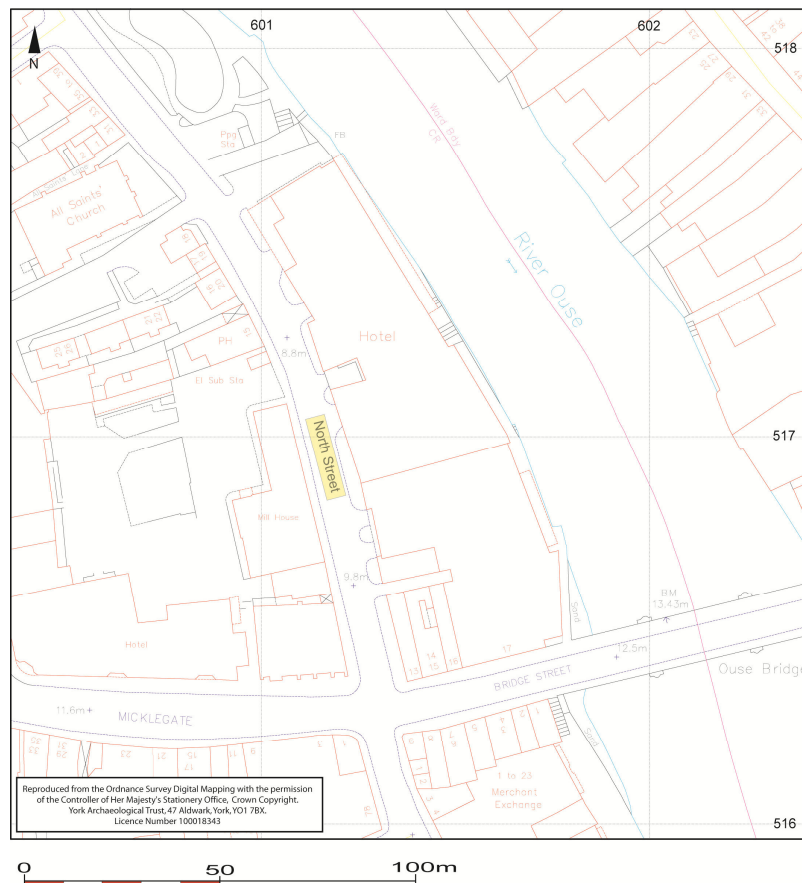




# YORK ARCHAEOLOGICAL TRUST REPORT

<b>SITE ADDRESS</b>	Utility trench, Micklegate – North Street, York		
<b>CLIENT NAME</b>	Morgan Est Plc		
<b>YAT PROJECT #</b>	5278	<b>YAT REPORT #</b>	2009/70
<b>MUSEUM ACCESSION #</b>	n/a	<b>GRID REFERENCE</b>	SE 6011 5170
<b>PLANNING REFERENCE #</b>	n/a	<b>AAI OPS NOTICE #</b>	09024

Between 23<sup>rd</sup> September and 15<sup>th</sup> October 2009 York Archaeological Trust undertook a watching brief on utility works between the junction of North Street / Micklegate and the junction of Tanner Row and North Street, York (Figure 1). The objective was to record any deposits, features or buried structures exposed during the replacement of a gas main along the eastern side of North Street. A total of twelve trenches (Figure 2) were observed although not all were recorded in detail. Most of the recorded or observed trenches lay within the road although two, Trenches 2 and 3, lay within the pavement. Most trenches were roughly rectangular and measured c.1m x 1.5m although there were some substantial variations in shape and size. After cutting out the tarmac initial excavation was by mini-digger although as the gas main, or any other known service, was approached excavation was normally by hand. The numbering sequence for the trenches and sections, which do not correspond as not all trenches were recorded in detail and sometimes more than one section was recorded from a single trench, is that of York Archaeological Trust and in each case begins in the road at the Micklegate / North Street junction and proceeds northwards along North Street. The sections recorded in detail are described below. Recording took the form of written notes and measured sketches supplemented with a number of colour digital photographs. The works were monitored on a regular basis and usually visited at least twice a day when excavation work was in progress.



**Figure 1** Location of North Street

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**Figure 2** Location of observed and recorded trenches, south

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### Section 1

This section was located within the north-eastern angle of Trench 1. The only two contexts seen in this section, concrete (1000) and tarmac (1001) were both clearly modern.

### Section 2

This section was situated close to the south-east corner of Trench 1. Again all the contexts recorded were of modern or relatively modern origin and consisted of brick rubble and mortar (1002) overlain by levelling deposits (1003 and 1004) sealed by the current road surface (1005).

