

Archaeological Services & Consultancy Ltd

HISTORIC BUILDING RECORDING: LUTON TOWN CENTRE TRANSPORT SCHEME

NGR: TL 0905 2170 – TL 0978 2115

for Pell Frischmann, on behalf of Luton Borough Council



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August 2013 (revised October)

ASC: 1616/LTC/2



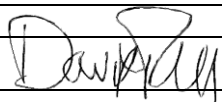
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CONTENTS

Summary.....	5
1. Introduction	6
2. Aims & Methods	10
3. Historical Background	12
4. Descriptions	23
5. Conclusions	44
6. Acknowledgements	46
7. Archive	46
8. References	47

Figures:

1. General location	4
2. Overall plan, showing locations of the recorded buildings	8
3. Proposed development.....	9
4. Luton Town Map, 1842.....	16
5. Map of Luton, 1855.....	17
6. Extract from First Edition Ordnance Survey sheet, 1880: sites of Station House and Buildings 1-3 (<i>scale 1:2500</i>).....	18
7. Extract from First Edition Ordnance Survey sheet, 1880: Cougar Garage site.....	18
8. Extract from Second Edition Ordnance Survey sheet, 1901: sites of Station House and Buildings 1-3 (<i>scale 1:2500</i>).....	19
9. Extract from Second Edition Ordnance Survey sheet, 1901: Cougar Garage site	19
10. Extract from the 1924 Ordnance Survey sheet: sites of Station House and Buildings 1-3 (<i>scale 1:2500</i>).....	20
11. Extract from the 1924 Ordnance Survey sheet: Cougar Garage site.....	20
12. Extract from the 1938 Ordnance Survey sheet: Cougar Garage site.....	21
13. Extract from the 1964 Ordnance Survey sheet: sites of Station House and Buildings 1-3 (<i>scale 1:2500</i>).....	22
14. Extract from the 1964 Ordnance Survey sheet: Cougar Garage site.....	22
15. Station House, plan and elevations.....	39
16. Goods Yard, Building 1: plan and elevations.....	40
17. Goods Yard, Building 2: plan and elevations.....	41
18. Cougar Garage, photo montage and elevation	42
19. Cougar Garage, floor plan	43

Plates:

Cover: Aerial view, Cougar Garage (www.google.com/earth)

1. Station House, west elevation looking east	23
2. Station House, north elevation looking south.....	23
3. Station House, east elevation of extension, looking west.....	24
4. Station House, south elevation of extension, looking north	24
5. Station House, east elevation of main building, looking west.....	24
6. Station House, south elevation of main building, looking north-west	24
7. Building 1, north elevation looking south-east.....	25
8. Building 1, west elevation looking east.....	26
9. Building 1, south elevation looking north-west.....	26
10. Building 1, east elevation looking north.....	26
11. Building 1, office area looking west.....	26

12. Building 1, interior looking south	27
13. Building 1, interior looking north.....	27
14. Building 1, cold store looking south.....	27
15. Building 2, east elevation looking west.....	28
16. Building 2, north elevation looking south	28
17. Building 2, west elevation looking south-east.....	29
18. Building 2, south elevation looking north	29
19. Building 2, interior, looking north west	29
20. Building 2, interior, looking south east	29
21. Building 3, east elevation looking west.....	30
22. Building 3, north elevation looking south	30
23. Building 3, west elevation looking east.....	31
24. Building 3, south elevation looking north-west.....	31
25. Cougar Garage, north-east frontage, detail of vehicle access.....	33
26. Cougar Garage, north-east frontage, detail of former vehicle access.....	33
27. Cougar Garage, north-east frontage, detail of central flat-roofed structure	33
28. Cougar Garage, north-east frontage, detail of office block at north-west corner.....	34
29. Cougar Garage, south-east elevation, looking north-west.....	34
30. Cougar Garage, north-west elevation, looking south-east.....	34
31. Cougar Garage, rear elevation, looking north-east.....	35
32. Cougar Garage, rear elevation, looking north-east.....	35
33. Cougar Garage, easternmost bay, looking north-east.....	35
34. Cougar Garage, easternmost bay, looking south-west	36
35. Cougar Garage, central bays, looking south.....	36
36. Cougar Garage, central bays, looking south-east	36
37. Cougar Garage, central bays, looking north west.....	37
38. Cougar Garage, central bays, looking north-west to offices	37
39. Cougar Garage rear extension, looking north-west.....	37
40. Cougar Garage offices, looking south-west	38
41. Cougar Garage offices, looking east.....	38
42. Cougar Garage offices, modern spiral stairs to upper floor	38

Appendices:

1. List of Photographs.....	48
2. ASC OASIS Form	51

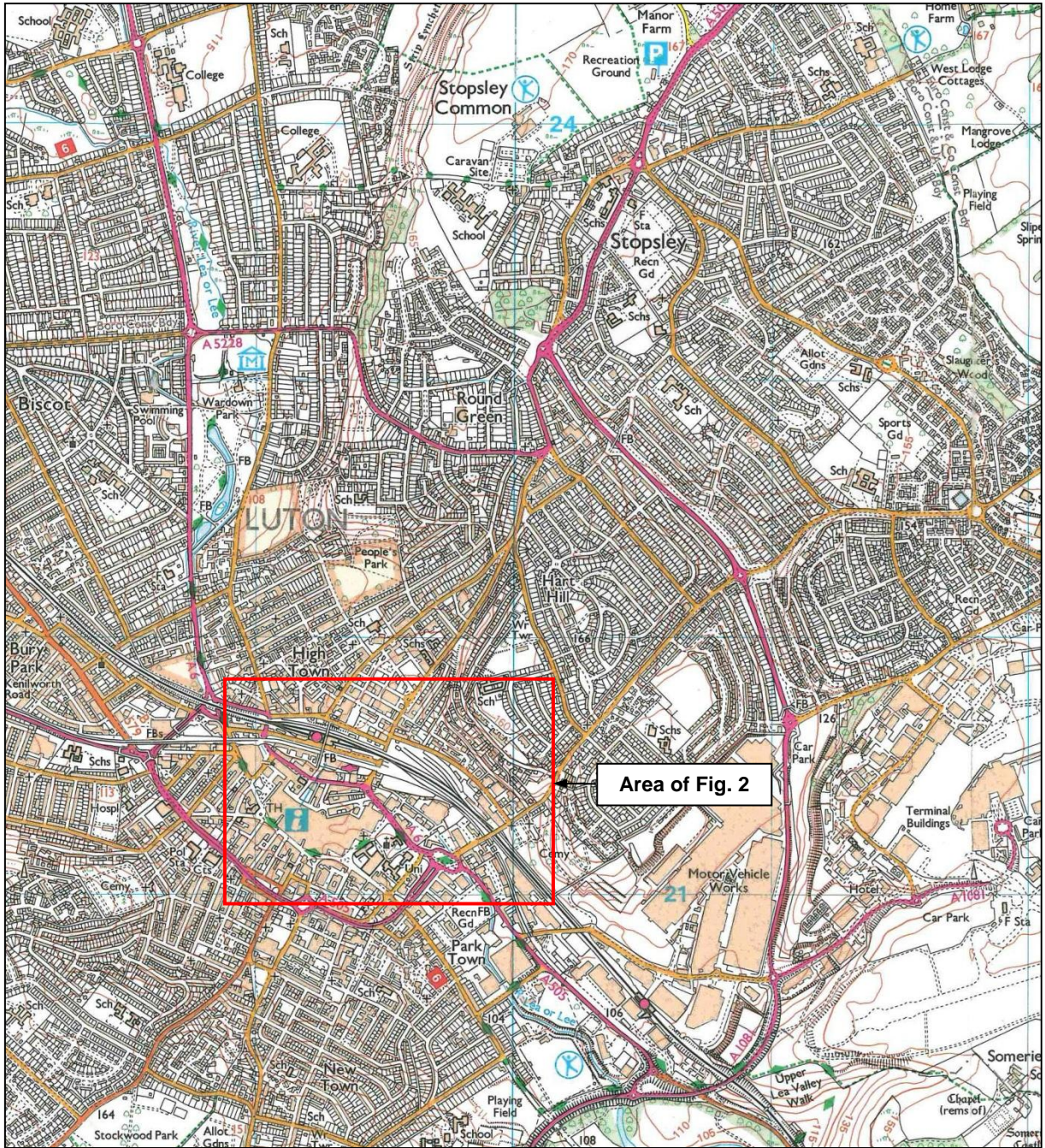


Figure 1: General location (scale 1:25,000)

Summary

In July 2013 historic building recording was undertaken of five buildings directly affected by, or in close proximity to, the proposed Luton Town Centre Transport Scheme. The buildings comprised Station House in Midland Road, three small structures within the former Midland Railway goods yard, off Church Street and Crescent Road, and Cougar Garage, at the junction of Crescent Road and Crawley Green Road.

Station House is a small double-fronted detached house of typical mid-19th-century style, with a more recent extension to the rear. It predates the coming of the railway to Luton in 1860, having been constructed as a private dwelling on the southern edge of the Hightown district of Luton. Following the opening of the Hertford, Luton and Dunstable Railway it became the station master's house, remaining as such until 1969. It is now a solicitor's office. Recording was limited to the exterior of the building.

Of the three Goods Yard buildings, Building 1, located on Crescent Road, is the largest, comprising a single-storey brick structure with a central section under a ridged corrugated sheet roof, with pent roof 'wings' built in similar materials on its east and west sides. The stock bricks and concrete window lintels used in its construction suggest a 20th-century date, and the building first appears on Ordnance Survey mapping in 1924. Internal features include a large cold room in the western 'wing' and a small office, most of the interior comprising a single open space. The building was last used as a pet food store, and before that an abattoir.

Building 2 is a single-cell brick structure beneath a roof of clay tiles, located at the Goods Yard entrance. Inside it has a small fireplace in the north-west corner. The building first appears on Ordnance Survey mapping in 1901. Cartographic evidence reveals it was a weigh house, controlling traffic to and from the Goods Yard.

Building 3 is a long, narrow building to the south of the Goods Yard entrance, adjacent to Church Road. Its construction, comprising orange-red bricks set in English bond, with detailing in cream bricks and a slate roof, suggests a late 19th or early 20th-century date: the building is first shown on Ordnance Survey mapping for 1901. No details of its past functions were found: it is currently used by Network Rail. Although identified originally for internal and external recording (as Buildings 1 & 2), only an external photographic record of Building 3 was made when it became apparent that it was not affected by the road scheme.

Cougar Garage is a large brick structure of five bays, beneath steel framed, glazed ridged roofs. The Crescent Road frontage, constructed in a style reminiscent of Art Deco, comprises four vehicular entrances, the centre two now reduced in size, all with prominent surrounds constructed in brick, and with brick detailing on the walls between. At the north-west end an upper storey has been added to the offices in that area: the materials used suggest a late 20th-century date for this. The upper part of the rear of the building is clad in large red ceramic tiles. Internally the building has a concrete floor throughout, and is subdivided and occupied by partitions and structures of relatively recent date. The entrances into the adjoining structure suggest that this extension is of relatively recent date. Cougar Garage first appears on Ordnance Survey mapping in 1938, and in 1964 it is labelled 'Depot'. On cartographic evidence the north-west extension was added in the latter half of the 20th century. Cougar Garage's most recent function is a car body repair shop: the 1964 designation 'Depot', the size of the building and the absence of typical 'garage' features suggest that its function has changed over time. The taller linked extension to the west, which appears from cartographic evidence to date from the late 1960s and resembles a warehouse, is apparently not scheduled for demolition.

1 Introduction

1.1 In July 2013 *Archaeological Services and Consultancy Ltd* (ASC) carried out historic building recording of five buildings in Luton, in relation to the *Luton Town Centre Transport Scheme*. The project was commissioned by Pell Frischmann on behalf of Luton Borough Council, and was carried out according to a project design prepared by ASC (Zeepvat 2013), and approved by the *Central Bedfordshire Council Archaeologists*, archaeological advisors (AA) to the local planning authority (LPA), *Luton Borough Council*. The relevant planning application references are detailed below (para. 1.5.3).

The monitoring of groundworks for the road, also described in the project design, will be the subject of a separate report.

1.2 *Planning Background*

This building recording project has been required under the terms of the *National Planning Policy Framework* (NPPF), as a condition of planning permission for development involving buildings on the site.

1.3 *Archaeological Services & Consultancy Ltd*

ASC is an independent archaeological practice providing a full range of archaeological services including consultancy, field evaluation, mitigation and post-excavation studies, historic building recording and analysis. ASC is recognised as a *Registered Organisation* by the Institute for Archaeologists and is also accredited ISO 9001, in recognition of its high standards and working practices.

1.4 *Management*

The project was managed by Bob Zeepvat BA MIFA.

