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Avon Place, Linlithgow:

Boundary Wall Survey

Report No 1635

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1. INTRODUCTION

1.1 General

1.1.1 This report presents the results of a structural survey undertaken in May 2009 by CFA Archaeology Ltd (CFA) on a boundary wall enclosing the site of a former coal siding at Avon Place, Linlithgow, West Lothian (NGR: NT 00203 76990 centred on) Fig 1. This work was part of an ongoing larger scope of archaeological works that include test pitting and an archaeological watching brief which will be reported on in due course.

1.1.2 The recording work was commissioned by EK:JN Architects in order to fulfill a planning condition for archaeological works prior to the redevelopment of the site. Although the boundary wall is not listed it is considered by West Lothian Council's Conservation Officer to make a substantial contribution to the character of the town's conservation area. West of Scotland Archaeology Service (WoSAS) requested that a Level 2 survey be carried out on the wall prior to its take-down and re-alignment. A Written Scheme of Investigation produced by CFA was approved by WoSAS prior to the commencement of the work.

1.2 Background

1.2.1 The site is presently a triangular-shaped vacant plot of land that has previously been used as a coal siding for the Edinburgh-Glasgow railway and is separated from Avon Place by a stone wall on the south side. The proposed erection of an office building on the site will require the take-down and realignment of the wall. The section to be taken down measures approximately 49m in length and varies in height and is topped by hewn coping stones. The rubble-built wall has been heavily repaired over the years and has been finished in parts with cement pointing. Blocking work is also visible.

1.2.2 Local tradition states that it is through a boundary opening somewhere in the vicinity of the proposed development site that Hamilton of Bothwellhaugh escaped after the assassination of Regent Moray in 1570. Tradition states that a lintel had been removed in advance from a boundary wall to allow the swift exit of horse and rider.

1.3 Objectives

1.3.1 The aims of the programme of archaeological works were:

1. to carry out a desk-based assessment of the cartographic sources to inform the relative age of the boundary wall and inform the archaeological watching brief;
2. to produce a Level 2 (English Heritage 2006) record of the wall prior to its take-down;
3. to carry out a watching brief on the site prior to its redevelopment;
4. to produce an illustrated data structure report on the archaeological results.

2. WORKING METHODS

2.1 General

- 2.1.1 CFA Archaeology Ltd follows the Institute for Field Archaeologists' Code of Conduct, and Standards and Guidelines for Historic Building Surveys as Appropriate.

2.2 Desk-based Assessment

- 2.2.1 The desk-based assessment is based on an abridged version of the historical report produced by Stuart Edemann which was considered sufficient for a Level 2 building survey report. Other historical information obtained from EK:JN Architects has also been included in this report. Cartographic sources have also been examined and selected extracts have been used.

2.3 Building Survey

- 2.3.1 A photographic record of the north and south-facing elevations of the boundary wall were made using a Nikon D100 professional SLR digital and 35mm camera. The wall outline was recorded using an industry-standard EDM to create a wire-frame in which to rectify the photographs. General characteristics of wall fabric were recorded using standard building recording forms. Alterations in height, width and character of build were also recorded.
- 2.3.2 The survey was carried out in good weather. At the west end of the north-facing elevation, trees partially masked the wall but access could still be gained to obtain measurements and to take photographs.

3. DESK-BASED ASSESSMENT RESULTS

3.1 Burgage plots

3.1.1 Linlithgow developed along the linear High Street with long rig gardens to the rear of the houses. The house plots were of a roughly standard width and length, which gave each an area of around one rood (1/4 acre). On the south side of the High Street frontages 8 yards wide resulted in gardens of around 150 yards in length (c.45m). Over time adjacent plots were combined to allow for larger houses and gardens and, particularly with tenement dwellings or where house building extended into the gardens, plots were often subdivided to provide separate space for individual owners (Edemann 2009). Dennison and Coleman (2000) mention the Town Council minutes of October 1635 which record the gathering of ‘vagabonds and beggars at the yearheadis’, the latter being the end of the burgage plots; all beggars were to be banished and the burgesses were instructed to build up the walls at the end of the plots.

3.2 Cartographic evidence

3.2.1 Edemann (op cit) notes that Slezer’s late seventeenth-century depiction of the town (Fig 2a) from the south suggests extensive, if relatively low, stone walls erected along the line of the back lane and viewed as a whole, the structure could be taken as constituting a non-defensive ‘town wall’. In common with other Scottish medieval towns, the burgh was never surrounded with strong walls although gates or ports were established at key entry and exit points to control the movement of livestock, goods and persons. The standardised plots and protected yard head resulted in a regular southern edge to the town and a south vennel or back lane developed as an alternative route west to east avoiding the High Street.

