

Archaeological Watching Brief
of works at
Blueschool Street and
Newmarket Street,
Hereford



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Archaeological watching brief at Blueschool Street and Newmarket Street, Hereford

Graham Arnold (project leader)

Summary

An archaeological watching brief was undertaken at Newmarket Street, Hereford (NGR SO 0897, 0200 and SO 1037, 0221). It was undertaken on behalf of Stanhope Plc, as part of a wider scheme for development of the former Cattle Market. Associated works within the carriageway included new service trenches and intrusive groundworks within a Scheduled Monument comprising the location of the former city walls, ramparts and ditch in this area of the city (Scheduled Monument HE124). Consent for these works was granted by English Heritage subject to conditions including archaeological observation of groundworks within and in the immediate vicinity of the scheduled area. Part of the works also lay within the designated Area of Archaeological Importance and consent was granted by Herefordshire Council.

The works included a new Welsh Water drainage service trench, excavation of tree pits, traffic lights, service ducting and highways surface drainage on the south carriageway of the street. Archaeological monitoring was undertaken in May 2013 and between February and March 2014. Modern made ground forming the base of the road and service trenches were encountered in all areas and natural gravels at a depth of 1.8m below current ground level.

No *in situ* archaeological deposits were encountered although during observation of the excavation of a tree pit, a number of worked grey sandstone ashlar masonry blocks were encountered within modern made ground. It is thought that these may represent fabric of the medieval city wall or associated structures disturbed during the construction of a modern subway in the mid 20th Century.

Report

1 Background

1.1 Reasons for the project

An archaeological watching brief was undertaken at Blueschool Street and Newmarket Street, Hereford (centred on NGR SO 50897, 40200 and SO 51037, 40221). It was commissioned by CgMs Consulting acting on behalf of Stanhope Plc. Scheduled Monument consent (set out in a letter from Tony Fleming, dated 14th September 2012) was granted by English Heritage for works within the City Walls, Ramparts and Ditch Scheduled Monument (HE 124) in Hereford as part of a wider scheme including development of the former cattle market subject to conditions including the provision of an archaeological watching brief of groundworks within the scheduled area. A Written Scheme of Investigation (CgMs 2012) was prepared which set out a methodology for the watching brief to which this project conforms. The project also conforms to the *Standard and guidance for an archaeological watching brief* (IfA 2008), and *Standards for archaeological projects in Herefordshire: issue 1* (Herefordshire Archaeology 2004).

The event reference for this project, given by the HER is EHE 80004.

2 Aims

The objectives of the archaeological monitoring are to:

- ensure that all works are carried out in accordance with the approved Scheduled Monument Consent;
- determine the presence or absence of archaeological deposits and define any existing level of truncation;
- to date and characterise any surviving archaeological deposits;
- to clarify, if possible, the role of the archaeological remains in Hereford's historic development.

The WSI also identified specific works to be monitored, including

- Tree pits within the central reservation of Newmarket Street (x 12)
- Tree pits on the southern side of Newmarket Street (x4)
- Highway surface drainage within Blueschool Street
- Highways ducting across Newmarket Street
- Traffic light works within Blueschool Street, in the location of Bastion 9 of the City Walls.

3 Methods

3.1 Personnel

The project was undertaken by Graham Arnold BA (hons) MSc; who joined Worcestershire Archaeology in 2009 and has been practicing archaeology since 2002. The project manager responsible for the quality of the project was Tom Rogers BA (hons) MSc. Illustrations were prepared by Carolyn Hunt MIFA.

Documentary research

An archaeological desk-based assessment (DBA) was undertaken by CgMs as part of the overall Cattle Market Development programme (Patrick 2010).

3.2 Fieldwork strategy

A detailed specification has been prepared by CgMs Consulting Ltd (WA 2012a). Fieldwork was undertaken between 13 May 2013 and 25th March 2014. The site reference number and site code is EHE 80004.

Nine separate areas were monitored, as well as various gullies and ducting trenches. The locations of these areas are indicated in Figure 2. A plan of all ducting is also shown on Figure 3.

3.3 Structural analysis

All fieldwork records were checked and cross-referenced. Analysis was effected through a combination of structural, artefactual and ecofactual evidence, allied to the information derived from other sources.

3.4 Statement of confidence in the methods and results

The methods adopted allow a high degree of confidence that the aims of the project have been achieved. Not all ducting across the site was archaeologically monitored due to practical issues as work was taking place over night when the traffic was minimal. However, it was established that ducting was above 0.60m below the modern ground level, and still within the road make-up, as shown in the tree pit and footpath excavations that were monitored. Gully pits between Wall Street and Blueschool Street and the traffic light excavations on Blueschool Street were monitored during the night.