1.5 *The Site*

1.5.1 *Location & Description*

The development area is located in the town and unitary authority area of Luton, in the historic county of Bedfordshire (Fig. 1). The development area (Fig. 2) comprises a network of roads, mostly on the northern side of the railway, running between Hucklesby Way and the St Mary's Road roundabout (south of the railway). The Hucklesby Way end of the scheme is situated at NGR TL 09054 21699 and the St Mary's Road roundabout end is situated at NGR TL 09782 21151. It currently contains a series of roads and associated footpaths, separated by the railway cutting for the Midland mainline from Bedford to St Pancras.

1.5.2 *The Buildings*

A number of buildings along the route of the road scheme are directly or potentially affected by its construction. Five of these (Fig. 2) were identified for recording:

- Station House, Midland Road
- Goods Yard: three small buildings (designated Buildings 1-3)
- Cougar Garage, Crescent Road

None of the above buildings are listed. Station House is recorded in the Central Bedfordshire & Luton Historic Environment Record (HER 10345).

1.5.3 *Proposed Development*

The development (Fig. 3) comprises three distinct elements:

- Gateway link alignment, eastern end to include dualling of carriageway on Crawley Green Road between Crescent Road and St Mary's roundabout to include construction of new road bridge and realignment and priority of the Crawley Green Road/Crescent Road junction and associated engineering works (08/01331/FUL and 11/01421/VARCON);
- Construction of a link road between Gillam Street and Midland Road for Luton Gateway (09/00267/FUL and 11/01419/VARCON);
- Gateway link alignment, western end to include construction of new section of link road, redesign and realignment of the junction of Old Bedford Road and Church Street, construction of new site access off Crescent Road, closure of parts of Midland Road and associated engineering works (09/01326/FUL and 11/01420/VARCON).

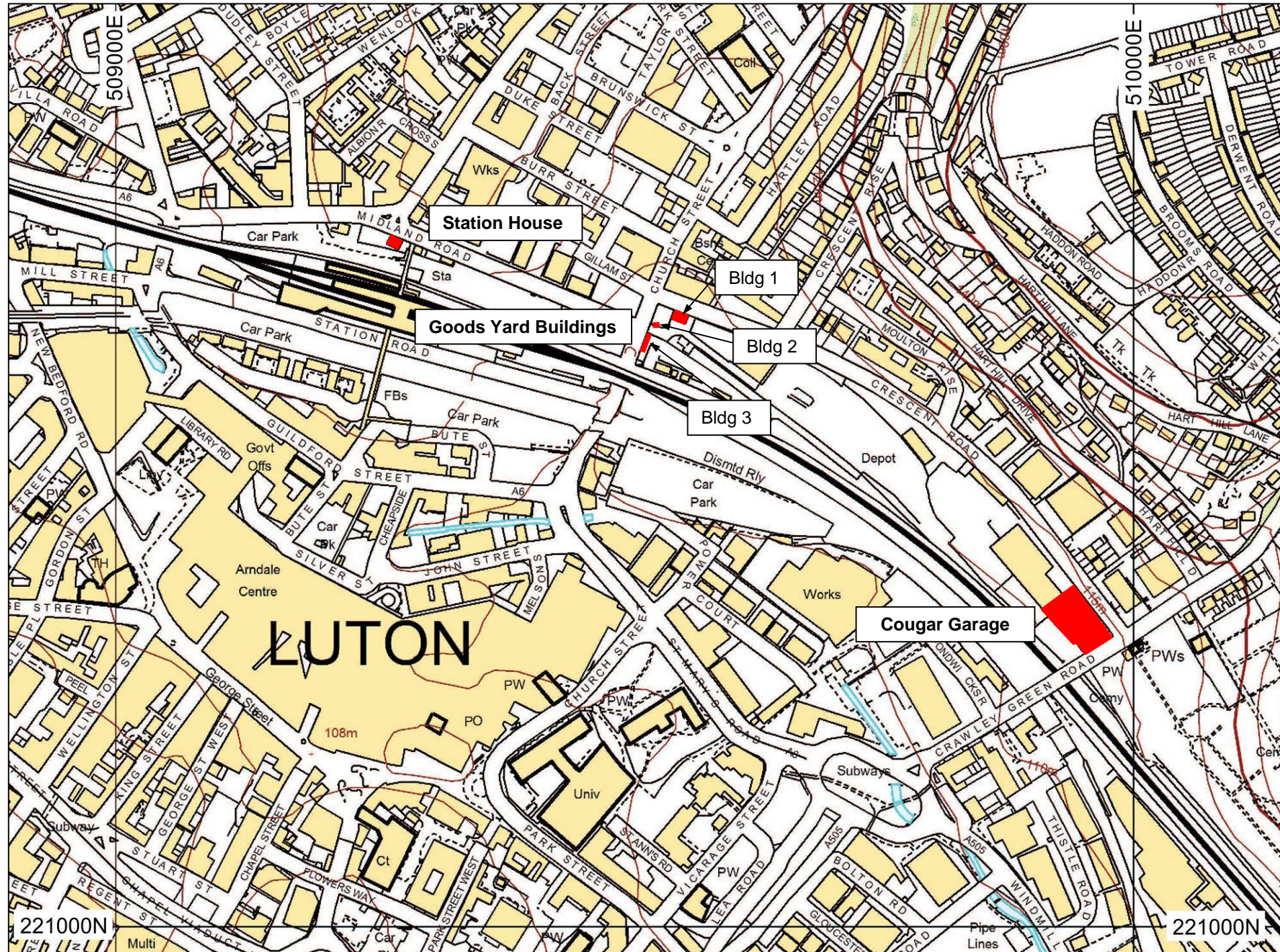


Figure 2: Overall plan, showing the locations of the recorded buildings (scale 1:5000)

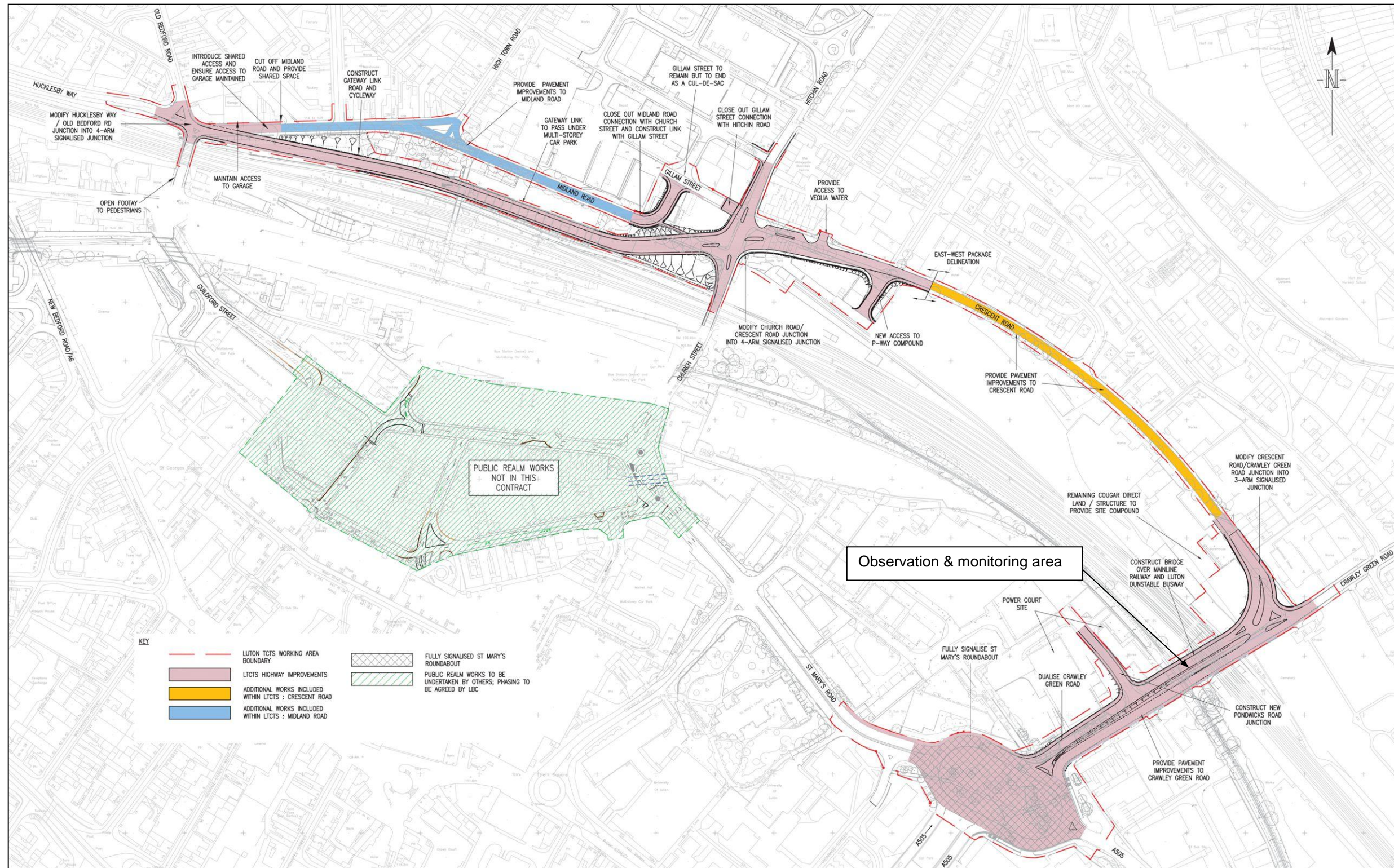


Figure 3: Proposed development (scale 1:3500 approx)

2 Aims & Methods

2.1 Aims

As described in the project design, the aims of the building recording were:

- To compile an accessible and accurate record of the structures concerned, prior to demolition
- To ascertain the nature, function, character, structural history and development of the structures concerned, within their local context

The project also considered what evidence the buildings provide of the expansion and industrialisation of Luton in the later post-medieval period (Oake *et al* 2007, 16 & 127-128).

2.2 Standards

The work conforms to the project design, to the relevant sections of the Institute for Archaeologists' *Code of Conduct* (IFA 2010) and *Standard & Guidance Notes* (IFA 2008), to current English Heritage guidelines (EH 2006a & b), to the Association of Local Government Archaeological Officers East of England Region *Standards for Field Archaeology in the East of England* (ALGAO 2003), and to the relevant sections of ASC's own *Operations Manual*.

2.3 Methods

The work was carried out according to the project design, which required a programme of historic building recording to English Heritage Level 2 (EH 2006a), specifically comprising:

Station House, Midland Road

- Plans and elevations (with dimensions indicated on them)
- External photographic record
- Written description of the building's development and use

Goods Yard (Buildings 1-3)

- Plans and elevations (with dimensions indicated on them)
- Internal & external photographic record
- Written description of the building's development and use

Cougar Garage, Crescent Road

- Plans and elevations (with dimensions indicated on them)
- Internal & external photographic record
- Written description of the building's development and use

2.4 Constraints

Recording of the east side of Building 1 was prevented by the presence of an active hive of bees, discovered during clearance of vegetation the day before the survey.

At the start of the survey it became apparent that Building 3 was not affected by the scheme, and was in a separate area of the goods yard, owned by Network Rail. Following discussions with the clients, the AA and the LPA, it was agreed that only a basic external photographic record of this building would be made.

Access to the rear of Cougar Garage for recording was limited by security fencing and the building's proximity to the railway. Health and safety issues prevented recording of the passage beneath the south end of the building.

3 Historical Background

3.1 The following section provides a summary of the readily available historical background to the site and its environs. This section has been compiled with information from with information from the brief, the Central Bedfordshire & Luton Historic Environment Record (HER), the *Luton Extensive Archaeological Urban Survey* (Albion Archaeology 2003) the Luton Local Studies Library and ASC's own reference collection.

3.2 *Medieval* (1066-1500)

At the time of the Domesday survey the manor of Luton was in the possession of the Crown, which it had been since the reign of Edward the Confessor (Page 1908, 350). It is recorded as having several mills and a market at this time, and is likely to have had a population of 500-800 people. It is possible that the church and its lands also formed part of the manor (*ibid.*). In the early 12th century the manor was granted to Robert Earl of Gloucester, though his tenure was short-lived, being terminated by King Stephen in 1139 following the earl's support for Matilda in her campaign for the throne. Stephen subsequently granted the manor and church to Robert de Waudari, a foreign mercenary. In the same year de Waudari constructed an unlicensed or adulterine motte and bailey castle (HER 186) on the south-western approach to the town. It was pulled down in 1153 following the Treaty of Winchester, which required the destruction of all unlicensed castles. Traces of its large defensive ditch and other associated structures have been recorded off Castle Street and Holly Street, initially in 1963 and again in 2002, 2004 and 2005 (Coles 2005; Abrams & Shotliff 2010).

