

Homes and Communities Agency
Plot 3, Temple Quay, Bristol
Archaeological Evaluation

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Summary

An archaeological evaluation at Plot 3, Temple Quay, Bristol, uncovered structural remains of Isambard Kingdom Brunel's early 1840s Dock and Goods Shed and associated dockside structures, together with structural remains of a late 1870s rebuilding. The Goods Shed and the south-west side of the Dock are well preserved, but the north-east side of the Dock appears to have been at least partially demolished in the 1870s.

1 Introduction

- 1.1** This report presents the results of an archaeological evaluation carried out by Arup and Bristol and Region Archaeological Services (BaRAS) on land at Plot 3, Temple Quay, Bristol, between the 18 and 27 of March 2014.
- 1.2** The evaluation was commissioned by the Homes and Communities Agency (HCA). The purpose of the evaluation was to determine the extent of survival and location of an early 1840s barge dock designed by Isambard Kingdom Brunel, and any associated dockside structures.
- 1.3** BaRAS will deposit the project archive with Bristol Museum and Art Galleries under accession number BRSMG 2014/21. A digital copy of the report will be sent to the Bristol Historic Environment Record (BHER) and the English Heritage Archive. The project has been entered in the Bristol Historic Environment Record as: BHER 25342 and in the OASIS Online Access to the Index of Archaeological Investigations as: bristola1-175998.

2 The Site

- 2.1** The site (centred on NGR ST 59700 72600) comprised an area of brick paving, compacted gravel surfacing and tarmac that is bounded by Temple Meads Railway Station to the south, Friary, Isambard Walk and The Square to the west, Temple Back East and Temple Meads Station Ferry Landing to the north, and the Floating Harbour to the north-east. The northern part of the site is currently used as a temporary food outlet; the central area is vacant; the south-eastern end is a car park. Ground levels range between 12.90m AOD in the east to 11.53m AOD in the north-west.
- 2.2** The geology of the site is Triassic Redcliffe Sandstone (BGS 2014), which is overlain by 2–4m of Pleistocene gravels of the Avon Formation, 4–12m of Holocene tidal flat deposits of the Wentlooge Formation and 2–6m of made ground (Arup 2002; Halcrow 2011; Wilkinson et al 2013).

3 Archaeological and Historical Background

- 3.1** Previous archaeological work on the site comprises a desk-based assessment (WS Atkins Consultants Ltd 1999) and a watching brief (Mason & Smith 2012). Watching briefs have also been undertaken on three adjacent sites (Parry 1999; Pilkington 1999; Edwards 2001).
- 3.2** Historically the site lay within Temple parish, outside Bristol's medieval town walls, in an area of low-lying agricultural land known as Temple Meads. Historic maps show that the site remained undeveloped until the construction of Temple Meads Railway Station (BHER 205M) in 1839–41.
- 3.3** In 1841–2 a Goods Shed (BHER 1254M) was built on land to the north-west of the Station. There is a plan of this building, which was located approximately 35m to the south-west of the site, in S.C. Brees' Railway Practice (reproduced in Binding 2001, 67, fig. 5.16). The interior of the first Goods Shed is also illustrated in a c 1841 lithograph by J. Bourne (BRSMG J619). At some point in the early 1840s a substantial extension was added to the north-east side of the Goods Shed; part of which extended into the south-west corner of the site. The north-eastern side of the enlarged Goods Shed opened onto a barge dock (BHER 1253M) that crossed the central part of the site. It is unclear exactly when the Dock was constructed but it was probably completed before 1845 (Binding 2001, 112, fig. 8.6). All three structures were designed by Isambard Kingdom Brunel (1806–1859).
- 3.4** The Goods Shed and Dock were designed as an integrated transport facility that enabled the transshipment of goods from the City Docks to the national rail network via barges that were low enough to pass under Bristol Bridge (Binding 2001, 113; Insole 2012, 10). The southern corner of the dock was uncovered and photographed during construction work in 1982 (pers. comm. John Bryant, 2 April 2014); further remains of the Dock and the adjacent Goods Shed were uncovered during watching briefs undertaken in 1998–2000 (Parry 1999; Pilkington 1999; Edwards 2001).
- 3.5** The Goods Shed and Dock are depicted on Ashmead's plans of 1854 (Fig. 10) and 1874. Ashmead's plans show four projections along the SW side of the dock, each of which has a dot in the centre; prior to the present evaluation it was thought these may indicate the positions of dockside cranes, mooring posts or capstans. The 1854 plan also depicts a large industrial building in the east corner of the site; this can be identified as wharf, covered shed and engine house that was built c 1844 for loading copper brought in by vessels used by the Copper Miners Company (Binding 2001, 115).

3.6

Brunel's dock remained in use for around 30 years, but it became obsolete when the Bristol Harbour Railway was constructed in 1868–72 as a link between Temple Meads and the City Docks. The Harbour Railway opened in 1872 and by the late 1870s the dock had been backfilled as part of a major rebuilding and extension of Brunel's Goods Shed (BHER 1255M), which involved re-aligning rail lines over the in-filled dock. The enlarged Goods Shed is depicted on the 1885 1:500 OS plan and the Goad Fire Insurance Plan of 1896 (Fig. 11). The Goods Shed was demolished in 1926 to make way for a Covered Goods Yard, which for a time was the largest in Western Europe. The 1926 Goods Yard was demolished in 1983; the site was subsequently developed as an industrial estate. The industrial estate was demolished in the late 1990s to make way for the redevelopment of the Temple Quay area. Between 2007 and 2012 the site was a temporary car park and since 2012 it has been used as a temporary performing arts venue known as Creative Common.

4 Aims and Methodology

- 4.1** The fieldwork complied with the methodology outlined in a Brief for Archaeological Evaluation (Insole 2013), a Written Scheme of Investigation WSI (Greig 2013) and followed the Standard and Guidance for archaeological field evaluation (IfA 2009). The aim of the evaluation is to make a full and accurate record (written, drawn, photographic) of the date, character, degree of survival, extent and location of archaeological deposits which might be preserved within the proposed development area.
- 4.2** The evaluation comprised four trial trenches, which exposed an area totalling 230m² at ground level. The trenches were dug using 13-ton 360° tracked excavator fitted with a toothless grading bucket under the direction of a BaRAS archaeologist. Toothed buckets were used to remove modern concrete obstructions. Mechanical excavation proceeded to the top of archaeological deposits, thereafter the excavated area was cleaned by hand and the archaeological features sampled, characterised and recorded.
- 4.3** Due to various on-site constraints all of the trenches apart from 3B were excavated in slightly different locations to those described in the WSI; all changes in trench locations were made in agreement with the client and Archaeological Officer for BCC. Trench 1 was relocated approximately 42m towards the south-east; Trench 2 was rotated slightly and moved 8.5m towards the south-east; Trench 3A was moved 5.5m towards the north-west.
- 4.4** The site was recorded in accordance with the BaRAS Site Recording Manual (BaRAS 2009). Archaeological features were planned at scales of 1:10 and 1:20. Sections were drawn at a scale of 1:20. A photographic record of all features was made using 35mm monochrome and digital colour photographs.

5 Results

- 5.1** Archaeological features are discussed below; detailed descriptions are provided in the Context summary (Appendix 2). Stratigraphic relationships are shown on the site matrix (Appendix 3). Trench locations and significant features are shown on the site plan (Fig. 2).

Trench 1

- 5.2** Trench 1 (Figs. 3–4; Plates 1–3) was located across the assumed line of the NE side of the Dock. The trench measured 10m NE–SW and 5m NW–SE at ground level, and was stepped in to 5.8m NE–SW and 2m NW–SE at the base of the trench. The trench was excavated to a maximum depth of 3.35m (8.45m AOD). The ground level around the trench was 11.82m AOD. Ground water was encountered at 8.8m AOD.
- 5.3** The earliest feature in Trench 1 was a substantial stone structure (118; Plate 3) that was constructed with roughly dressed Pennant Sandstone blocks bonded with a hard grey lime mortar. Structure 118 measured 1.4m from NW–SE, over 0.9m from NE–SW and was over 0.65m deep; the highest part of the structure was uncovered at a height of 9.28m AOD. Structure 118 is probably land-side buttress of the (robbed out) dock wall.
- 5.4** Structure 118 was overlain by a sequence of ground make-up layers (108–14), most of which contained large quantities of clinker and transfer-printed and painted whiteware pottery wasters. These deposits are probably contemporary with the construction of the dock, and are likely to have been dumped to create a hard surface for unloading and/or storing goods.
- 5.5** Layers 108–14 and structure 118 were truncated by robber cut 115. This cut represents the robbing of the dock wall in the 1870s. A 0.6m wide machine-cut slot was excavated along the SE side of the trench; this uncovered a solid mass of masonry at a depth of 8.45m AOD. Due to the rapid ingress of water it was not possible to determine if the masonry was an in-situ part of the dock wall or a large piece of masonry in the infill deposits.
- 5.6** The dock was in-filled with deposits of clean re-deposited natural silty and sandy clays (106–7), which were overlain by mixture of silts and brick/stone rubble (105) and a layer of railway track bedding (104). All of these deposits are likely to date from the 1870s and are associated with the infilling of the dock and the levelling of the site for use as a railway goods yard.
- 5.7** Layer 104 was overlain by modern levelling layers 100–3 and surfaces 116–17. It is unclear if Contexts 102–3 and 117 were associated with rebuilding of the Goods Shed in 1926 or the 1984 industrial estate. Layer 116 was probably associated with the 1984 industrial estate. Layers 100–1 date from the 1990s or later.

Trench 2

- 5.8** Trench 2 (Figs. 5–7; Plates 4–14) was located across the assumed line of the SW side of the Dock and within the footprint of the 1840s Goods Shed. The trench measured 10m N–S and 10m E–W at ground level, and was stepped in to 6.8m E–W and 6.9m N–S at the base of the trench. The trench was excavated to a maximum depth of 3.94m (8.14m AOD). The ground levels around the trench ranged between 11.91m and 12.08m AOD.
- 5.9** The earliest feature in Trench 2 was the SW wall of the Dock (210; Plates 4–7). The top of the dock wall was approximately 2.4m below the present ground level at a height of 9.53m AOD. Trench 2 was located over a projecting feature marked on Ashmead's plans of 1854 and 1874, which can be identified as the base for a dockside crane. The crane base comprised two 3.08m by 1.5m wide and 0.7m thick roughly dressed Pennant Sandstone blocks, with a fine hand-carved mounting for a crane near the dockside edge. The carved area comprised a 1.68m (5 feet 6 inches) square area that was recessed 0.03m (1 inch) below the surrounding dockside. There were four 0.27m square (10½ inch) mounting points in the corners of the carved area, each of which was recessed by a further 0.05m (2 inches). Each of the mounting points had a 0.05m (2 inch) wide circular hole in the centre, which held a treaded iron bolt. There were traces of melted lead around the mounting points. The crane base had a 1.22m (4 feet) wide and over 1m deep circular shaft in the centre, which had 12 evenly spaced chamfered notches around its edge. The mounting points and notches would have been used to lock a metal base plate and collar in place. The central shaft, which was backfilled with made ground layer 219, is likely to be several metres deep.
- 5.10** The SW side of the dock wall was stepped out by 0.6m at a depth of 0.7m below the dockside. It is unclear if the SW side of the dock wall is stepped out at a greater depth, but the recorded SW edge of the wall appears to be vertical until at least 8m AOD.
- 5.11** The dock wall was abutted and partially overlain by re-deposited grey alluvial clay 221, which was in turn sealed by made ground layer 208. Layer 221 is probably material derived from the excavation of the dock. There was no evidence of a contemporary ground surface adjacent to the dockside, but there was a surface of compacted gravel and tar (230) near the western edge of the trench. Layer 228 is part of the same surface. Surface 230 sloped from NE to SW and ranged in height between 9.32m and 9.49m AOD. Any contemporary surface between dock wall 210 and surface 230 appears to have been truncated when the Goods Shed was rebuilt in the 1870s. Surface 230 was overlain by made ground layer 218, which was in turn cut by drainage Trench 224 and overlain by timber beam 209.

