Lockridge Farm, was seemingly built to serve the mine at South Hooe. It is shown on the 1844 tithe map, but not on the 1st edition OS map (surveyed 1882). It is shown on the late 19th century MAP, where the leat crosses the field to the east of the engine house, then disappears (culverted?) just to the south-west of the crusher house, then emerges again from the south-west corner of the engine house, running through the woods towards North Hooe Farm.

There is now no trace of the leat in the field. To the west of the engine house the course of the leat has been superseded by the access track leading to the mine site. The track is very well defined, c.2.5m wide, and a greater part runs along the line of the contour. Along the southern (upslope) side of the track there are several small quarries (or possibly mineral prospecting? or both?), which presumably provide some of the material needed to build up the trackway.

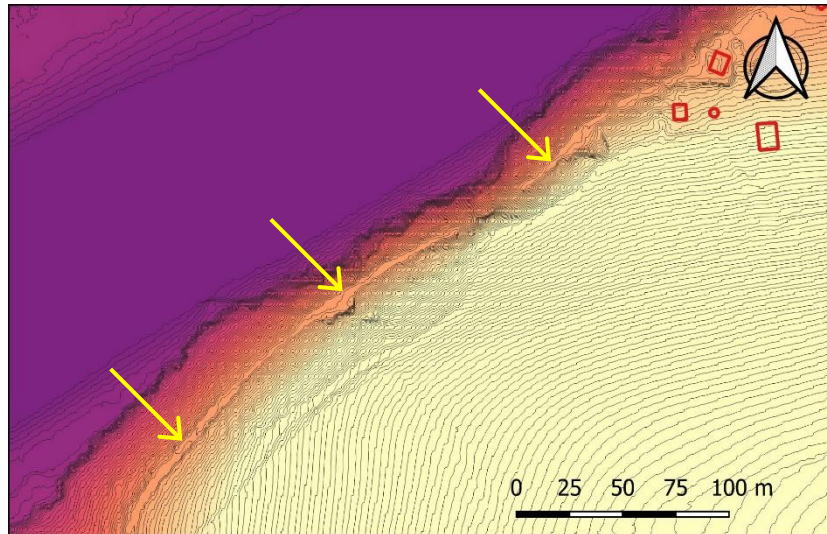


FIGURE 68: IMAGE BASED ON 1M DTM LIDAR DATA, SHOWING THE LINE OF THE TRACK (FORMER LEAT) (DATA USED UNDER THE OGL v3.0). CONTOURS EVERY 0.5M.