In 1194 the manor reverted to the Crown, and was granted initially to Baldwin de Bethune, Earl of Albemarle. It subsequently passed through a series of tenants including Faulk de Breaute. In 1221 de Breaute constructed Luton's second castle (HER 185), on a site between St Mary's Church and Lea Road. This was even more short-lived than de Waudari's castle, and was possibly demolished in 1224 or 1225. Archaeological investigations undertaken in response to the expansion of the University of Luton Park Square Campus have produced evidence of de Breaute's castle, including an extensive ditch and the partial footprint of a large timber-framed building (Keir forthcoming). It has been suggested that this "castle" was a fortified manor or ringwork rather than a traditional motte and bailey: nevertheless it was surrounded by a substantial ditch or moat, likely to have been created by diverting the river Lea. The castle was apparently succeeded by a moated house, known as Court House, situated on its southern corner, which may have re-used part of the castle moat and was recorded as extant in the early 17th century (*ibid.*).

After 1274 the manor was divided, and until the Dissolution one of the subdivisions was held by Dunstable Priory. The medieval town probably originated as a small collection of timber-framed houses located around the Park Square area with a series of roads leading down to bridges and fording points on the Lea. The earliest crossing points may have been a ford in the Church Street area, and perhaps another in the North Street area, replaced by a bridge in the medieval period. In 1336 a fire destroyed much of the town (Albion Archaeology 2003).

3.3 *Post-Medieval* (1500-1900)

By 1546, the population of Luton included 1,500 'houselying people' (Page 1908, 348): in 1801 it was 3,095 (*ibid.*). The town's main period of expansion came in the 19th century, before which documentary evidence suggests it was a fairly typical and unremarkable market town. By 1861 the population of Luton had reached 15,000 (English Heritage 2011a). Until the mid-19th century most of the occupants of the town were engaged in agricultural occupations and, besides the market, malting, milling, brewing and brick-making were the main industries (*ibid.*). These last four trades had their roots locally in the medieval period, when there may have been as many as 60 malt kilns in the town (Albion Archaeology 2003). In addition, straw plaiting and hat making were probably two of Luton's most recognisable post-medieval trades, and contributed to the town's rapid expansion.

It has been suggested that the extent of the medieval town was largely still intact by the 18th century when recorded by Jeffrey in 1765. The main streets thought to be in place by the medieval period are Bridge Street, George Street, Church Street, Park Street, Castle Street and Dunstable Lane, although it is likely they were known by different names. The market place was located in the Market Hill area, at the junction of Castle Street and George Street (English Heritage 2011a).

Straw plait making is likely to have been established in the Luton area by the mid-17th century. It has been suggested that the chalky, silica-rich soils of the district helped to produce strong, flexible straw suitable for plaiting. During the Napoleonic Wars the industry received a boost when supplies of straw plait from Italy were cut off, though it was really the introduction of the straw splitter, which allowed much finer plaits to be created, that really led to a boom in straw plait making. In 1871, at the peak of the trade, there were 20,701 plaiters in Bedfordshire. When cheaper imports from China and Japan began to flood the market, many plaiters went to work for the newly established straw hat factories in Luton (*ibid.*).

The hatting industry in Luton is said to have originated in the 18th century, when it was established locally on a domestic level and was unsurprisingly linked to straw plaiting. In the 19th and 20th centuries the industry underwent rapid expansion. The Waller brothers were two of the town's local hatting pioneers, and Edmund Waller was partly responsible for helping to establish the county-wide trade, whereby domestic makers could sell on their work to the larger factories. The Luton industry was so successful that it attracted a number of London-based producers such as Munt & Brown, the Vyse family and Welch, Willis & Elliot. By 1900 there were over 400 firms in Luton directly making straw hats, employing over 11,000 individuals. Many of the earlier factories were located around Bute Street and Guildford Street (*ibid.*).

Bute Street became the epicentre of Luton's manufacturing trades with many businesses associated with the hat industry such as ribbon makers, thread spinners, cardboard box makers and sewing machine companies located among the hat factories. Many of the workers lived in High Town, where the first house was constructed in 1815, and by 1851 the area had a population of 1,900. Much of the early High Town housing stock was exceedingly poorly built and after the Health Enquiry of 1850 much of it was condemned (Albion Archaeology 2003).

The impact of the railways on the development of Luton is less obvious than in some other towns in the region. Luton was bypassed by the London & Birmingham Railway (later the London & North Western Railway) to the west, and the Great Northern Railway to the east, largely due to the inability locally to agree a route. It

was not until 1860 that the Hertford, Luton and Dunstable Railway Company completed the connection from the LNWR branch to Dunstable (HER 14086: opened 1848) through Luton the GNR at Welwyn (en.wikipedia.org/wiki/Dunstable_Branch_Lines). Much of the southern portion of High Town was demolished to make way for the railway. People's Park (HER 17749) was granted to the local population in compensation (Albion Archaeology 2003). In 1861 the HL&DR merged with the GNR: in Luton the main station was then at Bute Street. Following the decision in 1862 for the Midland Railway to open a line to Bedford from London (St Pancras) via Luton (HER 12933), a second station, subsequently Luton's main station, was opened on the opposite side of Station Road (Abrams & Shotliff 2010). The Midland Railway line opened in 1868.

3.4 *Modern* (1900-present)

The rapid increase in population and industry in Luton in the later 19th century continued well into the early part of the 20th century. Following the decline of the hat-making trade, engineering took over as the major industry in the town, although Luton still had a thriving felt hat-making trade in the inter-war years. The Vauxhall car company came to Luton in 1905: other products made in Luton included ball bearings, gas cookers and meters. Gradually the surrounding hamlets (e.g. Limbury, Leagrave and Stopsley) were absorbed into the boundaries of the town and the airport was opened in 1938 (Albion Archaeology 2003).

Luton's industrial heartland was a target during the Second World War: the airport, the Vauxhall works and the Park Street bus depot were all damaged, and many domestic properties were lost. In the later war years many schools in Luton had air raid shelters built to accommodate local children, who started returning from their evacuation homes in 1940. These included Beechwood School, which had three shelters (HER 13492). High Town also had one of the largest public air raid shelters (HER 17927), the entrance to which adjoins the development site.

3.5 *Site-Specific (buildings)*

Background historical research undertaken for the survey has revealed little useful information regarding the past history and functions of the five buildings, with the exception of Station House, the only one recorded in the HER.

Station House

Station House (HER 10345) is one of the only remaining buildings associated with the original southern edge of High Town, the rest having been demolished to make way for the railway in the late 19th century. It was subsequently purchased by the railway, and became the stationmaster's house. It was sold to the last resident stationmaster, William Thomas, upon his retirement in 1969.

Goods Yard, Building 1

This building was last in use as a pet food store, before which it was an abattoir, and there is no readily available documentary or physical evidence to show it has been used for another function. Its development, as recorded cartographically, is described below (Section 3.6).

Goods Yard, Building 2

There is no readily available documentary evidence about the use and development of this building. Its small size and location, adjacent to the entrance to the Goods Yard, suggests it may have been a weigh house, by which traffic to and from the Goods Yard

was controlled. Its development, as recorded cartographically, is described below (Section 3.6).

Goods Yard, Building 3

There is no readily available documentary evidence relating to this building. Its development, as recorded cartographically, is described below (Section 3.6).

Cougar Garage

There is no readily available documentary evidence relating to this building, which became the Cougar Car Body Repair Centre (technically not a garage in the accepted sense) in 1990. Its development, as recorded cartographically, is described below (Section 3.6).

3.6 *Cartographic Evidence*

The earliest readily available map showing the locations of the five buildings is the 1842 Luton Town map (Fig. 4). At this time the town essentially retained its medieval plan, though recent developments are apparent to the north and south-west of the town. Crescent Road and Midland Road had yet to be constructed, as had the railway. The 1855 map of Luton (Fig. 5) shows the proposed route of the proposed Luton, Dunstable and Welwyn Junction Railway, and further development in the Hightown area, though Crescent and Midland Roads still do not appear.

In 1880, when the First Edition 25" Ordnance Survey (OS) sheet was published (Figs 6 & 7) the railway had been constructed, encouraging further development to the north and east of the centre of Luton. Crescent Road and Midland Road had been established by this date, and Station House is shown in its original rectangular form, with a small extension to the rear (Fig. 6). At this time the north-west end of the Goods Yard contains no buildings. The site of Cougar Garage remained open ground (Fig. 7).

By the publication of the 1901 OS sheet (Figs 8 & 9) Station House remained unchanged in plan, though structural evidence suggests that the present larger extension may be of 19th-century date (see 4.1, below). The Goods Yard had further developed: Buildings 2 and 3 had been constructed, and a range of buildings had been erected fronting Hitchin Road, north of Building 2 (Fig. 8). Building 2 is shown as a weigh house. The site of Cougar Garage remained open ground (Fig. 9).

The 1924 OS sheet (Figs 10 & 11) shows Station House unchanged, though in the Goods Yard Building 1 appears for the first time. The site of Cougar Garage remains open ground. On the sheet for 1938 the buildings in Midland Road and the Goods Yard all appear unchanged (so the map is not reproduced), but Cougar Garage is shown for the first time (Fig. 12). To the north of the Cougar Garage at this time is shown an even larger building, labelled 'Bakery' – apparently belonging to the Co-operative Society.

On the 1964 OS 1:2500 sheet (Fig. 13) Station House is named for the first time. The Goods Yard buildings remain unchanged. Cougar Garage also retains its original plan: the building is now labelled 'Depot' (Fig. 14).

Since that date, recent Ordnance Survey mapping has updated the footprint of Station House. The Goods Yard buildings are shown unchanged from 1964, and by 1992 the north-western extension to Cougar Garage is first shown.



Figure 4: Luton Town Map, 1842 (*not to scale*)

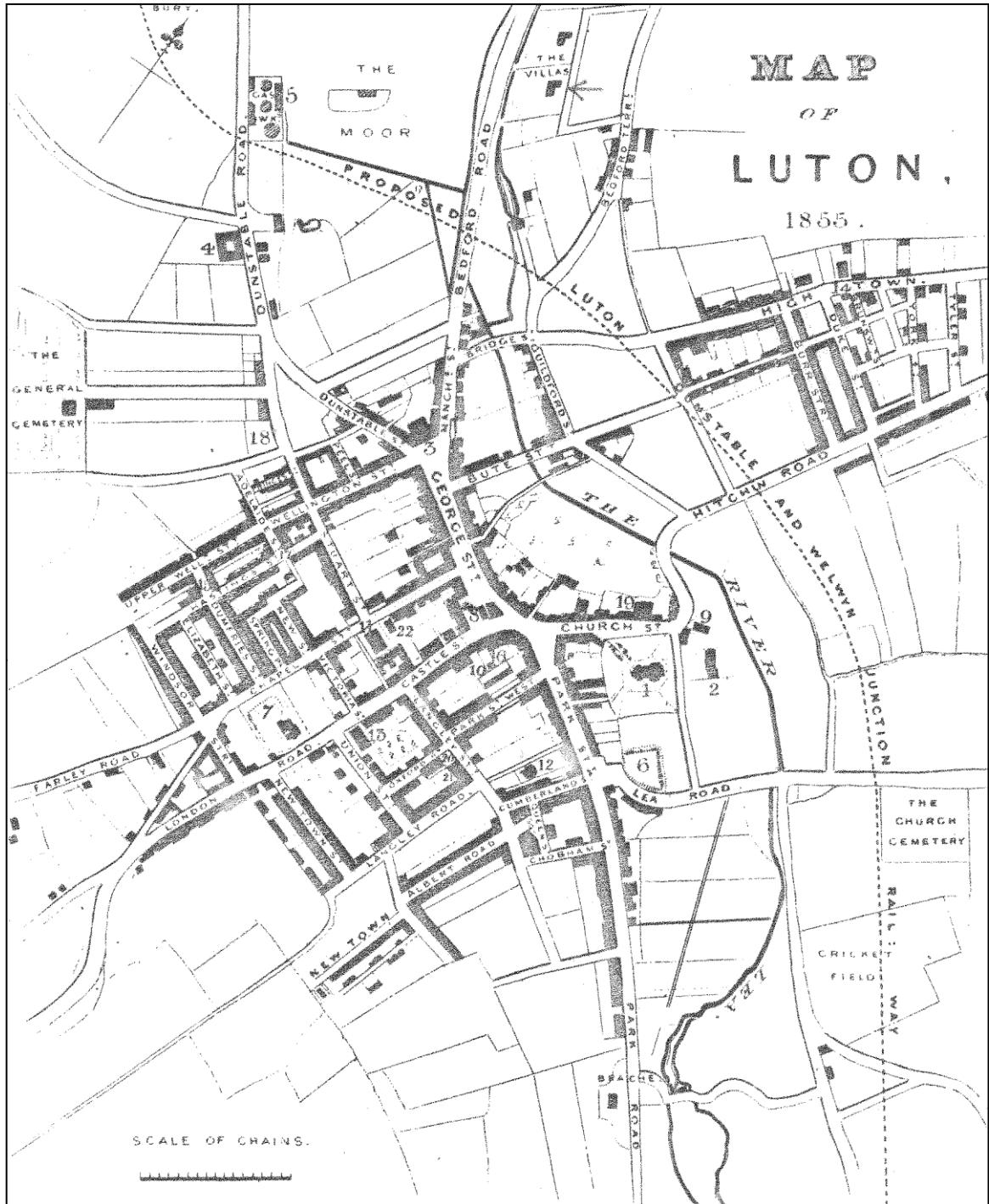


Figure 5: Map of Luton, 1855 (not to scale)