3.5 Topography, geology and archaeological context

The topography and geology of the site have been set out in the desk-based assessment of the wider site (CgMs 2010). The following information relating to the Scheduled Monument (HE124) is derived from the Historic Environment Record for Herefordshire.

In the mid 9th Century the main part of the city was enclosed with a gravel bank and ditch. At the end of the 9th century, probably in the time of Alfred (AD 871-901), the defences were completely rebuilt to the East to enclose St Guthlacs (now Castle Green). At the beginning of the 10th Century timber works were replaced in stone. In 1189 the defensive stone wall was extended to the north, which corresponds with the ring road today. The wall incorporated six gates and seventeen semi-circular towers in total.

Three separate areas relate to the current works. The section to the east from Widemarsh Gate to Bastion 9 (SMR 43832) was freestanding in Taylor's map of 1759, but had buildings incorporated into its North face by the time of the 1858 Curley plan and further additions on the 1st edition OS Map of 1886. These buildings were demolished along with the wall sometime after 1919 when Alfred Watkins observed part of it still existing 'at the back of stables and warehouses'. A short section is still incorporated into the north elevation of the Farmers Club on Widemarsh Street today.

Bastion 9 (SMR 43833) is recorded as a 'D-plan bastion or open-backed mural tower'. By 1886 the tower had been built over and presumably demolished by the time the area was surveyed for the 1st ed. OS map. The bastion's position was established by trenching in 1966 (Shoemith, 1974; Shoemith, 1982). Two machine cut trenches were excavated which showed that the wall existed at 1.10m below the ground level and that it was primary to the city wall and had been built into the side of an extramural ditch. It showed no trace of a walkway or berm around it. It was overlain by the remains of a 19th Century forge.

The city wall along Wall Street (SMR 43826) is the western most surviving length of the medieval wall on the north side of the city.

3.6 Current land-use

The site is currently a dual carriageway ring road around the city of Hereford, with footpaths, traffic crossings and associated roadside landscaping.

4 Structural analysis

The trenches and features recorded are shown in Fig 2. The results of the structural analysis are presented in Appendix 1.

4.1 Area 1; Water Main relocation trench

This trench was for the relocation of the water pipe (marked in blue on Fig. 2) and lay just to the north of the scheduled area, on the northern carriageway of Newmarket Street and was a total length of 90.00m, 2.10m wide and up to 3.00m in depth (Plates 1-4). The stratigraphy observed included natural gravels at 1.80m below ground level (103), overlain by made ground (102) and the modern road surfacing (100 and 101). The made ground was heavily truncated by a series of service trenches and the subway construction (Plate 4).

4.2 Areas 2, 3 and 8; Removal of road surface for re-surfacing

In these areas block paving (200,300 and 800) was removed for resurfacing and repaving. The block paving lay directly over made ground (201,202,301 and 801) which was observed to a maximum depth of 0.50m below ground level.

4.3 Area 4; Central Reservation Tree Pits

A total of 12 tree pits (Plates 9 and 10, light green on Fig. 2) were excavated to a total depth of 0.80m below the existing kerb line. Made ground (402) was recorded 0.55m below the ground level to the base of the tree pits. Modern road stone and tarmac lay over the made ground, frequently truncated by modern service trench backfill.

4.4 Area 7; Newmarket Street Tree Pits

Four tree pits (Plates 11 – 14, dark green on Fig. 2) measuring 2.70m square were excavated on the ground between Wall Street and Newmarket Street. They were located directly over modern services (703) and redeposited material for service trench backfill (704) was excavated to a total depth of 0.90m below the road level. Tree pit 16 was located on a modern landscaped bank and a deposit of modern topsoil (700) 0.3m deep was recorded here. The existing footpath surface that overlay the other tree pits had been removed previously during Area 3 works. A number of squared grey sandstone ashlar masonry blocks (plates 15-16) were found within the backfill of the service trench but these were loose and not in situ. It is likely that these had been disturbed previously when the subway was constructed and backfilled with the other waste material.