3.2.2 Armstrong’s map of 1773 (Fig 2b) depicts West Wynd, which was formally created in 1692 when the council purchased a vacant plot of land. The map depicts the boundary wall with a distinct outshot break in the alignment of the wall close to the current pedestrian underpass. Whether this break marks a formal opening is not clear.

3.2.3 John Wood’s Town Plan of 1820 (Fig 2c) shows the ‘back road’ running along the south edge of the town. No breaks in the boundary wall are shown on this map. The town plan names the property owners in that year. West Wynd was renamed Dogwell Wynd.

3.2.4 The 1856 Ordnance Survey plan of the town (Fig 2d) shows the proposed development site was then a railway siding. The land immediately to the south was a coal yard with a weighing machine and it is assumed that the siding was associated with supplying and transferring coal by road rail and the Union Canal.

3.2.5 The 1897 Ordnance Survey plan (Fig 2e) depicts two small structures on the coal siding site each directly opposite the houses on Avon Place. It seems unlikely that these were

for domestic use but rather were sheds associated with the coal handling operations carried out on the site.

3.3 Historical information

- 3.3.1 Historical research carried out EK:JN Architects on the 19th-century ownership of properties sold to the Edinburgh and Glasgow Railway Company between 1840 and 1843 is shown in Fig 3. The names of the vendors and the year each plot was sold are also depicted.

4. SURVEY RESULTS

4.1 General

4.1.1 Both the north and south-facing elevations were recorded (Figs 4a-b). A list of the photographs taken are presented in Appendix 1. The principal features including fabric type and blocked openings were afforded a unique identifying number which is shown on Figs 4a-b and listed in Table 1.

Feature	Summary description	Dimensions/extent	Approximate age
1	Section of dressed stonework demarcated by a large crack where it abuts the earlier wall.	Six courses of stugged sandstone on the corner of the pedestrian railway underpass	19th century
2	Semi-circular copings on average 0.50m by 0.32m .	Continues along the length of the wall	18th-19th century
3	Inserted doorway with brick surrounds surmounted by two lintels using sections of railway line. Doorway blocked by tongue-and grooved planks	1.6m high and 1.36m wide. Splayed surrounds 0.34m deep.	20th century
4	Blocked doorway infilled with random rubble. Roughly dressed quoins partly visible behind heavy pointing	1.72m high and 0.90m wide	18th century
5	Blocked opening with chamfer-moulded surrounds. Infilled with nine courses of sandstone. The chamfer-moulded elements are hidden behind modern (20th-century) pointing work.	1.52m high and 0.93m wide	18th century
6	Section of wall which is bowed out	3.5m wide and 3.2m high	18th century
7	Section of wall with a combination of random rubble and whinstone boulders. Heavily rendered with cement.		18th century
8	Section of new build with regular stone blocks and fresh looking mortar		20th century

Table 1. Principal recorded features shown on Figs 4a-b.

4.2 Summary descriptions of the wall and features

South-facing elevation (Fig 4a)

4.2.1 The south facing elevation is 59m long and stands to a height of 2.2m at the east end. The wall drops to 1.6m at the modern galvanized iron gate and 1m at the west end. A section of rebuild is present at the east end and is represented by six courses of rough dressed sandstone (1), which is in stark contrast to the rest of the wall of random rubble. Semi-circular hewn sandstone copings (2) have been used throughout its length.

4.2.2 Three blocked openings are present. Feature (3) appears to be a possible 20th-century insert with brick-surrounds and lintels using sections of railway line. The doorway is

blocked by timber planking and its brick surrounds have been rendered with cement. To the west of this is another blocked opening (4) which is infilled with blocks of randomly laid rubble. Another blocked opening (5) has chamfer-moulded surrounds which are heavily masked by cement render. Its infill contains nine courses of coursed sandstone laid with lime mortar. The wall fabric beyond this blocked opening is uniform in terms of its content and is a heavily pointed random rubble build.