- 5.12** Drainage Trench 224 was over 2m long, up to 1.25m deep and sloped steeply from NE to SW (Plate 8). There was a ceramic drain (225) in the base of the trench, which was constructed with pipes measuring 0.7m long and 0.25m wide. The drainage trench was backfilled with a dump of stoneware wasters (223) from the Bristol manufacturer J & C Price & Brothers. Some of the wasters were stamped with a diamond registration mark issued by the Patent Office on the 4th of November 1876. This indicates that fill 223 and any overlying Contexts must be later than this date.
- 5.13** A row of timber beams (209; Plates 9–10) were recorded to the SW of wall 210. The beams cross over the top of drain 224, but it is unclear if they post-date it, as the drain could have been dug below and across the line of the beams after they were laid. Beam 209 was 280mm wide and 120mm thick, and was joined end-to-end with another beam (removed during machining) by two small pieces of roof slate that were fitted into slots that had been cut into the ends of the beams. Both beams were soaked with a bituminous substance and had a zig-zag pattern of large coach screws on its upper side. The beams clearly originated as railway sleepers for a ‘baulk road’ type of track (where sleepers are joined end to end to form a continuous timber bearing for the tracks, rather than the more familiar ‘cross-sleeper’ track), but it is unclear if they were in-situ or not.
- 5.14** Layer 208, timber 209, dock wall 210 and surface 230, were overlain by the walls of the 1870s re-build of the Goods Shed (203-4 & 211; Plates 11–13). Structure 222 is part of the same phase of re-building. Structures 203-4, 211 and 222 were all constructed with roughly squared Pennant Sandstone blocks bonded with a soft grey lime mortar. Upstanding remains of the 1870s Goods Shed were up to 1m high, and were recorded at a maximum height of 10.65m AOD. Walls 203 and 211 were abutted by made ground layers 206-7. There were mortar splashes from the construction of wall 203 on the upper surfaces of these layers, which indicates that they are contemporary with the wall. Layers 226-9 are of a similar date.
- 5.15** There was a small area of truncation (cut 231) on the SW side of the dock wall 210. The purpose and date of this cut (which would have involved chiselling through solid Pennant Sandstone blocks) is unclear, but it may have been associated with the rebuilding of the goods station in the 1870s.
- 5.16** Layer 207 was overlain by a dump of black clinker (205) used as track bedding in the rebuilt Goods Shed. Layer 205 contained a number of decayed SE–NW aligned railway sleepers, at least some of which are likely to have been in-situ. Layer 205 was only present to the NW of wall 203, which is unsurprising given that this is where rail tracks are depicted entering the building on the 1885 OS 1:500 plan (surv. 1883) and Goad’s Fire Insurance Plan of 1896 (Fig. 11).

- 5.17** Layer 205 was cut by trenches for two NW–SE aligned two cast iron pipes (213-5) which ran parallel and to the NE of wall 204. The relationship between pipe Trench 213 and track bedding layer 217 is unclear, but pipe trench fill 214 was overlain by track bedding layer 216, which suggests that the pipes date from between the 1870s and 1925.
- 5.18** There is no evidence of a contemporary floor surface to the SE of wall 203, which suggests that there was a suspended wooden floor in this area. Support for this suggestion is provided by the Goad Fire Insurance Plan of 1896, which labels the bays between the rail tracks as ‘wood loading banks’; this is likely to be a reference to the nature of the floor covering rather than a description of their function.
- 5.19** Layer 206 was overlain by 1920s demolition dump 202. There were a number of zinc sheeting fragments lying on the interface between layers 206 and 202; these may have come from the roof of the Goods Shed when it was being demolished.
- 5.20** Layers 202 and 216 were overlain by 0.25m thick reinforced concrete floor slab 201 (Plate 14). It is unclear if the floor slab was part of the 1926 goods station or the 1984 industrial estate. Concrete floor 201 was cut by modern truncation 232 (probably a service trench), which was filled with angular gravel 220. Fill 220 was overlain by angular gravel 200, which dates from the 1990s or later.

Trench 3A

- 5.21** Trench 3A (Plate 15) was located across the assumed line of the SW side of the Dock. The trench measured 6m NE – SW and 3.2m NW - SE at ground level. Ground levels around the trench ranged between 11.85m and 11.95m AOD. The trench was excavated to a maximum depth of 3.5m (8.35m AOD). Due to the excessive depth and instability of the trench sides it was not possible to enter the trench for detailed recording.
- 5.22** The earliest feature in Trench 3A was the SW wall of Brunel’s dock (306). The top of the dock wall was located approximately 2.4m below the present ground level at a height of 9.57m AOD. The wall was approximately 1m wide and was constructed with large Pennant Sandstone blocks bonded with a hard grey lime mortar. Wall 306 was overlain by made ground layer 305 and cut by modern truncation 304.
- 5.23** Layer 305 comprised a mixture of black, red and grey sand, gravel, ash and clinker layers up to 1.3m deep dating from the 1870s–1925. Layer 305 was cut by modern truncation 304.
- 5.24** Cut 304 was over 3.5m deep, over 2m wide and over 6m long. The cut was backfilled with pipe bedding (fine rounded gravel) and angular gravel (303), which suggests that it was probably a modern service trench (possibly a storm drain) associated with the 1984 industrial estate.
- 5.25** Backfill 303 was overlain by modern angular gravel 302, which was sealed by path surface 300-1.

Trench 3B

- 5.26** Trench 3B (Figs. 8–9; Plate 16) was located across the assumed E corner of the Dock. The trench measured 10m NE–SW and 6m NW–SE at ground level. The ground levels around the trench ranged between 11.68m AOD and 11.7m AOD. The trench was excavated to a maximum depth of 3.4m (8.58m AOD). Ground water was encountered at 8.8m.
- 5.27** The earliest features in Trench 3B were wall 408 and made ground layer 410; the relationship between the two is unclear, but they are both likely to be contemporary with the construction of the Dock.
- 5.28** Layer 410 comprised a dump of grey clinker, ash and whiteware pottery wasters. One of the potsherds was marked with the initial ‘P & G’. This can be identified as a product of Pountney & Goldney dating from the period 1836–1849 (Pountney 1920, 10 & 120; Price 2006, 61). Pountney & Goldney were based at the Water Lane Pottery, 250m to the NW of the site. A similar group of wasters was uncovered during construction work along Temple Back in 1972 (Price 2006, 61–114). Layer 410 was overlain by surface 407.
- 5.29** Wall 408 was constructed with Pennant Sandstone rubble bonded with a soft pale grey lime mortar. The foundations were 0.5m wide; the wall was 0.35m wide. There was a slight indication that there may have been a return to the wall near its SW end. The width of the wall suggests that it is unlikely to have been particularly high. The wall was abutted by surface 407 and cut by 409. Some of the 1870s made ground layers (403–5) do not appear to have extended any further SE than the line of wall 408, which could indicate that it was extant in the 1870s. This is slightly puzzling as the wall does not appear on any contemporary plans, which suggests that it was probably a fairly minor feature; possibly a boundary wall or a small dockside building.
- 5.30** Truncation cut 409 is likely to represent the line of the NE side of the dock wall, which appears to have been at least partially robbed out prior to the infilling of the dock in the 1870s. The dock was in-filled with a deposit of soft re-deposited natural sand (406). Layer 406 was overlain by made ground layers 403–5 and 411. Layers 403 and 405 comprised a soft dark grey silty clay that contained large quantities of industrial waste and domestic refuse, including pottery wasters (mostly ginger beer bottles) from the Bristol stoneware manufacturer J & C Price & Brothers. One of the wasters was stamped with a diamond registration mark issued by the Patent Office on the 4th of November 1876. Sherds with identical marks were recovered from a waster dump 223 in Trench 2, which suggests that the rebuilding of the Goods Shed and the raising of the ground level over the in-filled dock are likely to have been contemporary. Made ground layer 404 was a deposit of soft re-deposited natural red sand. Made ground layer 411 comprised a mixture of brick/stone rubble and silty clay; it is unclear if this deposit was contemporary with layers 403–5 or not.

5.31

Made ground layer 411 was overlain by brick wall 402, which was in turn overlain by clinker track bedding layer 401. Wall 402 was founded on concrete and probably dates from the 1926 re-build of the Goods Shed. Layer 401 was overlain by modern angular gravel 400.

6 The Finds

6.1 A total of 683 finds were recovered during the excavation and watching brief at Plot 3. The finds comprise 611 sherds of ceramic, 36 pieces of glass, ten fragments of clay tobacco pipe, nine pieces of ceramic building material (CBM), five animal bones (one worked), five pieces of slag, four metal objects, one piece of struck flint, one lump of mortar and a crab claw. The finds were cleaned, identified and catalogued according to material type. Apart from a small quantity of residual medieval pottery, all of the finds are post-medieval or modern. The ceramic assemblage includes a significant proportion of 19th-century pottery kiln waste. A representative sample of the kiln waste and a small number of finds of local interest were retained; all other finds were discarded after assessment. Retained finds will be marked with the accession number BRSMG 2014/21 and a Context number. The finds are discussed separately by type below and quantified in Appendices 4–5.

Ceramics

Introduction

6.2 The ceramic assemblage comprises 85 pieces of kiln furniture and 526 sherds of pottery, 99.5% of which is post-medieval; the remaining 0.5% is medieval. The ceramics were examined with reference to the Bristol Pottery Type (BPT) Series and other published sources, using the widely accepted name codes based on the system adopted by the Museum of London (LAARC 2007). The ceramics are quantified by ware type in Table 1 and by ware type, Context and form in Appendix 4. Full descriptions are available in the site archive.

Table 1: Ceramics Quantified by Ware Type

Ware type	BPT	Name code	Date range	Source	Quantity
Bristol C ware	2	BRISTOL C	1050 – 1150	Bristol	1
Ham Green glazed ware	26	HG	1150 – 1250	Ham Green, North Somerset	1
Bristol Redcliffe ware	72	BR	1250 – 1500	Bristol	1
Westerwald stoneware	95	WEST	1600 – 1800	Germany	1
Somerset redware	96	SSOM	1550 – 1900	Somerset	1
Tin-glazed ware	99	TGW	1640 – 1800	Mostly Bristol	2
Bristol/Staffordshire yellow slipware	100	STSL	1650 – 1800	Mostly Bristol	1
Improved 'Bristol glazed' stoneware	200	ENG BRIS	1835 +	Bristol	304
Improved 'Bristol glazed' stoneware kiln furniture	200	ENG BRIS KILNF	1835 +	Bristol	2
Locally produced red earthenware	201	LMPLOC	1750 – 1950	Bristol area	2

Ware type	BPT	Name code	Date range	Source	Quantity
Bristol/Staffordshire mottled manganese ware	211	STMO	1690 – 1800	Mostly Bristol	3
Refined whiteware	202	REFW	1780 +	Bristol	91
Refined whiteware kiln furniture	202	REFW KILNF	1780 +	Bristol	71
‘Mocha’ decorated whiteware	223	REFW MOCHA	1790 – 1900	Bristol	10
Local red earthenware	264	-	1700 – 1900	Bristol area	43
English stoneware	277	ENGs	1700 – 1900	Mostly Bristol	13
Transfer-printed whiteware	278	TPW	1780 +	Mostly Bristol	51
Creamware	326	CREA	1765 – 1830	Mostly Bristol	3
Pearlware	349	PEAR	1770 – 1900	Mostly Bristol	10
Crucible	-	CRUC	1750 – 1900	Bristol	5
Saggars	-	SAGG	1780 +	Bristol	7
Yellow ware	-	YELL	1840 +	England	2
Total					683

Medieval

- 6.3** The medieval pottery comprises one sherd of 11th–12th-century Bristol C ware, one sherd of 12th–13th-century Ham Green ware and one sherd of 13th–15th-century Bristol Redcliffe ware, all of which were recovered as residual finds in 1870s dump layer 105.

Post-Medieval

- 6.4** The post-medieval assemblage comprises a small quantity of wares commonly found in 17th to 19th-century Contexts in Bristol, such as English tin-glazed wares, stonewares, slipwares, earthenwares and whitewares, and a moderately large assemblage of late-1870s Bristol glazed stoneware and early-1840s whiteware kiln waste. The kiln waste dumps are discussed separately by Context below.

Pottery and Kiln Furniture from Contexts 105 and 107

- 6.5** These deposits, which are likely to have been dumped in the late 1870s, contained small quantities of white clay kiln furniture, including handmade stilts and rolls and strips of clay similar to those recovered from Context 112 (see below), and a few sherds of biscuit-fired whiteware wasters. A few sherds of other post-medieval wares (Pearlware, refined whiteware and transfer-printed ware) were recovered; it is unclear if these are wasters or domestic refuse.

Pottery and Kiln Furniture from Contexts 108-10

- 6.6** These deposits, which are likely to have been dumped at some point between the early 1840s and the late 1870s, contained small quantities of white clay kiln furniture, including handmade stilts and rolls and strips of clay similar to those recovered from Context 112 (see below), and a few sherds of biscuit-fired whiteware wasters. All of the identifiable waster sherds are plates. Three sherds of other wasters (Tin-glazed ware, English stoneware and locally produced red earthenware) were recovered from Context 109 and 110, but these are not thought to be part of any significant waster deposits. A few sherds of other post-medieval wares (English stoneware, locally produced red earthenware and Somerset redware) were also recovered; these are likely to be derived from domestic refuse.