4.5 Area 6 and 10; Gully Pits

Four gully pits (Marked in black on fig 3) were monitored along the south footpath in the Eastern section of the study area, along Wall Street (plate 17) and the corner between Widemarsh Street and Blueschool Street (plates 18-20). The gully pits were a maximum of 1.20m in depth and were often shallower due to the presence of live services. The gully pit on Wall Street encountered very

compact sandy gravel / marl (603) thought to be natural or redeposited natural ground at 0.80m below the ground level. Gully pits between Widemarsh Street and Blueschool Street were excavated entirely through made ground (1002) backfilled from the laying of services.

4.6 Gullies and ducting trenches

Drainage gullies and traffic light ducting (shown in orange on Fig. 3) were only monitored along the footpaths (in Area 8 and central reservations; plates 7 - 8) and were a maximum of 0.50m in depth, placed within the existing modern make up for the road, central reservations and pavements.

4.7 Area 9 Traffic light and control box

A pit 0.45m in depth and 0.45m square was excavated over the location of Bastion 9. Only modern road surfacing and made ground (900-902) was encountered, with frequent modern services (plate 21).

5 Synthesis

No significant archaeological deposits or features were observed during the works, with the excavations only impacting upon 20th Century deposits and ensuring all new services were laid within the backfill of existing service trenches. The works highlight that this stretch of road lies over a considerable depth of made ground both as a base material and due to the laying of services. The city wall and ditch, if they survive along this stretch, lie at a greater depth than the impact levels of the development.

Some grey sandstone ashlar blocks encountered during excavation of tree pits in Area 7 may have derived originally from the City Wall or related structures, but were clearly not in their original location. They may have been disturbed during excavations for a subway beneath this road during the mid 20th Century.

6 Publication summary

Worcestershire Archaeology has a professional obligation to publish the results of archaeological projects within a reasonable period of time. To this end, Worcestershire Archaeology intends to use this summary as the basis for publication through local or regional journals. The client is requested to consider the content of this section as being acceptable for such publication.

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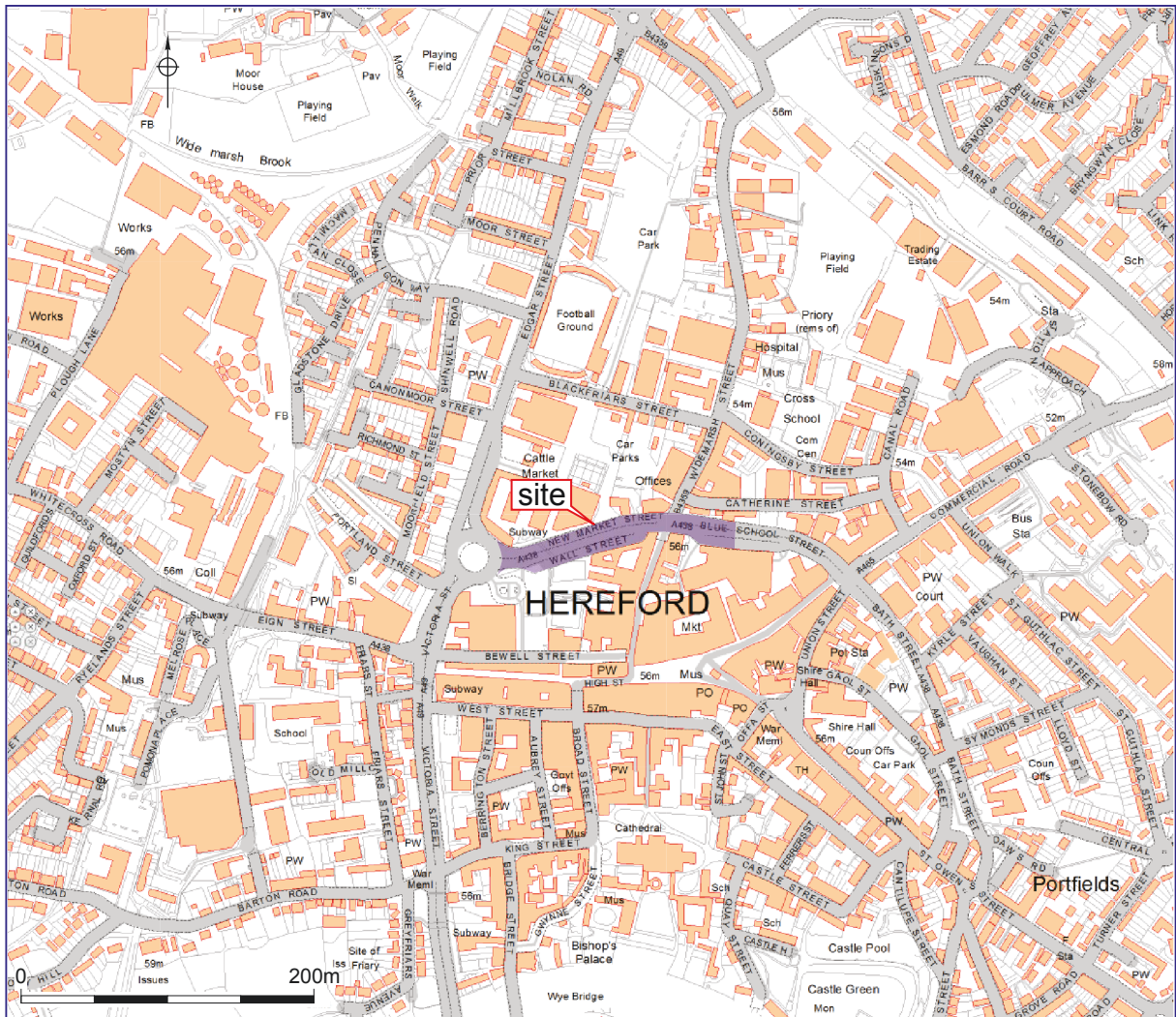
7 Acknowledgements

