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Historic Building Recording at Locks Building, Leeman Yard, York

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YAT Historic Building Report 2020/108 September 2020





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NON-TECHNICAL SUMMARY

On the 24th August 2020 York Archaeological Trust conducted a building recording exercise at Locks Building, Leeman Yard, York, an area of railway buildings and sidings to the north-west of York Railway Station (SE 58765 52116).

The work was undertaken for Arup prior to the redevelopment of the site and was based on a scope of works produced by Arup. The works involved a Level 1 recording of the Locks Building, Leeman Yard.

KEY PROJECT INFORMATION

Project Name	Historic Building Recording at Locks Building, Leeman Yard, York
YAT Project No.	6118
Document Number	2020/108
Type of Project	Historic Building Recording
Client	Arup
NGR	SE 58765 52116

REPORT INFORMATION

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	Initials	Date	Initials	Date	Initials	Date
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1 INTRODUCTION

On the 24th August 2020 York Archaeological Trust conducted a Level 1 Historic Building Record of the Locks Building, Leeman Yard, York, an area of railway buildings and sidings to the north-west of York Railway Station (SE 58765 52116; Figure 1). The work was undertaken for Arup prior to the redevelopment of the site and was based on a scope of works produced by Arup.

2 METHODOLOGY

The methodology followed the Scope of Works produced by Arup. In summary the Scope of Works required

- A level 1 record of the Locks Building including photography, a written description and a plan.
- The interior will be included to note any significant features.

The brief corresponds to a Level 1 historic building survey as defined in Historic England's guidance *Understanding Historic Buildings* (2016).

The site was visited by Christopher Curtis on the 24th August 2020. Certain parts of the building were inaccessible due to restricted access, and were therefore not entered.

2.1 Desk-Based Research

The historical background of the site has largely been covered in a Heritage Statement produced by Arup in 2018, which will form the basis of the historical background in this report. This has been supplemented with information from historic maps and plans.

2.2 Written Record

The written record is based on notes based on visual observations made during the site visit.

2.3 Photographic Recording

A photographic survey of the site was conducted using a 24-megapixel DSLR. The site photographs have been indexed and plotted on building and site plans (Figure 2).

Photographs include:

- general views of the buildings in the wider setting;
- the external appearance of the buildings, showing all external elevations;
- the overall appearance of all rooms and circulation areas;
- external and internal decorative detail relevant to the buildings' design, development and use;

2.4 Drawn Record

The drawn record is based on a measured survey of the building (Figure 2). The record consists of floor plans of the building.

3 LOCATION

The Locks Building is located to the north-west of York Railway Station, at the western end of Leeman Road (Figures 1-2; NGR SE 58765 52116). The building is situated within a characteristic teardrop shape formed by the conversion of three mainlines to the west of York Station.

4 ARCHAEOLOGICAL AND HISTORICAL BACKGROUND

Prior to the development of the railway the study site was mostly agricultural land outside the walls of the City of York. The first station was a temporary structure built south of Queen Street in 1839 whilst a permanent station was being built within the city walls.

A second mainline was built in 1841 along the southern boundary of the site towards Darlington and a branch towards Scarborough was built in 1845. This early configuration can be seen in the 1853 OS map showing an agricultural landscape divided by the railway lines. The present station was built in 1870 along with new mainline tracks to the north of the site completing the teardrop shape. The North Eastern Railway subsequently built a range of buildings to the west of the new station. The 1892 OS map shows the site as almost fully developed with a large track fan, the extensive wagon works, goods shed and engine shed all built by this point. The 1909 and 1931 OS maps show a further intensification of the site with sidings leading to the Albion Iron Foundry and Engineering Works to the north of the site.

5 DESCRIPTION

5.1.1 Overview

The building currently consists of a double height single-storey building aligned east/west, with a single-storey later addition on the west elevation. The building is constructed of brick, the open gable pitched roof is clad with slate tiles, while the modern addition has a flat roof.

