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Advice on Archaeology & Planning

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Field Evaluation & Excavation

Historic Building Recording

Site & Landscape Survey

Geophysical Survey

**Temporary Bus Terminal
250m North West of the A823 and A9 Junction,
Gleneagles,
Perth & Kinross.**

Archaeological Evaluation

Report No. 3024

CFA ARCHAEOLOGY LTD

The Old Engine House
Eskmills Business Park
Musselburgh
East Lothian
EH21 7PQ

Tel: 0131 273 4380
Fax: 0131 273 4381
email: info@cfa-archaeology.co.uk
web: www.cfa-archaeology.co.uk

Author	Ian Suddaby MA FSA Scot MifA
Illustrator	Kevin Hicks BA FSA Scot
Editor	Bruce Glendinning BSc PgDip MifA
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This document has been prepared in accordance with CFA Archaeology Ltd
standard operating procedures.

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1. INTRODUCTION

1.1 General

This report presents the results of an archaeological evaluation undertaken by CFA Archaeology Ltd (CFA) in May 2013 within the area of a temporary bus terminal to serve the 2014 Ryder Cup at Gleneagles, Perth & Kinross (Planning ref: 12/01770/FLL) (Fig.1 inset). The work was commissioned by Ryder Cup Europe Ltd.

A Written Scheme of Investigation (WSI) for the evaluation was produced by CFA. This included an indicative trench layout and was approved in advance by Perth & Kinross Heritage Trust (PKHT). The WSI was based on a Terms of Reference (TOR) for the work which was produced by PKHT. A 10% trial trenching evaluation was required. This report covers the results of the works in accordance with the WSI.

1.2 Background

Planning permission has been granted by Perth and Kinross Council for the formation of a temporary bus terminal and associated works at Land 250 Metres North West of A823 and A9 Junction, Gleneagles (12/01770/FLL).

The development site is considered to be archaeologically sensitive given the adjacent Loaninghead Fort Scheduled Monument (SM no. 7593) and given the density of recorded archaeological sites within the surrounding landscape, many of which have also been designed as nationally important scheduled monuments. Loaninghead Fort, a multi-vallate prehistoric hillfort, is visible as a series of upstanding earthworks in the trees to the south-west of Area 1. The monument lies at around 150m AOD, on a natural spur overlooking the valley that links Strathallan to Strathearn.

1.3 Objectives

The objectives of the project were:

- To conduct an archaeological trial trenching evaluation in order to establish the presence/absence, extent, condition, character, quality and date of any archaeological features or deposits within the proposed development area and establish the vulnerability of any archaeological features to the proposed development;
- To produce a data structure report on the results of the evaluation;
- To provide suitable mitigation for further archaeological works if the results merit it.

2. WORKING METHODS

2.1 General

Work was conducted with regard to the Institute for Archaeologists' (IfA) *Standard and Guidance for an Archaeological Evaluation and Code of Conduct*.

2.2 Evaluation Strategy

The site was divided into 3 areas (Areas 1-3). Area 1 consists of a pasture field. Area 2 consists of a materials storage area surrounded by bunded topsoil. Area 3 consists of undulating ground with areas of gorse. It was agreed in advance with PKHT that no trenching would take place in Area 2.

A 10% trial trenching evaluation was required. Areas 1 and 3 total 1.15ha with a 10% sample covering 1150m². Twenty trenches (Trenches 1-20) amounting to 1139m² were excavated (Fig. 1).

In Area 3, onsite discussions with William Dunbar (Chartered Civil & Structural Engineer) and Scott Fenwick (Golf Courses and Estates Manager at Gleneagles) led to changes in the proposed trench layout. These changes aimed to concentrate the trenches in the areas of greatest disturbance and to minimise any safety risk to the golf course users by avoiding excavating trenches close to the buggy track which runs along the northern edge of Area 3.

The trenches were excavated by machine under direct archaeological supervision to remove topsoil and other soil deposits down to natural subsoil or the first significant archaeological horizon, whichever was reached first.

Samples of all features of archaeological interest were hand excavated in order to establish their likely date, nature, extent and condition.

All hand excavation and on-site recording was carried out according to standard CFA procedures, principally by drawing, by digital photography and by completing standard CFA record forms.

Trench locations were recorded using industry standard surveying equipment and the trenches were backfilled on completion of recording.