Worcestershire Archaeology would like to thank the following for their kind assistance in the successful conclusion of this project, Steve Draper (Project Manager, Fitzgerald Civil Engineering), Jim McTaggart (Robert McAlpine), Cathy Patrick (CgMs Consulting), Tony Fleming (Inspector of Ancient Monuments, English Heritage) and Julian Cotton (Archaeological Advisor, Herefordshire Council).

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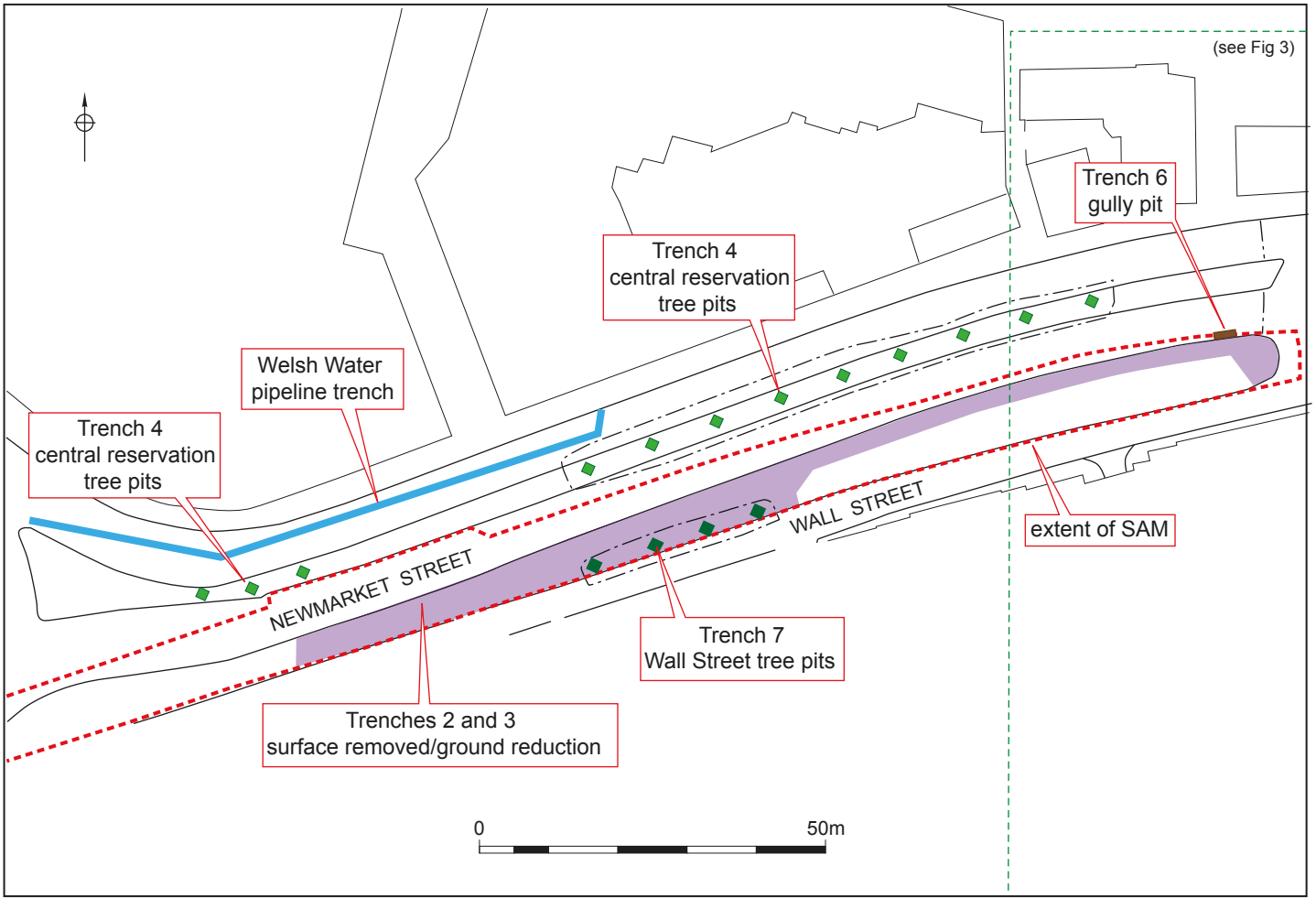
Figures