### Section 3

This section was in the central southern part of Trench 1. Within this part of the trench the remains of three brick walls, two (1006 and 1007) aligned approximately north-south. Both these appeared to be keyed into an east-west aligned brick wall (1008) projecting east of 1007 towards the pavement in Bridge Street. The spaces between these walls had been partially backed with brick rubble and mortar (1009) which was sealed by a modern levelling deposit of concrete (1010) and then the road surface (1011).



**Plate 1** Gas main and wall 1006 in Trench 1, looking north-west

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### Section 4

This was located at the southern end of Trench 2. Only modern levelling deposits (1012 – 1014) sealed by the pavement surface of concrete slabs (1015) were recorded in this section.

### Section 5

Trench 2 was extended northward and this section was situated towards the centre of the lengthened trench. Modern services (1016), levelling deposits (1017 and 1018) and the concrete slabs (1019) were all that were recorded.

### Section 6

This section was located within Trench 4. Between 0.8m and c.1.5m below ground level (BGL) there was a deposit of dark brown very slightly clayey silt with very occasional flecks of pale brown mortar (1020) and this probably was a build-up deposit. Sealing this was a 0.2m deep layer of very dark brown very slightly clayey silt with frequent brick / tile (1021) and above this a 0.2m deep deposit of dark brown very slightly clayey silt with occasional charcoal and pale brown mortar (1022). It is possible that both 1021 and 1022 may have been build-up deposits. Above 1022 there were modern levelling deposits (1023 – 1025) and the modern concrete pavement (1026).

### Section 7

This section was situated along the western side of Trench 5. The earliest contexts noted in this trench were a pair of brick walls. One of these (1027) lay towards the southern end of the trench and only a 0.5m long stub penetrated into the trench. This wall was aligned approximately east-west, c.0.23m wide, bonded with a moderately hard mortar and seen between c.0.25m and 1.5m BGL. The second wall (1028), lying c.2m to the north of 1027, was virtually identical except that it was aligned approximately north-west / south-east. The original insertion of the gas main, and probably other services, had destroyed any physical relationship between these two stretches of wall which were observed at least 2m out from the current modern building line. Butting these walls was a mass of brick rubble, crushed limestone, pale brown mortar and brown silt (1029) very similar to that seen in Trench 1. The walls and rubble had been sealed by modern levelling deposits (1030 and 1031) and the modern pavement surface (1032).

### Section 8

This section was recorded along the east side of Trench 6. The lowest deposit noted, at between 0.65m and at least 1.5m BGL, was a mid brown silt with moderate pebbles and pale brown mortar (1033). It was sealed by a 0.2m deep layer of dark brown silt with moderate brick / tile and pale brown mortar (1034). Both of these deposits may have been disturbed build-up deposits. Above them were clearly modern levelling deposits (1035 and 1036) and the modern road surface (1037).