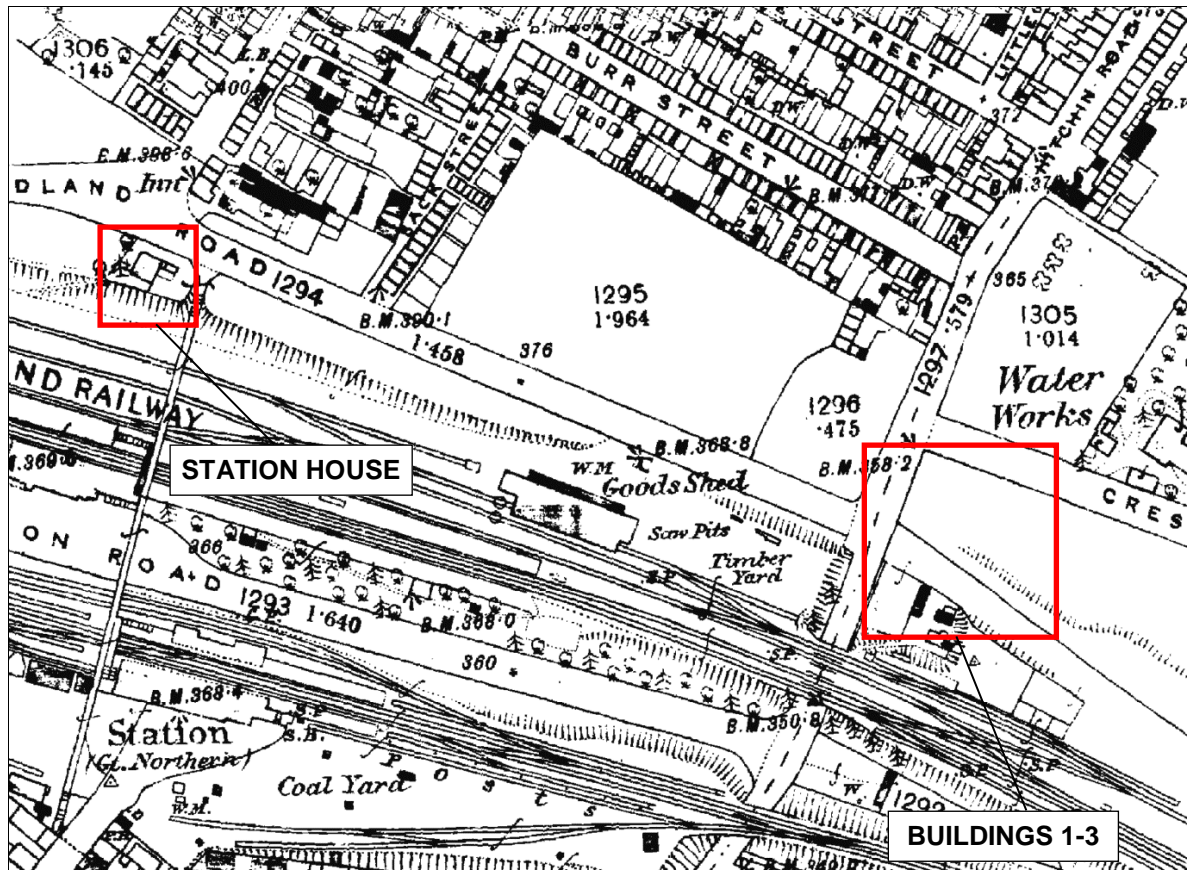


Figure 6: Extract from First Edition Ordnance Survey sheet, 1880: sites of Station House and Buildings 1-3 (scale 1:2500)

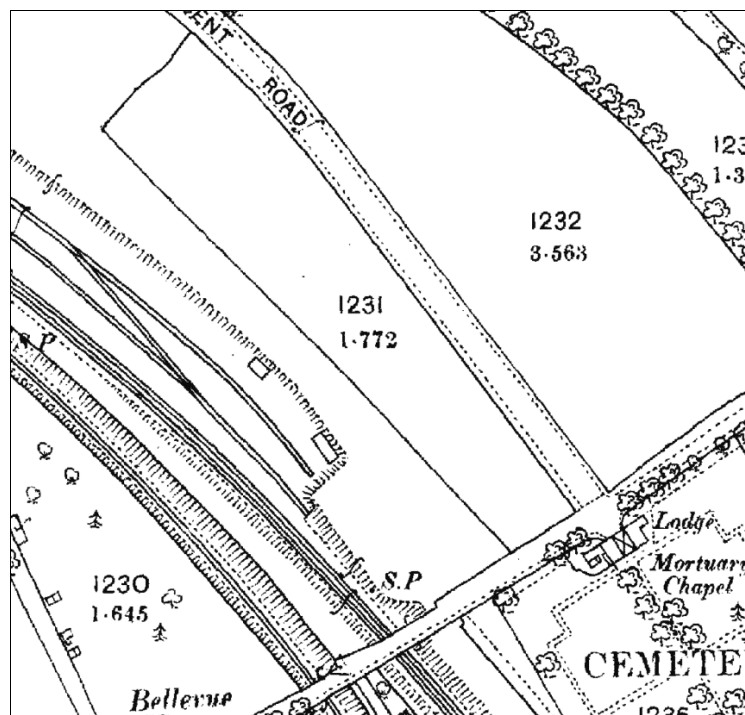


Figure 7: Extract from First Edition Ordnance Survey sheet, 1880: Cougar Garage site (scale 1:2500)

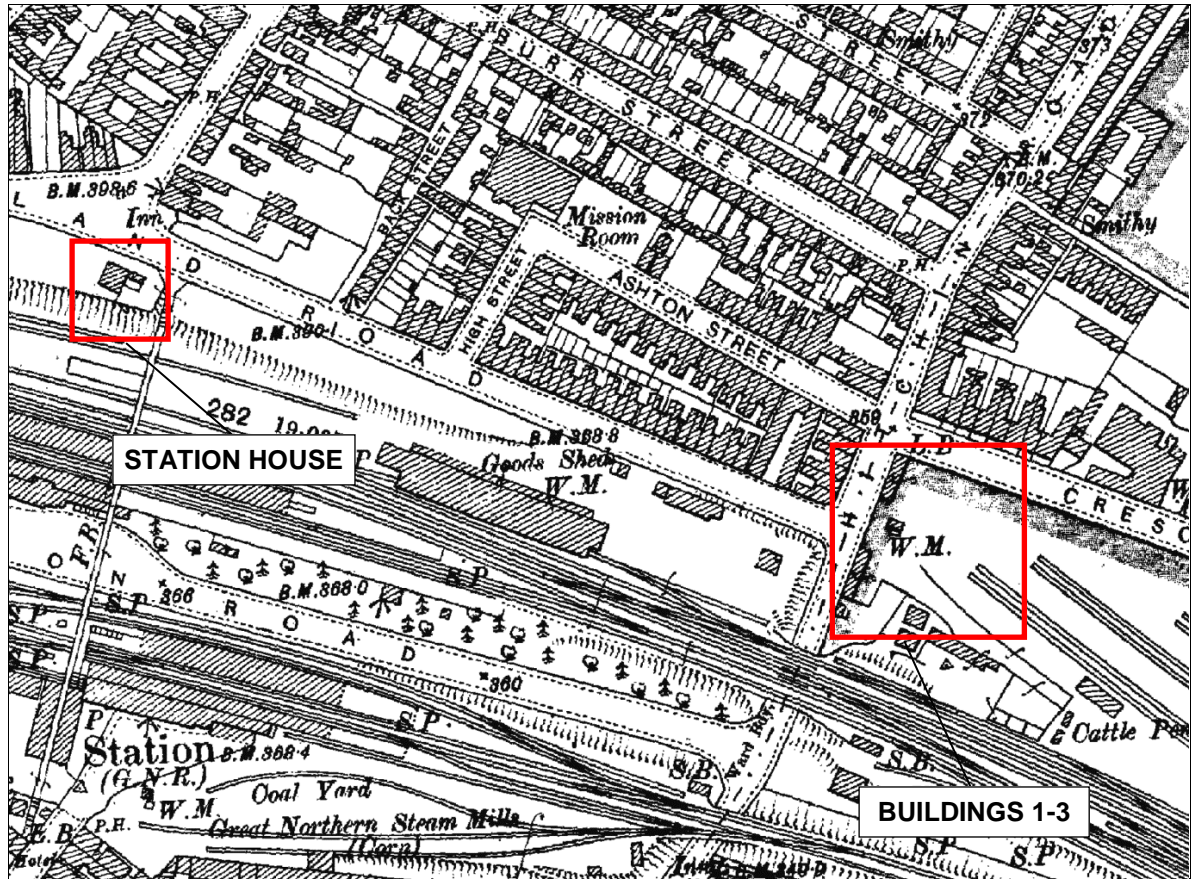


Figure 8: Extract from Second Edition Ordnance Survey sheet, 1901: sites of Station House and Buildings 1-3 (scale 1:2500)

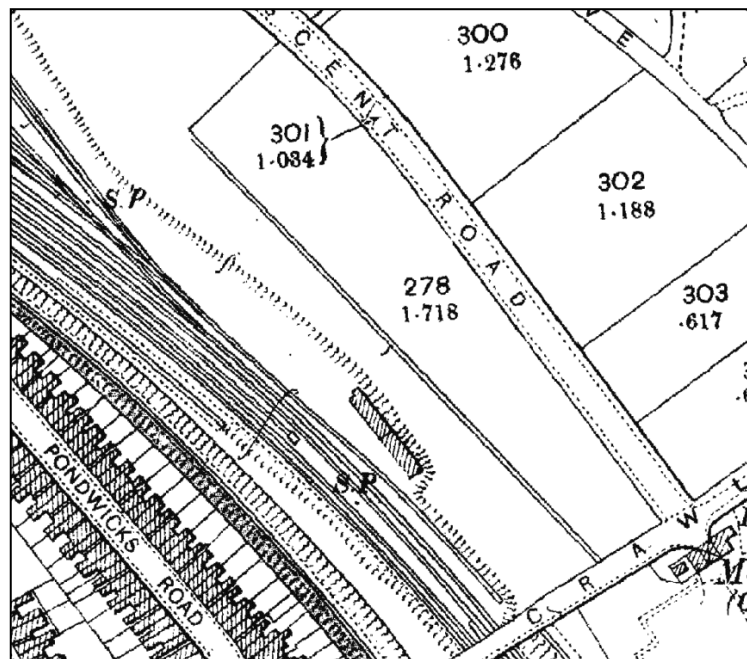


Figure 9: Extract from Second Edition Ordnance Survey sheet, 1901: Cougar Garage site (scale 1:2500)

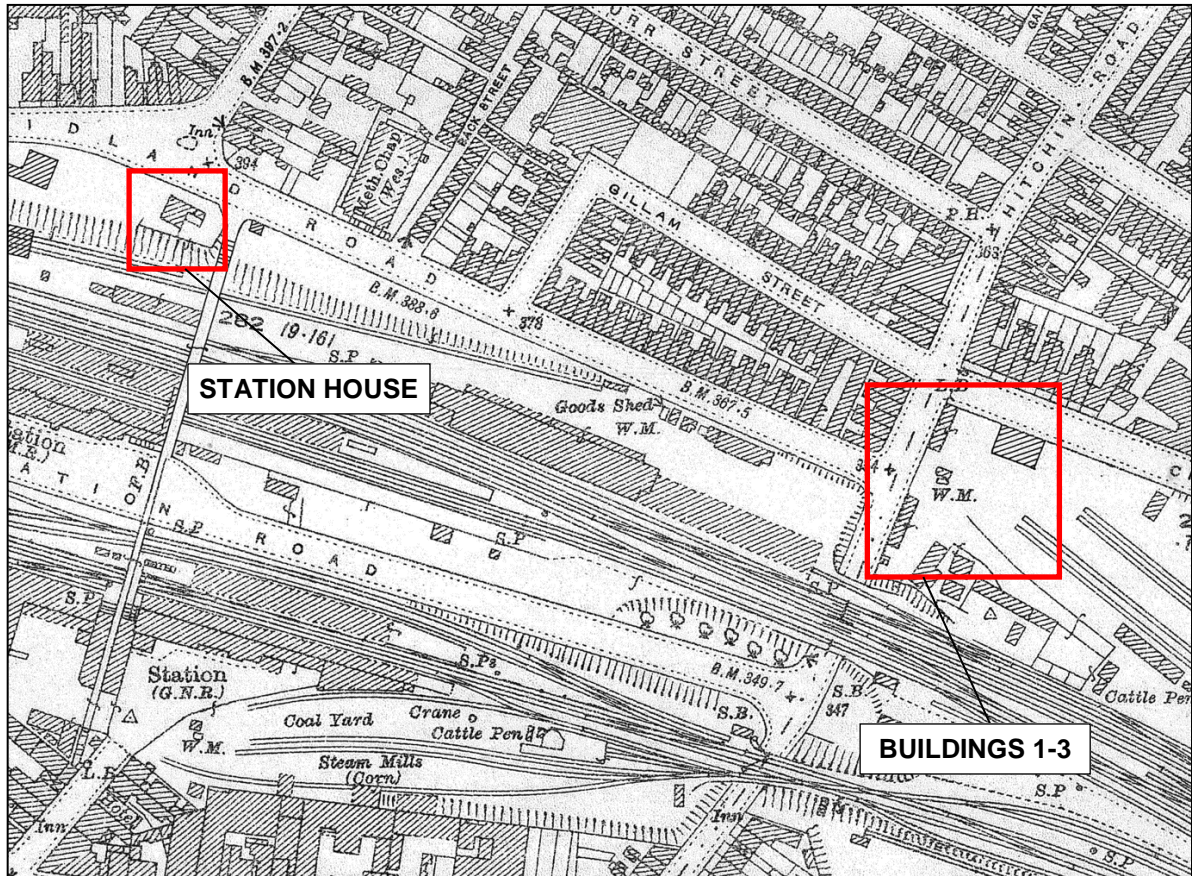


Figure 10: Extract from the 1924 Ordnance Survey sheet: sites of Station House and Buildings 1-3 (scale 1:2500)