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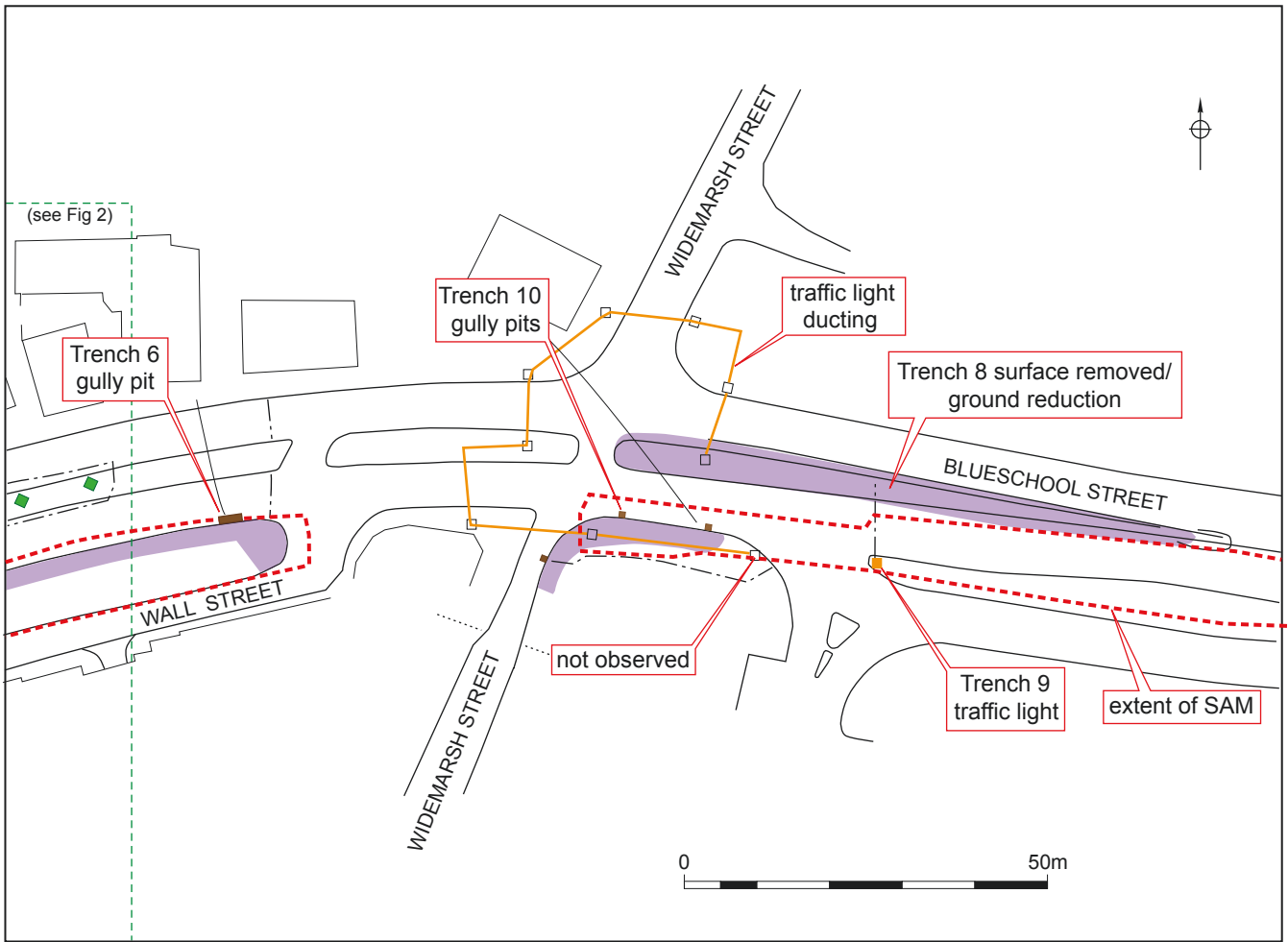
Location of the site