North-facing elevation (Fig 4b)

- 4.2.3 The north-facing elevation is higher than the south-facing elevation owing to the difference ground levels. The north-elevation is 3.2m high providing a height difference of 1m. The principal features include blocked openings 3, 4 and 5. To the west of opening 3 the wall is heavily pointed and is constructed with an assortment of random rubble. Below the blocked doorway (3) there are the remains of brick-built steps with a threshold measuring 1.5m and 1.3m and 0.5m high. The brick surrounds of the doorway are more visible on the right-hand side. The section of wall between blocked doors 3-4 has a greater quantity of whinstone boulders. Blocked doors 4 and 5 are visible by their blocking work and the surrounds on each side. To the right of blocked door (5) there is a noticeable thickening and projection of the random rubble stonework which is 3.5m wide and 3.2m high (6). Beyond this section the wall is fairly uniform with no visible breaks in construction throughout its length (7). On the right-hand side of the galvanized-steel gateway, the section of wall has been rebuilt recently (8). Here the wall includes more regular rectangular blocks of sandstone and its appearance is fresh, strongly suggesting that this section has indeed been rebuilt during the last ten years.

5. DISCUSSION

- 5.1 The wall is fairly uniform in build, but it has been much repaired over the years. It is very difficult to establish when these repairs were carried out without detailed examination of historical accounts, which may or may not be available for scrutiny. The wall incorporates both squared blocks of masonry and naturally sub-rounded and rounded whinstone. The source of the latter may have been collection from local field clearance material and brought to the site during primary construction.
- 5.2 The break in construction at the eastern terminus of the wall where well-dressed stonework meets random rubble with great non-conformity is suggested to date from the construction of the pedestrian railway underpass, installed by 1842. The Linlithgow section of the Edinburgh to Glasgow railway was completed by this year (Butt 1995).
- 5.3 Door opening (3) is a secondary feature placed in the wall perhaps when the site was used as railway siding. The door surrounds incorporate brickwork which, based on their 'industrial' character, would strongly suggest a late 19th or early 20th-century date for insertion. The use of three sections of standard gauge railway line for the lintel support strongly suggests the latter.
- 5.4 Assigning a chronology for the door openings (4) and (5) is more difficult but the use of chamfer moulding in the door surrounds for opening 4 points towards the 18th century or earlier. These doorways probably served as formal yardhead entries into the properties they served.
- 5.5 Slezer's 17th-century depiction of the walls surrounding Linlithgow does not show the presence of formal entries into the properties. Despite this, bearing in mind the non-fortified nature of the wall, it is likely that formal rear yard entrances would have been common at the end of each property. It appears that there was no martial intent with the construction of the Linlithgow yardhead walls. McKean (2001) stated that Scotland's towns were not walled for serious defence, and cites Bishop Lesley (1527–96) who, on the Scots' dislike of being confined within walls, said 'our towns we fortify not with walls, save first the borders of the realm, thereafter the fields, but where need is, they fence about and defend, with the force of their body and armour, according to the laws of their elders'.

6. CONCLUSION

- 6.1 The Level 2 survey carried out on the boundary wall has recorded its architectural character prior to its demolition and re-alignment. The wall is constructed of randomly-coursed rubble throughout its length with the exception of two short sections of rebuild at each end of the wall. The first section is associated with the construction of the pedestrian railway underpass in c.1845 and the second is a more recent section immediately on the west side of the modern site 1 gate. The survey has also shown that the wall height is greater on the north side of the wall and relates to site reduction probably related to the construction of the Edinburgh to Glasgow railway.
- 6.2 Three blocked openings are present. The most recent is of probable 19th or 20th-century date and has been inserted into the wall possibly when the site became a railway siding. Two other blocked openings are of an earlier date and are likely to belong to the 18th century.
- 6.3 There is no evidence within the section of wall for the creation of an artificial opening made to assist the escape by Hamilton of Bothwellhaugh after his assassination of the Earl of Moray in 1570. The present wall appears to largely date to the 18th century and has been much repaired and re-built during the 19th and 20th centuries.

7. RECOMMENDATIONS

- 7.1 The section of wall designated for demolition and re-alignment has been recorded in sufficient detail commensurate with a Level 2 structural survey and no other further recording work on the wall is required.
- 7.2 Following the walls take-down, the depth of its foundations will be recorded in four test-pits in accord with the Written Scheme of Investigation.
- 7.3 A report in *Discovery and Excavation in Scotland* (Appendix 3) will disseminate the results of this work and include a report on the watching briefs at a later stage.

8. REFERENCES

Bibliographic

Butt, R.V.J. 1995. *The Directory of Railway Stations: details every public and private passenger station, halt, platform and stopping place, past and present* (1st Edition ed.). Sparkford: Patrick Stephens Ltd.