5.1.2 Exterior

The north elevation has an extended canopy slate roof supported on extended steel roof trusses (Plate 14). Three of the four windows have been bricked up, leaving one five by four pane window with a straight lintel and brick sill. There are two wooden doors on sliders in the centre of this elevation. A raised brick platform carried on railway track used as beams runs along the north elevation and continues around the east elevation. The brick platform is open at the northern extent, revealing unglazed brick openings at ground level (Plate 15). The north elevation of the later brick addition has two wooden doorways and a boarded-up window with brick sill.

The east elevation has a metal-framed wooden doorway in the northern half, with a large opening with a metal roller door in the southern half (Plates 18-20). The patchy, whitewashed and irregular style of bricks may indicate possible reuse of bricks. The doorway appears to have been inserted at a later date, there is a concrete lintel and a lack of queen closures. The opening for the roller door has been widened to the south, with queen closures on the north jamb but cut brick to the south jamb. A continuation of the raised brick platform is present on this elevation.

The south elevation features five windows and a wooden door (obscured by foliage) in the original building (Plates 7-10). The five by four pane windows have arched brick lintels and stone

sills. A brick string course runs along the top of the windows. The later addition is constructed from modern brick and has a single aluminium-framed window with a brick sill. There is evidence for the removal of tracks in front of the southern elevation.

The west elevation of the original Locks Building is partially obscured at ground-floor level by the later addition, which contains a wooden doorway and a boarded-up window with brick sill (Plate 11).

5.1.3 Interior

The main space inside the Locks Building is open to the roof and features whitewashed stone walls and a concrete slab floor (Plates 21-23). A brick partition partially separates the eastern end from the rest of the interior space. The steel trussed roof is supported on the top of the brick walls; the trusses extend through the north wall to support the canopy roof. The single remaining window in the north wall is present to the immediate east of the brick partition, with a blocked window between it and the east wall. A modern mesh and timber-framed cage runs along the north wall, which has two further blocked windows and a blocked doorway.

The east wall contains a doorway that was probably inserted at a later date, and a metal roller door which has been widened to the south (Plate 27), while the west wall contains a blocked doorway with a brick arch lintel (Plate 28). The southern wall has five windows and a double wooden doorway with a brick arch lintel (Plates 25-26).

The interior of the modern addition to the west of the Locks Building was not able to be accessed and so cannot be described.

6 DISCUSSION

The Locks Building was built prior to 1931 when it appears on the Ordnance Survey map in an area labelled as 'Engineer's Works'. The original form of the building, comprising one building with canopy and raised platform, can be seen in this map and is evidenced by the fabric of the building. The Locks Building had an enclosed tramway on the northern side, rather than a railway, although tracks are shown on the southern side of the building.

While the majority of the building contains fabric from its original construction, the additional building on the western elevation was added in the 1960-70s, comprising modern brick walls and a flat roof. The result is that the building is visually made up of a patchwork of styles.

7 DISSEMINATION AND ARCHIVING

A full archive has been prepared to recognised standards (Brown 2007). The archive comprises copies of correspondence relating to fieldwork, site notebooks/diaries, original photographic records, site drawings, and computer discs and printouts. An OASIS online record has been made and has the reference [yorkarch1-402958](#).

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ACKNOWLEDGEMENTS

The author would like to thank Arup for commissioning the report and arranging access to the site.