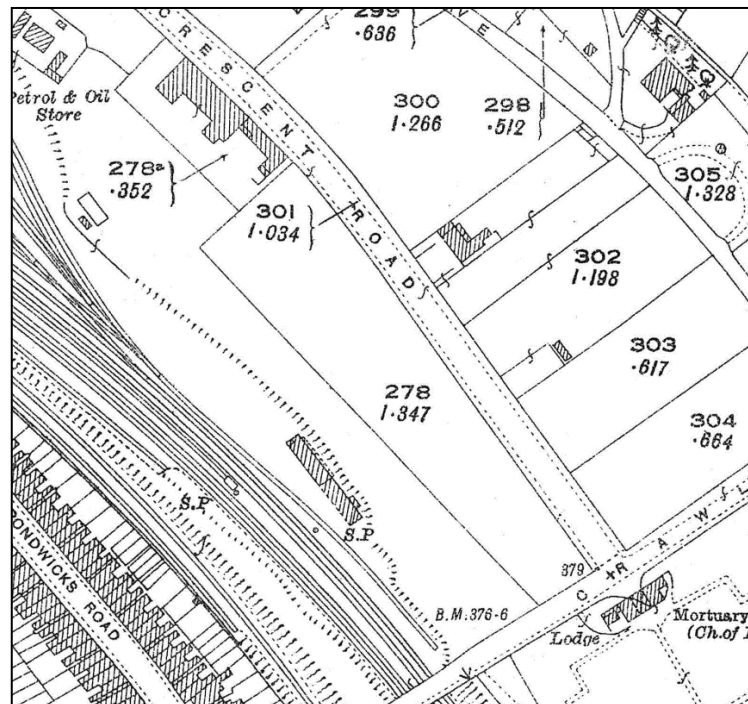


Figure 11: Extract from the 1924 Ordnance Survey sheet: Cougar Garage site (scale 1:2500)

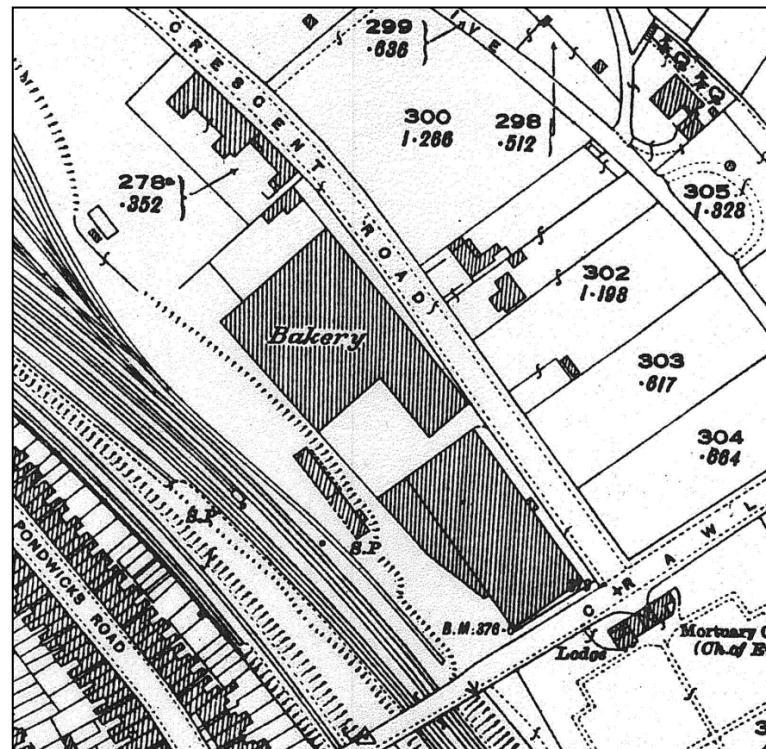


Figure 12: Extract from the 1938 Ordnance Survey sheet: Cougar Garage site (scale 1:2500)

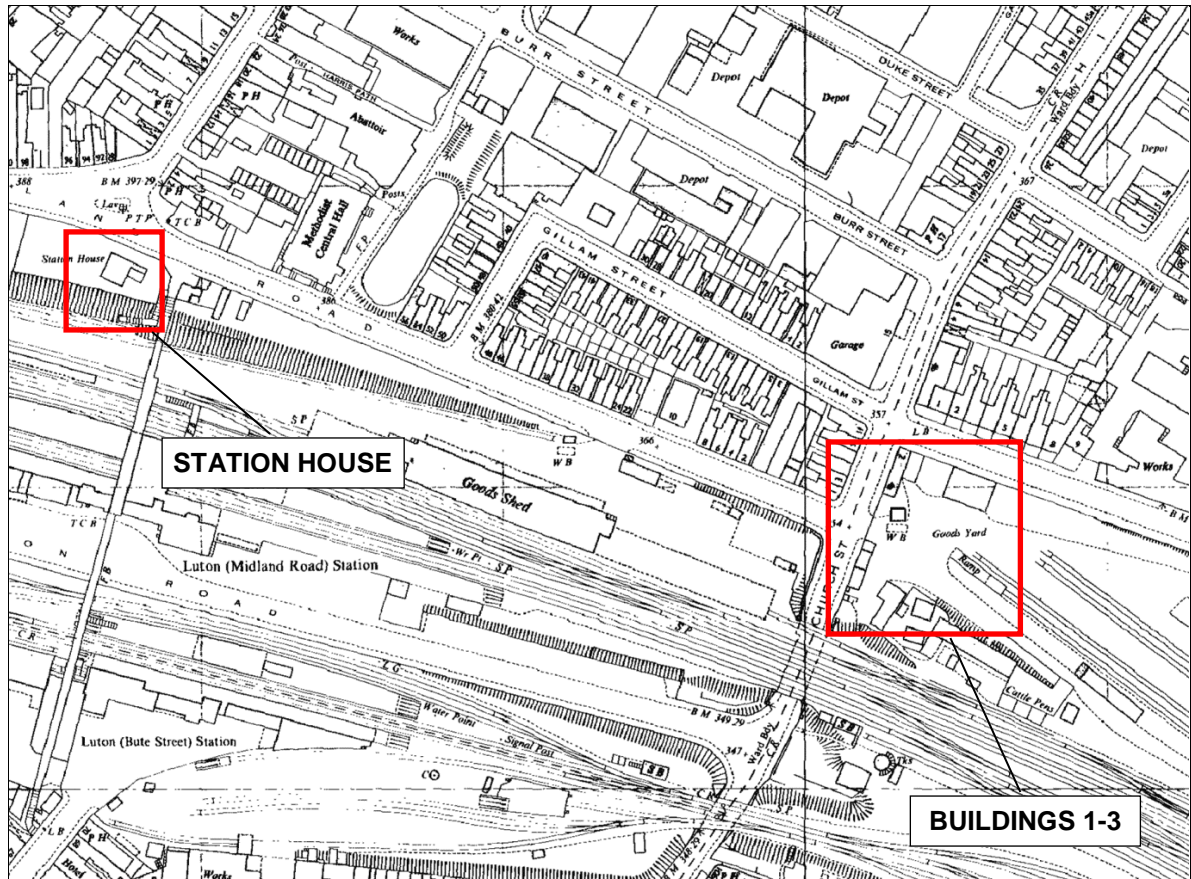


Figure 13: Extract from the 1964 Ordnance Survey sheet: sites of Station House and Buildings 1-3 (scale 1:2500)

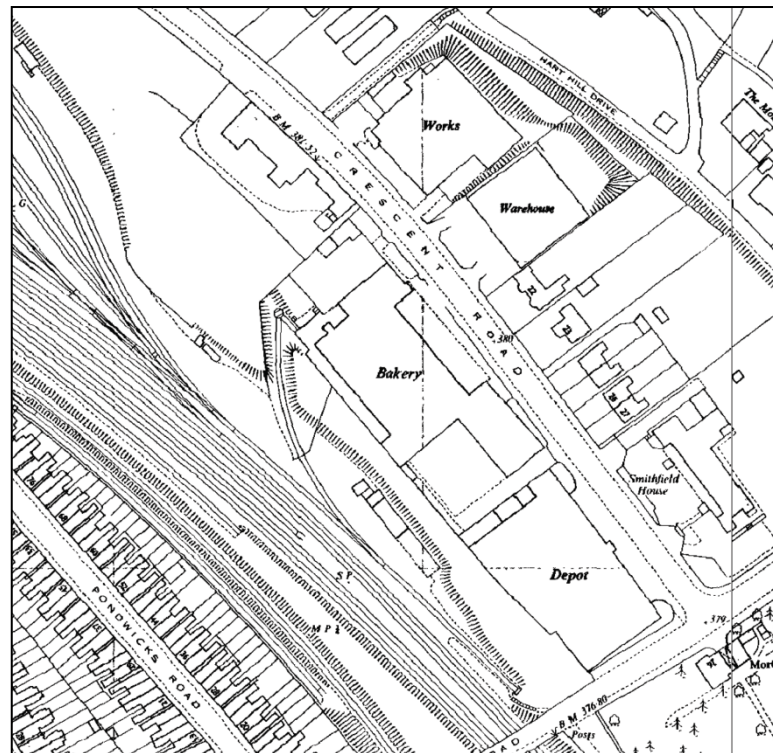


Figure 14: Extract from the 1964 Ordnance Survey sheet: Cougar Garage site (scale 1:2500)

4 Descriptions

4.1 *Station House* (Fig. 15: Plates 1-6)

Location: TL 0927 2167

Description: Two-storey L-shaped building, max. dimensions 13.06 × 9.02m, cement rendered and painted white. The main (western) part of the building is roofed with clay pantiles, the extension has a slate roof. Two or three rows of slates survive on the east face of the main roof, beneath the pantiles: these are presumably a vestige of the original roof cladding. There are two chimney stacks, located at the northern and southern ends of the building. The west elevation forms the front of the building, and has a central door surrounded by pilasters, entablature and a cornice. To the south of the door is a canted bay window, and there are three double-hung sash windows with moulded sills, one to the north at ground-floor level and two at first-floor level. The north and south gable ends of the main building are each dominated by substantial brick chimney stacks, and each has two or three small windows in the upper floor and attic. The eastern (rear) extension has a slate roof, with a lower roof-line than the main building, and no chimneys. The windows in the extension are all casements, and appear to be modern replacements.

The main part of Station House, with its central doorway and porch, gable end chimney stacks, bay window and general symmetrical appearance, is typically mid-19th century in date. The rear extension has a shallow-pitched slate roof and is therefore likely to be of 19th rather than 20th-century date, though other external diagnostic features such as windows have been replaced with modern items. The available cartographic evidence (Figs 6, 8, 10, 13) shows a much narrower extension to the building, with an open yard to the south: the extension and yard combined cover the same footprint as the present extension.