Dennison, E. P. and Coleman, R 2000 *Historic Linlithgow: the Scottish burgh survey*.

Edemann, S 2009 Unpublished research on Linlithgow.

English Heritage 2006, *Understanding Historic Buildings – A guide to good recording practice*.

McKean, C 2001 *The Scottish Chateau The Country House of Renaissance Scotland*.

Cartographic

Ordnance Survey 1856 Town Plan of Linlithgow 1856 Scale 1:1056 Sheet 3

Ordnance Survey 1897 Airdrie Sheet 31

Slezer, J 1693 The Prospect and Town of Linlithgow

Wood, J 1820 Town Plan of Linlithgow

APPENDIX 1: DIGITAL PHOTOGRAPHIC RECORD

Photo No.	Contexts/description	Taken from	Conditions
1	South-facing elevation, west end of the boundary wall	S	Fair
2	South-facing elevation, west end of the boundary wall, with site gate	S	Fair
3	South-facing elevation, site gate and adjoining wall	S	Fair
4	South-facing elevation, section of wall to the east of the site gate	S	Fair
5-10	South-facing elevation, contiguous wall sections	S	Fair
11	South-facing elevation, section of wall with blocked opening 5	S	Fair
12-13	South-facing elevation, section of wall with blocked openings 4 and 5	S	Fair
14	South-facing elevation, detail of blocked opening 4	S	Fair
15	South-facing elevation, detail of blocked openings 4 and 5	S	Fair
16-18	South-facing elevation, section of wall with blocked opening 3	S	Fair
19-20	South-facing elevation, section of wall to the west of blocked opening 3	S	Fair
21	South-facing elevation, section of wall adjacent to the railway underpass	S	Fair
22	South-facing elevation, ashlar-built stonework at the corner of the boundary wall	S	Fair
23-25	Looking north down the steps to the railway underpass with site boundary wall	SW	Fair
26-27	Looking up the steps leading to the railway underpass with site boundary wall	NW	Fair
28-30	Looking east along Avon Place with the site boundary wall	E	Fair
31-32	Looking west along Avon Place with the site boundary wall, south-facing elevation	W	Fair
33	North-facing elevation, section of new build at the east	N	Fair
34	North-facing elevation, site gate	N	Fair
35	North-facing elevation, site gate and section of wall on its west side	N	Fair
36-44	North-facing elevation, contiguous wall section	N	Fair
45	North-facing elevation, section of wall to the east of blocked opening 3	N	Fair
46	North-facing elevation, blocked opening 3	N	Fair
47-50	North-facing elevation, contiguous wall sections behind trees.	N	Fair
51	North-facing elevation, oblique shot of the wall	W	Fair
52-53	Stone sets on the ground in front of the north elevation	W	Fair
54-56	North-facing elevation, oblique view	E	Fair
57	General view, northwards over the Edinburgh to Glasgow railway line	S	Fair
58-59	General shot of the site and the north-facing elevation	E	Fair
60-61	General shot of the site entrance	S	Fair
62	North-facing elevation, section of new build (8)	W	Fair
63	Stone sets on the edge of the railway line	E	Fair
64-65	Looking north over the railway line	S	Fair
66	North-facing elevation, general shot	E	Fair