Figure 1



Trench location plan: western area (based upon Corun Drg 511)

Figure 2



Trench location plan: eastern area (based upon Corun Drg 511)

Figure 3

Plates



Plate 1 Tarmac stripped before excavation commenced. View North-east



Plate 2 North facing section of trench 1, showing truncation by modern services. View South-east.



Plate 3 South facing section of trench 1.



Plate 4 part of the modern subway under the road made of reinforced concrete.



Plate 5 Ground reduction of Blueschool Street central reservation view East.



Plate 6 Newmarket Street ground reduction view West.

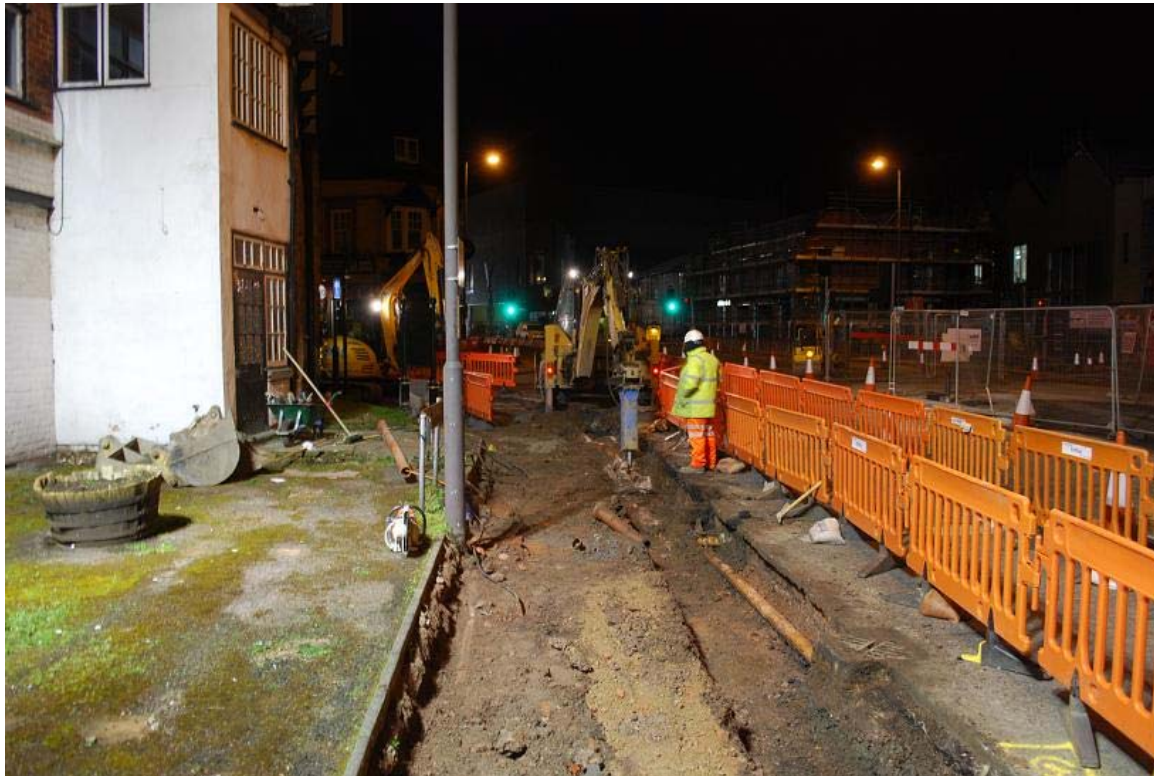


Plate 7 Blueschool Street; ground reduction for refurbishments. View west.



Plate 8 Example depth of ducting in central reservation and across carriageways after installation and backfilling.



Plate 9 Example of Tree pit 1 in plan showing made ground, a service trench. View west.



Plate 10 North-facing section of tree pit in central reservation.



Plate 11 Plan shot of Tree pit within Newmarket Street South (Trench 7) showing modern service pipes on north side. View east.



Plate 12 North facing section of Tree pit within Newmarket Street (Trench 7) showing made ground service trench backfill.