Pottery from Context 112

- 6.7** This deposit comprised a dump of transfer printed and painted whiteware wasters dating from the early 1840s. Many of the sherds were small, which may suggest that they had been deliberately crushed for use as hardcore. A few sherds of other post-medieval wares (English stoneware, Bristol/Staffordshire slipware and locally produced redwares) were recovered; these are not thought to be wasters. The wareware wasters include biscuit fired and glazed vessels. The waster sherds are quantified by form in Table 2.

Table 2: Bristol Glazed Stoneware Wasters from Context 112 Quantified By Form

Form	No. of Biscuit Fired Sherds	No. Glazed Sherds	Total
Teacups	7	5	12
Plates	1	4	5
Small pot lids	-	3	3
Bowl	-	1	1
Drainers	-	3	3
Small cylindrical jar	1	-	1
Unidentified	25	17	42
Total	34	33	67

- 6.8** Decorative motifs include mocha ware and transfer printed ware. The transfer-printed wares include ‘willow’ pattern, ‘flow blue’ and a sherd printed with the word ‘___lian’ on the reverse. The presence of ‘flow blue’ wares indicates that the pottery post-dates 1830.

Kiln Furniture from Context 112

- 6.9** The kiln furniture from Context 112 comprises 13 strips or rolls of white clay, 17 handmade white clay stilts, eight machine-made refractory clay spurs and three saggar fragments.
- 6.10** During firing refined earthenware is placed within saggars to protect it from direct contact with the kiln furnace. The saggar fragments collected all have a coarse pale creamy yellow fabric with calcareous and red clay pellet inclusions. The insides of the saggars are splattered with glaze, some of which has a bluish cast.

- 6.11** Stilts are used to separate pots within saggars in the kiln. The stilts have three ‘arms’, some of which have spurs pointing up and/or down on the ends. The stilts came in three sizes and had triangular or diamond section arms. The medium sized stilts had 35mm long arms.
- 6.12** Spurs, which are also known as cockspurs, were used to separate plates within saggars during firing. Three sizes of spur were recovered; 35mm, 25mm and 18mm wide. The larger two sizes are embossed with the words ‘CF PATENT’ and the numbers ‘6’ or ‘4’ on the base; the smallest ones are embossed with the words ‘CF PT’ and the number ‘2’ on the base.

Pottery from Context 113

- 6.13** Most of the sherds from this Context are un-diagnostic locally produced glazed redwares; these sherds could potentially be kiln waste from the Redware Pottery on Pipe Lane (90m north-east of the site), but there were no obvious wasters in the assemblage to confirm this suggestion.

Pottery and Kiln Furniture from Context 114

- 6.14** A single white clay stilt and seven sheds of undiagnostic biscuit fired whiteware were recovered from Context 114.

Pottery and Kiln Furniture from Context 223

- 6.15** This deposit comprised a dump of Bristol glazed stoneware wasters that had been used to backfill a drainage trench. The wasters comprise bottles, flagons and jars produced by J & C Price & Brothers, four of which were marked with a diamond registration mark for a design that was registered on the 4th of November 1876. The vessels are quantified by form in Table 3. Stratigraphic evidence indicates that this Context was deposited during the rebuilding of the Goods Shed in the late 1870s.
- 6.16** Many of the vessels were marked with an impressed manufacturer’s mark. Some of the vessels were marked ‘PRICE BRISTOL’ surrounding a letter (type 1 mark; Plate 17), but most of the flagons were marked ‘PRICE BRISTOL’ surrounding a Maltese cross (type 2 mark; Plate 18); one of the flagons was marked with a rectangular mark ‘PRICE BRISTOL’ (type 3 mark; Plate 19).

Table 3: *Bristol Glazed Stoneware Wasters from Context 223 Quantified By Form*

Form	No. of Sherds
Sprit flagons	214
Plain flat jar lids	10
Bottles or jars	12
Household product bottles	11
Plain straight sided jars (preserve jars)	6
Hot water bottles	2
Flat sided spirit flasks	2
Water filter	1

Form	No. of Sherds
Decorated jar lid	1
Total	259

- 6.17** Only one of the jars from Context 223 was an obvious waster, but given the fact that virtually all of the sherds were similar vessels produced by a nearby manufacturer, it seems likely that they are all wasters from a late stage in the production process. A significant proportion of the sherds could be cross-fitted, which suggest that many of the vessels were probably broken immediately prior to deposition.
- 6.18** Spirit flagons were by far the most common vessel in Context 223. These vessels were 130–270mm in diameter and had strap handles between the lip and shoulder. All of the flagons had a honey coloured glaze on the upper half of the body and most were impressed with proprietary marks on the shoulder (a full catalogue of which is available in the site archive).
- 6.19** All of the identifiable proprietors were British wine and spirit merchants, based in Chard, Frome, Glasgow, Launceston, Manchester, Market Harborough, Reading, Towcester, Welshpool and Wiveliscombe. Interestingly there were no marks for Bristol proprietors in the assemblage and there were no vessels for the export or London markets.
- 6.20** Identifiable proprietors include; Mansford & Baily of Frome (Plate 20); Nichols & Co. of the Reform Club Buildings, Manchester; Smith & Gibb of the Oxford Road Distillery, Manchester; Mitchell & Co. of Chard; and Charles Mytton of Welshpool.
- 6.21** The remaining vessels comprise plain white household bottles and jars, hot water bottles, flat-bodied spirit flasks and part of a water filter. All of these vessels are depicted in contemporary adverts (reproduced in Henrywood 1992, 62-63).
- 6.22** The only kiln furniture from Context 223 was a single roll of red earthenware.

Pottery from Context 405

- 6.23** This deposit contained a dump of Bristol glazed stoneware wasters (Plates 21–23). The wasters comprise bottles, flagons and jars produced by J & C Price & Brothers, one of which was marked with the same diamond registration mark as the vessels from Context 223. The vessels are quantified by form in Table 4. Stratigraphic evidence indicates that this Context was deposited during the infilling of the dock in the 1870s.
- 6.24** Many of the vessels were marked with impressed manufacturer's marks. The ginger beer and most of the household product bottles were marked with a Price type 1 mark; the flagons were marked with a type 2 mark (see Context 223 above). One of the household product bottles was marked 'PRICE BRISTOL' (type 3 mark).

Table 4: Bristol Glazed Stoneware Wasters from Context 405 Quantified By Form

Form	No. of Sherds
Ginger beer bottle	14
Household product bottles	8
Sprit flagons	6
Plain straight sided jars (preserve jars)	2
Ink bottles	2
Flat sided spirit flask	1
Porter beer bottle	1
Total	36

6.25 The most frequent ginger beer bottles were plain all white types with no proprietary marks. A single example of a plain ginger beer with a honey coloured top and a white lower half was also recovered.

6.26 Two complete honey coloured ginger beer bottles with impressed proprietary marks were also recovered. The first was impressed with the words 'TRADE MARK' surrounding an image of the young Queen Victoria and the words 'VICTORIA BREWERY Co MANCHESTER'. The other was impressed with the words 'BARRATT & SHORTRIDGE OLDHAM' and 'REGISTERED TRADE MARK' surrounding an image of a cockerel.

6.27 Context 405 also contained a few sherds of refined white earthenware and transfer-printed wares that are indicative of household refuse. Of note amongst these wares were a near complete polish paste pot for a Bath based company printed with the words:

'BIDMEAD'S CELEBRATED ECONOMICAL POLISHING PASTE. Warranted to be one of the best ever produced For giving a Brilliant Polish equal to new with little labour & in half the usual time. on BRASS, COPPER, TIN, ZINC, & ALL OTHER METALS, (IN POTS AT 6 PENCE EACH.) DIRECTIONS FOR USE. Take a small portion of the Paste on a wet Flannel rub it briskly over the Article then with a soft dry cloth clean it off in _____ent the Paste becoming dry allo _____ the top of the Pot after using. MAY BE HAD / Of any respectable Druggist. Grocer. Ironmonger. Oilman. Tinman & c. in Town and Count _____ any Quantities of the MANUFAC _____ W. BIDMEAD WIDCOM _____'

6.28 The other notable sherd is part of a 'Prattware' paste pot lid printed with an image of game (dead pheasants and stag) and the words 'TATNELL & SON. PEGWELL BAY. RAMSGATE'.

Pottery from Context 410

6.29 This deposit comprised a dump of transfer printed and painted whiteware wasters dating from the early 1840s. The assemblage from Context 410 is very similar to the group from Context 112. The wareware wasters include biscuit fired and glazed vessels. The waster sherds are quantified by form in Table 5.

Table 5: Whiteware Wasters from Context 410 Quantified By Form

Form	No. of Biscuit Fired Sherds	No. Glazed Sherds	Total
Teacups	9	1	10
Plates	9	1	10
Bowls	1	2	3
Jars	-	3	3
Cups	2	-	2
Unidentified	8	5	13
Total	29	12	41

6.30 Decorative motifs include mocha ware and transfer-printed ware. The transfer-printed wares include ‘willow’ pattern and a sherd printed with the word ‘HONG’ and ‘P & G’ on the reverse; this can be identified as a product of Pountney & Goldney dating from the period 1836–49. Some of the plates were feather-edged or wavy-edged ‘Queen’s ware’. The glazed sherds have a clear glaze that is greenish or bluish where it has pooled.

Kiln Furniture from Context 410

6.31 The kiln furniture from Context 410 comprises 15 handmade white clay stilts, three saggar fragments, four strips or rolls of red earthenware, two rolls of white clay, two one handmade white clay spur.

6.32 Apart from the handmade spur, the kiln furniture from Context 410 is identical to the kiln furniture from Contexts 105, 109-10, 112 and 114. Three sizes of stilt were identified; the arms are 50mm, 40mm and 30mm long.

Ceramics from Context 411

6.33 This deposit was probably deposited in the 1870s or possibly slightly later. The ceramics from this Context include part of a Bristol glazed stoneware flagon and five sherds from a 300mm wide glass crucible. The glass on the interior of the crucible is aqua (slightly greenish) coloured. The crucible has a hard gritty white fabric, with a black vitrified exterior. The crucible probably came from a nearby glassworks, such as the Pheonix Works, Avon Street.

Clay Tobacco Pipes

6.34 A total of ten fragments clay tobacco pipe were collected, nine of which are un-diagnostic stems. A marked pipe bowl was recovered from Context 405. The pipe bowl was identified with reference to the Bristol tobacco pipe bowl typology (Jarrett 2013, 215-37), and other published sources (Jackson & Price 1974; Price 2011).

- 6.35** The marked pipe is a BRST Type 15 bowl dating from the period c 1690–1740. The pipe is marked ‘IS’ incuse on the back of the bowl. Pipes with this mark are frequently found during excavations in Bristol and it seems probable that some of them were made by John Squibb, who was born c 1671, made free in 1704 and died in 1738 (Price 2011, 3791-3808).

Animal Bone

- 6.36** The assemblage comprises five fragments of animal bone from two 19th-century Contexts. Context 105 contained three cattle metapodial shafts that had the ends neatly sawn off with a fine-toothed saw, and a sheep/goat tibia. One of the cattle metapodials had also been longitudinally sawn in half. These cattle bones can be identified as bone working waste (Rixon 1989, 49-62). The straight dense section of the metapodial provided material that could be used for a number of purposes including pin, dice and handle making. Context 105 comprises material brought in from elsewhere in the city for use in ground raising; these finds are not therefore considered to be evidence of bone working on the site.
- 6.37** A near complete bone toothbrush from Context 205. The handle of the toothbrush is slightly curved and has holes drilled halfway through the stock to accommodate bristles. There are further thin holes, drilled from the head of the stock, which would have accommodated threads that secured the bristles in place. Toothbrushes were introduced to Europe from China in the mid-17th century, but they remained relatively uncommon until the end of the 18th century. The modern toothbrush was invented in 1780 by a London tanner named William Addis. His method of manufacturing bone-handled toothbrushes became common in the 19th century before being superseded by synthetic replacements towards the end of the century (Mattick 1993, 162).

Glass

- 6.38** The glass assemblage comprises 31 shards of bottle glass, 3 chunks of glass working waste and 2 shards of window glass, all of which is likely to date from the 19th century. The glass is quantified by Context in Appendix 5.
- 6.39** Most of the glass bottles are beer, wine or mineral water bottles dating from the second half of the 19th century, some of which are embossed with markers and/or proprietary marks. Five of the bottles (two round bottomed ‘torpedo’ types and three flat bottomed cylindrical bottles) from Contexts 403-5 are marked with the words ‘___ BATH & BRISTOL ___’, the name of the proprietor is missing from all five bottles. Two bottles (from Contexts 207 and 405) are ovate mineral waters for Schweppes & Co. A beer bottle from Context 405 is embossed ‘IMPERIAL’ on the shoulder; this bottle has the manufacturers mark ‘S & G BRISTOL’ on the base.