APPENDIX 2: 35MM PHOTOGRAPHIC RECORD

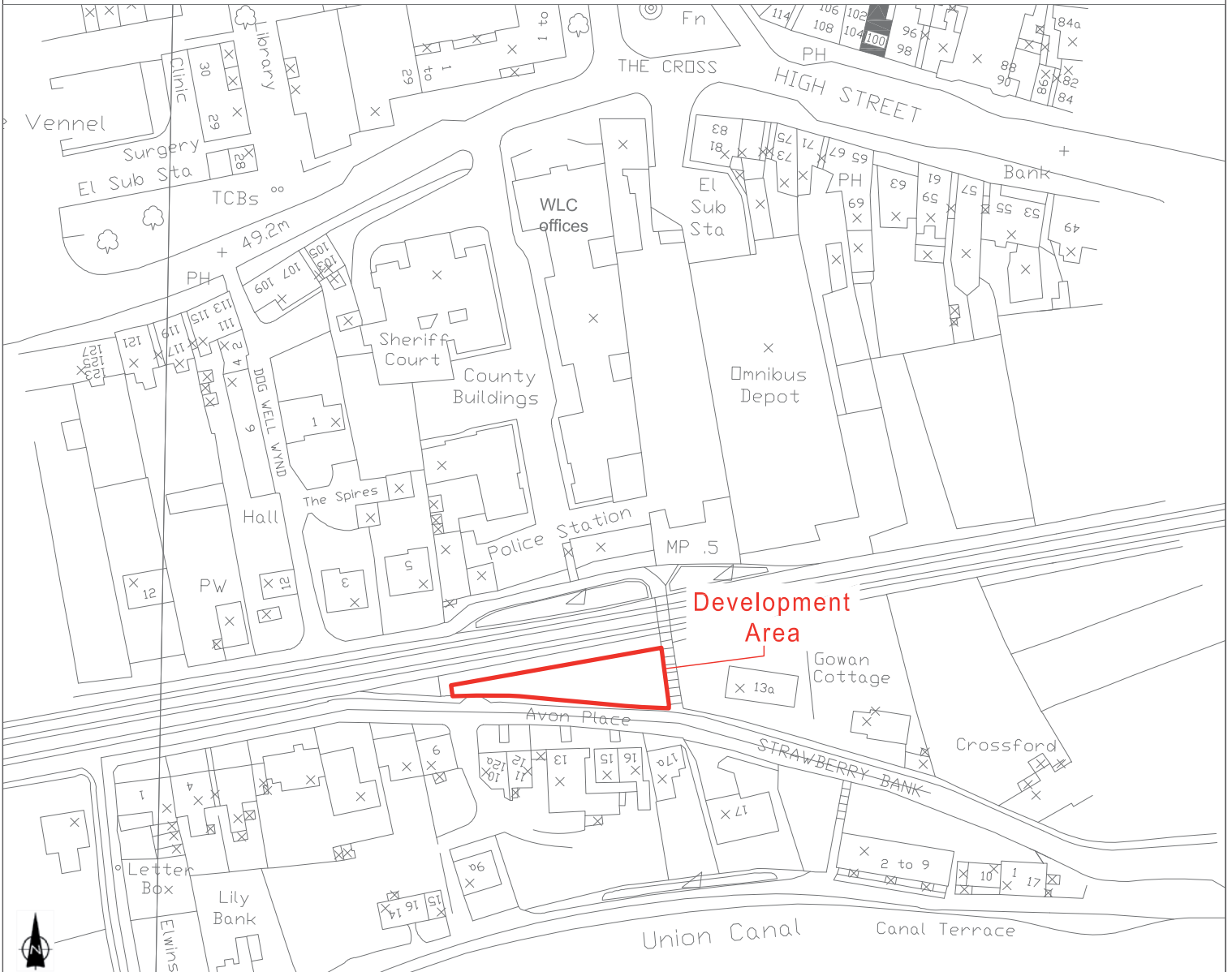
Photo No.	Contexts/description	Taken from	Conditions
1-2	South-facing elevation. North-south trending wall of the railway underpass boundary	SE	Fair
3	South-facing elevation. Stonework at the corner of west end boundary wall	S	Fair
4-5	South-facing elevation. Blocked doorway, Feature 3, general shot	S	Fair
6-7	South-facing elevation. Blocked doorway, Feature 4, general shot	S	Fair
8-9	South-facing elevation. General shot of the west end of the south elevation	SE	Fair
10	North-facing elevation. Section of wall to th east of the main gate	N	Fair
11	North-facing elevation. Section of wall at west end	N	Fair
12	North-facing elevation. Section of wall (middle)	N	Fair
13	North-facing elevation. Blocked doorway, Feature 5	N	Fair
14-15	North-facing elevation. Blocked doorway, Feature 5 (oblique shot)	N	Fair
16	North-facing elevation. Blocked doorway, Feature 4.	N	Fair
17-18	North-facing elevation. Blocked doorway, Feature 3	N	Fair
19	North-facing elevation. West end hidden by trees	N	Fair
20-21	North-facing elevation. General shot	W	Fair
22-23	General shot of the N-S trending pedestrian railway underpass.	S	Fair