Plate 13 Eastern tree pit within Newmarket Street south. It shows a loose sandstone ashlar block in the section and modern services on the north side. View east.



Plate 14 South facing section of eastern tree pit within Newmarket Street south.



Plate 15 Example of ashlar block from Newmarket Street tree pit within modern backfill.



Plate 16 Example of ashlar block from Newmarket Street tree pit within modern backfill.



Plate 17 Gully pit adjacent to Wall Street showing layers of road make up and very compacted red gravel. View west.



Plate 18 Widemarsh Street Gully Pit, showing modern water main and backfill of modern service trench. View West.



Plate 19 Plan shot of Gully pit in Blueschool Street. View north.



Plate 20 South facing section of Blueschool Street gully pit showing backfilled modern disturbed made ground.



Plate 21 Trench 9 traffic light signal box and traffic light on Blueschool Street. View north.

Appendix 1 Trench descriptions

Trench 1

Site area: Welsh Water pipe, Newmarket Street

Maximum dimensions: Length: 90m Width: 2.10m Depth: 3.00m max

Orientation: E-W – See fig 2

Road level : 55.90m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
100	Tarmac	Tarmac road surface	0 – 0.30m
101	Modern levelling hardcore	Road stone, type 1 gravels and bricks	0.30 – 0.80m
102	Layer	Compact dark brownish black mixed silty clay with frequent brick and concrete rubble. Made ground.	0.80 – 1.80m
103	Natural	Pale orange to red sand and gravels with frequent rounded unsorted gravels. Truncated by a number of service trenches and concrete subway construction.	1.80 – 3.00m

Trench 2

Site area: Central reservation, Blueschool Street

Maximum dimensions: Length: m Width: 3.00m Depth: 0.40m max

Orientation: E-W – See fig 2

Road level: 55.40m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
200	Brick pavement surface	Pedestrian block paving footpath surfacing	0 – 0.08m
201	Modern levelling hardcore	Concrete, tarmac and sand with frequent modern services and traffic light ducting	0.08 – 0.40m
202	Modern levelling	Type 1 road stone. Levelling material for road	0.40 – 0.50m +

Trench 3

Site area: Newmarket Street South Wall Street footpath and subway

Maximum dimensions: Length: 90m Width: 2.10m Depth: 0.40m max

Orientation: E-W – See fig 2

Road level : 55.60m – 56.00m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
300	Tarmac	Pedestrian block paving footpath surfacing	0 – 0.08m
301	Modern levelling hardcore	Mixed road stone, type 1 gravels and rubble bricks, sand , pea grit concrete	0.30 – 0.50m

Trench 4

Site area: Central reservation Newmarket Street Tree Pits x 12

Maximum dimensions: Length: 3.00m Width: 2.50m Depth: 0.80m and 1.00

Orientation: E-W – See fig 2

Road level : 55.76m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
400	Tarmac	Tarmac road surfacing	0 – 0.30m
401	Modern levelling hardcore	Road stone, type 1 gravels	0.30 – 0.60m
402	Layer	Compact mid reddish brown silty clay with frequent gravels and moderate inclusions of modern cbm, tarmac, concrete, kerb stones and brick. Made ground.	0.60 – 0.80m +
403	Modern service trench	Compacted modern brick, concrete, breeze blocks and modern refuse backfill of service trench	0.60 – 0.80m +
404	Cut of trench	Linear modern trench aligned east – west. Unexcavated	0.60 – 0.80m+

Trench 5

Site area: Street lights x 6

Maximum dimensions: Length: 0.50m Width: 0.50m Depth: 1.20m

Orientation: See fig

Road level: 55.60m – 56.00m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
500	Tarmac	Tarmac road surfacing	0 – 0.30m
501	Modern levelling hardcore	Road stone, type 1 gravels and bricks	0.30 – 0.50m
502	Layer	Compact mid reddish brown silty clay with frequent gravels and moderate inclusions of modern cbm, tarmac, concrete, kerb stones and brick. Made ground	0.50 – 1.20m

Trench 6

Site area: Wall Street Gully Pit

Maximum dimensions: Length: 1.50m Width: 0.60m Depth: 1.00m

Orientation: E-W

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
600	Tarmac	Tarmac road surfacing	0 – 0.30m
601	Modern levelling hardcore	Road stone, type 1 gravels	0.30 – 0.50m
602	Modern bedding material	Heavily compacted yellow stone chippings	0.50 – 0.80m
603	Natural	Extremely compact red sandy gravel / marl	0.80 – 1.00m

