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| Commissioned by | WSP Environmental Ltd for Network Rail                            |
| Date issued     | June 2009   |
| Version         | Final   |
| OASIS Reference | cfaarcha1-60730   |
| Grid Ref        | NS 854 671 (centred on)   |

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### **Airdrie to Bathgate Railway and Linked Improvements: Archaeological Standing Building Survey of Forrestfield Railway Platform.**

**Data Structure Report  
No. 1642.1**

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## **1. INTRODUCTION**

### **1.1 General**

1.1.1 This report presents the results of a photographic survey (English Heritage 2006) of the remains of Forrestfield Railway Station Platform (NGR: NS 85412 67129, Fig 1). The work was carried out in June 2009 by CFA Archaeology Ltd (CFA) on behalf of Network Rail Ltd as part of the Airdrie to Bathgate Railway and Linked Improvements works.

### **1.2 Historical Background**

1.2.1 The Ardrrie and Bathgate line was built by the Monkland Railway Company which was later merged with the Monkland and Kirkintilloch Railway, Ballochrey Railway and Slamannan Railway. It began carrying freight in 1861 and passengers a year later. Although it constituted a direct line between Edinburgh and Glasgow it was never intended to function as an inter-city line. The route relied on carrying freight from the numerous coal mines and settlements along its route. The Monkland Railway was absorbed by the Edinburgh and Glasgow Railway in July 1865 and was bought by the North British Railway on the next day. The passenger service between Airdrie and Bathgate came to an end in 1956, although Forrestfield Station was closed before this on 22nd of September 1930. By the 1960s heavy industry was in decline and in 1982 the line finally closed altogether (text taken from the now removed interpretation notice board).

1.2.2 Forrestfield Station is recorded in the National Monument Record of Scotland (NMRS No. NS86NE 46). The entry mentions that at the time of the RCAHMS survey, that there were few surviving remains of Forestfield Station which is situated to the east side of the road overbridge carrying Woodside Road. No other bibliographic details support the NMRS entry.

### **1.3 Aims and Objectives**

1.3.1 The objectives of this study were as follows:

- Carry out a photographic survey of the Forrestfield railway station platform prior to its demolition.
- Produce an illustrated report on the survey results.

### **1.4 Methods**

1.4.1 All upstanding remains were examined and where possible and critical measurements taken. A wireframe template of the platform was created using an industry standard EDM linked to PENMAP software and later converted into AutoCAD to rectify photographs used in the montage. Standard building recording forms were completed to record significant features. Digital photographs were taken of significant features to support the field record. A full list of the photographs taken are listed in Appendix 1.

## **2. SURVEY RESULTS**

### **2.1 Station Platform**

- 2.1.1 The station platform measures 67.3m long from the end of the loading ramps at each end (Fig 2-3, Plates 1-2). The height from track-bed level to the top of the platform is 0.9m high. Recently the top of the platform had been cleared of pioneer vegetation.
- 2.1.2 The outer-face of the platform is constructed of timber planks 0.23m wide and held together with iron bolts. The edge of the platform comprises a timber curb measuring 0.60m wide and 0.15m deep (Plate 3). The curb projects over the outer-facing by 0.30m. The curb has been laid in sections and is connected using bolted scarf joints. The curb is supported by moulded timber joists that have been placed 1.4m apart and fixed in place by large nuts and bolts. Situated between the lamp room and the road bridge abutment are three ceramic pipes with a diameter of 0.20m. The pipes have been inserted to allow rainwater to drain through the centre of the platform (Plate 4). The construction of the platform is a simple box-type composite structure probably infilled with rubble. The platform deck was probably covered with tarmac or gravel chippings laid directly on top of timber.
- 2.1.3 Two wooden benches were originally situated on the platform. Only one now survives on the east side of the overbridge. The bench is probably made from a re-used sleeper (Plate 5).
- 2.1.4 The loading ramps at each end of the platform are in a poor condition and have at some time in the past been reinforced using scaffolding battens.