APPENDIX 3: DISCOVERY AND EXCAVATION IN SCOTLAND ENTRY

LOCAL AUTHORITY:	West Lothian
PROJECT TITLE/SITE NAME:	Avon Place, Linlithgow
PROJECT CODE:	AVPL
PARISH:	Linlithgow
NAME OF CONTRIBUTOR:	Michael Cressey
NAME OF ORGANISATION:	CFA Archaeology Ltd
TYPE(S) OF PROJECT:	Standing building recording, test-pitting & watching brief
NMRS NO(S):	N/a
SITE/MONUMENT TYPE(S):	Boundary wall
SIGNIFICANT FINDS:	None
NGR (2 letters, 8 figures)	NT 00203 76990 (centred on)
START DATE (this season)	May 2009
END DATE (this season)	TBA
PREVIOUS WORK (incl. <i>DES</i> ref.)	None
MAIN (NARRATIVE) DESCRIPTION: (May include information from other fields)	<p>A level 2 standing building survey was conducted on a boundary wall enclosing a proposed development site on Avon Place, Linlithgow. The site had been previously used as a coal siding by the Edinburgh to Glasgow Railway. A survey of the wall confirmed it to be constructed of random rubble build using both rough dressed sandstone and whinestone boulders. The wall incorporated three blocked doorways two of which are probably 18th-century or earlier in date. One doorway incorporates brickwork and re-used sections of railway as a lintel. The wall has been much repaired over the years and has been rebuilt in the recent past at the east end.</p> <p>Local tradition states that it was through a boundary opening somewhere in the vicinity of the proposed development site that Hamilton of Bothwellhaugh escaped after the assassination of Regent Moray in 1570. Tradition states that a lintel had been removed in advance from a boundary wall to allow the swift exit of horse and rider. No archaeological evidence relating to this event was present within the section of wall examined.</p>
PROPOSED FUTURE WORK:	Test-pitting and a watching brief
CAPTION(S) FOR ILLUSTRS:	None



Site Location



Key:

Fig No:	1	Revision:		Client:	AK:JN Architects
Title:	Location Map				
Project:	Avon Place, Linlithgow: Boundary Wall Survey				
Scale:	1:1500 @ A4				

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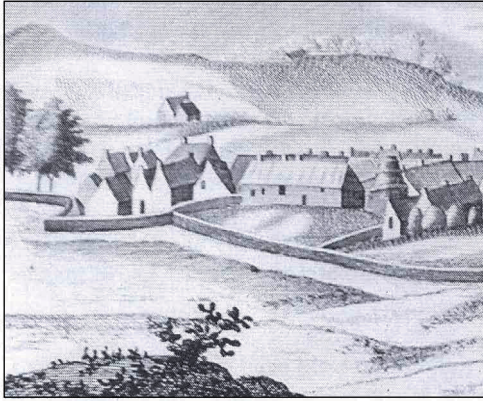