PLATES



Plate 1: West elevation of the Locks Building



Plate 2: West elevation of the Locks Building



Plate 3: North and west elevations of the Locks Building



Plate 4: North and east elevations of the Locks Building



Plate 5: North and east elevations of the Locks Building



Plate 6: South and east elevations of the Locks Building



Plate 7: South and east elevations of the Locks Building



Plate 8: South elevation of the Locks Building



Plate 9: Window in south elevation, 1m scale



Plate 10: South elevation of the later 20th century addition to the west elevation, 1m scale



Plate 11: West elevation of the later 20th century addition to the west elevation, 1m scale



Plate 12: North elevation of the later 20th century addition to the Locks Building, 1m scale



Plate 13: North elevation of the Locks Building, 1m scale



Plate 14: North elevation of the Locks Building showing raised platform and extending steel roof trusses for canopy



Plate 15: Underneath the raised platform, north elevation



Plate 16: Doorways under canopy on north elevation



Plate 17: Window in north elevation



Plate 18: East elevation of the Locks Building, 1m scale



Plate 19: East elevation of the Locks Building, 1m scale



Plate 20: East elevation of the Locks Building, 1m scale



Plate 21: Interior of the Locks building, view west



Plate 22: Interior of the Locks Building, view east



Plate 23: View of the steel roof trusses, looking east



Plate 24: Blocked doorway in west wall, 1m scale



Plate 25: Double doorway with brick arch lintel in south wall, 1m scale



Plate 26: Interior view of window in south wall, 1m scale



Plate 27: Interior view of north wall and brick partition, 1m scale

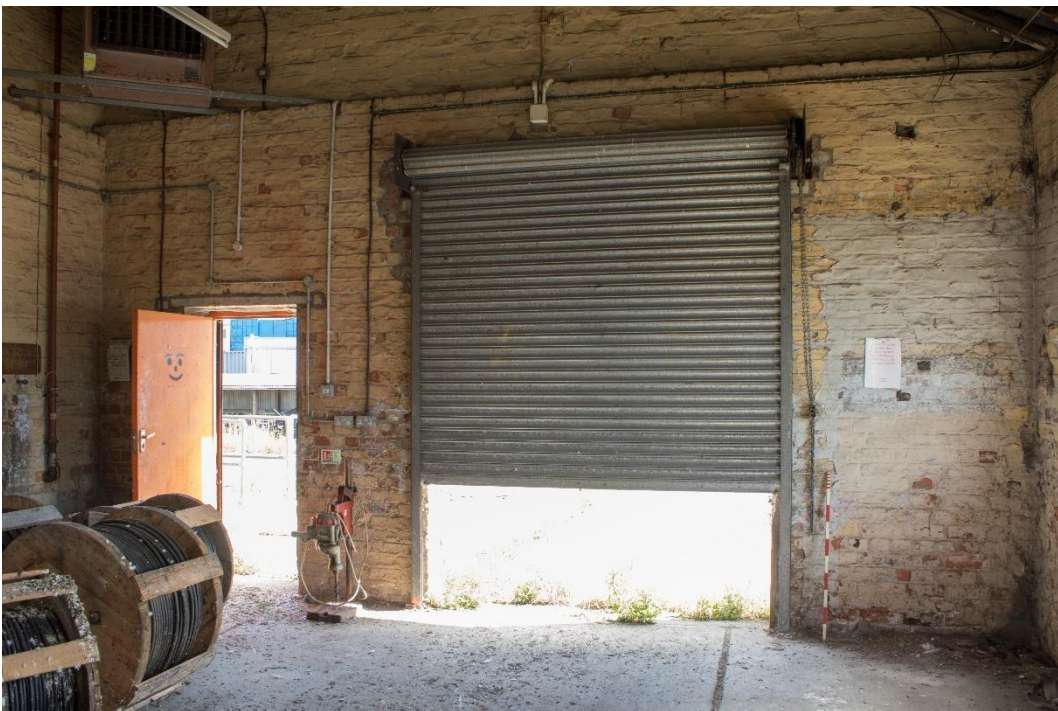
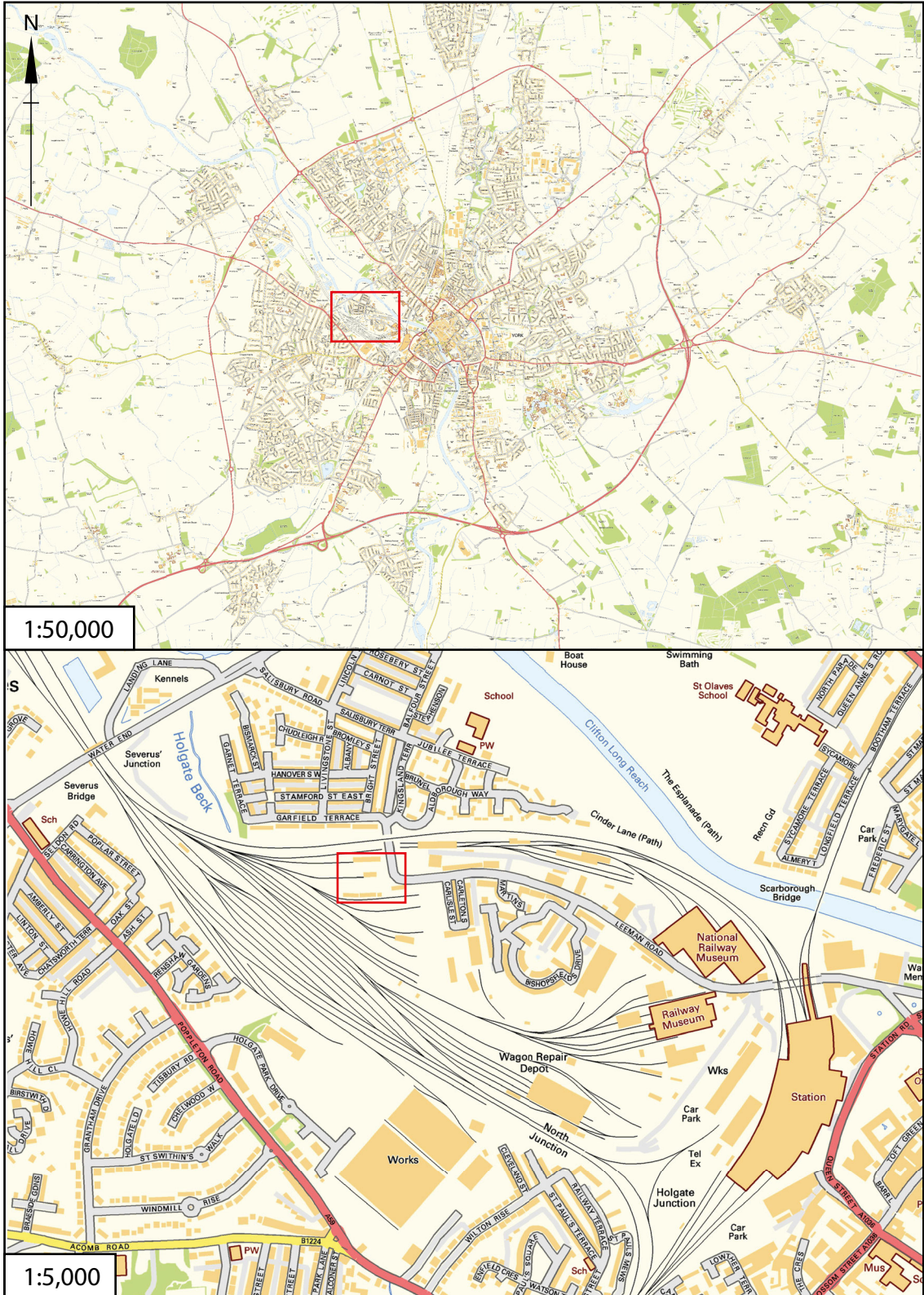