- 6.40** Of note amongst the assemblage is a mineral water bottle embossed with the words 'BOYCE & SONS, CHAMPAGNE GINGER BEER' on one side and 'MEADOW STREET, ST PAULS, BRISTOL'. The bottle is marked 'P & R B' on the base, which identifies it as a product of Powell & Ricketts of Bristol. This company formed when Powell's Glass amalgamated with the adjoining Phoenix Bottle Works in 1853. By the late 19th century, the company had become one of the largest manufacturers in Britain, but a failure to invest in modern bottle making machines lead to the factory's closure in 1922.

Ceramic Building Material

- 6.41** The ceramic building material comprises seven post-medieval roof tile fragments (from Contexts 110, 218 and 223), a glazed white ceramic wall tile from Context 219 and a Bristol Type 1 medieval roof tile from 19th-century Context 105. The medieval tile must have been imported onto the site with soil derived from elsewhere in the city.

Metal

- 6.42** The metal assemblage comprises three iron objects and a run of lead, all of which date from the 19th century. The iron objects comprise a nail from Context 219, an iron bolt from Context 210, and an iron coach screw from Context 209. The iron bolt is part of a mounting for a dockside crane attached to early 1840s wall 210. The bolt is 50mm wide, 120mm long and has a 6mm thread. The coach screw was one of a number that were screwed into the upper side of longitudinal railway sleeper 209. The screw is 160mm long, 15mm wide and has a 5mm thread. The run of lead was recovered from the crane mounting in wall 210. The lead is likely to have been used as a filler/sealant where the iron mountings were attached.

Other Finds

- 6.43** The other finds comprise 5 pieces of glassy slag from Context 223, a struck flint fake from Context 105, a lump of wall plaster from Context 110 and an edible crab claw (cancer pagurus) from Context 403. The flint flake must have been imported onto the site with soil derived from elsewhere in the city. The crab claw is probably food waste.

Discussion and Conclusions

- 6.44** Apart from a residual struck flint flake and few sherds of medieval and 17th/18th pottery, all of the finds are 19th century. The finds assemblage is dominated by industrial waste derived from nearby manufactories, including potteries and glass works. Of note amongst the assemblage are two groups of ceramic wasters dating from the early 1840s and the late 1870s.

- 6.45** The earlier group comprises whiteware wasters (mostly tablewares) from Contexts 112 and 410. A single sherd from Context 410 can be identified as a product of Pountney & Goldney's Bristol Pottery in Temple Back (which was situated approximately 230m to the north-east of the site) dating from the period 1836–49. Both Contexts are likely to be part of an extensive waster dump, which formed a hard surface along the north-east side of Brunel's dock. This deposit is likely to have been dumped during the construction of the dock in the early 1840s.
- 6.46** The latter group comprises Bristol glazed stoneware wasters from drain fill 223 and made ground deposit 405, both of which contained sherds marked with a diamond registration mark issued on the 4th of November 1876. The wasters can be identified as a product of Joseph & Charles Price & Brothers' Thomas Street/Victoria Street pottery. The wasters from Context 223 were predominantly spirit flagons; the most common wasters from Context 405 were ginger beer bottles. J & C Price & Brothers were a major manufacturing company who produced goods for the national market, which is reflected in the geographic spread of the proprietary marks on the vessels from Contexts 223 and 405. The fact that the wasters were produced after the 4th of November 1876 indicates that the final infilling of the dock and re-building of the Goods Shed cannot have been completed until after this date.
- 6.47** The finds from Temple Quay demonstrate the potential of the site for producing significant deposits of pottery wasters from at least two local manufacturers, which should be borne in mind when devising archaeological mitigation strategies during any future redevelopment of the site.

7 Discussion and Conclusions

- 7.1** The archaeological evaluation uncovered structural remains of Isambard Kingdom Brunel's early 1840s Dock and associated structures together with Goods Shed. Structural remains of the 1870s rebuilding of the Goods Shed and deposits associated with the infilling of the dock were also recorded.
- 7.2** Trenches 2 and 3A demonstrated that the south-west side of the dock wall and the adjoining Goods Shed are well-preserved and incorporate a crane base and internal features of the Goods Shed. The top of the dock wall was recorded at a height of 9.53m AOD. Structural remains of the 1870s rebuild of the station were recorded above and to the south-west of the dock wall at a height of 10.65m AOD.
- 7.3** Trenches 1 and 3B demonstrated that a significant amount of the north-east wall of the dock had been removed prior to the infilling of the dock in the 1870s. A substantial stone-built feature was recorded in Trench 1 at a depth of 9.28m AOD; this feature is probably a structural buttress that was either too difficult or uneconomic to remove when the dock wall was dismantled. A deep machine cut slot was excavated across the line of the robbed-out of the dock wall in Trench 1 to determine if there were any remains of the wall at greater depth. This trench uncovered a masonry obstruction at a depth of 8.45m AOD, but due to the rapid ingress of groundwater at 8.8m AOD it was impossible to determine if this was an in-situ part of the dock wall or a large lump of masonry in the infill of the dock.
- 7.4** A dockside surface of compacted clinker was recorded at a depth of 9.28m AOD in Trench 3B, but there was no indication of any surviving remains of the dock wall. The trench was excavated to a maximum depth of 8.58m AOD, but the rapid ingress of water below 8.8m AOD made it impossible to determine if there were any remains of the wall at greater depth. A small wall was uncovered in Trench 3B is probably best interpreted as a boundary wall that segregated the open area to the north-east of the Dock from the railway tracks to the south-east.
- 7.5** Previous geotechnical investigations within the basin of Brunel's dock uncovered alluvium at depths of between approximately 4–5m AOD (Arup 2002, figs. 2a-c); this is approximately 4.5m below the level of the dock wall in Trench 2, but only 1.5–2.5m below the mean level of the Floating Harbour (6.5m AOD), which is clearly too shallow for large barges. It seems likely therefore that the upper part of the alluvium recorded in the geotechnical investigations is actually silting within the Dock dating from the period immediately before it was infilled. The water level in the dock would have been 3m below the dockside. The true depth of the dock remains unknown.
- 7.6** A dump of wasters from Pountney & Goldney's Water Lane Pottery dating from the period 1836–49 were used to form a metalled surface around the north-east side of the dock; this is consistent with the early 1840s date for the construction of the dock.

- 7.7** Wasters from J & C Price & Brothers' Thomas Street/Victoria Street stoneware manufactory were recovered from Contexts dating from the re-building of the Goods Shed. Some of the potsherds were stamped with a registration mark issued on the 4th November 1876; this indicates that the re-building of the Goods Shed must have been completed after this date.
- 7.8** The evaluation demonstrated that the SW side of the dock wall is well preserved and there are well-preserved remains of the goods station to the SW of the dock wall. Cartographic and other documentary sources indicate that this part of the proposed development area is likely to contain walls, column bases, roadways, rail tracks, loading banks and turntables associated with the 1840s Goods Shed. There are also likely to be remains of walls, rail tracks, and roadways of the late 1870s Goods Shed in the same area. The remains of other features such as steam cranes, goods wharves, an engine house and other ancillary structures may survive in other parts of the site, but much of the NE side of the dock wall is likely to have been demolished in the 1870s. It remains unknown if the SE end of the dock wall survives and exactly how much of the NE dock wall was removed in the 1870s. It may be possible to resolve these uncertainties by drilling closely spaced geoarchaeological borehole transects across the projected lines of the NE and SE dock walls or possibly, subject to specialist advice as to suitability of the site conditions, by carrying out a ground-penetrating radar survey.
- 7.9** The dock is a significant feature from the early years of railway development in Britain, designed by one of the most important engineers of the time, Isambard Kingdom Brunel. Interchanges between water-borne and wagonway/tramway were fairly common in the 18th and early 19th centuries, but this was an especially large example associated with one of the earliest modern main line railways. Its short life demonstrates the rapid development and supremacy of rail transport. The well-preserved western wall still retains original features such as the crane base. Contemporary map evidence suggests that the other walls never possessed such facilities, and it appears that they are now in a much poorer state of preservation, having been largely removed in the 1870s.
- 7.10** It is likely that the planning authority will require the western side of the dock to be preserved in-situ below the development, but it will probably be acceptable for the development to have an impact on the remainder. It is also likely that archaeological recording will be necessary where archaeological remains will be exposed or impacted. However, the final decision, and the precise requirements, will need to be determined by, or in conjunction with, the Bristol City Council Senior Archaeological Officer and Archaeological Officer, once they have considered the results of the evaluation presented in this report.