Fig. 2a - Extract from Slezer's 1693 drawing of Linlithgow



Fig. 2b - Armstrong's map 1773

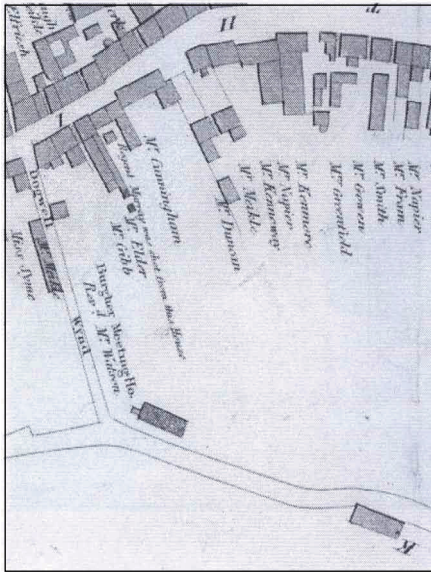


Fig. 2c - John Wood's Town Plan 1820

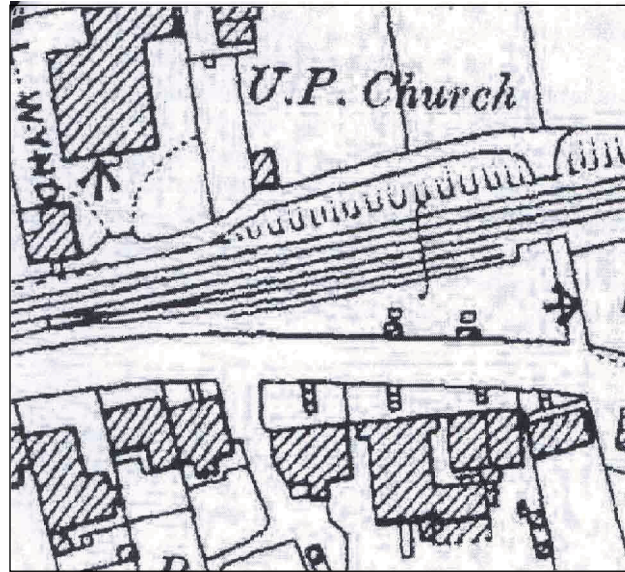


Fig. 2e - 1897 Ordnance Survey Map



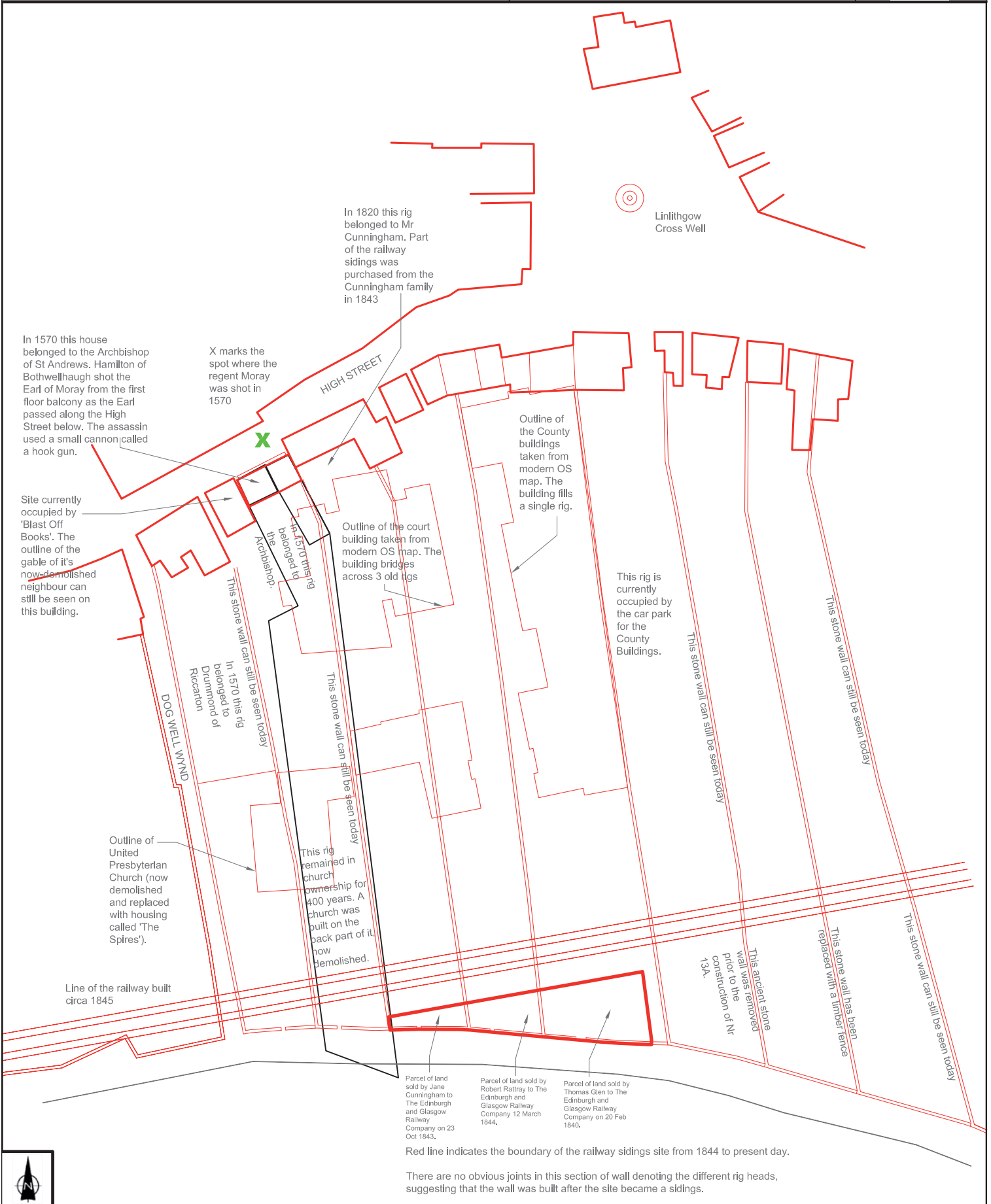
Fig. 2d - 1856 Ordnance Survey Map

Key:



Fig. No:	2	Revision:	A	Client:	AK:JN Architects
Title:	Historical Map Regression				
Project:	Avon Place, Linlithgow: Boundary Wall Survey				