Plate 1: Station House, west elevation looking east



Plate 2: Station House, north elevation looking south



Plate 3: Station House, east elevation of extension, looking west



Plate 4: Station House, south elevation of extension, looking north



Plate 5: Station House, east elevation of main building, looking west



Plate 6: Station House, south elevation of main building, looking north-west

4.2 **Goods Yard, Building 1** (Fig. 16: Plates 7-14)

Location: TL 0955 2216

Description: Single-storey building, rectangular, overall dimensions 16.00 × 10.73m, max. height to ridge 6.81m. Comprises a single-storey brick structure with a taller central section under a ridged corrugated sheet roof, with pent roof 'wings' built in similar materials on its east and west sides. The building is constructed in Fletton Rustic bricks, set in English bond with cement mortar. The windows all have concrete lintels and brick sills. The north elevation fronting onto Crescent Road has a roller shutter door and two windows in the central section, with a further two windows in the west wing and one to the east. All windows in this elevation have been bricked up: those to the east of the door with stock bricks, while on those to the west the bricks are covered with cement render. The sides of the door opening are constructed in blue/grey bullnose bricks, with a concrete lintel above. In the gable end is a square louvred vent. The west elevation has a single metal-framed window near its south end. The south elevation has a roller shutter door similar to that in the north elevation, with a door to the west, a door and window to the east, and a louvred opening in the gable end. The east elevation has a door at the south end and two windows, the northernmost being bricked up. This elevation was not closely examined (see Section 2.4).

The interior of this building reflects its former use as an abattoir. The floor is concrete, with an inset north-south drainage gully, and there are extant internal fittings for ventilation. The walls of the north-east corner are lined with galvanised metal sheet, and together with the open central area this was presumably used for butchery processes. Most of the eastern 'wing' of the building comprises a large cold store, with a small office and machine room (the latter accessed externally) to the south. In the south-east corner of the building is another small room, also accessed externally: this was not entered (see above). The iron-framed roof is carried on seven substantial east-west girders.



Plate 7: Building 1, north elevation looking south east



Plate 8: Building 1, west elevation looking east



Plate 9: Building 1, south elevation looking north-west



Plate 10: Building 1, east elevation looking north



Plate 11: Building 1, office area looking west



Plate 12: Building 1, interior looking south



Plate 13: Building 1, interior looking north



Plate 14: Building 1, cold store looking south

4.3 **Goods Yard, Building 2** (Fig. 17: Plates 15-20)

Location: TL 0953 2159

Description: This is a single cell, single-storey red brick building with a roof of clay pantiles, overall dimensions 3.80 × 4.40m, height to ridge 3.66m. The walls are constructed in English bond with cement mortar. There is a raised brick band four courses deep at the top of the walls, aligning with the concrete window and door lintels, and the roof ends overhang the building, with projecting roof joists. The upper parts of the walls, above window sill height, appear to have been rebuilt at some stage. The entrance is in the east gable end, set off-centre. In the south elevation are two windows of different widths. The west gable end has a single off-centre window, and the north elevation has no openings.

Internally, there is a suspended timber floor, and walls and ceiling are plastered. Set diagonally across the north-west corner is a chimney breast, with a blocked fireplace. The external chimney has been removed, presumably when the building received its present roof. On the west wall adjacent to the fireplace is a wall cupboard containing electrical equipment.



Plate 15: Building 2, east elevation looking west



Plate 16: Building 2, north elevation looking south

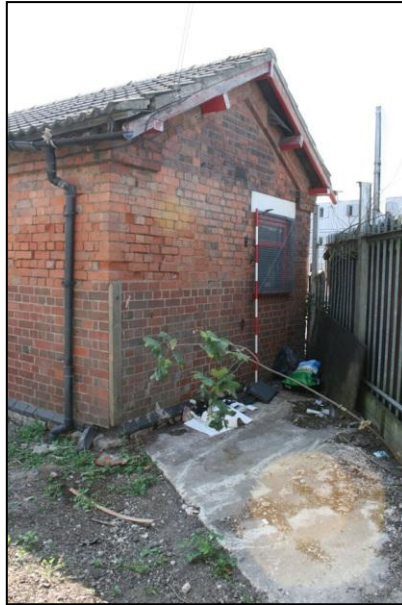


Plate 17: Building 2, west elevation looking south-east



Plate 18: Building 2, south elevation looking north



Plate 19: Building 2, interior, looking north west



Plate 20: Building 2, interior, looking south east

4.4 **Goods Yard, Building 3** (Plates 21-24)

Location: TL 0952 2157

Description: Single-storey brick building under a slate (imitation?) roof. Red brick, laid in English bond: brick plinth capped with engineering brick, above which are two bands of contrasting cream/buff brick, at sill and lintel height respectively. The east elevation has (from south to north) a door, two windows, two doors, four windows and a door: all openings are beneath cream/buff brick arched lintels. Doors are plank and batten, and appear original: windows are all modern replacements. There is a large window inserted in the north gable end, and a smaller window in the west elevation: the south elevation has no openings. The 1964 OS sheet (Fig. 13) seems to indicate that the building contains at least three rooms. The building's function is uncertain.



Plate 21: Building 3, east elevation looking west



Plate 22: Building 3, north elevation looking south



Plate 23: Building 3, west elevation looking east



Plate 24: Building 3, south elevation looking north-west

4.5 **Cougar Garage** (Figs 18 & 19: Plates 25-42)

Location: TL 0994 2130 (centre)

Description: Cougar Garage comprises a large structure of five bays, measuring 60.38 × 30.74m, of brick construction with a steel-framed roof. The Crescent Road (north-east) frontage of the building (Fig. 18) is constructed in Art Deco style, with four vehicle entrances (the centre two have been bricked up, with person-sized doors), each with a prominent fluted surround in red brick, set against the darker brick of the front wall, which has a tall plinth capped with red brick, a red brick band at door lintel height, and a darker brick parapet, 5.8m high, with concrete capping above. Only the plinth is carried around to the north-west and south-east elevations. The parapet of the westernmost bay is slightly lower than the adjoining bay, and the top of the surround of the vehicle entrance is flush with it, instead of rising above it, as it does in the other bays. In the centre of the north-east frontage is a small flat-roofed structure, accessed from inside the building. It was most recently used as a store: its original function is not known. At the north-west end is a two-storey flat-roofed block, the upper part of which is of noticeably modern construction. The south-east elevation has five large fixed two-light windows, covered by mesh screens: the north-west elevation is dominated by a fire escape leading to the upper floor, which has two modern 3-light casement windows and a fire door. The ground floor has several bricked-up windows and a door, which is bricked up internally. The rear of the building is dominated by a large, flat-roofed extension running most of its length: the lower part of this is plain brickwork, with cladding comprising large red ceramic tiles above, and grey metal (?) cladding to the gable ends of the roof. In the easternmost bay a large vehicle door with a roller shutter provides access by a steep concrete ramp to the rear yard, which is surrounded by a high security fence.

As noted above, the roof is steel-framed. It comprises a ridged roof over each bay, with a gable end to the south, and hipped to the north. Each bay has three sets of west-facing skylights except the westernmost bay, which has none, and a number of extractor vents rise above the spray booths on the centre bays.

Internally the building has a concrete floor throughout, and is subdivided and occupied by partitions and structures of relatively recent date. The easternmost bay is partitioned off from the rest of the building, which contains a bank of spray booths at its centre, and a row of offices along its west end, linked to the two-storey office block at the north-west corner. All the body shop machinery apart from the spray booths had been removed by the time of the survey, so with the exception of those features mentioned above, it was not possible to identify functions of separate areas and process flows. In the second bay from the east end a stairway leads to a very low-roofed passage leading to the rear of the building. The passage is too small for pedestrian access, and must have been used only for maintenance.

Cougar Garage first appears on Ordnance Survey mapping in 1938, and in 1964 it is labelled 'Depot'. Cougar Garage's most recent function is a car body repair shop, which opened in 1990.



Plate 25: Cougar Garage, north-east frontage, detail of vehicle access



Plate 26: Cougar Garage, north-east frontage, detail of former vehicle access



Plate 27: Cougar Garage, north-east frontage, detail of central flat-roofed structure



Plate 28: Cougar Garage, north-east frontage, detail of office block at north-west corner



Plate 29: Cougar Garage, south-east elevation, looking north-west



Plate 30: Cougar Garage, north-west elevation, looking south-east



Plate 31: Cougar Garage, rear elevation, looking north-east



Plate 32: Cougar Garage, rear elevation, looking north-east



Plate 33: Cougar Garage, easternmost bay, looking north-east



Plate 34: Cougar Garage, easternmost bay, looking south-west



Plate 35: Cougar Garage, central bays, looking south



Plate 36: Cougar Garage, central bays, looking south-east



Plate 37: Cougar Garage, central bays, looking north west



Plate 38: Cougar Garage, central bays, looking north-west to offices



Plate 39: Cougar Garage rear extension, looking north-west



Plate 40: Cougar Garage offices, looking south-west



Plate 41: Cougar Garage offices, looking east



Plate 42: Cougar Garage offices, modern spiral stairs to upper floor



Figure 15: Station House, plan and elevations (scale 1:125)
(location of plates shown in red)

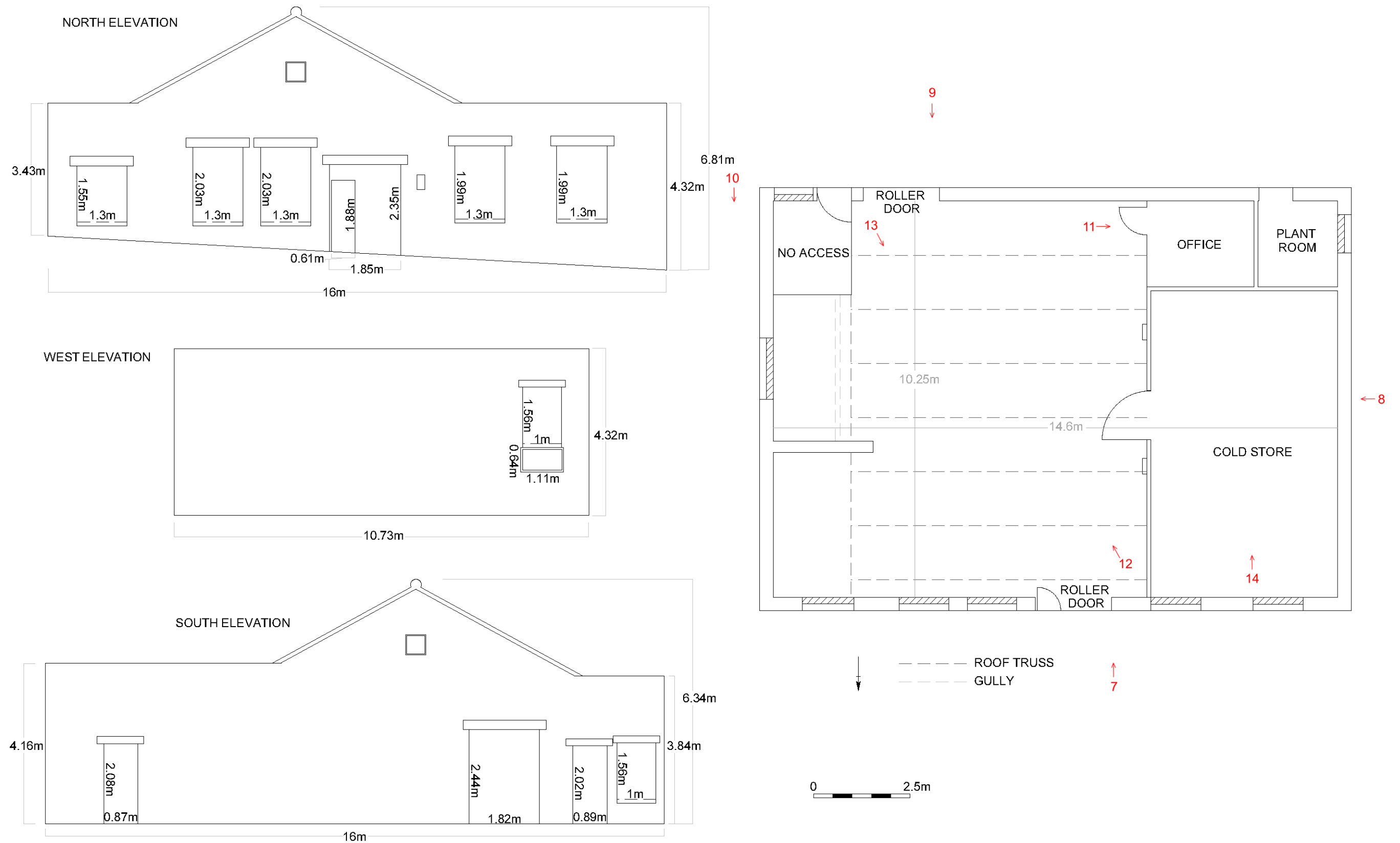


Figure 16: Goods Yard, Building 1: plan and elevations (scale 1:100)

(location of plates shown in red)

NB: due to health & safety issues (para. 2.4, above) detailed recording of the east side of the building was not possible