3. ARCHAEOLOGICAL RESULTS

3.1 General

Appendix 1 contains a tabulated summary of all the trenches. The remainder of the site records are described in Appendices 2-5. Finds are quantified in Appendix 6.

Topsoil was generally 0.2-0.3m deep, with a pinkish silty sand & gravel/cobbles subsoil present in much of Area 1 and in Trenches 16-17 in Area 3. Natural substrate consisted of, in the main, free-draining silty sands and gravels.

Twenty trenches (Fig. 1) were excavated, 13 in Area 1 and 7 in Area 3.

Shallow linear ditches and shallow pits were identified in Area 1. Area 3 contained modern made ground and the natural subsoil in the north of the area had been truncated in recent years.

3.2 Evaluation Area 1

Area 1 consists of a grass field adjacent to Loaninghead Fort (Fig. 2). No archaeological remains were found in Trenches 2-3, 5, 7-13.

Trench 1

In Trench 1, two parallel linear features (**101**, **103**, Fig. 3) were recorded. Both were aligned NE-SW and were filled with a homogeneous pinkish-brown silty sand with occasional gravel (**102**, **104**). Both ditches were 0.75m wide and they were 2.5m apart. A section (Fig. 4) showed **103** to be <0.1m deep. No finds were recovered.

Trench 4

In Trench 4, a pit (**401**) and a linear ditch (**403**) were recorded (Fig. 5). Both were sealed by subsoil (**002**). Pit **401** measured 1.7m by 1.3m and it was aligned ESE-WNW. A section (Fig. 6) showed it to be 0.1m deep. It was filled with a pinkish-grey cobble-rich silty sand (**402**). No finds were recovered.

Ditch **403** was 0.9m wide and it was aligned NE-SW. It was filled with a mottled pinkish-grey silty sand with gravel/cobbles, flecked with charcoal (**404**). A sherd of prehistoric pottery was recovered and a section (Fig. 6) showed it to be 0.1m deep.

Trench 6

In Trench 6, a single pit feature (**601**) was recorded (Fig. 7). The feature measured 1.2m by 0.75m. It was filled with a grey-black silty sand with cobbles (**603**) over a grey-pink silty sand with cobbles (**602**). No finds were recovered and the feature was not excavated.

3.3 Evaluation Area 3

Area 3 is undulating and uncultivated with stands of gorse and broom (Fig. 8). No archaeological remains were found.

In Trenches 14-16 and 18-20, there was evidence that the sandy gravel natural had been cut away in recent times (**1401, 1801, 1901, 2002**), truncating the natural surface, and the ground level then made up with re-deposited sandy gravels (**1402, 1501, 1601, 1901, 2001**). The cut extended to the south, probably under the banded topsoil which currently surrounds the materials storage area (Area 2).

At the western end of Trench 16, under thin topsoil (**001**), this re-deposited sandy gravel made ground (**1601**) was removed by machine (Fig. 10), revealing a dump of substantial pine tree trunks and similar tree stumps in a matrix of peaty soil (**1602**). At the eastern end of the trench, the made ground (**1602**) overlay a buried soil (**1603**) which overlay subsoil (**002**) and natural (**003**). The depth of the trench was 1.8m. The section is shown in Fig. 11.

At the southern end of Trench 18, this large modern cut (**1801**) was also filled with a deposit of substantial pine tree trunks in a matrix of peaty soil (**1803**) under a layer of re-deposited sandy gravels (**1802**). In the northern end of the trench, the natural gravels were truncated (**1805**) and overlain by alternating layers of topsoil and sandy gravels (**1804**), increasing in depth to the north.

In the northern end of Trench 19, a break of slope in the natural gravels (**1902**) was overlain by alternating layers of topsoil and sandy gravel made ground (**1903**), increasing in depth to the north (Fig. 12).

3.4 Finds, by Melanie Johnson

A single plain body sherd of handmade prehistoric pottery, weighing 11g, was recovered from context **404**. The fabric is thick and coarse, with sooting on the interior, and the sherd is very abraded, having lost much of its outer surface. The level of abrasion indicates that this sherd has likely been subject to post-depositional movement. It is not possible to assign a date to the sherd.

4. CONCLUSIONS

Twenty trenches were excavated at the proposed temporary bus terminal, amounting to 1139m² (c.10% of the available area).

In Area 1, archaeological features were confined to Trenches 1, 4 and 6. Three linear ditches and two oval pits were recorded, all cut into natural and sealed by subsoil where this was present. The ditches were all aligned NE-SW. One sherd of prehistoric pottery was recovered. All of the excavated features were shallow.