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Key:	Fig No:	3	Revision:		Client:	AK:JN Architects
	Title: Former property divisions within the proposed development area (source AK:JN Architects)					
	Project: Avon Place, Linlithgow: Boundary Wall Survey					
Scale: 1:1000 @ A4					Drawn by: LW	Report No: 1635

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Key:

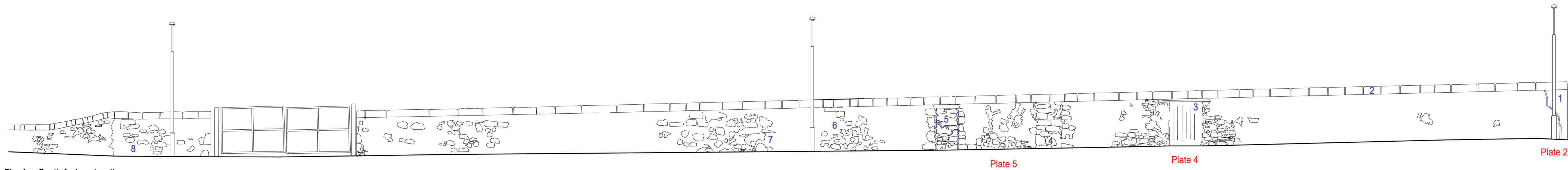


Fig. 4a - South-facing elevation



Fig. 4b - South-facing photo montage

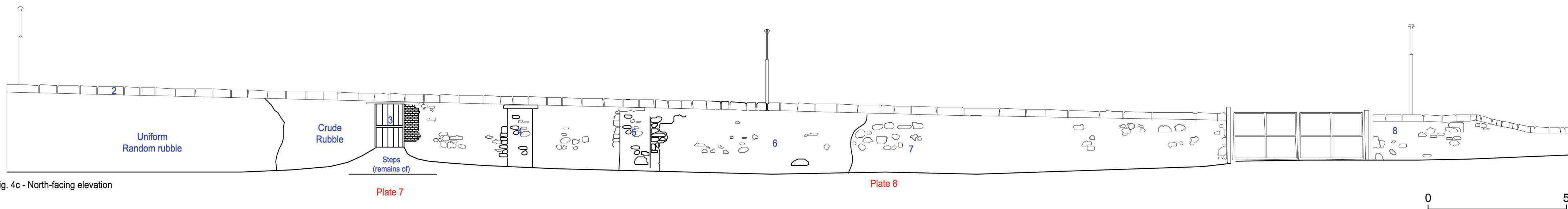


Fig. 4c - North-facing elevation



Fig. 4d - North-facing photo montage



Plate 1 - General view of the south-facing elevation at Avon Place, looking east



Plate 2 - Section of ashlar re-build associated with the railway underpass



Plate 4 - Blocked openings 3 and 4 on the south-facing elevation



Plate 3 - West-facing site boundary wall flanking the steps to the railway underpass

Key:

Fig. No:

Revision:

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Client:

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Title:

Plates 1 - 4

Project:

Avon Place, Linlithgow: Boundary Wall Survey

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Plate 5 - Blocked openings 4 and 5 on the south-facing elevation



Plate 6 - North-facing elevation, general view looking east



Plate 7 - North-facing elevation, blocked opening 3, general shot



Plate 8 - North-facing elevation, section of wall that is bulging outwards

Key:

Fig. No: Revision: **A** Client: **AK:JN Architects**

Title:
Plates 5 - 8

Project:

Avon Place, Linlithgow: Boundary Wall Survey



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AVPL_037.JPG



AVPL_038.JPG



AVPL_039.JPG



AVPL_040.JPG



AVPL_041.JPG



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AVPL_043.JPG



AVPL_044.JPG



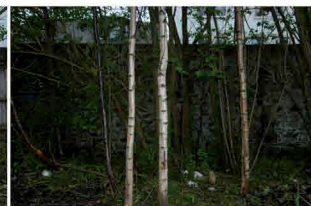
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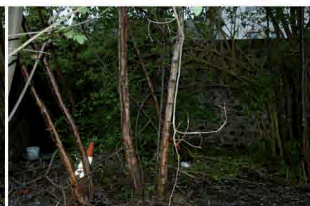
AVPL_046.JPG



AVPL_047.JPG



AVPL_048.JPG



AVPL_049.JPG



AVPL_050.JPG



AVPL_051.JPG



AVPL_052.JPG



AVPL_053.JPG



AVPL_054.JPG



AVPL_055.JPG



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AVPL_059.JPG



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