### **2.2 Lamp room**

- 2.2.1 Whilst not part of the wooden platform this structure is situated at the east end of the platform is a rectangular brick-built building measuring 4.6m long and 2.6m high (Fig 2-3). The building has been heightened using three courses of brick and now supports timber joists covered with a flat fibre-glass roof. A doorway with segmented-brick arch above has been blocked with a lighter coloured brick than that used for the construction of the building. The doorway measures 2m high and 0.9m wide. On its left-hand side there is a small window measuring 0.9m high and 0.7m wide and has been blocked with the same type of brick as the door.

### **2.3 Woodside Road road bridge**

- 2.3.1 Whilst not part of the wooden platform this structure is situated towards the west end of the platform is the steel-framed road bridge carrying Woodside Road (Plate 1). The abutment to the rear of the platform measures c.6m wide and is constructed of droved ashlar sandstone (Fig 2) with a steel parapet.

### **3. CONCLUSION**

- 3.1 The survey has shown that the platform is supported on a composite timber frame and has been faced with timber cladding held with iron bolts. A lamp room and part of the Woodside Road bridge have also been recorded.
- 3.2 The photographic survey has provided a permanent archaeological record of the structural remains of the former Forrestfield railway station platform prior to its demolition and no further recording work is required.
- 3.2 A copy of this report will be lodged with the NMRS and an entry provided to *Discovery and Excavation in Scotland*. No further archaeological work is required.

### **4. REFERENCE**

English Heritage 2006 Understanding Historic Buildings – A Guide to Good Recording Practice. London.

**APPENDIX 1 DIGITAL PHOTOGRAPHIC REGISTER**

| <b>Photo</b> | <b>Description</b>                                  | <b>From</b> |
|--------------|---|-------------|
| 1            | Bridge  | W           |
| 2            | Track   | W           |
| 3            | General view of platform, track and bridge          | E           |
| 4            | General view of platform, track and bridge          | E           |
| 5            | General view of platform, building and loading ramp | NE          |
| 6            | General view of platform, building and loading ramp | NE          |
| 7            | General view of platform, track and bridge          | W           |
| 8            | General view of platform, building and loading ramp | E           |
| 9            | General view of platform, building and loading ramp | E           |
| 10           | General view of platform, building and loading ramp | NE          |
| 11           | General view of platform, building and loading ramp | NE          |
| 12           | General view of platform, building and loading ramp | NE          |
| 13           | General view of platform, track and bridge          | E           |
| 14           | General view of Track and bridge                    | W           |
| 15           | General view of platform, track and bridge          | NW          |
| 16           | General view of platform, track and bridge          | NW          |
| 17-18        | North elevation of building                         | N           |
| 19-21        | North elevation of building                         | N           |
| 22-43        | North elevation of platform                         | N           |
| 44           | General view of bridge and platform                 | W           |
| 45           | General view of track                               | E           |
| 46           | General view of bridge and platform                 | NW          |
| 47           | General view of platform, track and bridge          | NW          |
| 48           | General view of platform, building and loading ramp | NE          |
| 49           | General view of track                               | E           |
| 50           | General view of bridge and platform                 | NE          |
| 51           | General view of building and loading ramp           | N           |
| 52           | General view of platform, building and loading ramp | NE          |
| 53           | View under bridge                                   | E           |
| 54           | Detail of decorated joists under platform           | NE          |
| 55           | Detail of decorated joists under platform           | NE          |
| 56           | Detail of decorated joists under platform           | NE          |
| 57           | Detail of drainage outlet under platform            | N           |
| 58           | Detail of drainage outlet under platform            | N           |
| 59           | North elevation of underside of bridge arch         | N           |
| 60           | Detail of Bridge                                    | NW          |

Key:

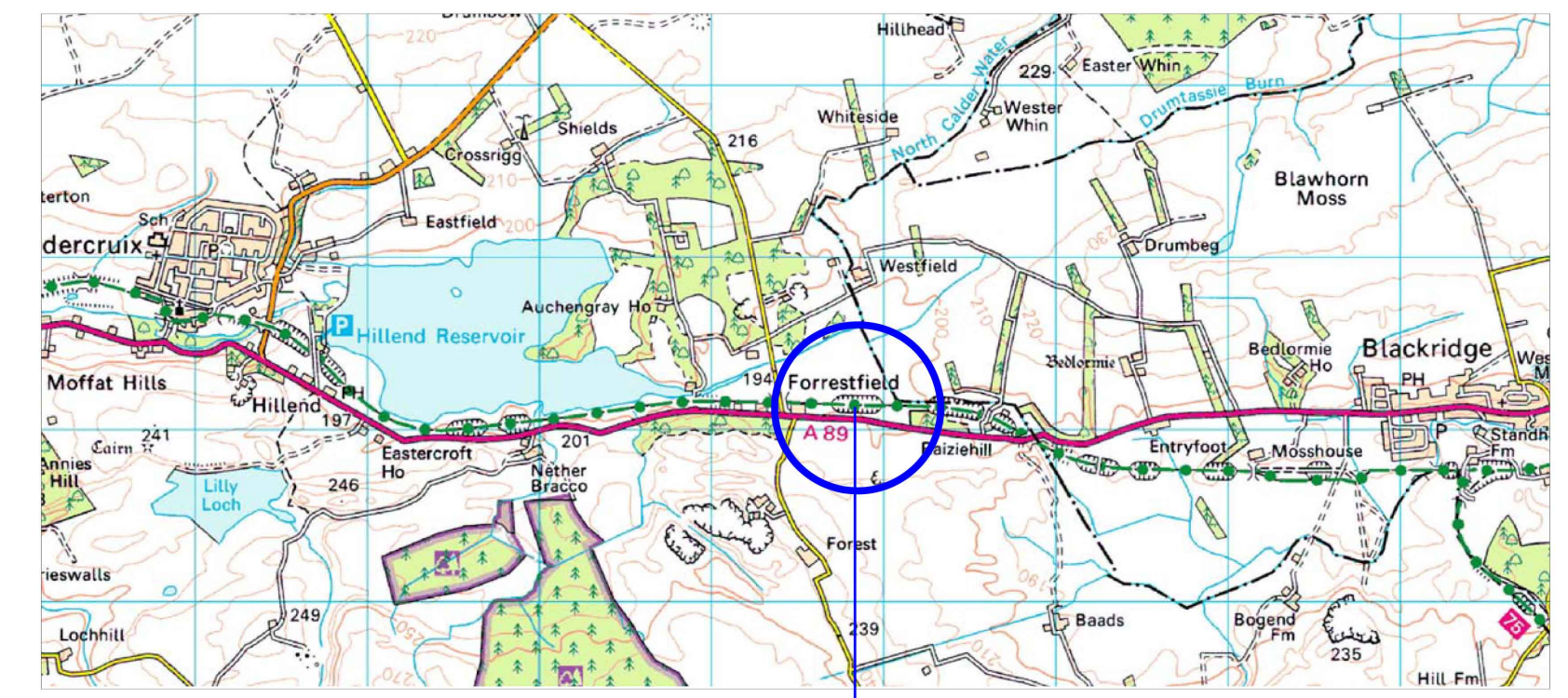


Fig. 1 - Location Map

Site Location

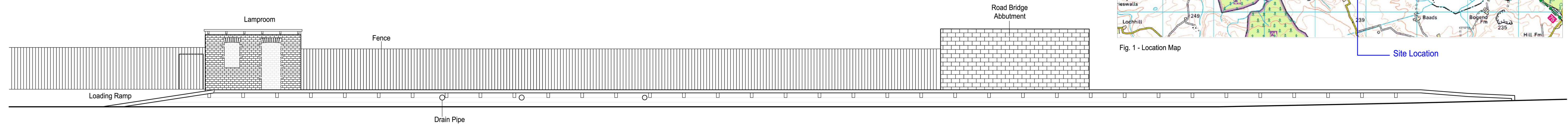


Fig. 2 - North-facing elevation of abandoned railway platform



Fig. 3 - Rectified Photograph of the North-facing elevation



Plate 1 - Woodside roadbridge near the platform



Plate 2 - Forrestfield station; general view of the west-bound platform

Key:

Fig. No:

Revision:

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Plate 3 - Moulded timber joist supporting the timber curb



Plate 4 - Timber curb with drain pipe through the lower cladding

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Plate 5 - A Platform bench still *in-situ*

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