In Area 3, the evaluation indicated that the level of the natural had been cut away in places and recent made ground was present, either dumped onto the existing ground surface or acting as levelling material to fill up the cut. The evaluation also indicated that all of the ground to the north of Trenches 18-20 has been recently truncated. A surface vegetation change supports this as dense stands of gorse are not present in the north of the area. Local information suggests that this may be the result of pathway excavations on the golf course dating to c.1995, resulting in an underpass.

It is understood that the final decision regarding any further work in mitigation rests with PKHT on behalf of the planning authority.

The project archive, comprising all CFA record sheets, maps and reports will be deposited with the Royal Commission on the Ancient and Historical Monuments of Scotland (RCAHMS) and copies of reports will be lodged with PKHT.

A summary statement of the results of this evaluation will be submitted for publication in *Discovery and Excavation in Scotland* and an online OASIS entry submitted on completion of the project.

APPENDIX 1: Summary of Evaluation Trenches

Trench No.	Size (m)	Depth of topsoil	Description
1	30 x 1.8	0.3m topsoil / 0.1m of subsoil at E end	2 ditches NE-SW
2	30 x 1.8	0.1-0.2m of gravel / 0.3m topsoil / 0.1m of subsoil	No Archaeological remains
3	30 x 1.8	0.3m topsoil / 0.1m of subsoil	No Archaeological remains
4	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil	1 pit and 1 ditch NE-SW. Ditch contained 1 sherd of prehistoric pottery
5	30 x 1.8	0.3m topsoil / 0.1m of subsoil	No Archaeological remains
6	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil	1 pit
7	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil	No Archaeological remains
8	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil	No Archaeological remains
9	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil	No Archaeological remains
10	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil	No Archaeological remains
11	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil	No Archaeological remains
12	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil at E end	No Archaeological remains
13	30 x 1.8	0.3m topsoil / 0.1-0.2m of subsoil	No Archaeological remains
14	30 x 1.8	0.3m topsoil	Cut for possible quarry
15	30 x 1.8	0.35m topsoil / 0.05m of subsoil	Made ground within possible sand quarry
16	30 x 1.8	0.4m topsoil / 0.1m of subsoil	Made ground consisting of cut pine trees under gravel capping
17	50 x 1.8	0.3m topsoil / 0.2m of subsoil	No Archaeological remains
18	34 x 1.8	0.3m topsoil / 0.2m of subsoil	Made ground consisting of cut pine trees under gravel capping within cut at South end. Natural truncated for underpass at North end.
19	34 x 1.8	0.3m topsoil / 0.2m of subsoil	Made ground consisting of gravel at South end. Natural truncated for underpass at North end.
20	35 x 1.8	0.3m topsoil / 0.1m of subsoil	Natural probably truncated for underpass at North end

APPENDIX 2: Context Register

Context	Fill of	Trench	Description
001		All	Topsoil. Mostly mid brown silty sand with occasional cobbles
002		Many	Subsoil under 001. Pinkish-grey silty sand with small cobbles
003		All	Natural subsoil. Pinkish silty sand with gravel in Area 1 Orange-pink or orange sandy gravels in Area 3 except Trench 14 which contained laminated sands & fine gravels
004		2-3	Modern crushed gravel over 001
101		1	Cut for NE-SW ditch
102	101	1	Pinkish-brown silty sand with gravel & cobbles
103		1	Cut for NE-SW ditch
104	103	1	Pinkish-brown silty sand with gravel & cobbles
401		4	Cut for egg/oval shape pit
402	401	4	Grey-pink silty sand with cobbles
403		4	Cut for NE-SW ditch
404	403	4	Grey-pink silty sand with cobbles
601		6	Cut for egg/oval shape pit
602	601	6	Grey-pink silty sand with cobbles
603	601	6	Grey-black silty sand upper fill
1401		14	Cut, possible quarry pit
1402	1401	14	Pinkish-brown sand & gravel
1501		15	Pinkish-brown sand & gravel
1601		16	Pinkish-brown sand & gravel
1602		16	Mixture of cut pine trees and peaty topsoil under 1601
1801		18	Cut, possible quarry pit
1802	1801	18	Pinkish-brown sand & gravel
1803	1801	18	Mixture of cut pine trees and peaty topsoil under 1802
1804	1805	18	Layers of slightly peaty topsoil and pinkish-grey sandy gravels
1805		18	Truncated natural for the golf course underpass to the N of Area 3
1901		19	Truncated natural for the golf course underpass to the N of Area 3
1902	1901	19	Layers of slightly peaty topsoil and pinkish-grey sandy gravels
1903		19	Pinkish-brown sand & gravel not clearly in a cut
2001		20	Layers of slightly peaty topsoil and pinkish-grey sandy gravels. Not clearly in a cut
2002	2001	20	Truncated natural for the golf course underpass to the N of Area 3