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Illustrations and Plates

Plot 3, Temple Quay, Bristol. BHER 25342

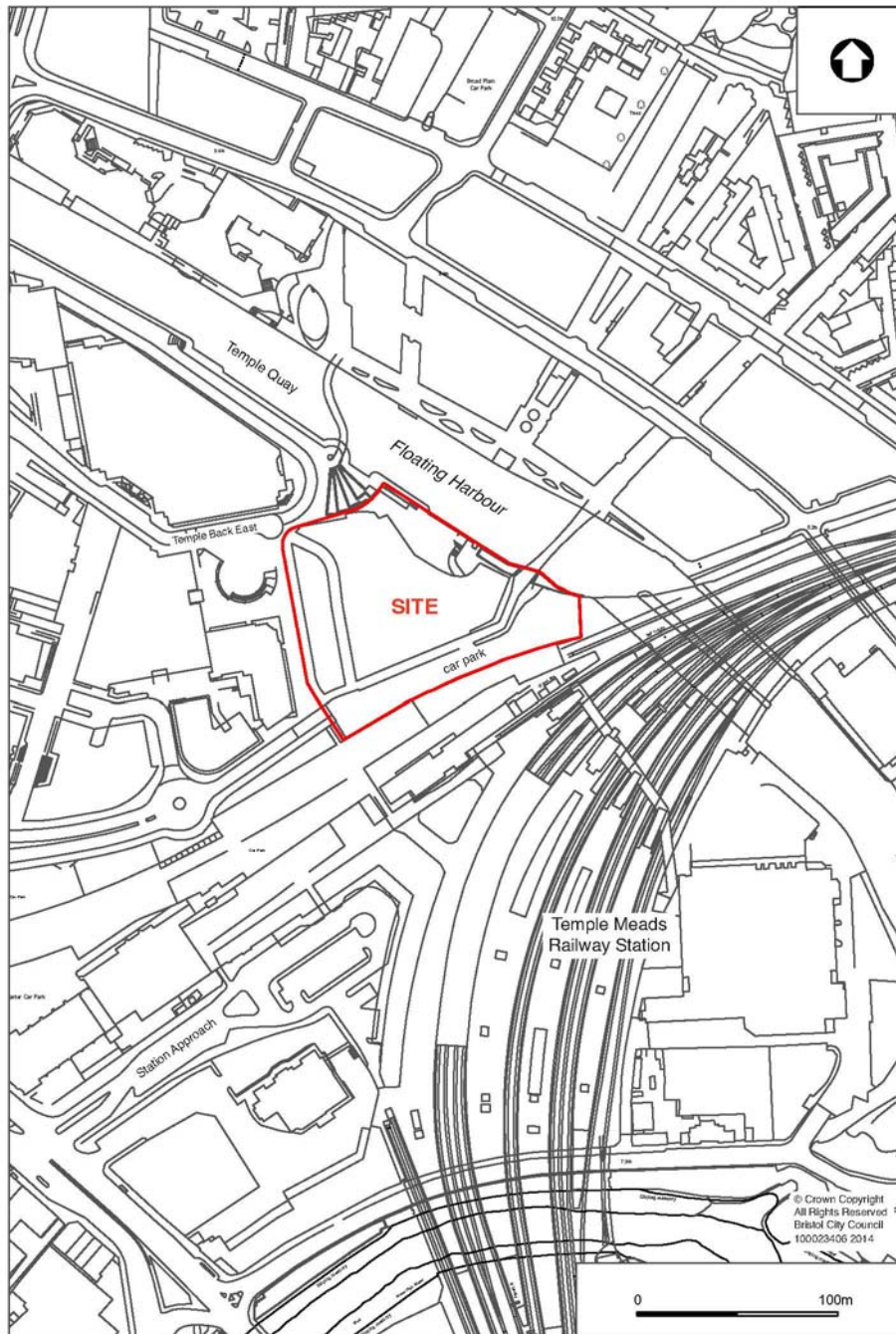


Fig.1 Site location plan, scale 1:2500

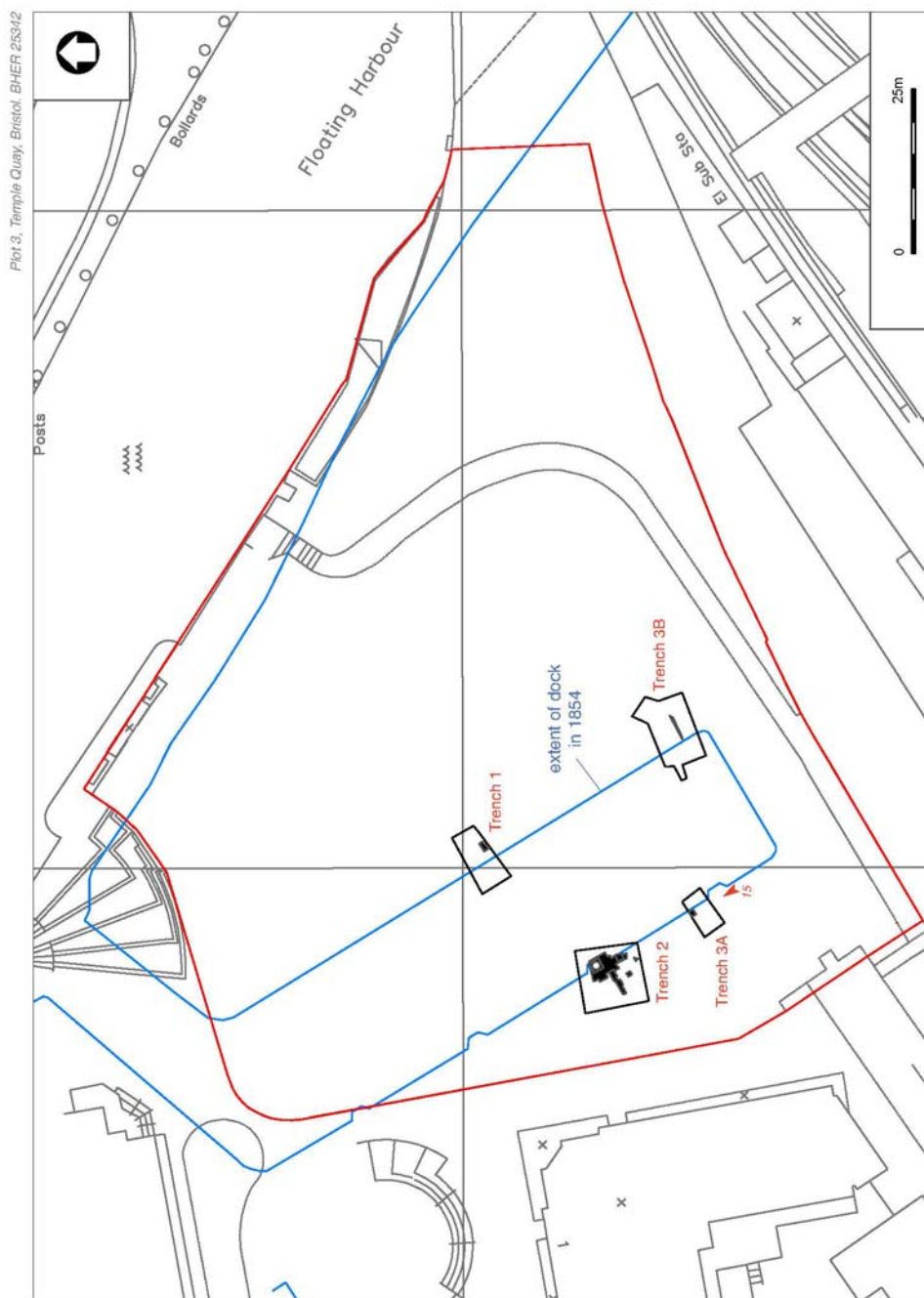


Fig 2 1:750 site plan, showing plate directions and extent of the Dock in 1854

Plot 3, Temple Quay, Bristol. BHER 25342

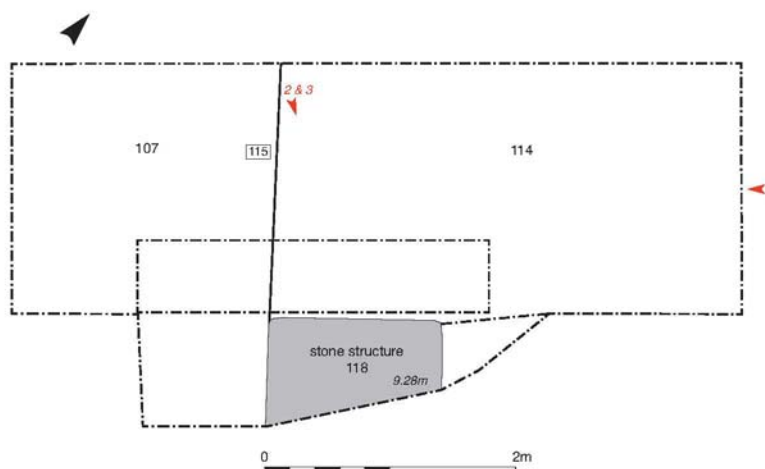


Fig.3 Plan of Trench 1, showing plate directions, scale 1:40

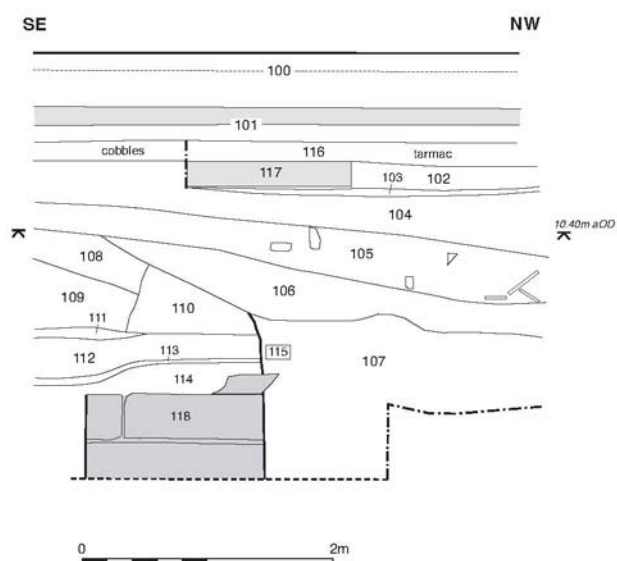


Fig.4 Section of features in Trench 1, scale 1:40

Plot 3, Temple Quay, Bristol. BHER 25342

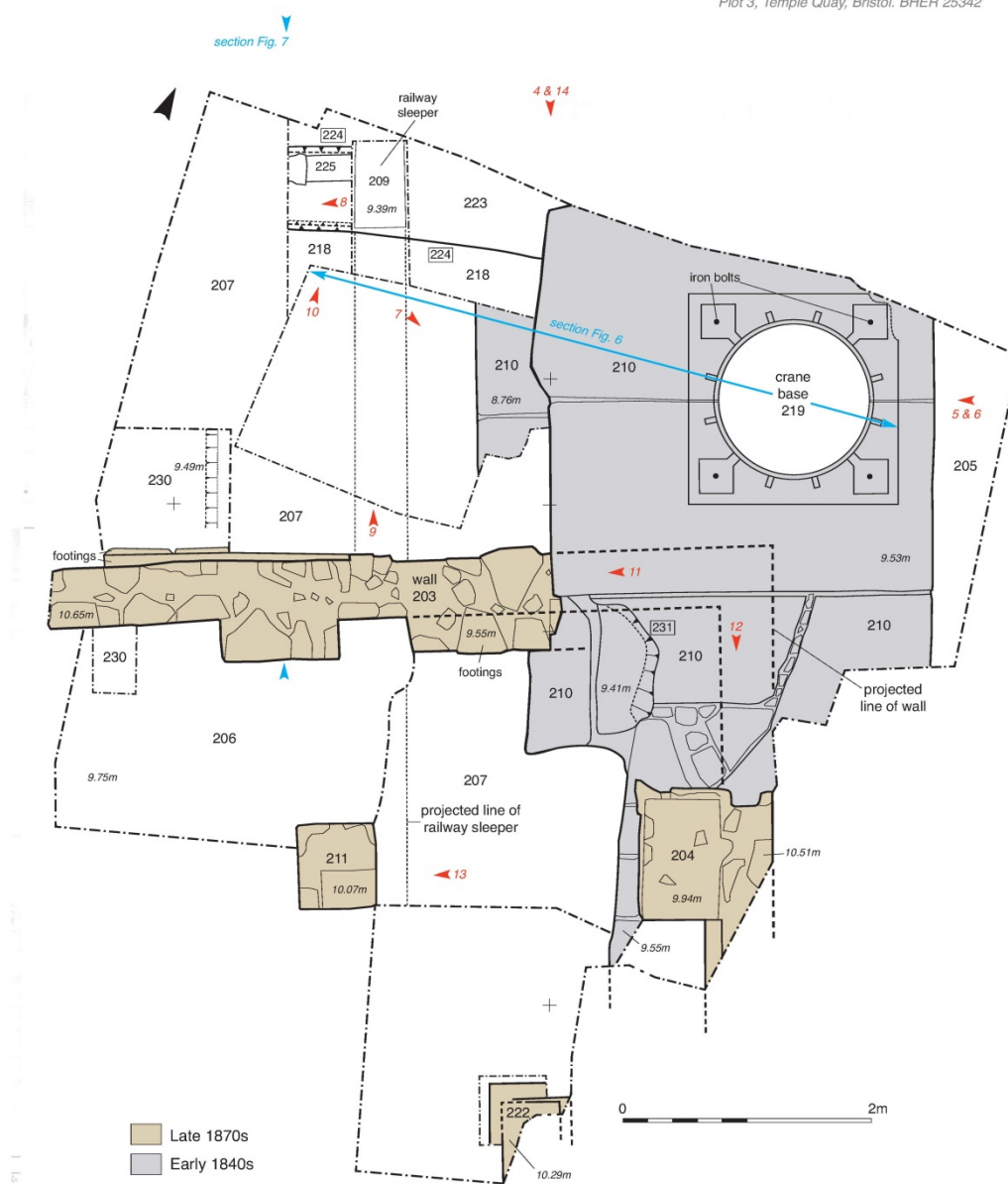


Fig.5 Plan of Trench 2, with plate directions in red, scale 1:40

Plot 3, Temple Quay, Bristol. BHER 25342

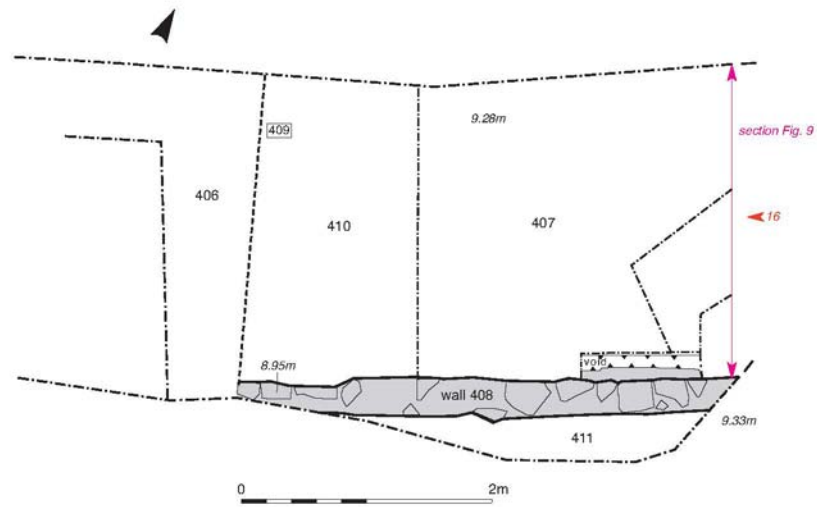


Fig.8 Plan of Trench 3B, with plate directions in red, scale 1:40

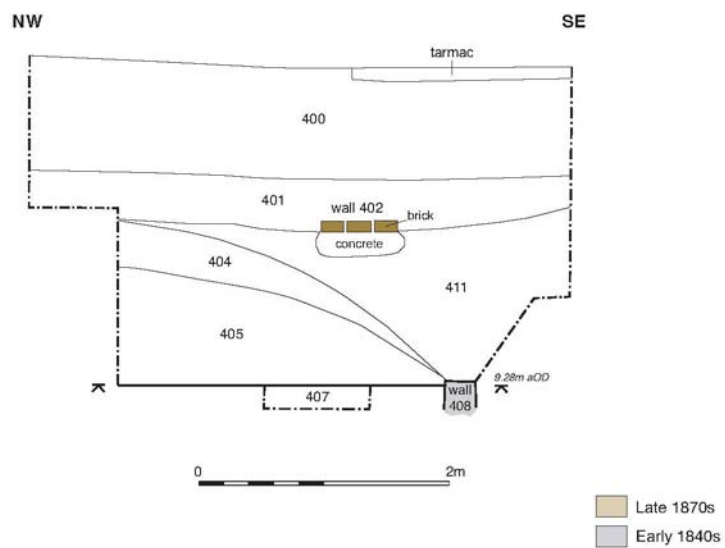


Fig.9 Section of features in Trench 3B, scale 1:40

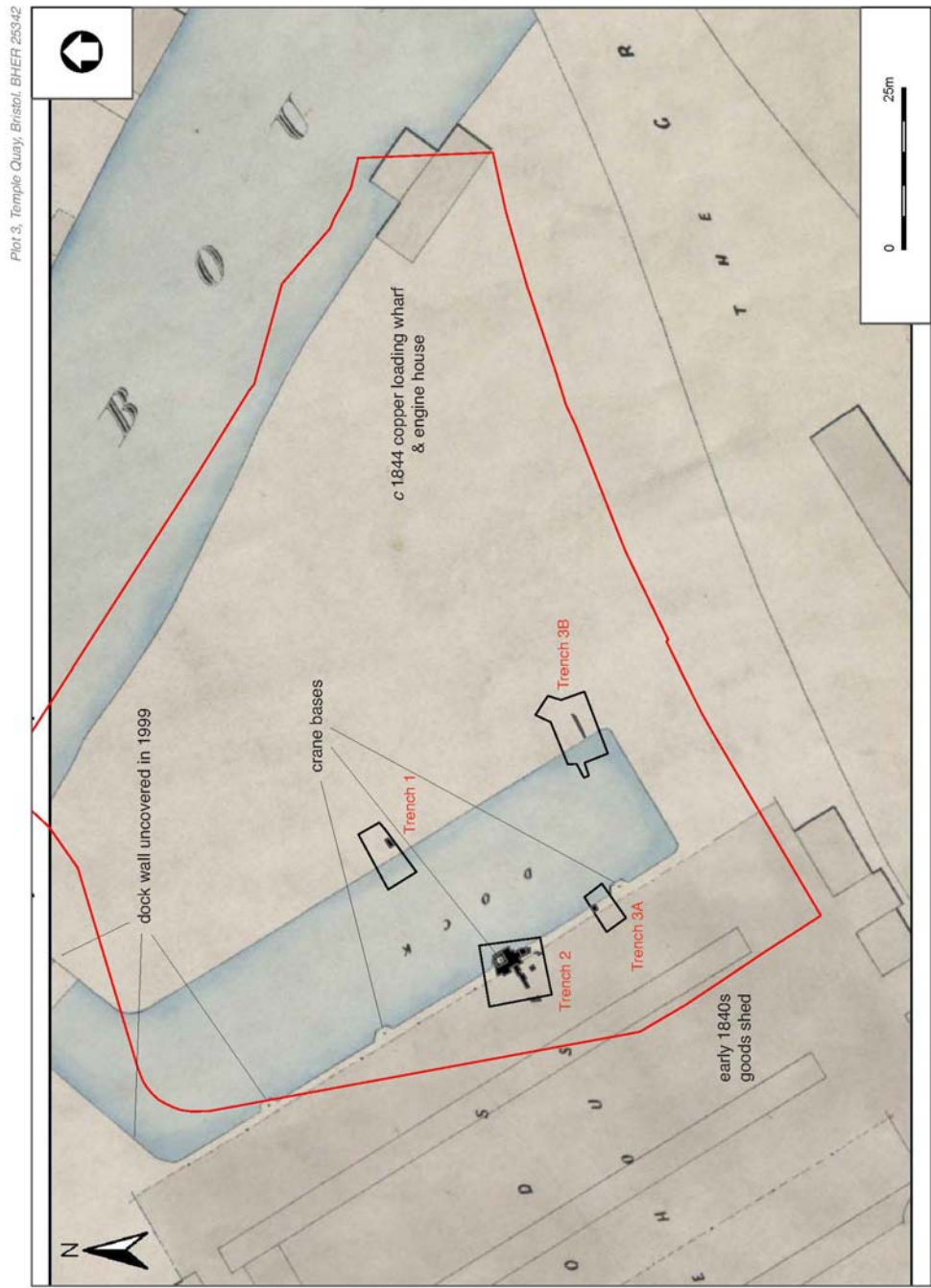


Fig. 10 Plan showing trench locations and structures shown in relation to Ashmead's plan of 1854, scale 1:750

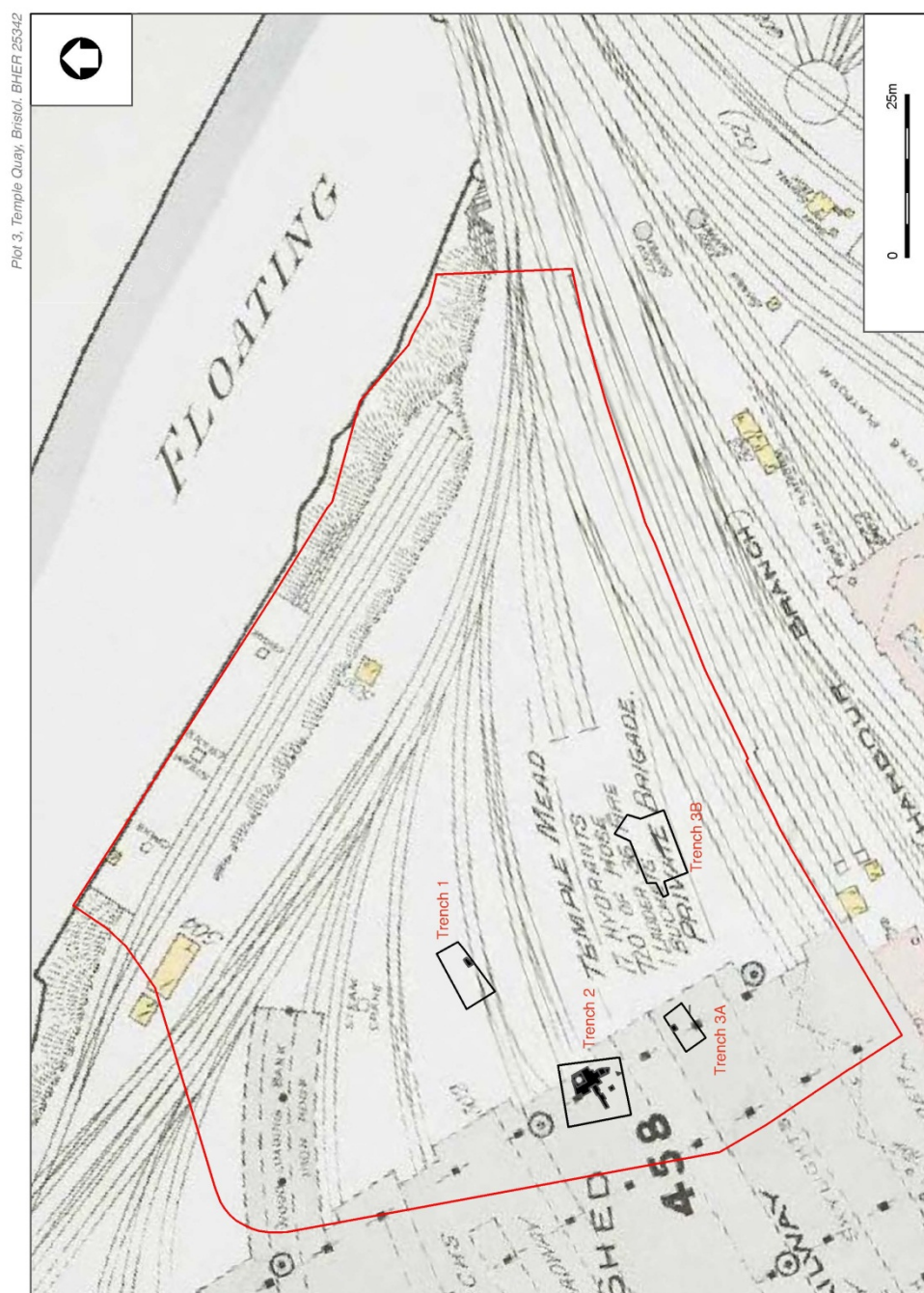


Fig.11 Plan showing trench locations and significant structures shown in relation to Goad's Fire Insurance Plan of 1896, scale 1:750

Plot 3, Temple Quay, Bristol. BHER 25342



Plate 1
Trench 1, looking south-west



Plate 2
Trench 1, looking south-east,
showing robber cut 115



Plate 3
Trench 1, looking south-east,
showing structure 118



Plate 4
Trench 2, looking south-east

Plot 3, Temple Quay, Bristol. BHER 25342



Plate 5
Trench 2, looking south-west,
showing crane base in dock
wall 210

Plate 6
Trench 2, looking south-west,
showing the NE edge of dock
wall 210 after extension of the
trench



Plate 7
Trench 2, looking east, showing
stepped foundations of dock
wall 210

Plate 8
Trench 2, looking south-west,
showing drain 224



Plot 3, Temple Quay, Bristol. BHER 25342



Plate 9
Trench 2, looking north-west,
showing section in the
north-west side of the
machine-cut slot in the
base of the trench



Plate 10
Trench 2, looking north,
showing timber beam 209



Plate 11
Trench 2, looking south-west,
showing wall 203



Plate 12
Trench 2, looking south-east,
showing wall 204

Figures

Figure 1 Site location plan, scale 1:2,500

Figure 2 Site plan, with plate directions in red and extent of the Dock in 1854, scale 1:750

Figure 3 Plan of Trench 1, showing plate directions, scale 1:40

Figure 4 Section of features in Trench 1, scale 1:40

Figure 5 Plan of Trench 2, showing plate directions, scale 1:40

Figure 6 Section of features in Trench 2, scale 1:40

Figure 7 Section of features in Trench 2, scale 1:40