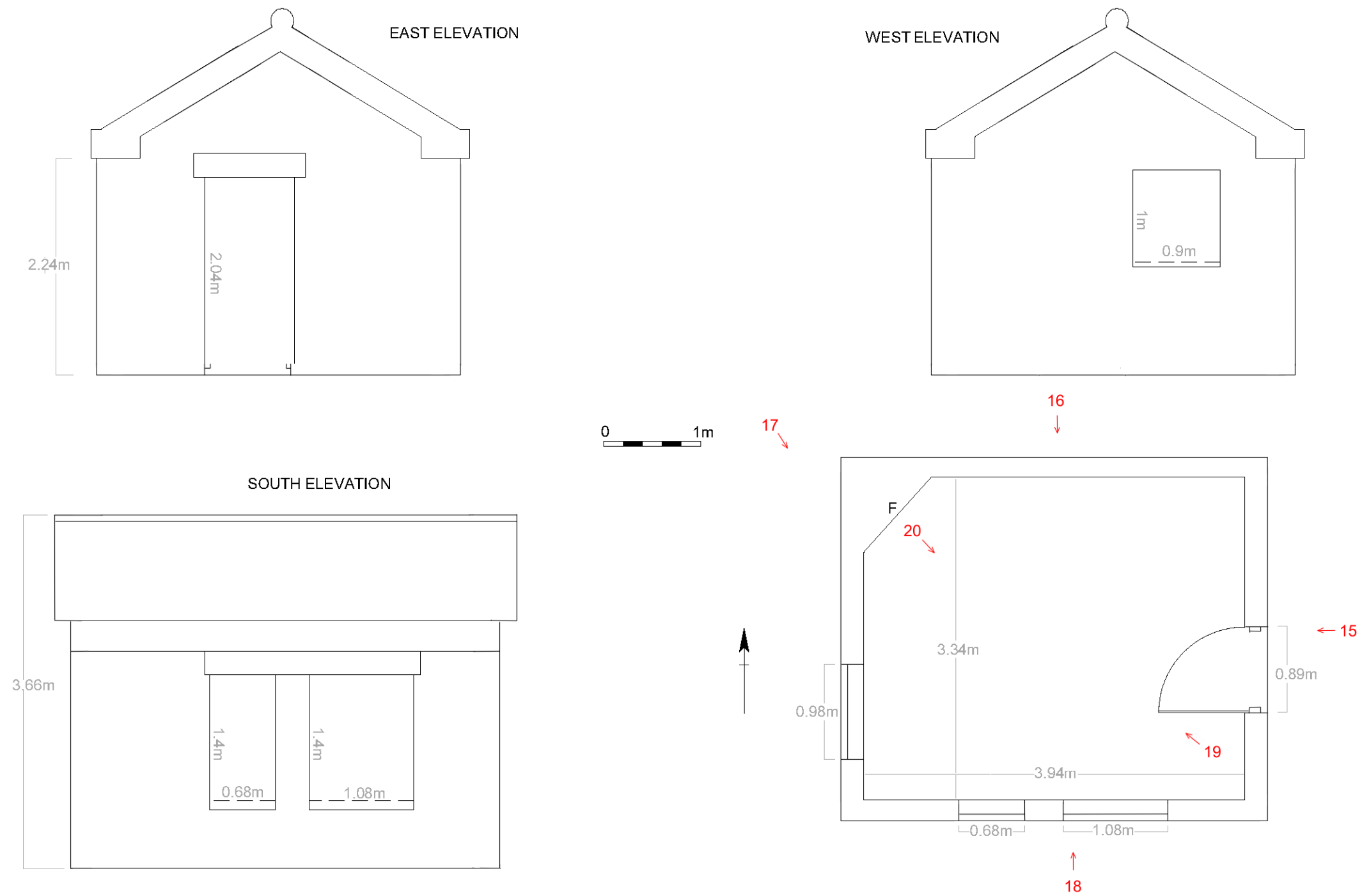


Figure 17: Goods Yard, Building 2: plan and elevations (scale 1: 50)
(location of plates shown in red)

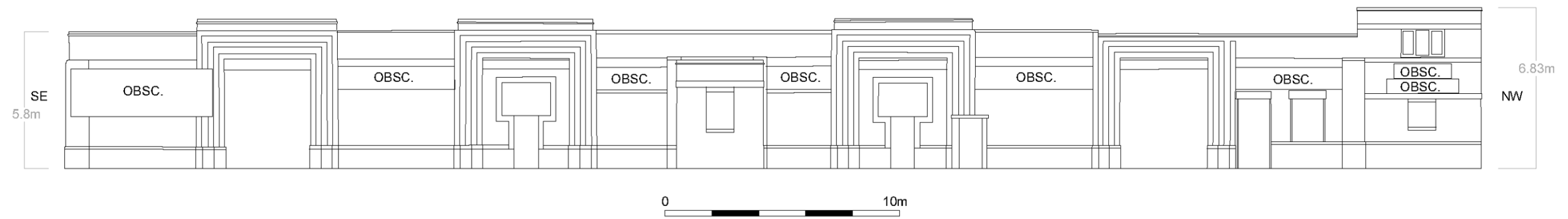


Figure 18: Cougar Garage, photo montage (*not to scale*) and elevation (*scale 1:100*) of frontage

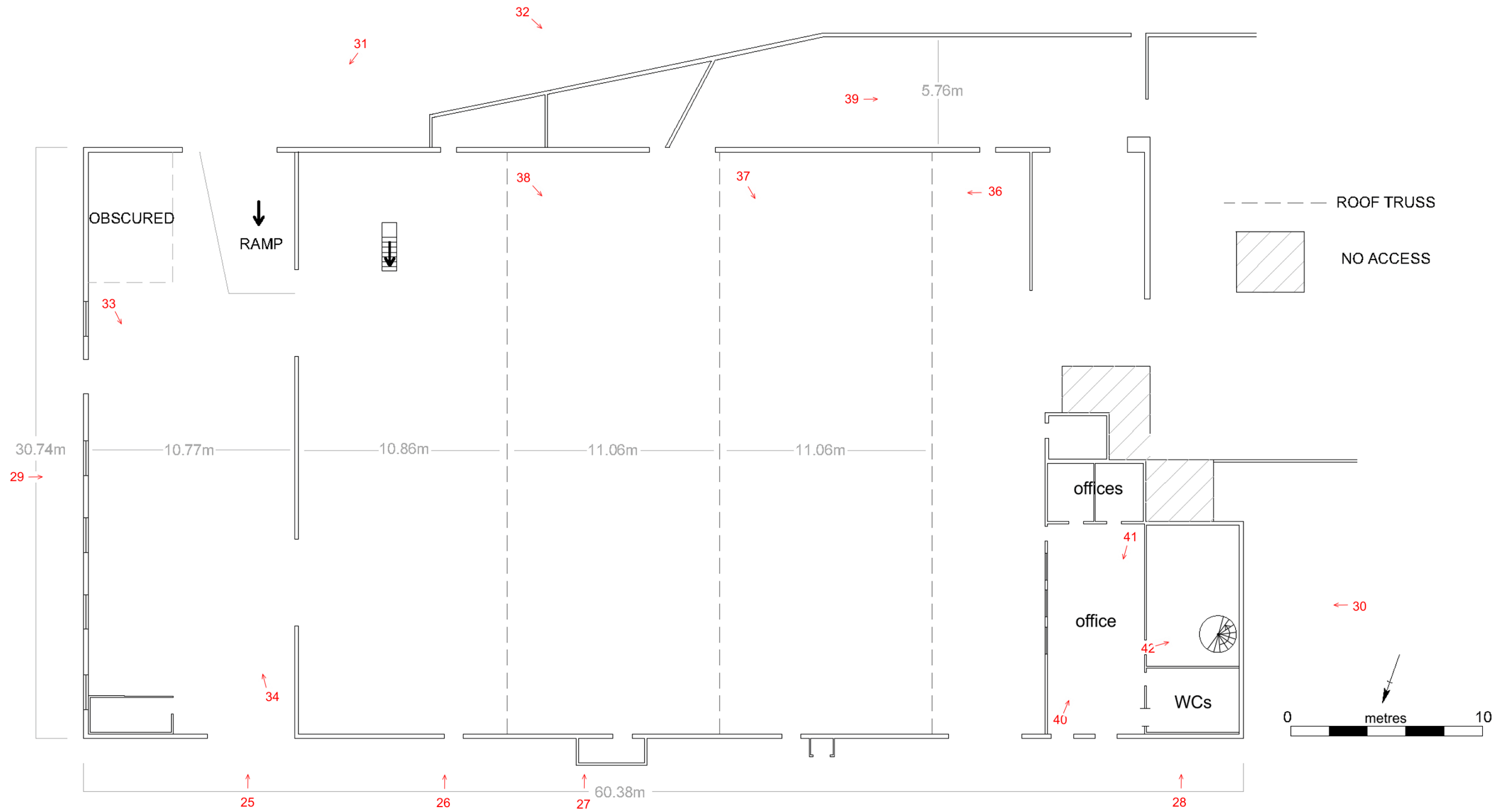


Figure 19: Cougar Garage, floor plan (scale 1:100)
(locations of plates shown in red)

NB: the extension of Cougar Garage west of the above was not being demolished, and was therefore not recorded.

5 Discussion & Conclusions

Five buildings threatened with or in close proximity to the proposed Luton Town Centre Transport Scheme were recorded in advance of the start of the development, which will involve total demolition of three of them, Buildings 1 and 2 in the Goods Yard and the Cougar Garage in Crescent Road.

Station House

Located south of the junction of Midland Road and High Town Road, this is a small double-fronted detached villa of typical mid-19th-century style, with a more recent extension to the rear. The date of the extension is uncertain: it does not appear on the available (pre-1964) Ordnance Survey mapping, which shows a smaller extension and yard within the same footprint, but its style and slate roof suggest a later 19th-century date. This discrepancy could just possibly be the result of a failure by the Ordnance Survey to re-survey after the extension was built. Station House predates the coming of the railway to Luton in 1860, having been constructed as a private dwelling on the southern edge of the Hightown district of Luton. Following the opening of the Hertford, Luton and Dunstable Railway it became the station master's house, remaining as such until 1969. It is now a solicitor's office. Recording was limited to the exterior of the building.

Goods Yard, Buildings 1-3

Building 1, located on Crescent Road, comprises a single-storey brick structure with a central section under a ridged corrugated sheet roof, with pent roof 'wings' built in similar materials on its east and west sides. The stock bricks and concrete window lintels used in its construction suggest a 20th-century date, and the building first appears on Ordnance Survey mapping in 1924. Most of the interior comprises a single open space. Internal features include a large cold room, associated machine room and a small office, all in the western 'wing'. The building was last used as a pet food store, and before that an abattoir: its original function is unknown.

Building 2, located at the Goods Yard entrance on Church Road, is a single-cell brick structure beneath a roof of clay tiles, which appear to be modern. Inside it has a suspended timber floor, and a small fireplace in the north-west corner. The building first appears on Ordnance Survey mapping in 1901. Cartographic evidence reveals it a weigh house, controlling traffic to and from the Goods Yard. Historic mapping shows the accompanying weighbridge to have been on its south side: no evidence for this remains.

Building 3 is a long, narrow structure to the south of the Goods Yard entrance. Its construction, comprising orange-red bricks set in English bond, with detailing in cream brick and a slate roof, suggests a late 19th or early 20th-century date. Like Building 2, this building is first shown on Ordnance Survey mapping for 1901. No details of its past function were found: it is currently used by Network Rail, and is not affected by the road scheme.

Cougar Garage

Located at the junction of Crescent Road and Crawley Green Road, this is a large brick structure of five bays beneath steel-framed, ridged roofs, hipped at the north end and gabled to the south. Its Crescent Road frontage is constructed in Art Deco style, and has four vehicular entrances, the centre two now reduced in size, all with prominent surrounds constructed in brick, and with brick detailing on the walls between. At the north-west end is a two-storey, flat-roofed office block, the upper storey of which appears to be no more than 20 or 30 years old. The upper part of the rear elevation of the building is clad in large red ceramic tiles,

while the gables above have grey sheet cladding. Internally the building has a concrete floor throughout, and is subdivided and occupied by partitions and structures of relatively recent date. Cougar Garage first appears on Ordnance Survey mapping in 1938, and on the 1964 edition sheet it is labelled 'Depot'. The building became 'Cougar Garage' – actually a car body repair shop – in 1990, and it is likely that some of the more modern alterations and additions date from this time. There is no evidence to show that the building has always been associated with the automobile trade: its 1964 designation 'Depot', the large size of the building and the absence of typical 'garage' features (e.g. forecourt, showrooms) suggest its function has changed over time. The form of the Cougar building, a large, single-storey structure with a number of doors providing access for large commercial vehicles, and relatively little office space, is reminiscent of a bus garage or possibly a distribution warehouse, though its original function remains uncertain.

Conclusion

The five buildings examined in this survey all relate to the main period of Luton's growth, from the development of Hightown in the first half of the 19th century, to the early and middle years of the 20th century. Station House predates the coming of the railways to Luton, though it later became directly associated with them. Buildings 2 and 3 are directly concerned with the development of the Midland Railway's goods yard at the end of the 19th century: Building 1 dates from the inter-war period, and though it stands on the north side of the Goods Yard its relationship with the railway, if any, is unknown. The present Cougar Garage also dates from the inter-war period, and is almost certainly purpose-built, though for some sort of service or transport-related structure, rather than a conventional garage. As such, all contribute in their own way to the history of the development of modern Luton, though none is of any great significance.