APPENDIX 3: Photographic Register

Photo	Contexts/Description	From	Conditions
1-8	Pre-ex views of Areas 1-3	Various	Overcast
9-10	Area 1, trenches excavated apart from Trench 1	N	Overcast
11	Trench 7, general view	W	Overcast
12	Trench 8, general view	NE	Overcast
13	Trench 9, general view	NNE	Overcast
14	Trench 10, general view	S	Overcast
15	Trench 11, general view	S	Overcast
16	Trench 12, general view	WSW	Overcast
17	Trench 13, general view	NNW	Overcast
18-19	Trench 3, general view	S	Overcast
20	Trench 5, general view	S	Overcast
21	Trench 4, general view	E	Overcast
22	Trench 4, pit 401 and ditch 403	E	Overcast
23	Trench 4, pit 401 plan view	N	Overcast
24	Trench 4, ditch 403 plan view	N	Overcast
25	Trench 4, ditch 403 plan view	NE	Overcast
26	Trench 4, ditch 403 section	NE	Overcast
27	Trench 4, N facing soil section	N	Overcast
28-29	Trench 4, pit 401 section	SW	Overcast
30	Trench 6, general view	E	Overcast
31-32	Trench 6, pit 601	E	Overcast
33	Trench 6, pit 601 plan view	N	Overcast
34	Trench 2, general view	E	Overcast
35-36	Trenches, 2, 4, 6, general view	ENE	Overcast
37	Trench 2, soil profile at E end	W	Overcast
38	Trench 1, general view	E	Overcast
39	Trench 1, ditches 101, 103	E	Overcast
40	Trench 1, ditches 101, 103	NE	Overcast
41-42	Trench 1, ditch 103 section	NE	Sun/Shade
43-45	General views of the area containing made ground	E to N	Bright
46-48	General views of Area 3	W	Bright
49	Trench 17, general view	NNW	Overcast
50	Trench 17, general view	SSE	Bright
51	Trench 14, general view	ENE	Bright
52	Trench 14, showing 1401-2	WSW	Bright
53	Trench 15, excavated to made ground 1501	WSW	Bright
54	Trench 16, general view	WSW	Bright
55	Trench 16, timbers in made ground 1602	WSW	Bright
56	Trench 16, general view	ENE	Bright
57-58	Trench 16, soil section to natural at ENE end	NE & NNW	Bright
59	Trench 16, view of 1601-2 at WSW end	NE	Bright
60	Trench 16, view of deep section at ENE end	NE	Bright
61-62	Trench 18, view of cut 1801 and fills 1802-3 at SE end. Ranging road is aligned with the cut	SE	Bright
63	Trench 18, soil section at N end	SE	Bright
64	Trench 18, soil section at N end	WNW	Bright
65	Trench 19, soil profile at N end	SE	Overcast
66	Trench 19, general view	NNW	Overcast
67	Trench 20, soil profile in 2002 at N end	S	Overcast
68	Trench 20, soil profile in 2002 at N end	SSE	Overcast
69	Trench 20, general view	SSE	Overcast
70-71	Area 3, backfilling shots	S	Overcast