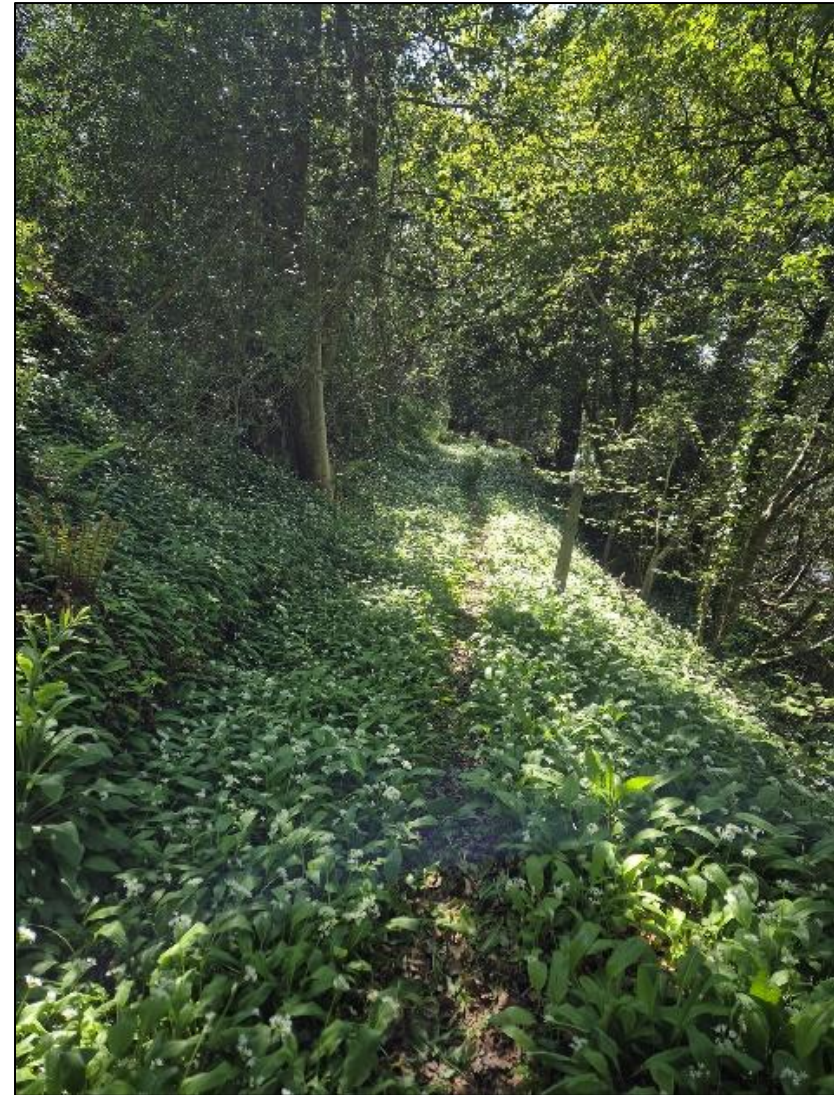


FIGURE 69: THE ACCESS TRACKWAY; FROM THE EAST (NO SCALE).

2.15.1 CONSERVATION MANAGEMENT

The leat (now track) appears to be in good and stable conditions. No interventions are required

2.16 RIVERSIDE STRUCTURE

Below the engine house, close to the base of the slope next to the river, is another masonry structure. This is not shown on the MAP but is shown on the 1st edition OS map (surveyed 1882). The structure is rectangular with a shallow projection to the south-west side, which is battered to both sides. The front (downslope) wall of the structure is battered and c.4m high, creating a platform c.4m across and running back into the slope c.2.5m. Above the platform and running up the slope behind it is a broad, shallow bank; at the time of the survey this bank was covered in Ramsons (wild garlic), whereas the slopes to either side were not; this implies the bank material was imported from elsewhere on the site.

The function of this structure is difficult to determine, given the terrain and vegetation cover. Nance & Nance¹⁵ suggest it was a wharf, but elsewhere it is noted that there is an adit, c.20m long, driven from the riverbank, which connects to the shaft under the engine house, so perhaps this was built to block the entrance to this adit¹⁶. The line of the adit, but not its exit, are shown on the c.1860s MAP.

2.16.1 CONSERVATION MANAGEMENT

The structure appears to be in a fair and stable condition. There remains some uncertainty here, as the structure was difficult to access and its downslope side was not readily visible. If it does mark the entrance to the adit, the interplay between the waters of the river, and the water in the mine, may become an issue.



FIGURE 70: THE STRUCTURE; FROM THE SOUTH-WEST.



FIGURE 71: DETAIL OF THE SW CORNER OF THE STRUCTURE; FROM THE SOUTH (SCALE 2M).



FIGURE 72: THE BANK OF RAMSONS RUNNING BACK UP THE SLOPE BEHIND THE STRUCTURE; FROM THE NORTH-EAST (NO SCALES).

3 STATEMENT OF SIGNIFICANCE

The ruined mid-19th century silver mine at North Hooe is significant for a number of interrelated factors. These factors are discussed below in terms of the conservation values as expressed by the NPPF¹⁷ and the relevant Historic England guidance¹⁸.

3.1.1 EVIDENTIAL VALUE [ARCHAEOLOGICAL INTEREST]

Evidential value derives from the potential of a place to yield evidence about past human activity.

The mine site will have a high evidential value as the limited amount of available documentary information makes its built and archaeological remains more critical to interpretation. The standing buildings can be regarded as being relatively well understood, but no archaeological investigations have been undertaken and – as the section on the pump house (below) makes clear – the more recent elements have been largely ignored.

3.1.2 HISTORICAL VALUE [HISTORIC INTEREST]

Historical value derives from the ways in which past people, events and aspects of life can be connected through a place to the present. It tends to be illustrative or associative. The idea of illustrating aspects of history or Prehistory – the perception of a place as a link between past and present people – is different from purely evidential value. Illustration depends on visibility in a way that evidential value (for example, of buried remains) does not... Association with a notable family, person, event, or movement gives historical value a particular resonance.

The principal value of the mine site lies in its historical significance, magnified by its inclusion within the Cornwall and West Devon Mining Landscape World Heritage Site (WHS). The site as a whole illustrates the individual components, layout and functional structure of a mid-19th century mine, including some less common ones, like the quayside. However, some of the components are in poor condition and difficult to recognise and interpret. The site was relatively short lived, and thus

largely presents as a more readily-intelligible single-phase mine; the early 20th century elements are readily identifiable. Another unusual aspect, and one it shares with South Hooe Mine, is the fact its workings extended beneath the bed of the River Tamar.

The site is closely associated with South Hooe Mine, the older and more significant mining site, and via South Hooe the long history of silver-lead mining on the Bere Peninsula from the medieval period. It formed part of the group of mines run by Percival Norton Johnson (PNJ), who trained as an assayer and founded (later with George Matthey) the firm *Johnson Matthey*. PNJ was a metallurgist with a reputation for accuracy, and he was also an innovator, introducing the ventilation systems to South Hooe Mine that allowed it to be worked deeper, and seemingly an early adopter (in Cornwall) of the shaking table (for concentrating sands and slimes)¹⁹. He was also a local philanthropist.