Trench 7

Site area: Wall Street Tree Pits x 4

Maximum dimensions: Length: 2.70m Width: 2.70m Depth: 0.80m

Orientation: See fig 2

Road level: 56.00m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
700	Modern turf/topsoil	Moderately compact mid brown silty loam Modern topsoil and turf bank landscaping. Eastern tree pit only.	0.00m – 0.30m Eastern tree pit only
701	Modern levelling hardcore	Modern type 1 hardcore and brick fragments	0.00m – 0.20m
702	Modern services trench	Storm and foul drainage incased in concrete and cast iron water main all orientated east-west parallel to the road	0.60m – 0.90m
703	Subway structure	Reinforced concrete modern brick wall of subway	0.10 – 0.90m
704	Made ground / backfill	Backfill of loose red gravels with frequent iron bars, breeze blocks, ash, charcoal, wood and occasional squared sandstone ashlar masonry blocks of varying size, measuring 0.50m x 0.50m x 0.25m max.	0.20m – 0.90m +

Trench 8

Site area: Blueschool Street South Pavement

Maximum dimensions: Length: Width: Depth:

Orientation: See fig 2

Road level: 55.35m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
800	Surfacing	Pedestrian block paving footpath surfacing	0 – 0.08m
801	Modern levelling hardcore	Concrete, tarmac and sand with frequent modern services and traffic light ducting	0.08 – 0.50m

Trench 9

Site area: Traffic Light and Signal control box (directly above Bastion 9 location)

Maximum dimensions: Length: 0.45m Width: 0.45m Depth: 0.50m

Orientation: See fig 2

Road level: 55.21m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
900	surface	Modern cubed shaped stone cobble surfacing	+ 0.10m – 0.00m
901	Layer	Concrete bedding for stone cobbles, traffic light post and kerbing	0.00 – 0.20m
902	Layer	Indurated conglomerate gravels with occasional modern brick fragments and frequent services and kerbing	0.20 – 0.45m

Trench 10

Site area: Blueschool Street South Gully Pits x 3

Maximum dimensions: Length: 1.50m Width: 0.60m Depth: 1.20m maximum

Orientation: See fig 2

Road level: 55.11m AOD

Main deposit description

Context	Classification	Description	Depth below ground surface (b.g.s) – top and bottom of deposits
1000	Tarmac	Tarmac road surfacing	0 – 0.30m
1001	Layer	Road stone, type 1 gravels and bricks	0.30 – 0.50m
1002	Made ground	Soft dark grey clay with frequent iron bars, brick, mortar, concrete fragments. Modern backfill of service trench along road.	0.50m – 1.20m +

Appendix 2 Technical information

The archive (site code: EHE 80004)

The archive consists of:

- 6 Field progress reports AS2
- 3 Photographic records AS3
- 286 Digital photographs
- 1 Drawing number catalogues AS4
- 2 Scale drawings
- 10 Trench record sheets AS41
- 1 CD-Rom/DVDs

1 Copy of this report (bound hard copy)

The project archive is intended to be placed at:

Hereford City Museum and Art Gallery
Broad Street
Hereford
HR4 9RU

Tel. Hereford (01432) 268121 extension 207/334

Summary of data for Herefordshire SMR

Report name and title	Archaeological Watching Brief of roadworks at Blueschool and Newmarket Street	
Contractor's name and address	Worcestershire Archaeology, The Hive, Sawmill Walk, The Butts, Worcester	
Site name	Blueschool Street and Newmarket Street, Hereford	
Grid Reference (8 fig)	SO 0897, 0200 + SO 1037, 0221	Planning Application Number
SMR number/s of site	EHE 80004	
Date of fieldwork	13 th May 2103 – 20 th May 2013 + 29 th January 2014 – 25 th March 2014	
Date of report	April 2014	
	Number and type of finds	
Pottery	Period	Number of sherds
None		
Other finds	Period	Quantity
	Number and type of samples collected - None	
Sieving for charred plant remains	Number of features sampled: Number of buckets:	
C14/scientific dates	Number and type: Result:	
Pollen	No of columns/spot samples: Name of pollen specialist	
Bone	Number of buckets sieved for bone <i>Quantity recovered</i> <i>Period</i>	
Insect	No of columns/spot samples Name of pollen specialist	
Other	Type and specialist	
Summary of the report	Negative Watching Brief monitoring roadworks and refurbishments as a condition to Scheduled Monument Consent. A4 written report including bibliographic references with illustrated figures and photographic plates.	