APPENDIX 4: Drawings Register

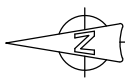
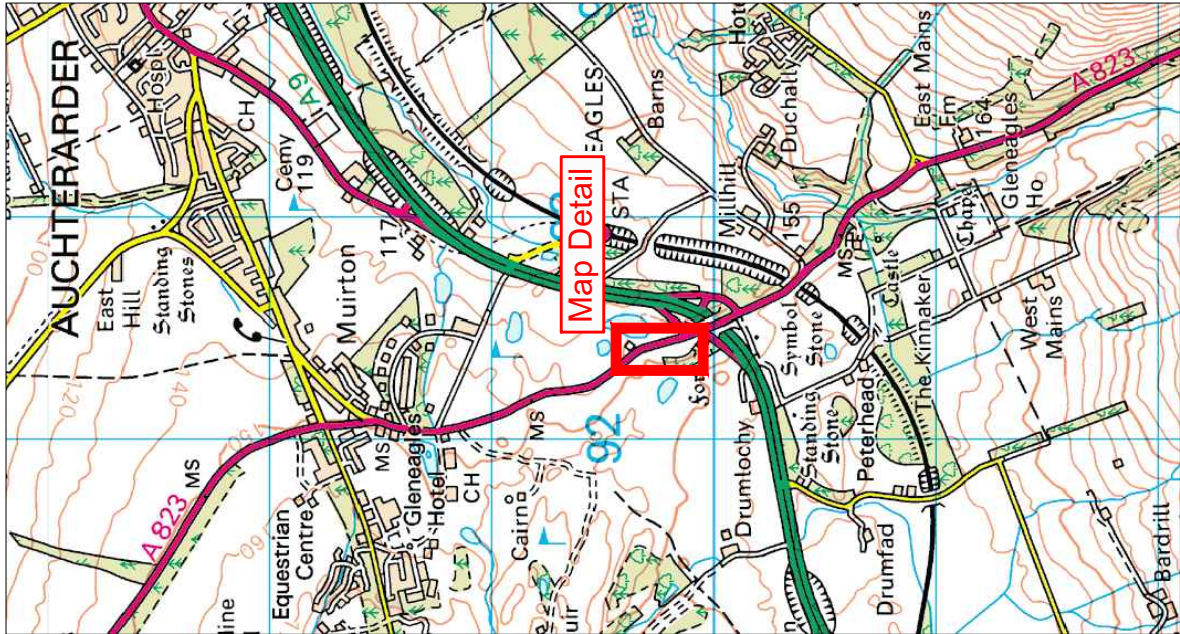
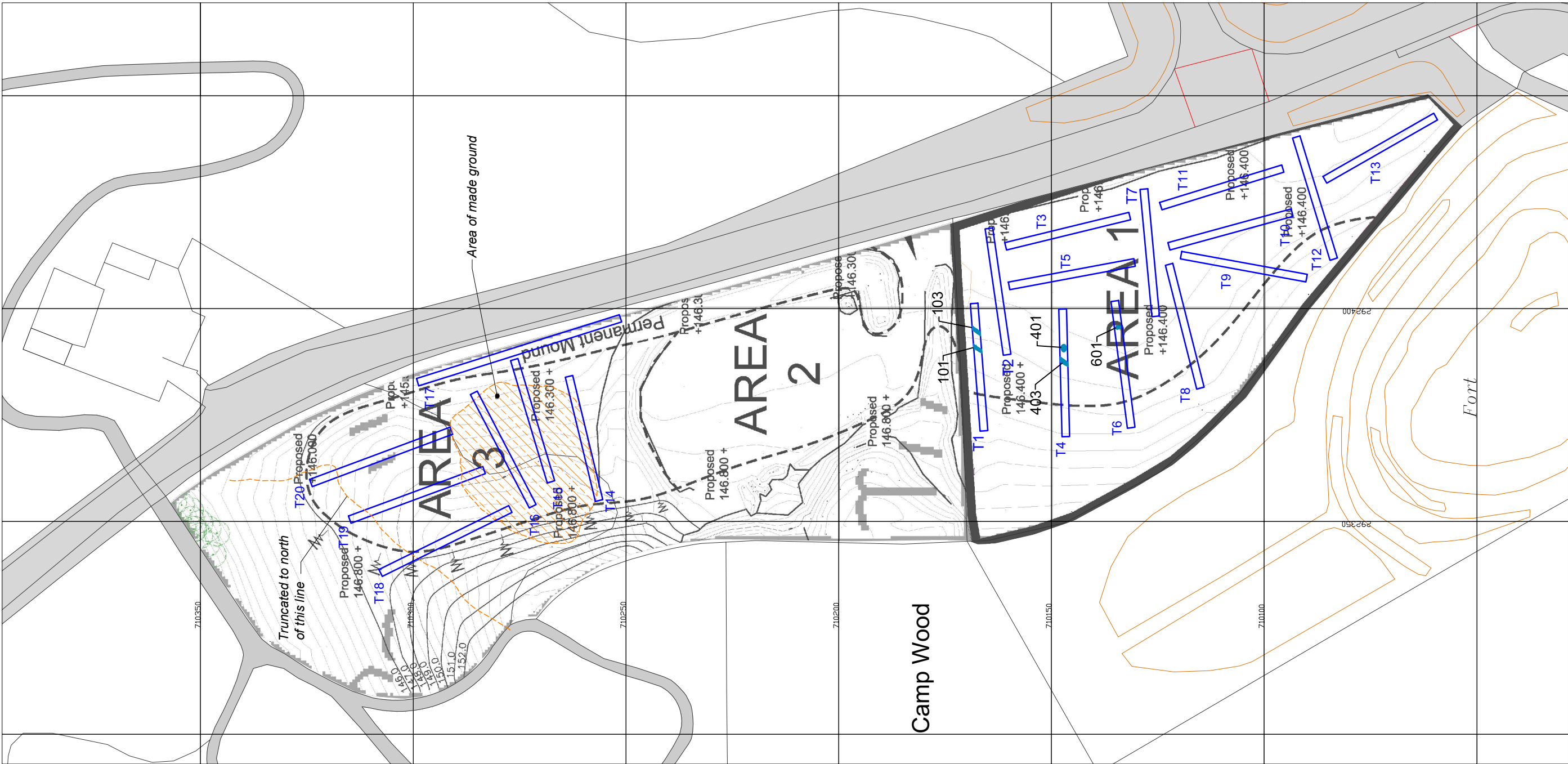
Number	Trench	Scale	Description
1	4	1:50	Trench 4, plan
2	4	1:20	Trench 4, pit 401 section
3	4	1:10	Trench 4, ditch 403 section
4	6	1:50	Trench 6, plan
5	1	1:50	Trench 1, plan
6	1	1:10	Trench 1, ditch 103 section
7	14	1:100	Trench 14, plan
8	16	1:100	Trench 16, plan
9	18	1:100	Trench 18, plan
10	19	1:100	Trench 19, plan
11	20	1:100	Trench 20, plan