Figure 8 Plan of Trench 3B, showing plate directions, scale 1:40

Figure 9 Section of features in Trench 3B, scale 1:40

Figure 10 Plan showing trench locations and structures overlaid on Ashmead's plan of 1854, scale 1:750

Figure 11 Plan showing trench locations and significant structures shown in relation to Goad's Fire Insurance Plan of 1896, scale 1:750

Plates

Cover Trench 2, looking south-east

- Plate 1 Trench 1, looking south-west
- Plate 2 Trench 1, looking south-east, showing robber cut 115
- Plate 3 Trench 1, looking south-east, showing structure 118
- Plate 4 Trench 2, looking south-east
- Plate 5 Trench 2, looking south-west, showing crane base in dock wall 210
- Plate 6 Trench 2, looking south-west, showing the NE edge of dock wall 210 after extension of the trench
- Plate 7 Trench 2, looking east, showing stepped foundations of dock wall 210
- Plate 8 Trench 2, looking south-west, showing drain 224
- Plate 9 Trench 2, looking north-west, showing section in the north-west side of the machine-cut slot in the base of the trench
- Plate 10 Trench 2, looking north, showing timber beam 209
- Plate 11 Trench 2, looking south-west, showing wall 203
- Plate 12 Trench 2, looking south-east, showing wall 204
- Plate 13 Trench 2, looking south-east, showing pillar 211 overlying the decayed remains of a railway sleeper
- Plate 14 Trench 2, looking south, showing concrete floor 201
- Plate 15 Trench 3A, looking north-west, showing dock wall 306
- Plate 16 Trench 3B, looking south-west, showing wall 408
- Plate 17 J & C Price & Brothers' type 1 mark
- Plate 18 J & C Price & Brothers' type 2 mark
- Plate 19 J & C Price & Brothers' type 3 mark
- Plate 20 Stoneware spirit flagon waster from Context 223
- Plate 21 Ginger beer bottles from Context 405
- Plate 22 Household product bottles from Context 405
- Plate 23 Spirit flask and porter beer bottle from Context 405

Appendix A

Policy Statement

A1 Policy Statement

This report is the result of work carried out in the light of national and local-authority policies.

NATIONAL PLANNING POLICY (ENGLAND)

The National Planning Policy Framework (NPPF) for England published by the UK Government in March 2012 states that the historic environment, which includes designated and non-designated heritage assets, is an irreplaceable resource and, as such, should be taken into account by Local Planning Authorities when considering and determining planning applications. This is taken to form part of a positive strategy set out in the respective Local Plan (i.e. Bristol Core Strategy) to ensure the conservation and enjoyment of the historic environment. The assigned significance of heritage assets will be key factor in terms of their conservation.