Johnson Matthey is now a multinational speciality chemicals firm based in London and listed on the FTSE 250. From 1822 the firm was based in Hatton Gardens and facilitated the jewellery businesses there. In 1852 the firm was appointed the official assayer to the Bank of England, to cope with the surge of gold imports arriving from the gold fields of California and Brazil. In 1874 the company manufactured the kilogram reference standard for the *Bureau International des Poids et Mesures*. Later work by the company in the 1960s led to the catalytic converter, and platinum-based anti-cancer drugs. Over the years the company produced a huge volume of gold, silver and platinum bars, marked with their distinctive crest (Figure 73).

While the site and structures have no formal statutory protection, they fall within the Tamar Valley Mining District with Tavistock part of the Cornwall and West Devon Mining Landscape WHS. The site embodies three of the seven attributes of the WHS site that convey its 'outstanding universal value' (OUV): mine sites (the mine); mine transport (the Tamar quays); and mineralogical sites (spoil heaps).



LEFT FIGURE 73: JOHNSON MATTHEY BULLION MARK (IMAGE CREDIT: WWW.INVESTORCRATE.COM).

RIGHT FIGURE 74: PERCIVAL NORTON JOHNSON (IMAGE CREDIT: JOHNSON MATTHEY).

3.1.3 AESTHETIC VALUE [ARCHITECTURAL AND ARTISTIC INTEREST]

Aesthetic value derives from the ways in which people draw sensory and intellectual stimulation from a place. Aesthetic values can be the result of the conscious design of a place... [or] the seemingly fortuitous outcome of the way in which a place has evolved and been used over time. Many places combine these two aspects...

The mine site is currently a highly romanticised version of itself: ivy clad ruins set within woods draped in wild garlic, embedded within attractive, rolling countryside in a dramatic riverside location. The mine complex was designed, and a strong case can be made that certain mining structures (engine houses, count houses) were built with one eye on impressing potential and current investors. The engine house at North Hooe, set high up on steep slope and overlooking a pronounced bend in the River Tamar, may well have been intended as an industrial eyecatcher. If so, the design was practical and rather utilitarian (and presumably, therefore, more reasonably priced), with no embellishment (i.e. no detail to the quoins, no bob wall of granite ashlar, no segmental arches of stone or brick etc.).

180 years later, the remains of the engine house (in particular) stand on the edge of the woods, swathed in ivy, a lost monument to a short-lived industrial enterprise. It presents as an attractive ruin, but it does not derive much of its significance from that appearance (indeed, its current state actively hinders an appreciation of its built significance).

3.1.4 COMMUNAL VALUE

Communal value derives from the meanings of a place for the people who relate to it, or for whom it figures in their collective experience or memory.

North Hooe Mine retains very little in the way of communal value.

3.1.5 INTEGRITY

This characteristic is an aspect of assessment under the WHS, relating to the current condition of the site and its component elements. In general, the integrity of the site is poor and – prior to the planned consolidation works – declining. The crusher house has disappeared, as have both chimneys, most of the boiler house, much of the smithy, and the engine house has numerous structural issues.

3.1.6 AUTHENTICITY

This characteristic is an aspect of assessment under the WHS, relating to how the site and its component parts authentically convey the OUV of the WHS. In this aspect, the site remains highly authentic – modern interventions here are minimal (the pump house and shaft capping). This aspect will be affected by the proposed consolidation works, which will introduce new elements (lintels, stonework) and strip back the ivy and other vegetation currently shrouding the buildings.

4 CONCLUSION

This report builds on existing surveys and provides a point-in-time record of the mine site prior to consolidation works taking place. Based on what is currently known, North Hooe Mine was established in 1838×42 as part of Tamar Consols, together with the renewed exploitation of South Hooe Mine and prospection elsewhere. This took place under the direction of the notable 19th century metallurgist-assayer Percival Norton Johnson. In 1855 the site was closed and the materials sold to support renewed investment at South Hooe. All the buildings are shown as roofless and derelict on the early 1880 OS maps, indicating they had been plundered for their materials. Attempts were made to revitalise the mine – a cross cut from South Ward mine in 1874, and a failed dewatering in the early 1900s – but the only significant addition to the site was a pump house dating to the c.1960s.

The principal surviving remains are those of the pumping engine house and the smithy, both of which are swathed in vegetation and in a poor structural state. Nonetheless, and despite a lack of statutory protection, the site is significant for its historical-illustrative and associative value, and the fact it embodies key attributes of the Cornwall and West Devon Mining Landscape World Heritage Site.

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