APPENDIX 5: Sample Register

Sample	Context	Volume	Description
1	104	8L	Fill of ditch 103 adjacent to prehistoric pottery

APPENDIX 6: Discovery & Excavation in Scotland Entry

LOCAL AUTHORITY:	Perth & Kinross
PROJECT TITLE/SITE NAME:	Temporary Bus Terminal 250m North West of the A823 and A9 Junction, Gleneagles, Perth & Kinross
PROJECT CODE:	RYDE
PARISH:	Blackford
NAME OF CONTRIBUTOR:	Ian Suddaby
NAME OF ORGANISATION:	CFA Archaeology Ltd
TYPE(S) OF PROJECT:	Evaluation
NMRS NO(S):	None
SITE/MONUMENT TYPE(S):	Ditches, Pits
SIGNIFICANT FINDS:	None
NGR	NN 9238 1019
START DATE (this season)	May 2013
END DATE (this season)	May 2013
PREVIOUS WORK (incl. <i>DES</i> ref.)	None
MAIN (NARRATIVE) DESCRIPTION: (May include information from other fields)	An archaeological excavation was carried out prior to the formation of a temporary bus terminal to accommodate visitors to the 2014 Ryder Cup. Twenty trenches were excavated, comprising 1139m ² . In the south of the site, adjacent to Loaninghead Fort (SM 7593), shallow ditches and two pits were recorded. One sherd of prehistoric pottery was recovered. In the north of the area, modern deposits and features relating to the golf course were recorded.
PROPOSED FUTURE WORK:	Post-excavation analysis
CAPTION(S) FOR ILLUSTRS:	N/A
SPONSOR OR FUNDING BODY:	Ryder Cup Europe Ltd
ADDRESS OF MAIN CONTRIBUTOR:	The Old Engine House, Eskmills Park, Musselburgh, EH21 7PQ
EMAIL ADDRESS:	cfa@cfa-archaeology.co.uk
ARCHIVE LOCATION (intended/deposited)	Royal Commission on the Ancient and Historical Monuments of Scotland Perth & Kinross Council Sites and Monuments Record



CFA ARCHAEOLOGY LTD
The Old Engine House
Eskmills Park
Musselburgh
East Lothian, EH21 7PQ
t: 0131 273 4380
f: 0131 273 4381
e: info@cfa-archaeology.co.uk
w: www.cfa-archaeology.co.uk