Plate 28: Interior view of east wall with later door and roller door, 1m scale



Plate 29: Interior view of north wall showing blocked doorways, 1m scale



Contains Ordnance Survey data © Crown copyright and database right 2017

Figure 1 Site location

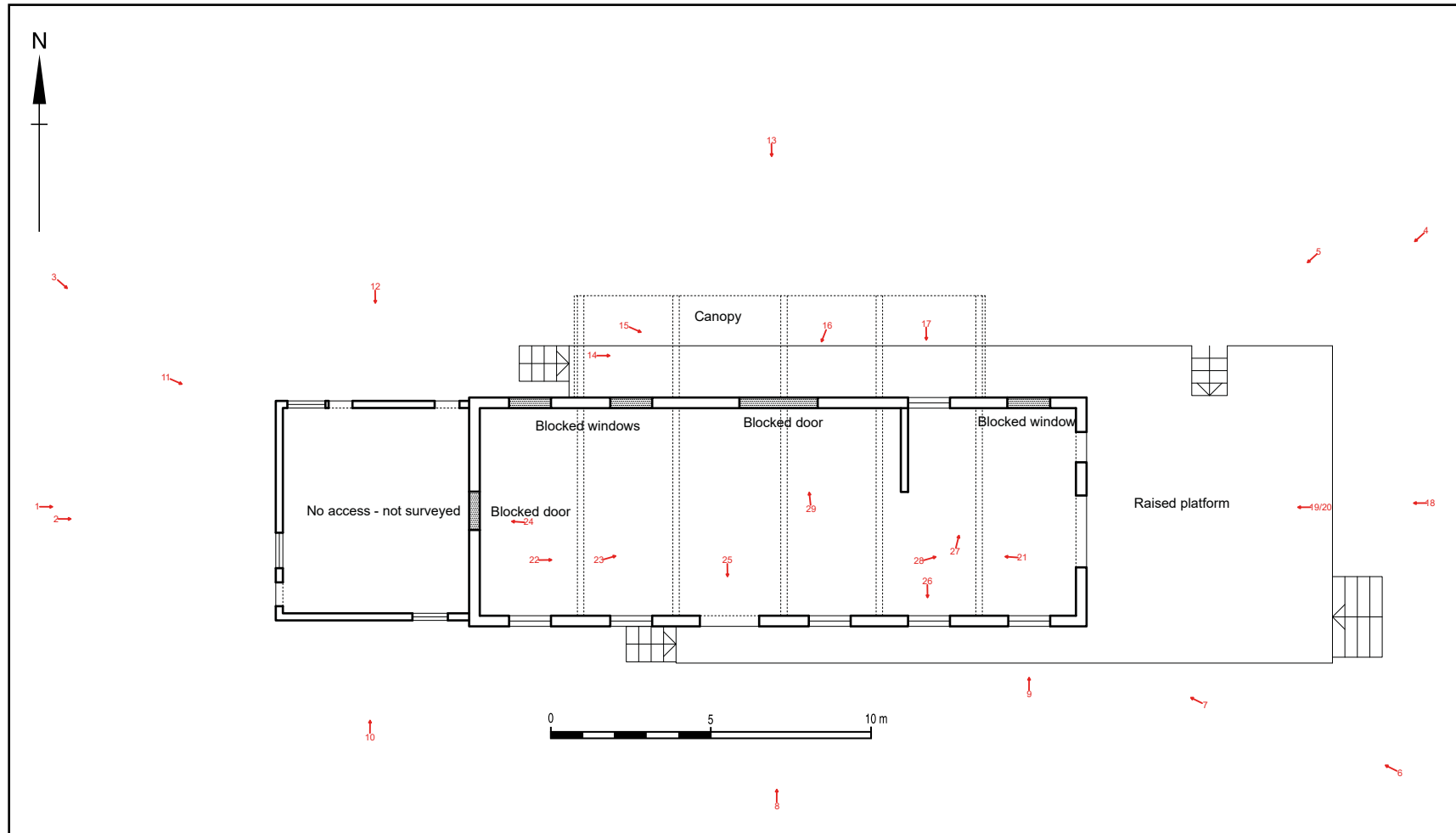


Figure 2 Building plan and photograph directions

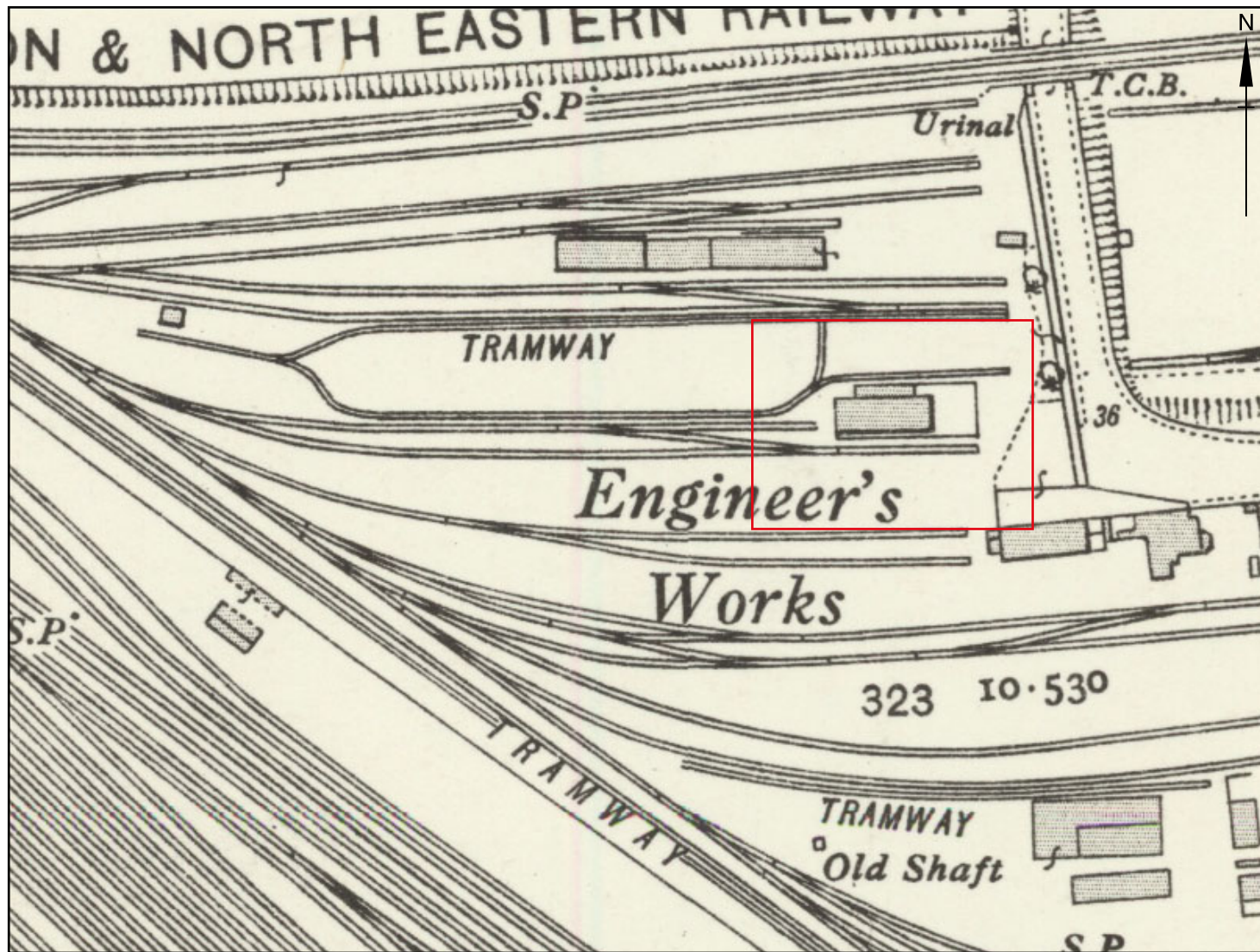


Figure 3 The Locks Building on the Ordnance Survey 1931 map



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