### Section 9

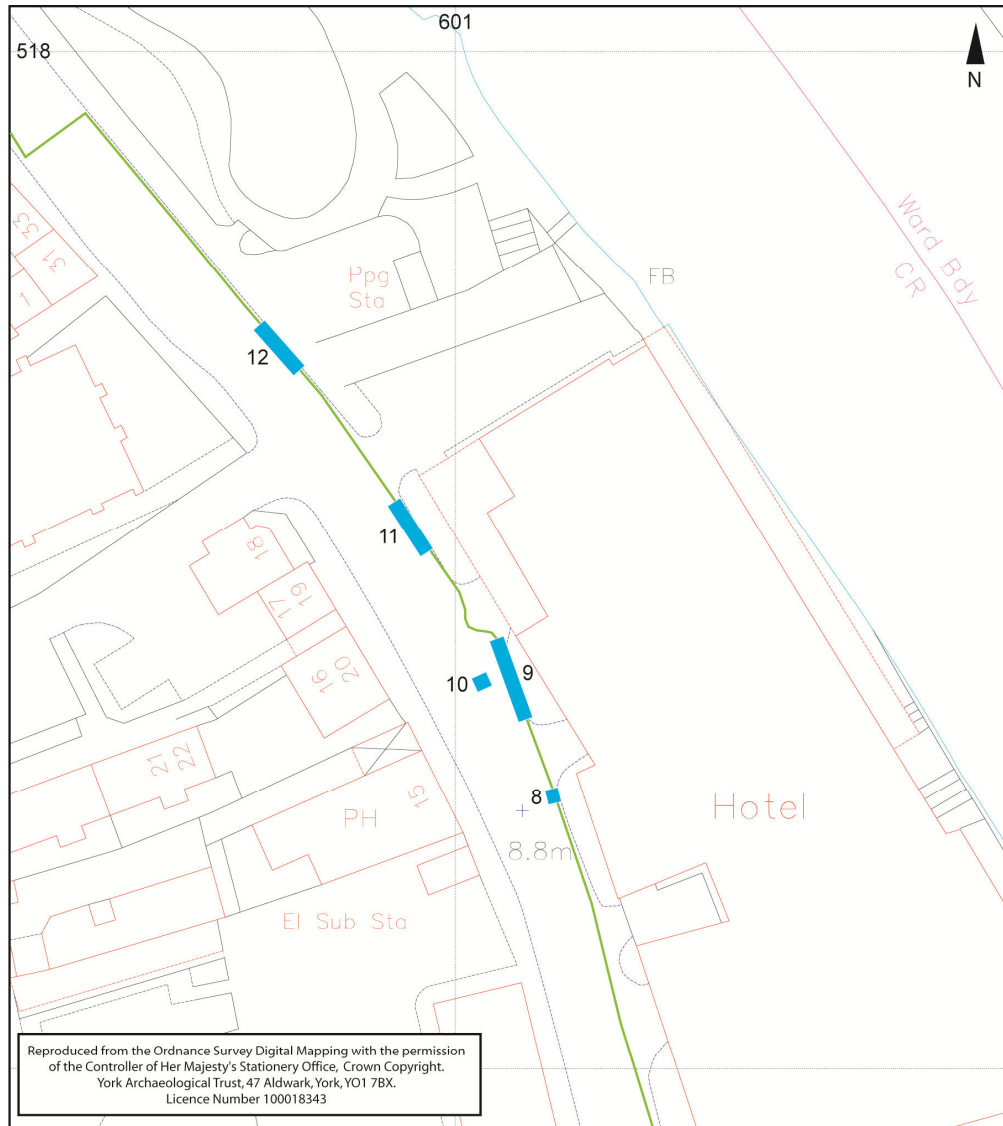
This section was along the west side of Trench 7. All the contexts recorded within this trench were of modern origin and consisted of levelling deposits (1038 – 1041) sealed by the current road surface (1042).

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**Figure 3** Location of observed and recorded trenches, north

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### Section 10

This section was recorded along the northern side of Trench 7. All of the contexts seen within this trench were of modern origin and comprised levelling deposits (1043 – 1045) below the modern road surface (1046). Within the lowest levelling deposit (1043) there was a squared, worked limestone block probably originally from a medieval building.

### Section 11

This section was recorded at the north end of Trench 9. The earliest deposit, between 0.6m and at least 1.2m BGL, was a mid brown silt with occasional pale brown mortar (1047) and may have been a build-up deposit. It was overlain by clearly modern levelling deposits (1048 and 1049) and the modern road surface (1050).



**Plate 2** Modern deposits and services in Trench 11, looking north

### Section 12

This section was at the north end of Trench 10. All contexts were definitely modern and comprised levelling deposits (1051 – 1053) underlying the present road surface (1054).

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### Section 13

This section was located at the north end of Trench 11. All contexts recorded were believed to be modern and comprised probable or definite levelling deposits (1055 – 1057 and 1059 – 1061) a short stretch of possible brick wall (1058) and a service trench and its backfill (1062 and 1063) all below the existing road surface (1064).

### Section 14

This section was recorded along the south-west side of Trench 12. The earliest deposit noted, between 1.5m and c.2m BGL, was a dark greyish-brown very slightly clayey silt with occasional flecks of white mortar (1065) and this may have been a build-up deposit. It was overlain by modern levelling deposits (1066 – 1068) below current the road surface (1069).

### Conclusions

No dating evidence was recovered from any of the trenches except for modern material and it was apparent that most of what was exposed was modern. The brick walls seen in Trench 1 almost certainly belong to cellars and those noted in Trench 5 probably do. In both cases they suggest that North Street has been widened considerably at some point in the past. Possible build-up deposits were recorded in the lower parts of Trenches 4, 6, 9 and 12 but were probably badly disturbed by modern activity although they may hint at the survival of similar deposits lower down and away from modern services. The limestone block in Trench 8 may be derived from a medieval building associated with the waterfront but no definite trace of any surviving structures of the period was noted from any of the trenches. It is believed that the works undertaken did little or no damage to any archaeological remains.

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