Fig. No: 1	Revision: A	Drawn by: KH	Checked: BG	Report No: 3024	Client: Ryder Cup Europe Ltd
Title: Location map (inset) and plan of the trenches in Area 1			Project: Temporary Bus Terminal 250m North West of the A823 and A9 Junction, Gleneagles, Perth & Kinross		Scale at A3: 1:1000





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Fig. 2 - General view of Areas 1 and 2 from the north-west



Fig. 3 - Trench 1, ditches 101, 103 from the east

Fig. No: 2-3		Revision: A	Project: Temporary Bus Terminal 250m North West of the A823 and A9 Junction, Gleneagles, Perth & Kinross			CFA ARCHAEOLOGY LTD The Old Engine House Eskmills Park, Musselburgh East Lothian, EH21 7PQ T: 0131 273 4380 F: 0131 273 4381 e: info@cfa-archaeology.co.uk w: www.cfa-archaeology.co.uk
Drawn by: KH	Checked: BG	Report No: 3024	Client: Ryder Cup Europe Ltd			

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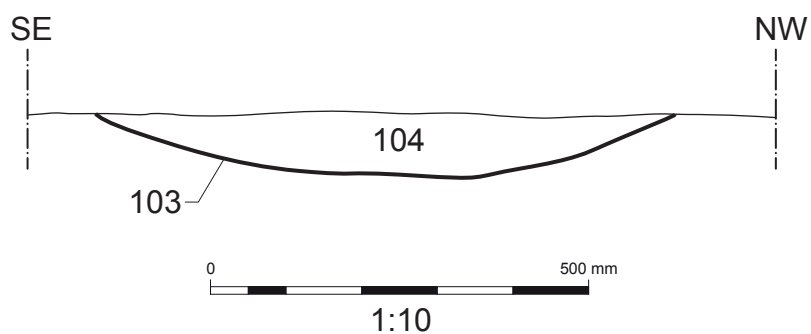


Fig. 4 - Trench 1, ditch 103 north-east-facing section



Fig. 5 - Trench 4, showing pit 401 and ditch 403 in mid-trench

Fig. No: 4-5		Revision: A	Project: Temporary Bus Terminal 250m North West of the A823 and A9 Junction, Gleneagles, Perth & Kinross	REGISTERED ifA ORGANISATION	CFA ARCHAEOLOGY LTD The Old Engine House Eskmills Park, Musselburgh East Lothian, EH21 7PQ T: 0131 273 4380 F: 0131 273 4381 e: info@cfa-archaeology.co.uk w: www.cfa-archaeology.co.uk
Drawn by: KH	Checked: BG	Report No: 3024	Client: Ryder Cup Europe Ltd		

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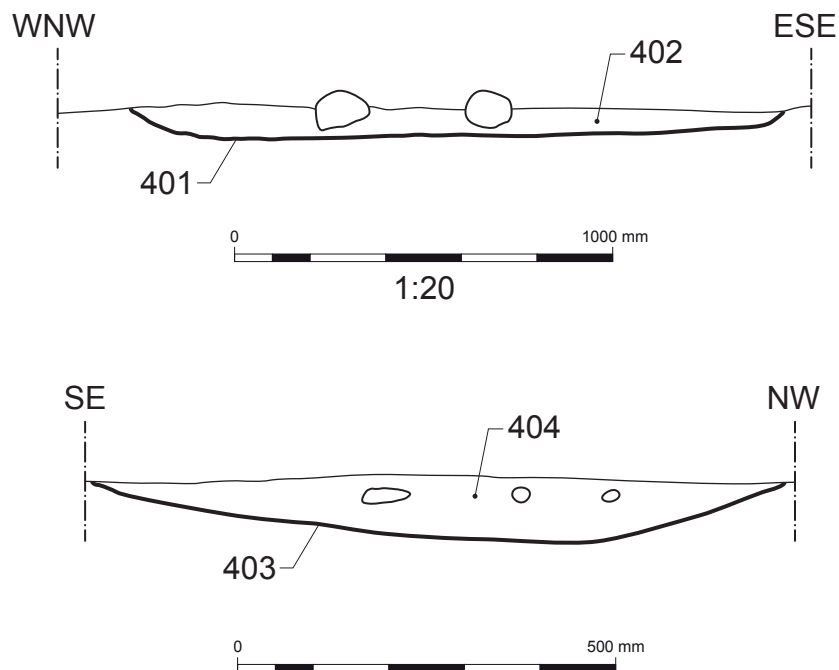