Given their irreplaceable nature, any harm to, or loss of, a heritage asset, or heritage assets, should be clearly and convincingly justified as part of a planning application. As part of this, applicants are required to describe the significance of any heritage assets affected by a proposal, including any contribution made by their setting. Where a heritage asset, or assets, are to be harmed or lost as the result of a proposal, the applicant will be required to record and advance the understanding of the significance of that asset or assets, to include making the evidence arising publicly accessible, but this will be in proportion to the significance of the asset/assets in question. While the NPPF takes into account the historic environment as a whole, additional protection is afforded to designated heritage assets under current English Law. Any proposal that would result in harm or loss of a designated heritage asset is also required to be justified by the applicant in meeting strict criteria set out in the NPPF.

LOCAL POLICY

Bristol City Council Supplementary Planning Document 7 Archaeology and Development (SPD 7, adopted 2006) has been carried forward for use under the present Bristol Core Strategy (adopted 2011). SPD 7, page 4 states that:

- (i) There will be a presumption in favour of preserving any archaeological features or sites of national importance, whether scheduled or not;
- (ii) Development which could adversely affect sites, structures, landscapes or buildings of archaeological interest and their settings will require an assessment of the archaeological resource through a desk-top study, and where appropriate a field evaluation. Where there is evidence of archaeological remains, development will not be permitted except where it can be demonstrated that the archaeological features of the site will be satisfactorily preserved in situ, or a suitable strategy has been put forward to mitigate the impact of development proposals upon important archaeological remains and their settings; or, if this is not possible and the sites are not scheduled or of national importance, provision for adequately recording the site prior to destruction is made, preferably by negotiating a planning agreement to ensure that access, time and financial resources are available to allow essential recording and publication to take place.

The Bristol Core Strategy (2011) retains some policies from the 1997 Bristol Local Plan including for the protection of the historic environment as Policy BCS22 which states that development proposals will safeguard or enhance heritage assets and the character and setting of areas of acknowledged importance including:

- Scheduled ancient monuments;
- Historic buildings both nationally and locally listed;
- Historic parks and gardens both nationally and locally listed;
- Conservation areas; and
- Archaeological remains.

Appendix B

Context Summary

B1 Context Summary

Context No.	Type	Description	Date
Trench 1			
100	Layer	Tarmac surface. 0.14m thick tarmac overlying a 0.3m thick angular gravel bedding layer.	Modern
101	Structure	Concrete surface. Reinforced concrete surface overlying an angular gravel bedding layer. 0.3m thick.	Modern
102	Layer	Made ground. Compact black and dark brown slag and gritty silt up to 0.2m thick.	Late 1870s +
103	Layer	Made ground. Friable yellow sandstone gravel and brown silt up to 0.05m thick.	Late 1870s +
104	Layer	Railway track bedding. Compact dark bluish grey slag and brown silt up to 0.6m thick.	Late 1870s +
105	Layer	Made ground. Compact mixture of orangey brown, grey, yellow, brown, bluish grey and greenish-brown silt, clay and rubble up to 0.4m thick.	Late 1870s
106	Layer	Infill of dock. Compact brownish-pink silty and sandy clay with occasional yellow sandstone inclusions. Up to 0.45m thick.	Late 1870s
107	Layer	Infill of dock. Compact mixture of pink, brownish-pink, bluish grey and yellow silty and sandy clay with occasional yellow sandstone inclusions. Over 1.4m deep.	Late 1870s
108	Layer	Made ground. Compact brownish to yellowish grey silty clay with occasional sandstone and charcoal inclusions. Up to 0.4m thick.	Early 1840s – 1870s
109	Layer	Made ground. Compact mixture of brown and grey silty clay with brick and stone rubble and ceramic waster inclusions. Up to 0.5m thick.	Early 1840s – 1870s
110	Layer	Made ground. Compact brownish to greenish grey and grey silty clay with stone rubble and ceramic waster inclusions. Up to 0.55m thick.	Early 1840s – 1870s
111	Layer	Made ground. Compact reddish brown silt with stone inclusions. Up to 0.05m thick.	Early 1840s
112	Layer	Made ground. Compact very dark grey and brown ash clinker and silt with ceramic waster inclusions. Up to 0.32m thick.	Early 1840s
113	Layer	Made ground. Compact brown and red ash clinker and silt with angular sandstone inclusions. Up to 0.1m thick.	Early 1840s
114	Layer	Made ground. Compact grey slag and ceramic wasters. Over 1m thick.	Early 1840s
115	Cut	Robber cut. Vertical NW–SE aligned cut along the line of the NE side of the dock wall.	1870s
116	Layer	Tarmac and granite sett surface. 0.15m thick.	Modern
117	Structure	Concrete foundations/surface. 0.2m thick.	Modern

Trench 2			
200	Layer	Gravel surfaces. At least four layers of angular gravel. 1–1.3 m thick.	Modern
201	Structure	Concrete floors. Two layers of concrete. Up to 0.4m thick.	Modern
202	Layer	Demolition dump. Friable brick and stone rubble up to 0.7m thick.	1926
203	Structure	Wall of Goods Shed. Constructed with roughly dressed Pennant Sandstone blocks bonded with a soft pale grey lime mortar with coal and lime inclusions. Foundations 0.75m wide, wall 0.5m wide. Over 4m NE–SW, up to 1m high, with a 0.35m by 0.95m wide buttress on the SE side.	Late 1870s
204	Structure	Wall of Goods Shed. Constructed with roughly dressed Pennant Sandstone blocks bonded with a soft pale grey lime mortar with coal and lime inclusions. Foundations 0.55m wide, wall 0.4m wide. Over 1.7m NW–SE and up to 0.75m high, with a 1.05m by 0.65m wide buttress on the SW side.	Late 1870s
205	Layer	Railway track bedding. Friable black clinker, ash and slag up to 1.2m thick.	Late 1870s – 1925
206	Layer	Made ground. Soft dark grey and orangey brown sandy clay, clinker and ash with common angular sandstone inclusions. 0.5–0.7m thick.	Late 1870s
207	Layer	Made ground. Firm reddish brown, yellow, dark pink grey and greenish silty clay with angular sandstone inclusions. Up to 0.4m thick.	Late 1870s
208	Layer	Made ground. Firm brownish grey silty sand with occasional angular sandstone inclusions. 0.2m thick.	Early 1840s
209	Timber	Timber beam. Measured 280mm wide and 120mm deep. Joined to another beam (removed during machining) with two pieces of slate slotted into a cut in the end of the timbers. A zig-zag row of coach screws on the top of the beam indicate the position of a rail. It is unclear if the beam is <i>in-situ</i> .	Early 1840s – 1870s
210	Structure	Dock wall and crane base. Constructed with massive Pennant Sandstone blocks (up to 3.08m by 1.5m wide and 0.7m thick) bonded with a hard yellowish lime mortar with gravel, slag and coal inclusions. Finely carved crane mounting near the dock edge.	Early 1840s
211	Structure	Pillar within Goods Shed. Constructed with roughly dressed Pennant Sandstone blocks bonded with a soft pale grey lime mortar with coal and lime inclusions. 0.68m by 0.6m wide and 0.7m high.	Late 1870s
212	Cut	Trench for cast iron pipe. Linear SE–NW aligned concave cut. Up to 1.2m wide and 0.8m deep.	Late 1870s – 1925
213	Cut	Trench for cast iron pipe. Linear SE–NW aligned concave cut. Up to 1m wide and 1m deep.	Late 1870s – 1925
214	Fill	Fill of 212. Friable brownish grey silt and slag with small stone inclusions and a 0.15m wide cast iron pipe in the base.	Late 1870s – 1925
215	Fill	Fill of 213. Friable yellow, brown and grey gritty silt and slag with small stone inclusions and a 0.15m wide cast iron pipe in the base.	Late 1870s – 1925
216	Layer	Railway track bedding. Friable greyish brown and black gritty silt and slag with small stone and clinker inclusions. 0.2m thick.	Late 1870s – 1983
217	Layer	Railway track bedding. Friable greyish brown gritty silt and slag with small stone inclusions. Up to 0.2m thick.	Late 1870s – 1925

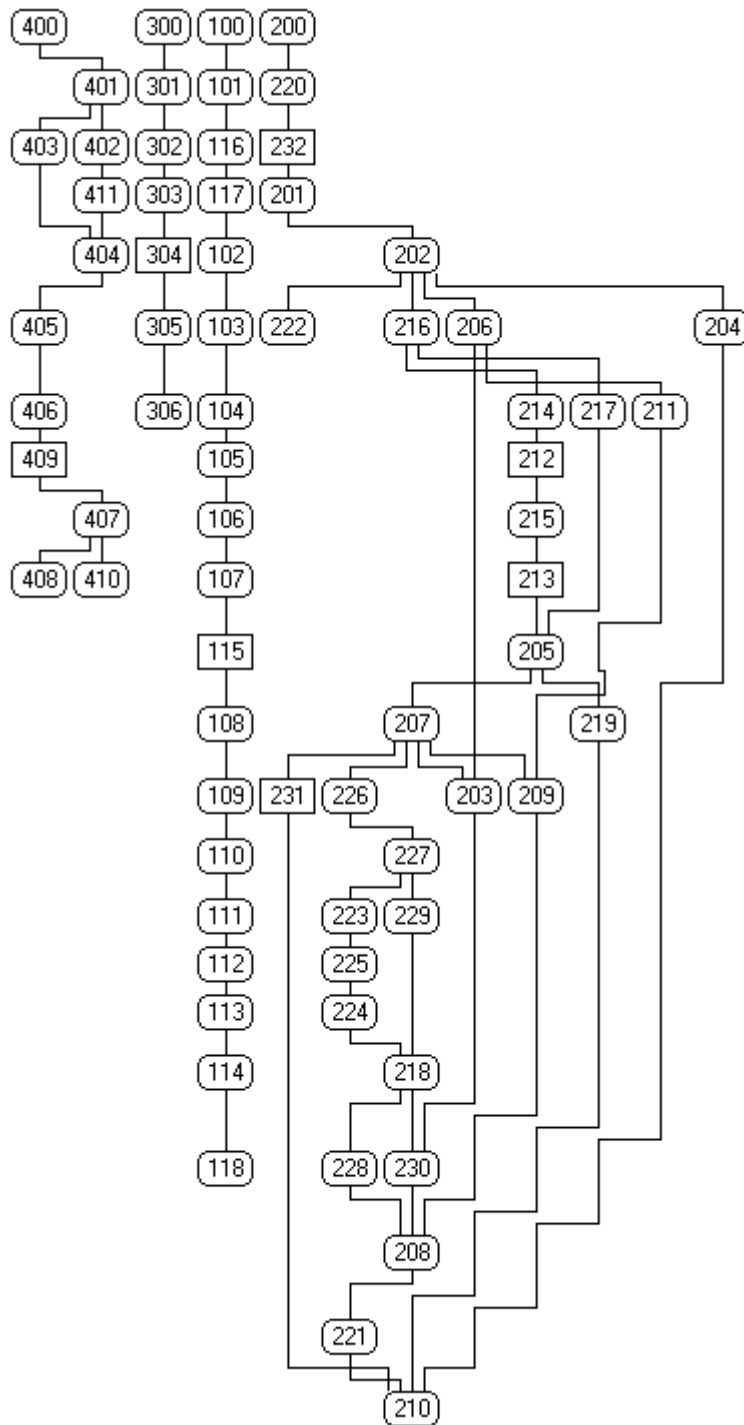
218	Layer	Made ground. Firm brownish grey gritty silty sand with occasional stone inclusions. Up to 0.2m thick.	Early 1840s – late 1870s
219	Fill	Fill of crane base shaft in 210. Soft dark brownish grey silty clay and stone rubble. 1.22m wide, over 1m deep.	Late 1870s
220	Fill	Fill of 232. Friable pink and grey angular gravel.	Modern
221	Layer	Made ground. Firm re-deposited natural brownish grey silty clay with occasional wood inclusions. Over 0.7m thick.	Early 1840s
222	Structure	Pillar within Goods Shed. Constructed with roughly dressed Pennant Sandstone blocks and red bricks (230 x 110 x 75mm) bonded with a soft pale grey lime mortar with coal and lime inclusions. 0.68m by 0.6m wide and 0.7m high.	Late 1870s
223	Fill	Fill of 224. Dump of Bristol-glazed stoneware wasters.	Late 1870s
224	Cut	Drainage trench. Linear NE–SW and NW–SE aligned cut with vertical sides. Slopes steeply towards the SW. Over 2m long, 0.7m wide and up to 1m deep	Late 1870s
225	Fill	Ceramic drain in 224. Constructed with drain pipes measuring 0.7m long and 0.25m wide.	Late 1870s
226	Layer	Made ground. Firm brownish grey silty sand with occasional mortar inclusions. Up to 0.2m thick.	Late 1870s
227	Layer	Made ground. Firm brownish grey silty clay with occasional stone inclusions. Up to 0.2m thick.	Late 1870s
228	Layer	Made ground. Friable grey ashy silt with occasional coal inclusions. 0.1m thick.	Late 1870s
229	Layer	Made ground. Friable sand, silt and brick/tile rubble. Up to 0.2m thick.	Late 1870s
230	Layer	Surface within Goods Shed. Compact angular gravel with tar on the upper surface. NE edge truncated. 0.25m thick	Early 1840s
231	Cut	Irregular area of truncation. Cut chiselled into the edge of dock wall 210, 1m x over 0.6m wide, up to 0.15m deep.	Early 1840s – 1870s
232	Cut	Service trench.	Modern
Trench 3A			
300	Layer	Concrete sett pavement. 0.1m thick.	Modern
301	Layer	Tarmac base for 300. 0.1m thick.	Modern
302	Layer	Gravel surfaces. Compact angular gravel and weak concrete. 1m thick.	Modern
303	Fill	Fill of 304. Friable angular gravel.	Modern
304	Cut	Service trench. Over 2.5m deep, over 2m wide, over 5m long.	Modern
305	Layer	Made ground. Layers of black, red and grey clay, sand, gravel, clinker, ash and slag. Up to 1.3m thick.	Late 1870s+
306	Structure	Dock wall. Constructed with Pennant Sandstone blocks bonded with a hard yellowish lime mortar with gravel, slag and coal inclusions.	Early 1840s