6 Acknowledgements

The project was commissioned by Pell Frischmann on behalf of Luton Borough Council. The writer is grateful to David Homan of Pell Frischmann for his assistance. The project was monitored by the Central Bedfordshire Council Archaeologists on behalf of the local planning authority. Thanks are also due to Henry Abel, senior engineer for the contractors, Volker Fitzpatrick, and his assistant Sam Hussey, for their assistance and hospitality.

The project was managed for ASC by Bob Zeepvat BA MI(A). Fieldwork was carried out by Bob Zeepvat and Calli Rouse BA AIA. The report was prepared by Calli Rouse and Bob Zeepvat and edited by David Fell BA MI(A).

7 Archive

7.1 The building recording archive will comprise:

1. Brief
2. Project Design
3. Report
4. Historical & Survey notes
5. Survey drawings
6. List of photographs
7. B/W prints
8. B/W negatives
9. CDROM with copies of all digital files.

7.2 The building recording and watching brief archives will be combined, and deposited with Luton Museum.

8 References

Standards & Specifications

- ALGAO 2003 *Standards for Field Archaeology in the East of England*. East Anglian Archaeology Occasional Paper **14**.
- EH 2006a *Understanding Historic Buildings: a guide to good recording practice*. English Heritage (London).
- EH 2006b *Management of Research Projects in the Historic Environment (MoRPHE)*. English Heritage.
- IFA 2010 Institute for Archaeologists' *Code of Conduct*.
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- Soil Survey 1983 *1:250,000 Soil Map of England and Wales, and accompanying legend* (Harpenden).

Maps

- 1842 Luton Town Map (*Luton Local Studies Library*)
- 1855 Map of Luton (*Luton Local Studies Library*)
- 1880 Ordnance Survey First Edition 25" sheet (www.oldmaps.co.uk)
- 1901 Ordnance Survey Second Edition 25" sheet (www.oldmaps.co.uk)
- 1924 Ordnance Survey 25" sheet (www.oldmaps.co.uk)
- 1938 Ordnance Survey 25" sheet, XXXIII.6
- 1964 Ordnance Survey 1:2500 sheet, TL 0921 NE
- 1976 Ordnance Survey 1:2500 sheet, TL 0921 NE
- Modern digital Ordnance Survey mapping

Appendix 1: List of Photographs

SITE NO/CODE: 1616/LTC			Site Name: Luton Town Centre Transport Scheme
Shot	B&W	Digital	Subject
1	x	x	Building 1, exterior, looking south east
2	x	x	Building 1, exterior, looking north west
3	x	x	Building 1, interior, looking south east
4	x	x	Building 1, interior, looking south west
5	x	x	Building 1, interior, looking north west
6	x	x	Building 1, interior, looking north east
7	x	x	Building 1, interior, roof structure
8	x	x	Building 1, interior, office area, looking north west
9	x	x	Building 1, interior, cold store, looking north west
10	x	x	Building 1, exterior, looking south east
11	x	x	Building 1, exterior, looking north north east
12	x	x	Building 1, exterior, looking north
13	x	x	Building 1, exterior, looking north
14	x	x	Building 2, exterior, looking west
15	x	x	Building 2, exterior, looking south
16	x	x	Building 2, exterior, looking south east
17	x	x	Building 2, interior, looking north west
18	x	x	Building 2, interior, looking south east
19	x	x	Building 2, interior, looking south
20	x	x	Cougar Garage, exterior, looking south west
21	x	x	Cougar Garage, exterior, looking south west
22	x	x	Cougar Garage, exterior, looking south west
23	x	x	Cougar Garage, exterior, looking south west
24	x	x	Cougar Garage, exterior, looking south west
25	x	x	Cougar Garage, exterior, looking south west
26	x	x	Cougar Garage, exterior, looking south west
27	x	x	Cougar Garage, exterior, looking south west
28	x	x	Cougar Garage, exterior, looking south west
29	x	x	Cougar Garage, exterior, looking south west
30	x	x	Cougar Garage, exterior, looking south west
31	x	x	Cougar Garage, exterior, looking south west
32	x	x	Cougar Garage, exterior, looking south east
33	x	x	Cougar Garage, exterior, looking north west
34	x	x	Cougar Garage, exterior, looking north west
35	x	x	Cougar Garage, exterior, looking north west
36	x	x	Cougar Garage, exterior, looking north west
37	x	x	Cougar Garage, exterior, looking north east
38	x	x	Building 3, exterior, looking south south west
39	x	x	Building 3, exterior, looking east south east

40	x	x	Building 3, exterior, looking east south east
41	x	x	Building 2, exterior, looking north east
42	x	x	Building 3, exterior, looking north west
43	x	x	Building 3, exterior, looking north
44	x	x	Cougar Garage, Interior, south east end, looking north east
45	x	x	Cougar Garage, Interior, south east end, looking east
46	x	x	Cougar Garage, Interior, south east end, looking south west
47	x	x	Cougar Garage, Interior, south east end, looking south west
48	x	x	Cougar Garage, Interior, south east end, looking west
49	x	x	Cougar Garage, Interior, south east end, looking north west
50	x	x	Cougar Garage, Interior, main area, looking south
51	x	x	Cougar Garage, Interior, main area, looking east
52	x	x	Cougar Garage, Interior, main area, looking south east
53	x	x	Cougar Garage, Interior, main area, looking south west
54	x	x	Cougar Garage, Interior, main area, looking north
55	x	x	Cougar Garage, Interior, main area, looking north
56	x	x	Cougar Garage, Interior, main area, looking north east
57	x	x	Cougar Garage, Interior, offices, looking south west
58	x	x	Cougar Garage, Interior, offices, looking south west
59	x	x	Cougar Garage, Interior, offices, looking south west
60	x	x	Cougar Garage, Interior, offices, looking west
61	x	x	Cougar Garage, Interior, offices, looking north
62	x	x	Cougar Garage, Interior, offices, looking east
63	x	x	Cougar Garage, Interior, offices, looking north west
64	x	x	Cougar Garage, Interior, north west bay, looking south east
65	x	x	Cougar Garage, Interior, north west bay, looking south
66	x	x	Cougar Garage, Interior, north west bay, looking south west
67	x	x	Cougar Garage, Interior, north west bay, looking north west
68	x	x	Cougar Garage, Interior, north west bay, looking north east
69	x	x	Cougar Garage, Interior, north west bay, looking east
70	x	x	Cougar Garage, Interior, link area, looking south
71	x	x	Cougar Garage, Interior, link area, looking south west
72	x	x	Cougar Garage, Interior, link area, looking west
73	x	x	Cougar Garage, Interior, link area, looking north west
74	x	x	Cougar Garage, Interior, link area, looking south west
75	x	x	Cougar Garage, rear exterior, looking south west
76	x	x	Cougar Garage, rear exterior, looking north
77	x	x	Cougar Garage, rear exterior, looking north east
78	x	x	Cougar Garage, rear exterior, looking north east
79	x	x	Cougar Garage, rear exterior, looking north east
80	x	x	Cougar Garage, rear exterior, looking north east
81	x	x	Cougar Garage, rear exterior, looking north east
82	x	x	Cougar Garage, rear exterior, looking north east

83	x	x	Cougar Garage, Interior, south east end, roof structure
84	x	x	Cougar Garage, Interior, south east end, roof structure
85	x	x	Station House, exterior, looking south west
86	x	x	Station House, exterior, looking west
87	x	x	Station House, exterior, looking east
88	x	x	Station House, exterior, looking north west
89	x	x	Station House, exterior, looking north east
90	x	x	Station House, exterior, looking north west
91	x	x	Station House, exterior, looking north west
92	x	x	Station House, exterior, looking south east
93	x	x	Station House, exterior, looking south east
94	x	x	Station House, exterior, looking south east
95	x	x	Station House, exterior, looking north east

A CDROM containing copies of the digital photos listed above is included in the back cover of this report

Appendix 2: ASC OASIS Form

PROJECT DETAILS			
Project Name:	Luton Town Centre Transport Scheme	OASIS reference:	151719
Short Description:	<p>In July 2013 historic building recording was undertaken of five buildings directly affected by, or in close proximity to, the proposed Luton Town Centre Transport Scheme. The buildings comprised Station House in Midland Road, three small structures within the former Midland Railway goods yard, off Church Street and Crescent Road, and Cougar Garage, at the junction of Crescent Road and Crawley Green Road.</p> <p>Station House is a small double-fronted detached house of typical mid-19th-century style, with a more modern extension to the rear. It predates the coming of the railway to Luton in 1860, having been constructed as a private dwelling on the southern edge of the Hightown district of Luton. Following the opening of the Hertford, Luton and Dunstable Railway it became the station master's house, remaining as such until 1969. It is now a solicitor's office. Recording was limited to the exterior of the building.</p> <p>Of the three Goods Yard buildings, Building 1, located on Crescent Road, is the largest, comprising a single-storey brick structure with a central section under a ridged corrugated sheet roof, with pent roof 'wings' built in similar materials on its east and west sides. The stock bricks and concrete window lintels used in its construction suggest a 20th-century date, and the building first appears on Ordnance Survey mapping in 1924. Internal features include a large cold room in the western 'wing' and a small office, most of the interior comprising a single open space. The building was last used as a pet food store, and before that an abattoir.</p> <p>Building 2 is a single-cell brick structure beneath a roof of clay tiles, located at the Goods Yard entrance. Inside it has a small fireplace in the north-west corner. The building first appears on Ordnance Survey mapping in 1901. Cartographic evidence reveals it was a weigh house, controlling traffic to and from the Goods Yard.</p> <p>Building 3 is a long, narrow building to the south of the Goods Yard entrance, adjacent to Church Road. Its construction, comprising orange-red bricks set in English bond, with detailing in cream bricks and a slate roof, suggests a late 19th or early 20th-century date: the building is first shown on Ordnance Survey mapping for 1901. No details of its past functions were found: it is currently used by Network Rail. Although identified originally for internal and external recording (as Buildings 1 & 2), only an external photographic record of Building 3 was made when it became apparent that it was not affected by the road scheme.</p> <p>Cougar Garage is a large brick structure of five bays, beneath steel framed, glazed ridged roofs. A taller linked extension to the west, of similar construction, is apparently not scheduled for demolition. The Crescent Road frontage, constructed in a style reminiscent of Art Deco, comprises four vehicular entrances, the centre two now reduced in size, all with prominent surrounds constructed in brick, and with brick detailing on the walls between. At the north-west end an upper storey has been added to the offices in that area: the materials used suggest a late 20th-century date for this. The upper part of the rear of the building is clad in large red ceramic tiles. Internally the building has a concrete floor throughout, and is subdivided and occupied by partitions and structures of relatively recent date. The entrances into the adjoining structure suggest that this extension is of relatively recent date. Cougar Garage first appears on Ordnance Survey mapping in 1938, and in 1964 it is labelled 'Depot'. On cartographic evidence the north-west extension was added in the latter half of the 20th century. Cougar Garage's most recent function is a car body repair shop: the 1964 designation 'Depot', the size of the building and the absence of typical 'garage' features may suggest its function has changed over time.</p>		
Project Type:	Building recording (5 buildings)		
Previous work:	no	Site status:	none
Current land use:	Commercial	Future work:	no
Monument type:	n/a	Monument period:	19 th & 20 th century
Significant finds:	n/a		
PROJECT LOCATION			
County:	Bedfordshire	OS reference: (8 figs min)	TL 0905 2170 – 0978 2115
Site address:	Luton Town Centre Transport Scheme		
Study area: (sq. m. / ha)	n/a	Height OD: (metres)	n/a
PROJECT CREATORS			

Organisation:	Archaeological Services & Consultancy Ltd		
Project brief originator:	Hannah Firth, CBCA	Project design originator:	Bob Zeepvat BA MIFA
Project Manager:	Bob Zeepvat BA MIFA	Director/Supervisor:	Bob Zeepvat BA MIFA
Sponsor / funding body:	Luton Borough Council		
PROJECT DATE			
Start date:	July 2013	End date:	August 2013
PROJECT ARCHIVES			
	Location (Accession no.)	Content (eg. pottery, animal bone, files/sheets)	
Physical:	Luton Museum	none	
Paper:		Brief, project design, site drawings/notes/photos, report	
Digital:		CD with all digital files	
BIBLIOGRAPHY (Journal/monograph, published or forthcoming, or unpublished client report)			
Title:	Historic Building Recording: Luton Town Centre Transport Scheme		
Serial title & volume:	ASC Ltd Report ref. 1616/LTC/2		
Author(s):	Calli Rouse BA MIFA & Bob Zeepvat BA MIFA		
Page nos	52	Date:	8 th August 2013