Fig. 6 - Trench 4, pit 401 and ditch 403 sections

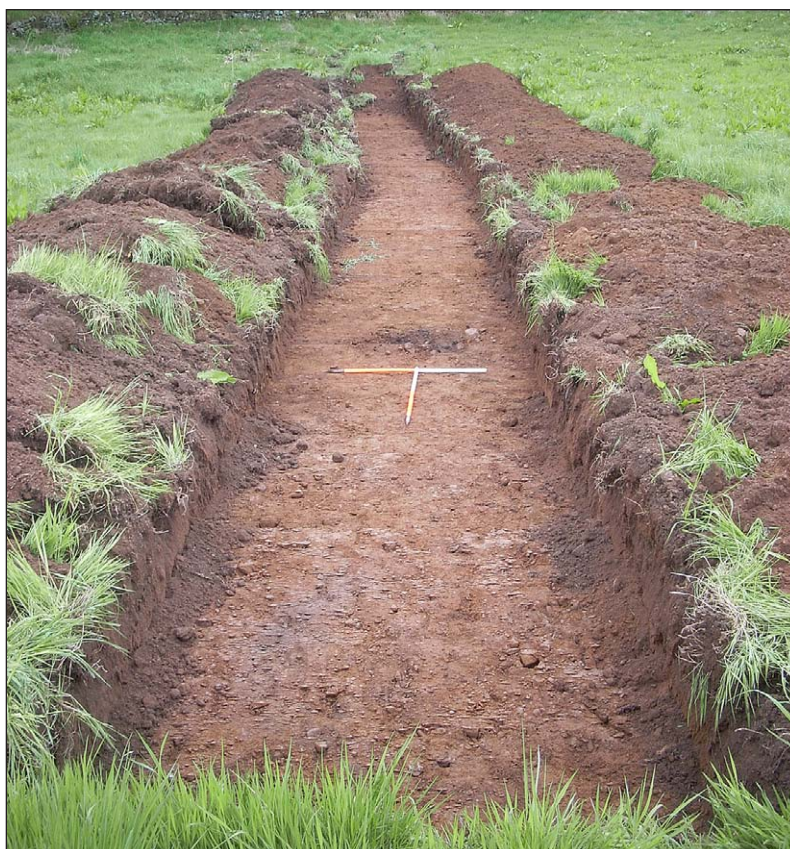


Fig. 7 - Trench 6, showing pit 601 in mid-trench

Fig. No: 6-7		Revision: A	Project: Temporary Bus Terminal 250m North West of the A823 and A9 Junction, Gleneagles, Perth & Kinross	REGISTERED IfA ORGANISATION	CFA ARCHAEOLOGY LTD The Old Engine House Eskmills Park, Musselburgh East Lothian, EH21 7PQ T: 0131 273 4380 F: 0131 273 4381 e: info@cfa-archaeology.co.uk w: www.cfa-archaeology.co.uk
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Fig. 8 - General view of Area 3 from the west



Fig. 9 - Trench 16, dumped trees and soil (1602) capped with gravel (1601)

Fig. No: 8-9		Revision: A	Project: Temporary Bus Terminal 250m North West of the A823 and A9 Junction, Gleneagles, Perth & Kinross			CFA ARCHAEOLOGY LTD The Old Engine House Eskmills Park, Musselburgh East Lothian, EH21 7PQ T: 0131 273 4380 F: 0131 273 4381 e: info@cfa-archaeology.co.uk w: www.cfa-archaeology.co.uk
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Fig. 10 - Trench 16, made ground (1601-2) overlying an orange-brown soil (1603) in the base of the section





Fig. 11 - Trench 19. Break of slope at the northern end where the natural subsoil has been truncated by the cutting for the underpass

Fig. No: 10-11		Revision: A	Project: Temporary Bus Terminal 250m North West of the A823 and A9 Junction, Gleneagles, Perth & Kinross	REGISTERED ORGANISATION 	 CFA ARCHAEOLOGY LTD	CFA ARCHAEOLOGY LTD The Old Engine House Eskmills Park, Musselburgh East Lothian, EH21 7PQ T: 0131 273 4380 F: 0131 273 4381 e: info@cfa-archaeology.co.uk w: www.cfa-archaeology.co.uk
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Fig. 12 - Trench 20. Break of slope at the northern end where the natural subsoil has been truncated by the cutting for the underpass

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