Trench 3B			
400	Layer	Gravel surfaces. Compact angular gravel. 0.8m thick	Modern
401	Layer	Railway track bedding. Friable black clinker. 0.4m thick.	Late 1870s +
402	Structure	Wall. Constructed with a concrete foundation supporting a wall of machine-made bricks (230 x 110 x 75mm) bonded with Portland cement.	1925 – 1983
403	Layer	Made ground. Soft dark grey silty clay with clinker, slag, pottery, glass, brick and stone inclusions. Up to 0.5m thick.	Late 1870s
404	Layer	Made ground. Soft red sand with occasional yellow sandstone inclusions. Up to 0.5m thick.	Late 1870s
405	Layer	Made ground. Soft dark grey clay, black clinker, and dark grey silt with abundant slag, pottery, glass, brick and stone inclusions. Up to 0.5m thick.	Late 1870s
406	Layer	Infill of dock. Soft red sand with common yellow sandstone inclusions. Over 1.1m thick.	Late 1870s
407	Layer	Dockside surface. Compact grey silty sand with common ceramic wasters and brick rubble. 0.42m thick.	Early 1840s
408	Structure	Wall. Constructed with Pennant Sandstone rubble bonded with a soft grey lime mortar with coal and lime inclusions.	Early 1840s
409	Cut	Robber cut. Vertical NW – SE aligned cut along the line of the NE side of the dock wall.	1870s
410	Layer	Made ground. Friable grey clinker, ash, slag and ceramic waster inclusions. Over 0.2m thick.	Early 1840s
411	Layer	Made ground. Firm grey silty clay and crushed mortar with abundant brick and stone rubble inclusions. Up to 1.3m thick.	Late 1870s – 1925

Appendix C

Site Matrix

C1 Site Matrix



Appendix D

Ceramics Quantified by Context

D1 Ceramics Quantified by Context

Context	No.	Bristol Pottery type (BPT)	Fabric	Form	Finds date
105	1	2	BRISTOL C	Jar	1835 – 1900
	1	26	HG	-	
	1	72	BR	Jug	
	1	200	KILNF ENGS BRIS	Stilt	
	2	202	KILNF REFW	Stilt	
	2	202	KILNF REFW BISC	Roll of clay	
	2	202	KILNF REFW BISC	Strip of clay	
	1	202	REFW	-	
	1	223	PEAR	Cup	
	1	264	Local red earthenware	Pancheon	
	1	277	ENGs	Bottle	
	1	278	TPW	-	
107	1	202	REFW	-	1780 – 1900
	1	202	REFW BISC WASTER	-	
109	1	99	TGW	Ointment pot	1835 – 1900
	1	99	TGW BISC WASTER	-	
	1	202	KILNF REFW BISC	Roll of clay	
	2	202	KILNF REFW BISC	Strip of clay	
	1	202	KILNF ENGS BRIS	Stilt	
	2	200	KILNF REFW	Stilt	
	3	202	KILNF REFW BISC	Stilt	
	2	202	REFW	-	
	3	202	REFW BISC	Tea cup	
	11	202	REFW BISC	-	
	1	264	Local red earthenware	-	
	1	277	ENGs WASTER	-	
	2	277	ENGs BRIS	-	
	1	278	TPW BISC	-	
	2	278	TPW3	-	
	2	-	KILNF SAGG	Saggar	
110	1	96	SSOM	Sgraffito jar	1780 – 1900
	1	202	REFW BISC KILNF	Stilt	
	1	264	Local red earthenware	-	
	3	264	WASTER Local red earthenware	-	

Context	No.	Bristol Pottery type (BPT)	Fabric	Form	Finds date
112	1	201	LMPLOC	-	1825 – 1900
	6	202	KILNF REFW BISC	Roll of clay	
	7	202	KILNF REFW BISC	Strips of clay	
	8	202	KILNF REFW BISC	Stilt	
	9	202	KILNF REFW	Stilt	
	8	202	KILNF REFW	Spur	
	23	202	REFW BISC WASTER	-	
	3	202	REFW BISC WASTER	Jar	
	5	202	REFW BISC WASTER	Tea cup	
	2	202	REFW	Plate	
	3	211	STMO	Plate	
	3	223	REFW BISC WASTER	-	
	1	264	Local red earthenware	-	
	1	277	ENGs	-	
	6	278	TPW	-	
	7	278	TPW FLOW	-	
	3	278	TPW FLOW	Tea cup	
	5	278	TPW	-	
	3	278	TPW WASTER	Tea cup	
	1	278	TPW	Drainer	
	2	278	TPW	Plate	
	1	278	TPW	Bowl	
	3	349	PEAR	-	
	3	349	PEAR	Pot lid	
	3	-	SAGG	Saggar	
113	31	264	Local red earthenware	-	1800 – 1900
	1	277	ENGs	Bottle	
114	1	202	KILNF REFW	Stilt	1780 – 1900
	7	202	REFW BISC WASTER	-	
202	1	349	PEAR	Jar	1800 – 1900
	1	349	PEAR	Bowl	
206	1	223	PEAR	Tankard	1810 – 1895
	1	278	TPW3	-	
207	1	95	WEST	-	1750 – 1900
	1	100	STSL	-	
	1	201	LMPLOC	-	
	2	264	Local red earthenware	Flower pot	
	1	277	ENGs	-	
	1	278	TPW	-	
214	1	264	Local red earthenware	-	1700 – 1900
218	1	264	Local red earthenware	-	1700 – 1900
	3	277	ENGs	-	

Context	No.	Bristol Pottery type (BPT)	Fabric	Form	Finds date
219	2	200	ENGS BRIS	-	1840 – 1900
	1	202	REFW CHROM	-	
	2	202	REFW	Jar	
	1	223	REFW	-	
	1	277	ENGS	-	
	1	278	TPW3	Teacup	
	1	278	TPW6	-	
	1	278	TPW3	Plate	
	1	278	TPW FLOW	-	
	1	278	TPW	-	
	1	-	YELL	-	
	1	-	YELL	-	
223	11	200	ENGS BRIS	Jar lid	1876 – 1900
	214	200	ENGS BRIS	Flagon	
	12	200	ENGS BRIS	Bottle or jar	
	11	200	ENGS BRIS	Bottle	
	2	200	ENGS BRIS	Sprits falsk	
	2	200	ENGS BRIS	Hot water bottle	
	1	200	ENGS BRIS WASTER	Jar	
	1	200	ENGS BRIS	Water filter	
	5	200	ENGS BRIS	Jar	
	1	264	Local red earthenware	-	
403	1	200	ENGS BRIS	Ginger beer bottle	1876 – 1900
	1	202	REFW	-	
	1	202	ENGS BRIS	Telegraph insulator?	
	1	277	ENGS	Bottle	
	1	277	ENGS	Ink bottle	
	1	278	TPW	-	
	1	-	YELL	Bowl	
	1	-	YELL	Bowl	
404	1	200	ENGS BRIS		1835 – 1900
405	14	200	ENGS BRIS	Ginger beer bottle	1876 – 1900
	2	200	ENGS BRIS	Porter beer bottle	
	1	200	ENGS BRIS WASTER	Bottle	
	7	200	ENGS BRIS	Bottle	
	1	200	ENGS BRIS	Sprit flask	
	3	200	ENGS BRIS	Ink bottle	
	2	200	ENGS BRIS	Jar	
	6	200	ENGS BRIS	Flagon	
	1	202	REFW CHROME	-	
	1	278	TPW3	Teapot	
	1	278	TPW	Dish	
	3	278	TPW	Plate	
	1	178	TPW3	Jar	
	1	202	PEAR TR4	Potlid	
	1	202	PEAR TR4	Potlid	

Context	No.	Bristol Pottery type (BPT)	Fabric	Form	Finds date
410	2	202	KILNF REFW BISC	Roll of clay	1836 – 1849
	5	202	KILNF REFW	Stilt	
	10	202	KILNF REFW BISC	Stilt	
	1	202	KILNF REFW BISC	Spur	
	8	202	REFW BISC WASTER	Bowl	
	1	202	REFW BISC WASTER	Plate	
	6	202	REFW BISC WASTER	Bowl	
	1	202	REFW BISC WASTER	Teacup	
	9	202	REFW BISC WASTER	Cup	
	1	202	REFW	Strip of clay	
	2	223	REFW BISC WASTER	Roll of clay	
	2	264	KILNF Local red earthenware	-	
	2	264	KILNF Local red earthenware	Teacup	
	3	278	TPW	Plate	
	1	278	TPW	Jar	
	4	278	TPW	Bowl	
	3	326	CREA	-	
	1	349	PEAR	-	
	1	349	PEAR	-	
	2	-	SAGG	Saggur	
411	1	200	ENGs BRIS	Flagon	1835 – 1900
	5	-	Crucible	Glass crucible	

Appendix E

Glass Quantified by Context

E1 Glass Quantified by Context

Context	No.	Description	Finds date
105	1 1 1	Greenish grey glass chunk Blue glass chunk Mould blown cylindrical bottle	1830 – 1930
207	1	Aqua coloured ovate mineral water bottle embossed '___WEPPE & Co./ ___PERIOR/___S'	1860 – 1910
214	1 1	Aqua window glass (possibly safety glass) Clear PM vessel glass	Post-med
219	1 1	Aqua window glass (possibly safety glass) Black glass chunk	Post-med
403	1 1 1 1 1 1 1 1 1 1 1 2	Dark green mould blown cylindrical wine bottle embossed 'S' on base Dark green mould blown cylindrical wine bottle Dark green mould blown cylindrical wine bottle Aqua mould blown cylindrical bottle with applied lip Aqua mould blown cylindrical mineral water bottle, embossed '___BATH & BRISTOL___ TRADE MARK' on one side and '___ & Co' on the other Aqua mould blown 'torpedo' shaped mineral water bottle, embossed '___ATH & BRISTO___ TRADE MARK P' on one side (same as above) Aqua mould blown cylindrical bottle with applied lip Aqua mould blown octagonal bottle Aqua mould blown rectangular bottle Dark green PM bottle glass Moulded white milk glass	1860 – 1900
404	1	Aqua mould blown 'torpedo' shaped mineral water bottle, embossed '___ BATH & BRISTOL ___ TRADE MARK P' on one side and '___thy & Co.' on the other	1860 – 1930
405	2 1 1 1 2 1 1 3 1 1 1	Aqua mould blown cylindrical mineral water bottle embossed '___BATH & BRISTOL___ TRADE MARK' and 'K' on one side and '___y & Co' on the other Dark green mould blown cylindrical beer bottle, embossed 'IMPERIAL' on the shoulder and 'S & G BRISTOL' on the base Near complete aqua mould blown cylindrical mineral water bottle, 1854 - 1922, embossed 'BOYCE & SONS / CHAMPAGNE / GINGER BEER' on one side and 'MEADOW STREET / ST PAULS / BRISTOL' on the other and 'P&R B' on the base) Aqua mould blown cylindrical bottle, embossed 'P&R BRISTOL' on the base Aqua mould blown square bottle Dark green mould blown cylindrical bottle Aqua mould blown octagonal bottle, embossed 'C & B' on the side and 'KE' on the base Aqua mould blown square bottles with applied lip Dark green mould blown cylindrical wine bottle, embossed 'P&R BRISTOL' on the base Clear mould blown burst top stoppered bottle with moulded vertical ribbing Aqua ovate mineral water bottle embossed '___ & Co./ ___OL / SUPERIOR / ___RS'	1860 – 1922
Total	36		